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**PLANNING COMMISSION WORKSHOP AGENDA
HUMAN RESOURCES TRAINING ROOM
CITY HALL, 250 N 5TH STREET
THURSDAY, AUGUST 22, 2024 - 12:00 PM
*Attend virtually: bit.ly/GJ-PCW-2024***

Call to Order - 12:00 PM

1. Discussion Regarding Neighborhood, Subarea, and Corridor Plans and Overlays

Other Business

Adjournment



Grand Junction Planning Commission

Workshop Session

Item #1.

Meeting Date: August 22, 2024
Presented By: David Thornton, Principal Planner
Department: Community Development
Submitted By: Dave Thornton, AICP, Principal Planner

Information

SUBJECT:

Discussion Regarding Neighborhood, Subarea, and Corridor Plans and Overlays

RECOMMENDATION:

EXECUTIVE SUMMARY:

Continue discussion from earlier 2024 workshops concerning neighborhood, subarea, and corridor plans and overlays.

BACKGROUND OR DETAILED INFORMATION:

Continue discussion from earlier 2024 workshops concerning neighborhood, subarea, and corridor plans and overlays. This workshop will focus on the five remaining adopted neighborhood plans/overlays: Horizon Drive, H Road/Northwest Area, Greater Downtown, North Avenue and North Avenue West.

Summaries of the analyses for these plans are below and the analyses matrices are attached.

Horizon Drive

The Horizon Drive commercial area (district) has adopted its own Strategic Operating Plan (plan) and the City has adopted an overlay zone of design standards but a subarea/neighborhood plan has never been created or adopted. Consequently, nothing needs to be repealed or revised concerning this subarea. It is recommended the overlay be kept intact as exists in the GJMC.

The Comprehensive Plan includes the following language that addresses the subarea and supports and reinforces the Strategic Operating Plan and the overlay standards.
Chapter 2 Plan Principle 2: Resilient and Diverse Economy

Horizon Drive is recognized as a Site of Commerce on page 16.

Chapter 2 Plan Principle 3: Responsible and Managed Growth

Horizon Drive is recognized as a major corridor for Guided Growth on page 19.

Chapter 2 Plan Principle 5: Strong Neighborhoods and Housing Choices

Horizon Drive is recognized as an on-street bicycle route on the map on page 27.

Horizon Drive is listed as a subarea that has design standards (no plan) in the chart on page 29.

Chapter 2 Plan Principle 6: Efficient and Connected Transportation

Horizon Drive is shown as an Active Transportation Corridor on the map on page 31 and, as such, identified as to be developed as a complete street in strategy 1.f.i.

Horizon Drive is identified as a Minor Arterial on the map on page 35.

Chapter 2 Plan Principle 7: Great Places and Recreation

Horizon Drive is shown as having an existing bike lane on the map on page 37.

Chapter 2 Plan Principle 8: Resource Stewardship

The natural drainageway along Horizon Drive is identified on the map on page 41 and, as such, addressed in strategy 4.c.

Chapter 3 Intensification and Growth Tiers

Horizon Drive is identified as Tier 2 on the map on page 57.

Horizon Drive is identified as Commercial on the map on page 59

Chapter 4 Area-Specific Policies

Horizon Drive is identified as a Commercial Areas Regional Center in the policy text on page 68 and the map on page 69.

A portion of Horizon Drive is identified as a Neighborhood Center on the map on page 73.

H Road/Northwest Area

Adopted in 2007, The H Road/Northwest Area Plan included both a Plan and an Overlay Zone. However, apart from the first two overarching policies, the Plan and the Overlay are completely one and the same. Thus, the policies of the Plan are redundant with the standards in the Overlay Zone and the Plan may be repealed but the Overlay kept intact as-is in Title 22 of the Zoning and Development Code. There is one suggestion for a future ZDC amendment that the portions of the Overlay pertaining to H-1/2 Road be re-examined to evaluate the re-classification of the roadway between 21-1/2 and 22 Roads in the most recent Circulation Plan.

Greater Downtown

Adopted in 2013, the Greater Downtown Plan included both a Plan and an Overlay Zone. The Overlay Zone has generally been utilized to implement the Plan and has been functioning well, with very few revisions having been made over the past decade.

Subsequently, the 2020 One Grand Junction was adopted and the Downtown Development Authority created its downtown plan, Vibrant Together: A Downtown Initiative. The Comprehensive Plan addresses downtown in many general policies as well as in some more detailed analyses. Similarly, Vibrant Together includes very specific priorities and a framework plan for downtown. The Comprehensive Plan acknowledges both the Vibrant Together and the Downtown Plan. In analyzing the policies of the Greater Downtown Plan, it was found that, between the Comprehensive Plan and Vibrant Together, the policies of the Greater Downtown Plan were redundant, particularly since the Overlay Zone has been incorporated into the Zoning and Development Code as Title 24. Thus, staff is recommending that the Greater Downtown Plan be repealed and the Zoning Overlay be kept intact. The only revision to the Comprehensive Plan that is suggested is incorporation of the maps that define the areas addressed by the Overlay Zone to better cross-reference with the Overlay Zone.

