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HISTORIC PRESERVATION BOARD AGENDA 250 NORTH 5TH STREET TUESDAY, APRIL 28, 2020 @ 4:00 PM

Call to Order/Announcements

Approval of Minutes

Approval of the minutes from the March 3, 2020 Historic Preservation Board meeting

Discussion Items

Consider a request by Chris and Heather Dennis for a Certificate of Appropriateness (COA) to: 1) demolish a 600-square foot garage; 2) construct a 2,160-square foot garage; and 3) erect a 6-foot fence on 0.24 acres located at 640 North 7th Street within the North Seventh Street Historic Residential District.

Updates

Other Business/Public Comment

Adjournment

Historic Preservation Board Meeting

Minutes - March 3, 2020

Board Members Present: Priscilla Mangnall, Ron Parron, Jody Motz, and Troy Reynolds

Board Members Not Present: Greg Gnesios, Mikhail Blosser

Others in Attendance: Landon Hawes, Senior Planner; Kristen Ashbeck, Principal Planner; John Shaver, City Attorney; 8 members of the public

1. Call to Order/Announcements

The meeting was called to order at 4:00 pm at City Hall.

2. Approval of Minutes

Jody Motz moved to approve the minutes of the February 4, 2020 meeting. Troy Reynolds seconded and the motion passed 4-0.

3. Discussion Items

Consider a request by Matthew Motz for a Certificate of Appropriateness to enclose a 99 square foot patio located at 706 N 7th Street

Troy Reynolds made a motion to approve the item. Priscilla Mangnall seconded and the item was approved 4-0, with the condition that the applicant resubmit plans to staff that more accurately reflect the details of the enclosure.

Planning for the May 9, 2020 Historic Preservation Block Party

The owner of the old church on 7th Street happened to be attending the meeting and was invited to assist with the planning for the May 9 block party. It was decided that the block party would include live music, with food trucks, events inside the church, and a walking tour of the neighborhood. Ron Parron volunteered to help arrange the walking tour.

Discussion of a proposed amendment to Section 21.01.130

Staff discussed the need to replace the DDA representative on the Historic Preservation Board. The Board indicated that an HPB member should contact the chair of the DDA to determine whether the DDA could appoint a new person to replace Brandon Stam, the DDA's executive director. Ron Parron agreed to contact the DDA chair to discuss the matter.

4. Updates

Staff gave an update on the Dennis COA and why it was postponed to the April 7 meeting.

5. Other Business/Public Comment

Timothy Stouffer spoke regarding potential demolition options for a historic garage on his property.

6. Adjournment

The meeting was adjourned at 5:22 PM.



Grand Junction Planning Commission

Regular Session

Item #

Meeting Date: April 28, 2020

Presented By:

<u>Department:</u> Community Development

Submitted By:

Information

SUBJECT:

Consider a request by Chris and Heather Dennis for a Certificate of Appropriateness (COA) to: 1) demolish a 600-square foot garage; 2) construct a 2,160-square foot garage; and 3) erect a 6-foot fence on 0.24 acres located at 640 North 7th Street within the North Seventh Street Historic Residential District.

RECOMMENDATION:

EXECUTIVE SUMMARY:

EXECUTIVE SUMMARY

The Applicants, Chris and Heather Dennis, are requesting a Certificate of Appropriateness to: 1) demolish a 600-square foot garage; 2) construct a 2,160-square foot garage; and 3) erect a 6-foot fence on 0.24 acres located at 640 North 7th Street. The existing one-story garage would be replaced by a two-story garage that features room for three cars on the ground floor and residential living space on the second floor. Two separate 6-foot cedar wood fences would also be erected as part of this COA request, one on the northeast side of the property and one on the eastern side.

BACKGROUND OR DETAILED INFORMATION

BACKGROUND

The Applicants, Chris and Heather Dennis, reside at 640 North 7th Street, which is a contributing structure in the North Seventh Street Historic Residential District ("the

District") known as the Ferbrache House. The home was built in 1905 and was originally owned by C.F. Ferbrache. It is of the Colonial Revival architectural style. The Ferbrache House also has two accessory buildings. One of these is a 220-square foot historic garage, which is a contributing structure to the District, which will remain on site. The other is a 600-square foot garage which the Applicant proposes to demolish and replace with a two-story 2,160 square foot garage. According to the Applicant, this garage was constructed in the late 1980s and is not historic. It had not yet been built when the North Seventh Street Historic Residential District was added to the National Register of Historic Places in 1984. Photos of the 600-square foot garage attached to the staff report show a dilapidated metal-roofed structure with wooden walls that does not match the color or architecture of the primary structure and is not consistent with the character of the District.

The Applicant proposes to construct a 2,160-square foot accessory structure in place of the existing garage. The structure would have two stories; the first story (which is also the building footprint) would be 1,290 square feet and would have parking for three cars, while the second story would be 870 square feet of residential living space. The roofing and paint for the new garage would match the existing house, while the garage doors would have a historic appearance that would fit with other doors in the neighborhood. More details are contained in the Analysis section of the report.

The Applicant plans to apply for a Planning Clearance to build an accessory dwelling unit on the 2nd floor of the new garage. This application will be reviewed by Community Development staff if the Historic Preservation Board approves the COA and would be reviewed administratively. The proposed ADU is an allowed use in the District per the underlying R-8 zoning for the property provided it meets zoning and building requirements.

All application for alterations, new construction or site improvements within the North Seventh Historic Residential District must be reviewed by staff and decided by the Historic Preservation Board in a public hearing.

As provided in Section 26.24.010, new construction in the North Seventh Street Historic Residential District is allowed, as long as the design, siting, and construction are congruous with the character of the District. District Guidelines and standards that are relevant to the review of this request are contained within the Analysis section of this report.

As provided in Section 26.32.020, an application for demolition for any accessory structure in the District is required to be reviewed by staff, with staff providing a recommendation to the Historic Preservation Board. The Board then makes a final decision on the application based upon review criteria found in this section. An appeal of the Board's decision shall be heard by City Council. To further inform the decision,

staff shall prepare a report with findings, including recommendation regarding historicity.

NOTIFICATION REQUIREMENTS

Notice was completed consistent with the provisions in Section 21.02.080 (g) of the Zoning and Development Code. The subject property was posted with an application sign on February 20, 2020. Mailed notice of the public hearing was sent to property owners in the North Seventh Street Historic Residential District on April 17, 2020. The notice of this public hearing was published on April 21, 2020 in the Grand Junction Daily Sentinel.

ANALYSIS

Any application for a Certificate of Appropriateness in the North Seventh Street Historic Residential District must demonstrate compliance with the District-Wide Design Guidelines and Standards found in Chapter 26.20 as well as the applicable regulations in Chapter 26.24 of the Guidelines and Standards, "Contributing Structures, Noncontributing Structures and New Construction". Applications for demolition must also meet Chapter 26.32, "Demolition of All or Part of All Structures Within the North Seventh Street Historic Residential District." Criteria from the relevant sections for this application are listed below, along with staff's recommendation regarding whether the application meets the specified criteria.

Chapter 26.20, District-Wide Design Guidelines and Standards

Applications for Certificates of Appropriateness must demonstrate compliance with this chapter including Sections 26.20.010 through 26.20.110. This section includes guidelines and standards such as minimum bulk requirements, landscaping, utility systems, and fencing, among others. With the exception of Section 26.20.010, 26.20.030 and 26.20.090 as provided below, staff has found the guidelines and standards are generally not applicable to the scope of the request. Staff finds that the relevant requirements of this chapter have been met.

Section 26.20.010 Minimum Bulk requirements/standards

There are bulk standards unique to the District that are included in the Guidelines and Standards that address accessory structure setbacks and height. Setback requirements are as follows: front setback on Seventh Street, 30 feet; front setback on side street, 10 feet; side setback, 3 feet; and rear setback, 5 feet. The height allowance is 35 feet. The current home and proposed garage meet all required bulk standards. Staff finds this criterion to be met.

Section 26.20.090, Fencing

Subsection (a), "Guideline," states that "fencing materials and styles should complement the character of the District. Fence styles, particularly in front yards, should be similar to those from the restoration era of the buildings on site. In addition to the regulations for fences applicable to the R-8 zone, fences in the District shall adhere to the requirements of this section." The implementing requirement for this subsection is 26.20.090(a)(3), which states that "side and rear yard fences should be a maximum of six feet high measured from the street side of the fence. They should be between 60 percent and 100 percent opaque. They may extend to and be placed on property lines."

The Applicant proposes to construct a 6-foot-tall cedar wood fence on the north and east sides of the property. The northern fence along Gunnison Avenue would have a gate to allow vehicular access to the existing historic shed that will be retained, while the eastern fence along the alley would provide secure access from the new driveway on the north to a parking space on the south side of the garage. The proposed fence materials are similar to materials and styles already existing in the District; the fence is 100% opaque and is located on the north and east property lines. As such the proposed fencing complies with the referenced guideline and standard; therefore, staff finds this criterion to be met.

Section 26.24.010. General Guidelines

This section states that "it is preferable to design congruous contemporary structures rather than duplicate or mimic the design of historic buildings in the District." The proposed garage matches several design elements with the primary structure on site and blends with surrounding buildings instead of duplicating them. Staff finds that this criterion has been met.