North Avenue and North Avenue West

The North Avenue and North Avenue West Corridor Plans were adopted in 2007 and 2011 respectively and an Overlay Zone that comprises ZDC Title 23 was adopted in 2013. North Avenue is a corridor for people and places, a crossroads of Grand Junction, a corridor to the City Center and Colorado Mesa University. A place where higher education facilities connect with medical facilities, downtown, sports facilities, historic neighborhoods, existing and future residential neighborhoods, regional retail and employment opportunities. As with other plans, the Overlay Zone has generally been utilized to implement the Plans and has been functioning very well, with few revisions made to the plans since adoption. The majority of the policies and strategies in the two plans are addressed in various sections of the Comprehensive Plan as indicated on the analysis matrices. There are a few modifications to the Comprehensive Plan suggested: 1) Add a description of the North Avenue Subareas on page 76 of the Plan and add the area to the accompanying map on that page. In addition, it is suggested that TEDS be updated to include the specific cross section for North Avenue that is contained in the subarea Plans. With the proposed changes, staff then recommends that both the North Avenue and North Avenue West Plans be repealed and the overlay in ZDC Title 23 be kept intact as is.

SUGGESTED MOTION:

This item is for discussion only.

Attachments

1. Subareas Location Map
2. Legend to Abbreviations in Plan Matrices
3. Horizon Drive District Overlay Zone - Tracking Matrix
4. H Road Northwest Area Plan - Tracking Matrix
5. Greater Downtown Plan - Tracking Matrix
6. North Avenue Corridor Plan - Tracking Matrix
7. Horizon Drive Overlay

8. H Road Northwest Area Plan
9. Greater Downtown Plan
10. Greater Downtown Overlay
11. NA Plan Res 174-07
12. NA West Plan Ord 4486
13. Page 76 Subareas - Comp Plan
14. North Avenue Street Cross-Section

GJ Zoning and Development Code

Zoning Overlays	Year Adopted
Greater Downtown	2013
H Road/Northwest	2007
North Avenue Corridor	2013
24 Road Subarea	2016
Horizon Drive	2020

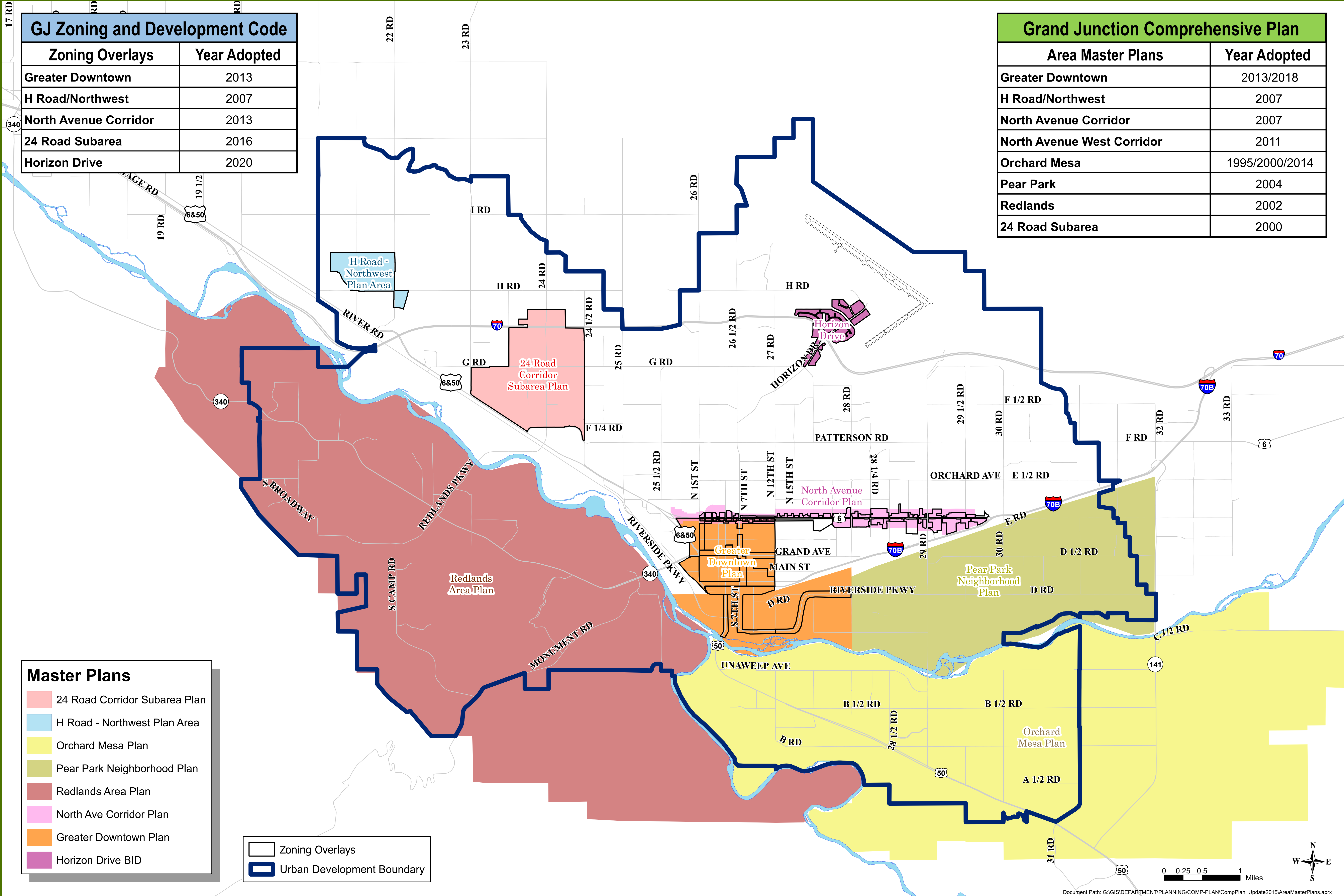
Grand Junction Comprehensive Plan

Area Master Plans	Year Adopted
Greater Downtown	2013/2018
H Road/Northwest	2007
North Avenue Corridor	2007
North Avenue West Corridor	2011
Orchard Mesa	1995/2000/2014
Pear Park	2004
Redlands	2002
24 Road Subarea	2000