Section 26.24.020, Building Proportions, Mass and Form

26.24.020(a), "Guideline," states that a "building's overall massing and form shall honor its historical style. In the North Seventh Street Historic Residential District, the building forms have historically reflected a human scale."

The relevant standards for implementing this guideline are 26.24.020(b)(6) and (b)(7), which indicate that "new buildings shall use massing and form similar to neighboring buildings. Design shall convey a human scale through the use of traditional mass, sizes, materials, and window openings," and "new construction shall incorporate design elements such as roof forms, lines, openings, and other characteristics commonly found in the district," respectively. The building proportions, mass, and form of the proposed garage are similar to neighboring buildings within the District. Most structures, including some accessory structures, are 1 ½ to 2 stories or greater. The

proposed garage has a pitched roof comparable to the existing house and other structures in the district, and includes new windows with patterns similar to those on the existing house. Staff finds that this criterion has been met.

Section 26.24.030, Orientation and Lot Coverage

Subsection (a) states that "lot coverage should be similar in proportion to the lot coverage of neighboring lots." The proposed garage does not disrupt the existing lot coverage pattern on the subject property located at 640 North 7th Street or that of other properties in the District. Lot coverage of adjacent properties in the District range from 20.6% to 47.9%. The proposed lot coverage for this project is approximately 30.5%. Staff finds this project has similar lot coverage of neighboring lots and therefore that this criterion has been met.

Section 26.24.050, Exterior Materials

Subsection 26.24.050(a), "Guidelines," provide that new construction should "use similar building materials as those found in the district." The relevant standard for this guideline is 26.24.050(b)(1), which requires that "exterior wall materials shall be those that are commonly present in the district."

The proposed exterior wall material for this project, wooden lap siding, matches what is currently present on the Applicant's house and is commonly found on other structures within the District. Therefore, staff finds that this criterion has been met.

Section 26.24.080, Windows and Façade Treatments

Subsection (a), "General Guidelines," includes the following statement: "Windows and window patterns in new construction should be of similar proportion and size to the windows of the other buildings within the neighborhood." The Applicant proposes to use single-hung windows that generally have the shape and proportion of windows on the primary structure. Therefore, staff finds that this criterion has been met.

Section 26.24.090, Roof Forms and Materials

Subsection (a), "Guidelines," suggests that any roof in the District should "use similar roof forms, slope ratios and materials drawn from historic structures in the district," and also suggests that roofs should be gabled or hipped and that the roof pitch should be greater than 6:12. The implementing standard for this section is 26.24.090(b)(1), which requires that "materials on a new roof or replacement roof should be similar to materials found on roofs in the District." The proposed accessory structure roof has an 8:12 pitch, is gabled, and uses asphalt shingles that match the ones on the primary structure. Staff finds that this criterion has been met.

Section 26.24.100, Trim and Details

Subsection (a)(1) states that "exterior trim details on new construction should provide a visual link between the old and new buildings," while (a)(2) states that "whenever possible, windowsills, moldings, and eave lines should be aligned with similar elements on adjacent buildings within the block."

The Applicant's proposed plan shows 1x4 window trim with sill and a 1x4 apron. The roof trim features a 1x4 frieze board below the soffits. Both windows and roof trim used on the new structure replicate those on the primary structure. Eaves are represented on the submitted building elevations as being nearly identical. Therefore, Staff finds that these criteria have been met.

Section 26.28, Contributing Structures Guidelines and Standards

Subsection 26.28.020(j) states that "new additions and adjacent or related new construction shall be undertaken in such a manner that, if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired." The proposed garage does not physically connect to the existing historic structures (house and garage/shed). Staff therefore finds the proposed demolition and construction do not destroy historic materials that characterize the property and could be removed without damage to the essential form and integrity of the property. Staff finds that these criteria have been met.

Chapter 26.32, Demolition of All or Part of Structures within the North Seventh Street Historic Residential District

Section 26.32.020 of the Code provides Approval of a Certificate of Appropriateness for a demolition may be issued upon consideration of the following:

(a) Whether the applicant has made a good-faith effort to pursue reasonable, cost effective alternatives to demolition.

The Applicants have found that the useful life of the structure has been reduced to a point where it is no longer desired or practical to continue use, to remodel or otherwise reuse the accessory structure. Staff supports this finding and believes that this criterion has been met.

(b) Whether the loss of part or all of the subject property would be detrimental to the quality and continuity of the site, District or surrounding neighborhood.

The Applicants have demonstrated through the use of photography that the structure to

be demolished is not historic and would not result in a loss of historical quality for the neighborhood. Staff finds this criterion to have been met.