Master Plans

- 24 Road Corridor Subarea Plan
- H Road - Northwest Plan Area
- Orchard Mesa Plan
- Pear Park Neighborhood Plan
- Redlands Area Plan
- North Ave Corridor Plan
- Greater Downtown Plan
- Horizon Drive BID

- Zoning Overlays
- Urban Development Boundary



Legend to Abbreviations in Plan Matrices

Comprehensive Plan

C – Chapter

PP – Plan Principle

G – Goal

S – Strategy

p. – page

Other Documents

IGA – Intergovernmental Agreement

PROS – Parks, Recreation, and Open Space Plan

TEDS – Transportation Engineering Development Standards

ZDC – Zoning and Development Code

Other Abbreviations

GIS – Geographic Information Systems

Horizon Drive District Overlay Zone Purpose	Add to Area-Specific Policies?	Current Comp Plan or Area-Specific Policy Reference and Text	Next Steps	Draft Area-Specific Policy	
The purpose of the Horizon Drive Zone District Overlay is to provide a consistent level of architectural character, quality and aesthetics of the Horizon Drive area as well as to improve and enhance pedestrian access, vehicular access, parking and circulation within the designated Horizon Drive Zone District Overlay.	Yes	Chapter 4 Area-Specific Policies Commercial Areas Regional Centers P. 68	Add Area-specific policy	See attached	

Horizon Drive District Overlay Zone Applicability	Add to Area-Specific Policies?	Current Comp Plan or Area-Specific Policy Reference and Text	Next Steps	Draft Area-Specific Policy	
The Horizon Drive Zone District Overlay includes all commercial properties within the general geographic area of Horizon Drive between G Road and H Road as identified on the zone district overlay map.					

Horizon Drive District Overlay Zone Standards and Guidelines			Add to Area-Specific Policies
27.12.010 Architectural features and materials.	Architectural features are intended to provide variations in massing, be at a human scale, and provide variety in design that work to reinforce the importance of the civic, public and open spaces.		
	(a)	All buildings facing Horizon Drive shall use materials that are durable, including but not limited to stone, brick, precast concrete and architectural metals.	
	(b)	Materials prohibited for any building facing Horizon Drive include metal-clad prefabricated buildings and building made from pre-cast concrete and tilt-up wall systems that are structural in appearance.	
	(c)	Building entries shall have a strong visual and pedestrian relationship to the street.	
	(d)	Buildings shall provide the following architectural design elements:	
	(1)	Buildings shall provide their main entry facing Horizon Drive. When not feasible, a side or rear of a building may face Horizon Drive. However, the facade fronting Horizon Drive shall give the appearance of a front facade in terms of quality of architecture, materials and detailing.	
	(2)	Building form shall provide recessed or projecting elements to provide facade articulation. This can be accomplished through the design of entryways, awnings, rooflines, projecting bays, pilasters, columns or other features. Articulation shall occur a minimum of every 30 feet for all sides of the building.	
	(3)	The first floor of a building designed to accommodate a restaurant or retail use shall have windows facing the public right-of-way that create visual interest to pedestrians and provide views from inside of buildings to the street.	
	(e)	In addition, the site shall exhibit a minimum of three of the following seven architectural design elements:	
	(1)	Variation in materials, material modules, expressed joints and details, surface relief and texture to break up building forms and wall surfaces. Such detailing may include sills, headers, belt courses, reveals, pilasters, window bays or similar features for all sides of the building.	
	(2)	Variation in roof lines/roof materials in order to add interest to and reduce the scale of buildings or expanses of blank wall. This can be accomplished through design elements such as overhangs, eaves, recesses, projections, raised cornice parapets over doors or bays and peaked roof forms.	
	(3)	Establishing the main building entrance on the street with facade features that emphasize the primary building entrance through projecting or recessed forms, detail, color and/or material.	
	(4)	Outdoor patio in combination with or without outdoor seating located between the building and the primary street.	
	(5)	Ground story transparency of at least 50 percent in the form of windows and/or door(s) for facades facing all public street frontages.	
	(6)	Public art, as approved by the Director.	
	(7)	Other architectural or site features that achieve the goals of the overall Horizon Drive District Overlay vision or concept, as determined by the Director.	
020 Site design.	Elements required or encouraged for site design are intended to minimize vehicular orientation and emphasize pedestrian activities such as ease of access from the public way and safe access to parking areas, increase walkability of the district especially between the public way, transit facilities and other buildings. They are also intended to provide safe access to businesses from the street and sidewalks, as well as parcel interconnectivity to minimize multiple access points to Horizon Drive.		
	(a)	Clearly visible and direct pedestrian paths with adequate lighting should be established between neighboring buildings, between buildings and outlying parking areas, and between buildings and transit facilities.	
	(b)	A six-foot-wide concrete sidewalk is required from the street to the front of the primary building main entrance.	
	(c)	New development shall be required to close redundant or multiple accesses to Horizon Drive from a single site. Access location and turning movements shall be limited to those which can be safely accommodated as determined by City of Grand Junction traffic engineers.	
	(d)	New development shall be required to consolidate accesses with neighboring properties to the extent practicable.	

27.12.	(e)	Bicycle parking shall be provided at locations that do not obstruct the flow of pedestrians, are easily identifiable and visible and convenient to customer entrances.			
	(f)	Where pedestrian circulation paths cross vehicular routes, a change in paving materials, textures, or colors shall be provided to emphasize the potential conflict point, improve visibility, enhance safety and enhance aesthetics.			
	(g)	On-site signage and traffic markings necessary to facilitate circulation and improve public safety and awareness are required.			
	(h)	Drive-up windows such as those used for banks, restaurants, groceries and pharmacies or drive-through facilities such as gasoline service islands and car wash or vacuum bays shall be oriented as to not be visible from the public right-of-way.			
27.12.030 Landscaping and public amenities.	This section is meant to ensure appropriate landscaping improvements and the creation of public amenities that enhance the character of the district, and to utilize xeric principles in landscaping design encouraging the planning of low water plants that are appropriate for Grand Junction’s arid climate. The intent is also to encourage future development to include civic areas and open space (parks).				
	(a)	Landscaping shall be designed to provide drought tolerant plant species that are native to the region or otherwise suitable to the climate.			
	(b)	For all development, street trees planted every 40 feet shall be planted along all public rights-of-way.			
	(c)	For all development with landscaped areas fronting Horizon Drive, any groundcover provided shall be visually similar to existing landscape rock (e.g., crushed red granite) in place on Horizon Drive.			
	(d)	Art, sculpture, transit shelters, benches, planters, bike racks, trashcans and other hardscape feature, plazas, landscaping and other amenities shall be included when appropriate.			
	Signage is intended to communicate information and reduce existing visual clutter as well as prohibit new visual clutter. Signage shall provide visual continuity within a single project.				
	(a)	Signs shall be consistent in design, color, typeface, materials and construction details with each project.			
	(b)	Freestanding signs shall comply with the following requirements:			
		(1)	Shall be placed perpendicular to the right-of-way.		
		(2)	Shall be constructed with a stone or veneer base. The sign may be single- or double-faced. If single the back side of the sign shall be painted the same color as the cabinet and poles.		
		(3)	Shall be located no closer than 10 feet from a property line and no closer than six feet from the curb of a street or drive.		
		(4)	The height shall be measured from finished grade.		
		(5)	One freestanding sign shall be allowed per street frontage.		
		(6)	Up to two small, freestanding directional signs may be allowed that are three square feet or fewer and no more than 30 inches in height.		
		(7)	The sign allowance per frontage can only be used on that frontage and shall not be transferred to any other frontage.		
		(8)	Maximum sign dimensions shall not exceed the following:		
			(i)	For properties fronting Horizon Drive between G Road and 27 1/2 Road and H Road:	
			(A)	Forty feet in height;	
			(B)	One hundred square feet for properties with up to 150 feet of linear frontage;	
			(C)	One hundred twenty square feet for properties with 150 to 200 feet of linear frontage;	
			(D)	One hundred sixty square feet for properties with 201 to 300 linear feet of frontage;	
			(E)	Two hundred square feet for properties with greater than 300 linear feet of frontage.	
			(ii)	For properties with frontage on Horizon Drive between 7th Street and G Road (27 1/2 Road), the maximum sign dimensions shall not exceed the following:	
			(A)	Twelve feet in height;	
			(B)	One hundred square feet.	
			(iii)	For properties with Interstate 70 frontage maximum sign dimensions are subject to GJMC 21.06.070.	
			(iv)	For any regulated cannabis business, the maximum sign dimensions shall not exceed the following:	
			(A)	Twenty feet in height;	
			(B)	Seventy-five square feet.	
	(c)	Flush wall signs may be either nonilluminated or internally illuminated.			
	(d)	No off-premises signs or outdoor advertising shall be permitted, except where existing at the time of adoption of the ordinance codified in this title, provided such signs are appropriately permitted through the City of Grand Junction.			
	(e)	Projecting signs shall be permitted as per GJMC 21.06.070.			

	(f) Roof signs are not allowed.	
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[illegible]