- (c) Whether denial of the application would result in an undue economic hardship for the owner/applicant. Based on a thorough analysis of the financial, economic, and engineering information described below, the Historic Preservation Board may determine that there is an undue economic hardship if the following criteria are met:
- (1) No economically viable use consistent with zoning of the property will exist unless the demolition is approved. (Note: inability to put the property to its most profitable use does not constitute an undue economic hardship.)

As previously noted, the existing garage structure has exceeded its useful life and as such the property owner would like to demolish the structure and replace the structure with a new garage. The property continues to have an economic viable use with or without the demolish and replacement of the accessory structure, therefore, staff has found this criterion to be not applicable.

(2) The hardship is peculiar to the building or property in question and must not be in common with other properties.

The proposed demolition affects only the property at 640 North 7th Street and the issue of this garage is specific to and not shared by other property owners within the District. Staff finds this criterion to be met.

(3) The hardship is not self-imposed, caused by action or inaction of the owner, applicant or some other agent.

The Applicants purchased this property in 2018 and have inherited the accessory structure from previous owners. Staff finds this criterion to be met.

(4) The Applicant has attempted and exhausted all reasonable alternatives which would eliminate the hardship, such as offering the property for sale.

As mentioned, remodel or continued use of the structure proposed for demolition has been found by the Applicant to be impracticable. Sale of the property is not a reasonable alternative for resolution of this issue and as such staff has found this criterion to be no applicable to the request.

Section 26.32.040 of the North Seventh Street Historic Residential District Guidelines and Standards provides that staff shall prepare a report with findings, including recommendation regarding historicity. This report describes the non-historic, nonconforming nature of the building proposed for demolition and staff finds that it is

not historic. This report also describes the new accessory structure proposed for construction and staff finds that this structure conforms to the requirements of the District Guidelines and Standards.

RECOMMENDATION AND FINDINGS OF FACT

After reviewing the request for a Certificate of Appropriateness, COA-2020-86, for demolition of an existing, non-historic accessory structure, construction of a new building, and new fencing on the property located at 640 N 7th Street, the following findings of fact have been made:

- 1. The request conforms to Chapter 26.20, District-Wide Design Guidelines and Standards for the North Seventh Street Historic Residential District
- 2. The request conforms to Chapter 26.24, Contributing Structures, Non-Contributing Structures and New Construction for the North Seventh Street Historic Residential District.
- 3. The request conforms to Chapter 26.28, Contributing Structures Guidelines and Standards.
- 4. Due consideration has been made to the criteria provided in Chapter 26.32 and it has been found that demolition is warranted in accordance with Section 26.32.010.

Therefore, Staff recommends approval of the request.

SUGGESTED MOTION

Mr. Chairman, on item COA-2020-86, an application for a Certificate of Appropriateness for the proposed demolition of an accessory structure, construction of an accessory structure, and construction of fences at 640 North 7th Street, I move that we approve the request.

BACKGROUND OR DETAILED INFORMATION:

SUGGESTED MOTION:

Attachments

- 1. Dennis project report
- 2. 07311 Dennis ADU and garage site A1
- 3. 07311 Dennis ADU and garage floor plan A1.1
- 4. 07311 Dennis ADU and garage elevations A2 (1)
- 5. 07311 Dennis ADU and garage elevations A3

- 6. Dennis photos
- 7. 20200219_161735
- 8. 20200219_161725
- 9. Street View
- 10. IMG_20200224_085630220

General Project Report



Date: 9/12/19

To: Whom it may concern,

This project is for Chris and Heather Dennis, 640 N. 7th St. Grand Junction, CO 81501

RE: Demolition of the existing detached garage and construction of a new garage with an ADU above located at 640 N. 7th Street Grand Junction, CO 81501.

Please be advised that Mor Storage LLC on behalf of Chris and Heather Dennis, is requesting approval from the City Council for a certificate of appropriateness for the following project;

Demolition of the existing 20' x 30' detached one story garage built in the late 1980's. The garage is situated in the south east corner of the lot and is approximately 9' from the alley and appears to be on the south property line. This building is not a part of the original property or house built in 1905. There is also an existing 11'x20' unfinished detached garage that is to remain.