H Road / Northwest Overlay Zone Policies and Performance Standards			Add to Area-Specific Policies?	Current Comp Plan or Area-Specific Policy Reference and Text	Potential Next Steps/Notes
PLAN ADOPTION		Contains language regarding amendment of City and County's Future Land Use Map and previous version of the Grand Valley Circulation Plan - both are now obsolete with recent adoption of Grand Junction Circulation Plan and the 2020 Comprehensive Plan.	No	Not applicable, references obsolete planning documents.	Repeal Plan but retain overlay
FUTURE LAND USE MAP		Contains map that shows previous Future Land Use Map that is now obsolete with recent adoption of the 2020 Comprehensive Plan which has new/different land use categories.	No	Not applicable, references obsolete planning documents.	Repeal Plan but retain overlay
POLICIES	22.12.010	Affected area. The following performance standards shall affect the entire H Road/Northwest Area Plan.	No	Completed, included in overlay ZDC 22.12.010.	
	22.12.020	Site design shall direct truck (operations) traffic to the 21 1/2 Road Corridor. All other traffic including customer or light vehicle traffic may also use 22 Road and H 1/2 Road.	No	Completed, included in overlay ZDC 22.12.020.	
	22.12.030	All signage as defined under the existing development codes and regulations of the City and County as off-premises signs are not allowed anywhere within the H Road/Northwest Area Plan boundaries.	No	Completed, included in overlay ZDC 22.12.030.	
ORDS	22.12.040	Development on all parcels abutting the west side of 22 Road from H Road to H 1/2 Road and the south side of H 1/2 Road from 21 Road to 22 Road shall be required to meet the following performance standards.	No	Completed, included in overlay ZDC 22.12.040.	
	22.12.050	All property frontages along these corridors shall provide at a minimum: (a) A 25-foot-wide landscaping strip the entire length of the frontage (excluding driveways). (b) A berm the entire length of the frontage with a minimum of 36 inches in height. Fencing shall not be allowed within the 25-foot landscape strip with the exception of split rail fences with up to three rails and not more than four feet in height.	No	Completed, included in overlay ZDC 22.12.050.	With future Code recommend update to amend H-1/2 corridor to only include 21-1/2 to 22 Road
	22.12.060	All loading docks and fleet/equipment parking shall be located in the rear half of the lot or behind the principal structure (i.e., south side of buildings fronting on H 1/2 Road and west of buildings fronting on 22 Road).	No	Completed, included in overlay ZDC 22.12.060.	
	22.12.070	Outdoor storage areas shall be: (a) Adequately screened so as not to be visible from adjacent public roads (i.e., H 1/2 Road and 22 Road); (b) In the rear half of the lot or behind the principal structure (i.e., south of buildings fronting on H 1/2 Road and west of buildings fronting on 22 Road); (c) Trash dumpsters shall be fully screened and located in the rear half of the lot or behind the principal structure.	No	Completed, included in overlay ZDC 22.12.070.	

PERFORMANCE STANDARDS	22.12.080	All parking lots located within the front half of the parcel or front of the principal structure (adjacent to 22 Road and H 1/2 Road rights-of-way) shall only be used for customer parking.	No	Completed, included in overlay ZDC 22.12.080.	
	22.12.090	Applies only to building facades facing the 22 Road and H 1/2 Road rights-of-way. Building form shall incorporate projected and recessed elements to provide architectural variety, such as entryways, special functional areas, rooflines, and other features, including the following requirements: (a) Blank, windowless walls are discouraged. Where the construction of a blank wall is necessary, the wall shall be articulated. (b) Large monolithic expanses of uninterrupted facades (greater than 50 feet) are not allowed. Pilasters, texture transitions, windows and stepping of the wall plane are required. (c) Buildings with flat roofs shall provide a parapet with an articulated cornice. (d) All primary buildings shall use materials that are durable, economically maintained, and of quality that will retain their appearance over time including but not limited to stone, brick, stucco, and pre-cast concretes.	No	Completed, included in overlay ZDC 22.12.090.	
	22.12.100	Only monument style signs at a maximum of eight feet in height with a maximum total of 64 square feet per sign face shall be allowed. Signs shall not be internally illuminated. External illumination is allowed.	No	Completed, included in overlay ZDC 22.12.100.	
	22.12.110	The following are adequately addressed under existing development codes and City of Grand Junction and Mesa County regulations and therefore conformance must be met through the development process under then-existing code requirements: (a) Retail sales/wholesale sales area; (b) Odors; (c) Glare; (d) Parking lots; (e) Lighting standards; (f) Noise (regulated in § 25-12-103, C.R.S., maximum permissible noise levels, and GJMC 8.16.010).	No	Completed, included in overlay ZDC 22.12.110.	

Area-wide goals and policies			Add to Area-Specific Policies?	Current Comp Plan or Area Specific Policy Reference and Text	Potential Next Steps/Notes
GOALS	1	Enhance the transportation system to accommodate automobiles, bikes and pedestrians, and provide adequate, convenient parking.	No	PP 6. p. 32 1. Continue to develop safe, balanced and well connected transportation system that enhances mobility for all modes. Also Vibrant Together: Connectivity p. 46.	
	a	Street design will accommodate travel lanes, parking, bike lanes, medians, sidewalks, and street trees, appropriate to and complementary of the adjacent land use.	No	PP 6. p. 32 1. Continue to develop safe, balanced and well connected transportation system that enhances mobility for all modes. Also Vibrant Together: Connectivity p. 46. TEDS	
	b	Street design will achieve a balance between travel mobility, land use access and livability and improve connections to the Greater Downtown area and the connections between subdistricts.	No	PP 6. p. 32 1. Continue to develop safe, balanced and well connected transportation system that enhances mobility for all modes. Chpt 4 Mixed Use Areas: Downtown Core Circulation and Access and Streetscape p. 71. Also Vibrant Together: Connectivity p. 46.	
	c	Emphasize “walkability” of the downtown area through street design that is pedestrian friendly to provide a foundation for a safe, active and livable area, including sidewalks, accessibility improvements, bicycle facilities, off-street trail connections and safe crossings, where appropriate.	No	PP 6. p. 32 1. Continue to develop safe, balanced and well connected transportation system that enhances mobility for all modes. Chpt 4 Mixed Use Areas: Downtown Core Circulation and Access and Streetscape p. 71. Also Vibrant Together: Connectivity p. 46.	
	2	Establish and improve entry points into the Greater Downtown area (refer to the wayfinding and signage map in GJMC 36.16.070).	No	Vibrant Together p. 35. and p. 46 C.4 Initiate a Gateway and Wayfinding study to improve ease of navigation for pedestrians, bikes and vehicles in downtown.	
	a	Street and streetscape design will include signage, landscaping and other design elements to delineate appropriate entry points into Greater Downtown.	No	Vibrant Together p. 35. and p. 46 C.4 Initiate a Gateway and Wayfinding study to improve ease of navigation for pedestrians, bikes and vehicles in downtown.	
	3	Promote downtown living by providing a wide range of housing opportunities, primarily in the Downtown District.	No	PP 5 p. 25 Expanded housing options p. 71 Area Specific Policies: Mixed and relationship of uses.	
	a	Support a regional housing strategy with an emphasis on infill, downtown housing.	No	Housing Strategy	City Council adopted a Housing Strategy in 2021
	b	Promote development patterns and regulations that accommodate vertical mixed-use development, primarily in the Central Business District.	No	PP 5 p. 25 Expanded housing options p. 71 Area Specific Policies: Mixed and relationship of uses. Building Form and Character	Completed in both adopted Comprehensive Plan and DDA Plan of Development

	4	Redefine the land use along key corridors to provide a mix that will offer the most opportunities for redevelopment and revitalization.	No	Chpt 3 Land Use and Growth: Mixed Use Land Use. Chpt 4 Area Specific Policies, Mixed Use, Downtown Mix and Relationship of uses.	
	i	Define subareas and corridor areas for groupings of land uses that are complementary to the rest of the Greater Downtown area.	Yes	Completed. Areas defined in adopted Overlay ZDC 24.04.010 Figures 2 and 3.	Amend Comprehensive Plan to include maps of subareas and corridor areas after existing page 75 in the Comprehensive Plan.
	ii	Mixed uses, including residential, will be encouraged in appropriate subareas and corridors.	No	Chpt 3 Land Use and Growth: Mixed Use Land Use. Chpt 4 Area Specific Policies, Mixed Use,	Completed

Downtown District goals and policies			Add to Area-Specific Policies?	Current Comp Plan or Area-Specific Policy Reference and Text	Potential Next Steps/Notes
ALS	1	Maintain and enhance the economic, cultural and social vitality of the Downtown District.	No	PP 4 p. 23 1. Cultivate energetic and livable greater downtown and university districts that balance the needs of residents, students and visitors. Vibrant Together Placemaking p. 51 Ideas 1-6	
	a	Define subareas and corridor areas for groupings of land uses that are complementary to the rest of the Greater Downtown area.	Yes	Completed. Areas defined in adopted Overlay ZDC 24.04.010 Figures 2 and 3.	Amend Comprehensive Plan to include maps of subareas and corridor areas after existing page 75 in the Comprehensive Plan.
	b	Implement infill and redevelopment policies that support downtown.	No	Infill Policy	Completed
	c	Encourage a wide mix of uses, offering retail and commercial services at ground level and business/office/residential on upper floors in all but residential areas.	No	Land use Plan and Vibrant Together	
	d	Maintain and expand public amenities and services in the Downtown District.	No	PP 4 p. 23 1. Cultivate energetic and livable greater downtown and university districts that balance the needs of residents, students and visitors. Vibrant Together Placemaking p. 51 Ideas 1-6	
	e	Enhance and preserve Whitman and Emerson Parks to integrate the space into the downtown fabric and encourage use by the community.	No	PROS. Vibrant Together Placemaking B4 p. 90	Completed. Parks are both being renovated with new uses proposed.
	f	The City with assistance from the Downtown Development Authority will explore the alternative street configuration to relocate the one-way couplet of streets that are currently Ute and Pitkin Avenues to utilize Pitkin and South Avenues for this purpose.	No	CDOT has moved ahead with plans for Ute and Pitkin Avenues that are currently under construction. Thus, this option is no longer valid.	
	g	Study alternatives for 4th and 5th Streets including returning these streets to the two-way grid system between Ute Avenue and North Avenue.	No		Study completed. Streets to remain one-way but currently being updated/under construction.
	h	Within the CBD, encourage shared parking, discourage single-use, surface parking and develop new means of paying for shared parking (e.g., develop a fee in lieu of required on-site parking that will be used to fund shared parking structures).	No	PP4 p. 24 2c. Continue to manage and improve upon the utilization of existing public parking facilities in Downtown. Vibrant Together 5. p. 51 Develop parking programs to better utilize parking.	
	2	Require density/intensity in the Downtown District as prescribed by the Comprehensive Plan, primarily within the Central Business District.	No	General policies in Overlay, ZDC	
	a	Strengthen means of implementation that promote vertically mixed-use structures, primarily within the CBD.	No	General policies in ZDC, Comp Plan, Vibrant Together	
	b	Require minimums in height and density/intensity for new development in the CBD.	No	Overlay 24.12.100 (a) minimum building height in Downtown Core.	