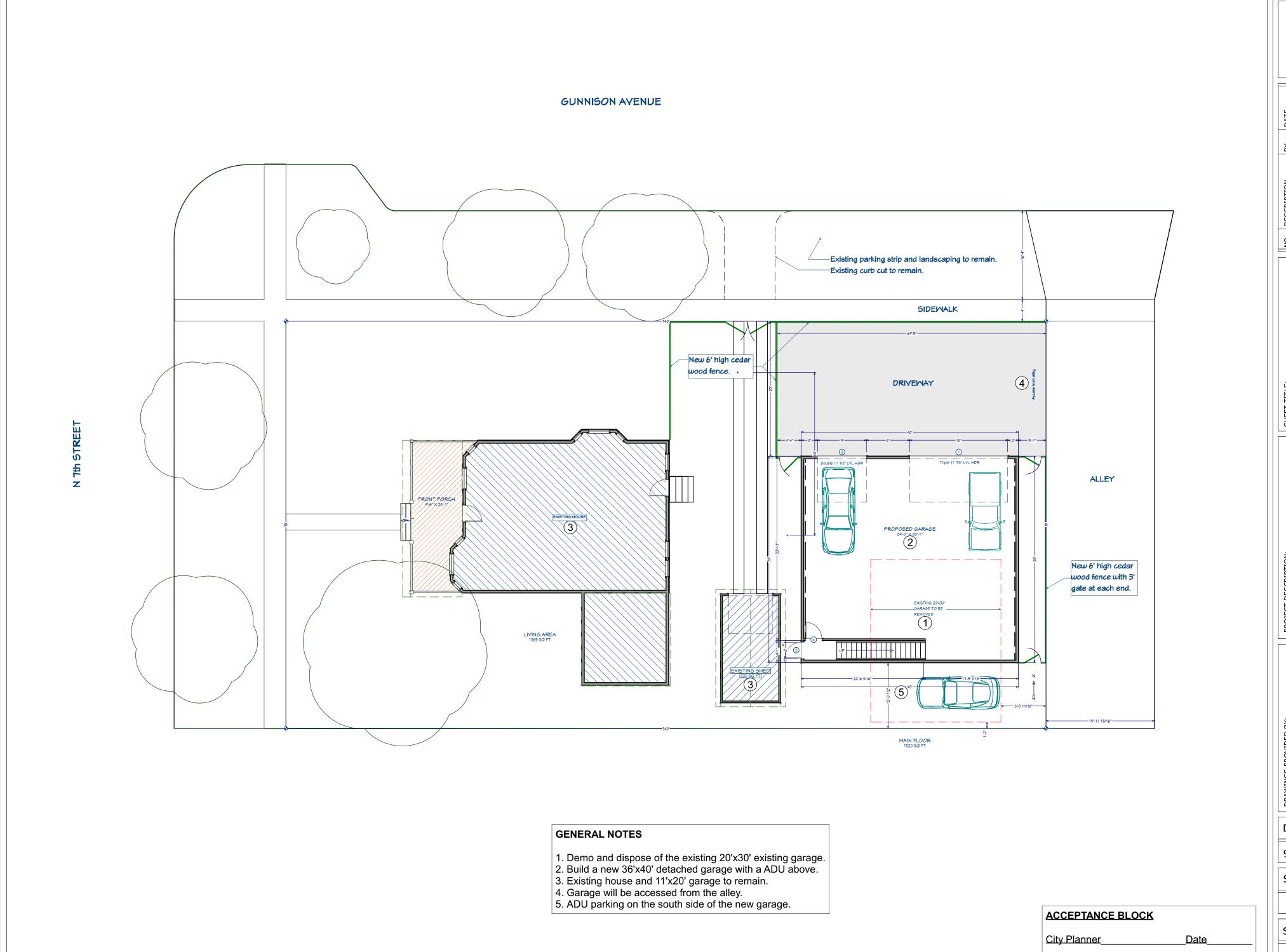
This will be phase 1 of a total planned building project to build a larger detached 1-story garage with a small accessory dwelling unit incorporated in the attic above. The new proposed accessory building will be built in approximately the same location as the old garage while observing city setback requirements. We are also requesting a fence be installed on the rear portion of the lot.

Thank You,

1

Contractor: Mor Storage, LLC

3010 I-70 Business Loop Grand Junction, CO 81504



SCRIPTION BY DATE
e Plan drk 9/19/19
vised drk 2/21/20

Site Plan

Dennis Garage and ADU 640 N 7th St Grand Junction, CO

MOR STORAGE
REES SALES BARNS
GARBERS - CARINS
GARBERS - CARRINS
GENERAL CONTRACTORS
970-254-0460

MOR STORAGE LLC
3010 I- 70 Business Loop
Grand Junction, CO 81504

DATE:

9/19/19

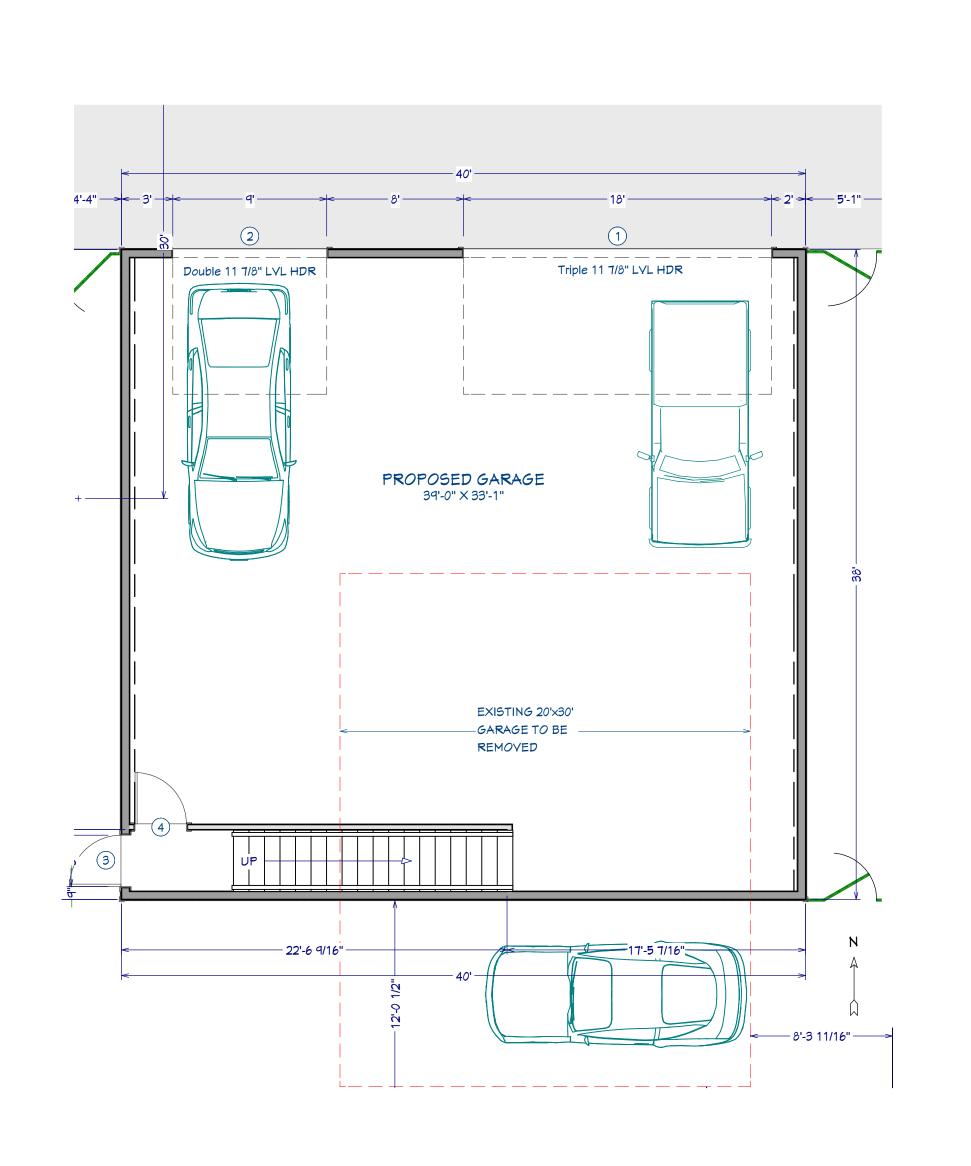
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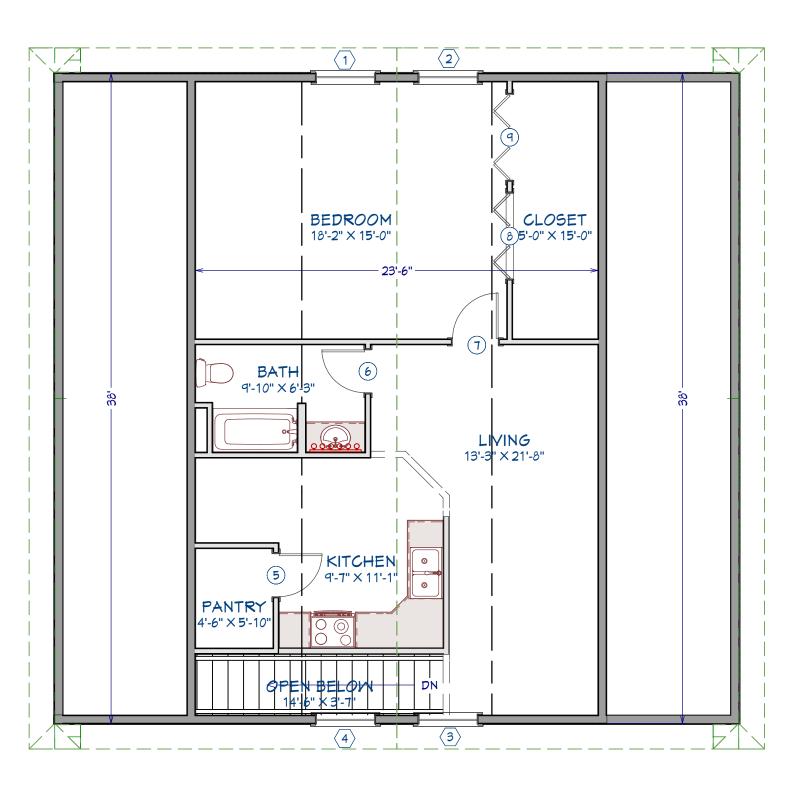
SHEET:

A-1

<u>Date</u>

City Engineer_





SECOND FLOOR 870 SQ FT

DOOR SCHEDULE							
NO.	Door Size	Туре	Frame	Notes			
1	16'-0"x 8'-0"	Wayne Dalton Sonoma	None	With Opener			
2	9'-0"x 8'-0"	Wayne Dalton Sonoma	None	With Opener			
3	3'-0"x 6'-8"	ThermaTru TS210	Ext. 6-9/16"				
4	3'-0"x 6'-8"	ThermaTru TS210	Ext. 4-9/16"	Self Closing			
5	2'-6"x 6'-8"	JeldWen Colonist	Int. 4-9/16"				
6	2'-6"x 6'-8"	JeldWen Colonist	Int. 4-9/16"				
7	2'-6"x 6'-8"	JeldWen Colonist	Int. 4-9/16"				
8	5'-0"x 6'-8"	JeldWen Colonist	None	Bi-fold			
9	5'-0"x 6'-8"	JeldWen Colonist	None	Bi-fold			

NO.	Window Size	Туре	Frame	Notes
1	3'-6"x 5'-0"	Single Hung Mezzo	Vinyl w/ 3 1/2" trim	Low-E
2	3'-6 x 5'-0"	Single Hung Mezzo	Vinyl w/ 3 1/2" trim	Low-E
3	3'-6"x 5'-0"	Single Hung Mezzo	Vinyl w/ 3 1/2" trim	Low-E, Temp
4	3'-6"x 5'-0"	Single Hung Mezzo	Vinyl w/ 3 1/2" trim	Low-E, Temp

	DATE	9/19/19	2/28/20		
	ВУ	drk	drk		
	DESCRIPTION	Floor Plan	Revised		
	NO.	-	2		

Floor Plan

SHEET TITLE:

PROJECT DESCRIPTION:

Dennis Garage and ADU
640 N 7th St
Grand Junction, CO



DRAWINGS PROVIDED BY:

MOR STORAGE LLC

3010 I- 70 Business Loop
Grand Junction, CO 81504

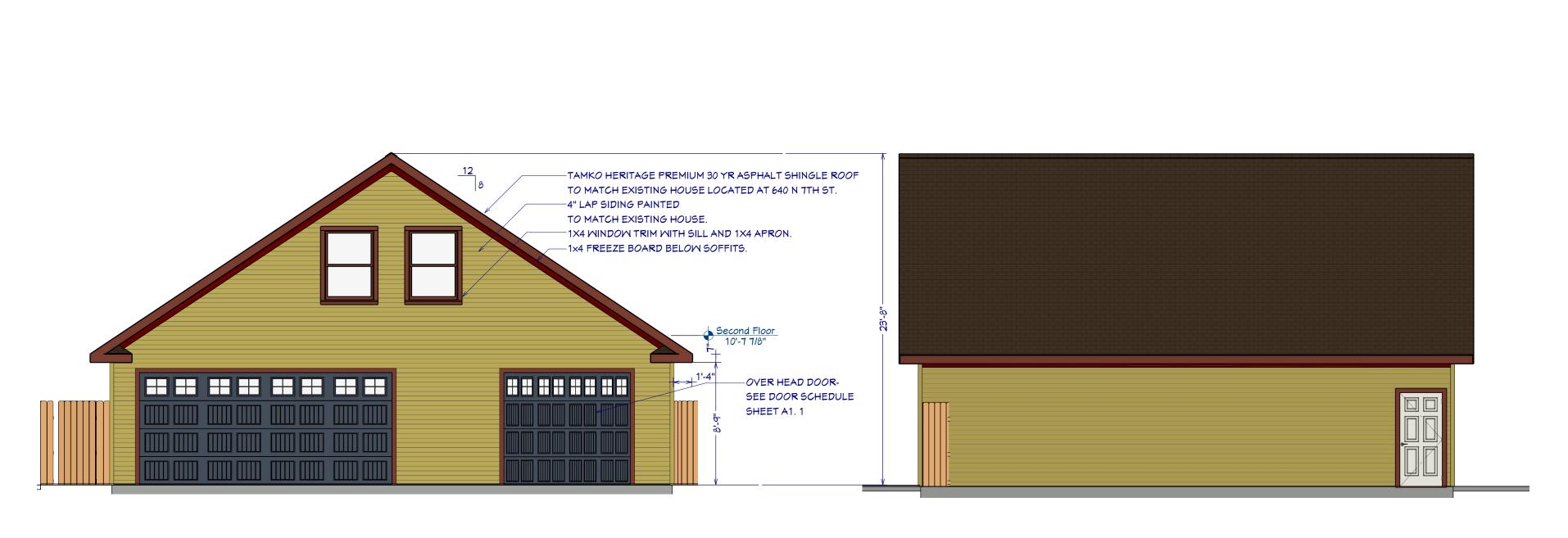
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9/19/19