c	Require minimal or no building setbacks within the Downtown Core to maximize site intensity/density.	No	Code, Overlay 24.12.100 (b)(1) Max building setbacks, Vibrant Together	
3	Develop a pedestrian-oriented, walkable Downtown Core.	No	General policies in ZDC, Overlay, Vibrant Together	
a	Discourage uses on ground level that do not support pedestrian activity.	No	Overlay c. architectural character. Vibrant Together Big Ideas 2. p. 35 Prioritize pedestrian and bike improvements to improve mobility throughout downtown and to the river	
b	Require building facade details that activate the ground floor, particularly on corner buildings to activate north-south streets.	No	Overlay c. architectural character. Vibrant Together Big Ideas 2. p. 35 Prioritize pedestrian and bike improvements to improve mobility throughout downtown and to the river	
4	Stabilize and enhance the residential subareas.	No	Overlay. Residential Areas Standards and Guidelines	
a	Discourage further encroachment of nonresidential uses into the established residential neighborhoods.	No	Overlay 24.12.120 (a) and (b)	
b	Establish design standards to address conservation and enhancement of the residential development patterns and streetscape.	No	Overlay 24.12.130 Architectural considerations	
c	Establish design standards for the transitional subareas to emphasize use and development compatibility with adjacent residential areas.	No	Overlay Transitional Areas Guidelines and Standards	
d	Promote the establishment of neighborhood watch and neighborhood organizations.	No	PP10 Safe, Healthy and Inclusive Community; p. 47 1.g	
5	Recognize and promote opportunities to build subareas/neighborhoods, each with a unique identity.	No	Overlay established subareas and corridors with unique guidelines and standards for each.	
a	Develop a set of guidelines within each subarea to address building and facade design, streetscape, landscape and other elements of site development.	No	Overlay completed/adopted	Completed
6	Jump-start the revitalization and reinvestment in the Downtown District with strategic catalyst projects.	No	Vibrant Together: Some projects underway or in Planning Phases. Development Ideas p. 77 Catalyst Projects	
a	Plan and budget for strategic property acquisition for future development.	Yes	Vibrant Together: Development Idea 4 p. 76 Leverage the Downtown Partnership financial tools to incentivize development. PP 11	
b	Identify locations for and promote concepts of catalyst projects, including public building/housing/mixed use, live/work units, mixed-use retail/residential and mixed-use retail/office.	No	Vibrant Together: Development Idea 4 p. 76 Leverage the Downtown Partnership financial tools to incentivize development. PP 11	

Rail District Goals and Policies			Add to Area-Specific Policies?	Current Comp Plan or Area Specific Policy Reference and Text	Potential Next Steps/Notes
GOALS	1	Preserve the opportunity for heavy industry and rail service that supports it.	No	PP 1. p. 15 g. Agricultural and Industrial Roots	
	a	The City will maintain industrial zoning in those areas served by rail lines and sidings.	No	Chapter 3 Land Use Plan	
	2	Recognize distinction between “industrial” streets such as 9th and 12th Streets and “public” streets 7th Street and Riverside Parkway.	No	Chapter 4 Area-Specific Policies - for Downtown Core address Circulation and Access. Overlay Corridor Guidelines for Commercial/Industrial ZDC 24.08 II. And III..	Completed. Most recent Circulation Plan and TEDs include classification and street sections of these streets as major collectors.
	a	Develop street sections that reflect the differences in development patterns along and the use of the street.	No	Completed. TEDS recently updated and adopted.	
	b	Discourage industrial and commercial traffic from using C-1/2 Road and direct this traffic to travel north and east using the Riverside Parkway.	No	Completed. Circulation to get to C 1/2 is circuitous - limits traffic on C 1/2	Completed
	3	Promote higher quality, customer and pedestrian friendly development along 7th Street and Riverside Parkway.	No	Commercial area specific policies p. 75. Identifies 7th and parkway as commercial area and lower Downtown Subarea character areas	
	a	Implement design guidelines and standards along corridors that will result in site and building design improvements along the corridors.	No	Overlay corridor Guidelines and Standards	
	4	Re-establish and improve a street grid in the Rail District.	No	Overlay corridor Guidelines and Standards. Functional improvements in Comp Plan Site Specific p. 78; TEDS	
	a	The City will implement the Grand Junction Circulation Plan in the Rail District, including construction of new streets as development occurs in the industrial and commercial/industrial areas.	No	Ongoing. TEDS, Circulation Plan, Bike Ped Plan	

River District goals and policies			Add to Area-Specific Policies?	Current Comp Plan or Area Specific Policy Reference and Text	Potential Next Steps/Notes
GOALS	1	Create/maintain/enhance a green waterfront.	No	PP 1 Retaining Character. PP 7 Great Places and Recreation p. 37 1.e. p. 38 Colorado River Trail. PROS plan	
	a	Take advantage of and create opportunities and partnerships to enhance the riverfront trail system.	No	PP 8 Resource Stewardship 4.b. p. 42 Collaborate with partners on resource preservation. PROS plan	
	b	Take advantage of and create opportunities and partnerships to develop Las Colonias Park and open space areas within the Jarvis property.	No	PP 8 Resource Stewardship 4.b. p. 42 Collaborate with partners on resource preservation. PROS plan	
	2	Create retail, general commercial and mixed use opportunities that complement the uses along the riverfront.	No	Chapter 5 Land Use Plan	
	a	Utilize zoning, overlay districts and incentives for development and redevelopment of complementary uses.	No	Complete. Overlay districts; incentive areas established	Complete
	3	Create/enhance redevelopment opportunities and partnerships.	No	Vibrant Together, ongoing	
	a	The City will work with the Downtown Development Authority to expand its boundaries.	No	Vibrant Together, ongoing	
	b	The City will consider implementation of incentive strategies for redevelopment.	No	Vibrant Together, ongoing	
	c	The City will consider redevelopment opportunities for the Jarvis property including the potential for public-private partnerships.	No	Complete - Dos Rios Underway. Vibrant Together	Complete