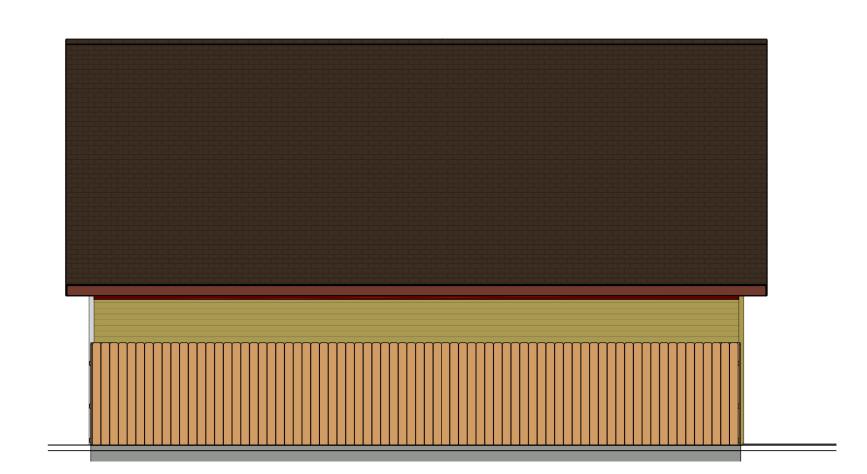
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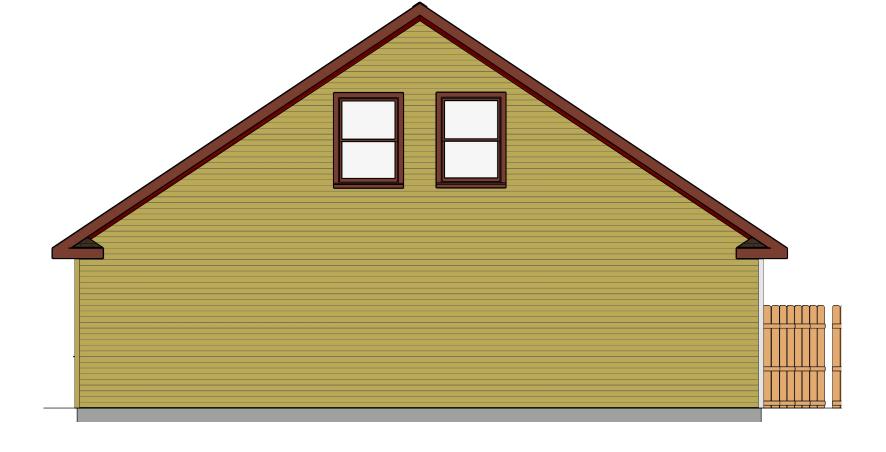
SHEET:

A1.1



North Elevation West Elevation





East Elevation South Elevation

NO. DESCRIPTION BY DATE

1 Elevations drk 9/19/19

DDS

Elevations

SHEET TITLE:

PROJECT DESCRIPTION:

Dennis Garage and ADU
640 N 7th St
Grand Junction, CO



MOR STORAGE LLC
3010 I- 70 Business Loop
Grand Junction, CO 81504

DATE:

9/19/19

SCALE:

SHEET:

A-2



NO. DESCRIPTION BY DATE

1 Elevation drk 3/2/20

HEET TITLE:
Elevation

PROJECT DESCRIPTION:

Dennis Garage and ADU
640 N 7th St
Grand Junction, CO



DRAWINGS PROVIDED BY:

MOR STORAGE LLC

3010 I- 70 Business Loop
Grand Junction, CO 81504

DATE:

9/19/19

SCALE:

SHEET:

A-3