North Avenue Preferred Alternative		Add to Area-Specific Policies?	Current Comp Plan or Area-Specific Policy Reference and Text	Notes	Potential Next Steps
The predominant elements of the preferred alternative are:					
a	A student and entertainment district in proximity to Mesa College would also provide student housing, after 5:00 p.m. activities and the opportunity for a quality hotel to serve the area.	Yes	This section west of 27 1/2 Road is in the Mixed Use Area-Specific Policies. It is also in Ch 2, pp 4 Downtown and University Districts as part of the University District Area and much of it also in the Mixed Use area on the map found in Ch 4. p. 73 Mixed Use Area-Specific Policies.	Previous visioning and planning for the North Avenue Corridor has identified the 4-mile corridor into separate subdistricts. These subdistricts include a student and entertainment district in proximity to Mesa College where student housing, after five activities and the opportunity for a quality Hotel to serve the area would enhance and revitalize the corridor. A Neighborhood Center where Senior Housing located in proximity to a quality grocer, neighborhood retail, and the potential for a hotel to serve the hospital and surrounding neighborhood is visioned for the area between 23rd Street and 28 ¼ Road. A multi-family and attached residential district connecting to the neighborhoods to the north, and potential for office space to the south is identified. for the corridor between 28 ¼ Road and 28 ¾ Road. A mixed-use area with residential over retail incorporated with the governmental functions and potential public plazas for the area from 28 ¾ Road to just east of 29 Road is envisioned. This can create a gateway for the 29 Rd. intersection that blends into a regional retail area to anchor the east end of the corridor.	Add language to Subareas p. 76 by adding the North Avenue Overlay boundary on the map and adding language on North Avenue Corridor including 1) Mix and Relationship of Uses, 2) Circulation and Access, 3) Building Form and Character, and 4) Streetscape and Signage
b	A neighborhood center where senior housing is located in proximity to a quality grocer, neighborhood retail, and the potential for a hotel to serve the hospital and surrounding neighborhood.				
c	A multifamily and attached residential district connecting to the neighborhoods to the north, and potential for office space to the south.		Ch 4. p. 69 Commercial Area-Specific Policies and		
d	A mixed use area with residential over retail incorporated with the governmental functions and public plazas to create a gateway for the 29 Road intersection.		Ch 4. p. 69 Commercial Area-Specific Policies and		
e	Regional retail to anchor the east end of the corridor.		Ch 4. p. 69 Commercial Area-Specific Policies and		

North Avenue Key Projects		Add to Area-Specific Policies?	Current Comp Plan or Area-Specific Policy Reference and Text	Potential Next Steps/Notes
a	A civic plaza where the intersection treatments would provide for clear and safe pedestrian traffic movement. The civic space would serve as a gathering space and as an entry marker giving the North Avenue Corridor an identity. Existing restaurant spaces could be maintained while new residential development would be tucked around existing building pads. Such projects could be realized at the intersection of North Avenue and 12th Street, where a student and entertainment district would create a marker and gathering space for students as well as the surrounding community. This potential project could serve the needs for higher quality restaurants, entertainment, and a student housing district. This key project ultimately could serve as the catalyst to entice development and the further revitalization of the greater North Avenue Corridor.	No	Commercial Areas: Commercial Corridor - Mix and Relationship of Uses, Building Form and Character (p. 68) and Mixed Use Areas: University District - Mix and Relationship of Uses, Streetscape (p. 71)	Civic space/infrastructure enhancements created in 2022 with the Stocker Stadium project at 12th/North.
b	Improvements to the streetscape are another key project that would further promote the revitalization of this corridor. A pedestrian-friendly environment using native, xeric plant materials that are easily maintained while yet providing an appealing and beautiful streetscape are important to attract pedestrian traffic. Median cuts and numerous parking lot accesses will be consolidated. Traffic level of service and access will not be impacted. Intersections and medians would be improved while a cohesive kit-of-parts would bring character to the corridor. Improved sidewalks, lighting, street furniture and signage will improve the pedestrian, bike and vehicular access, allowing for retail and commercial to thrive in a safe environment.	Yes	North Avenue from 27 1/2 Road east is part of the Commercial Areas "Commercial Land Use" designation in Chapter 4 Area-Specific Policies.. This chapter supports increasing sidewalk width and designation of an appropriate and safe space for bicycles. Also supports Streetscape improvements including ped signage, benches and street trees, wayfinding and connectivity to enhance streetscape functionality and safety. Implemented through North Avenue Overlay Zone and the 2023 Zoning and Development Code. Much has been accomplished though recent CDOT and City infrastructure improvements. Additional improvemetns have been funded and will be completed in 2025/26	Add language to Subareas p. 76 by adding the North Avenue Overlay boundary on the map and adding language on North Avenue Corridor including 1) Mix and Relationship of Uses, 2) Circulation and Access, 3) Building Form and Character, and 4) Streetscape and Signage
c	Assembly of parcels of land where a large development can build momentum and entice greater investment would bring the vitality back to North Avenue. The illustration presents retail and commercial uses with residential above combined with a plaza and patios. Senior housing, a grocer and neighborhood retail provide an environment to attract individuals interested in living within a vibrant community. Surrounding neighborhoods would further sustain the community node. Infill development such as this will assist in a neighborhood center as well as a regional destination promoting activity beyond 5:00 p.m. and on weekends.	No	Mixed Use Areas: Neighborhood Center - Mix and Relationship of Uses, Building Form and Character (p. 70)	

North Avenue Phasing Concept			
	Add to Area-Specific Policies?	Current Comp Plan or Area-Specific Policy Reference and Text	Potential Next Steps/Notes
The phasing of development would allow for existing buildings and uses to slowly become integrated into the larger vision of the North Avenue Corridor. Illustrated below is an example of how such phasing could occur:	No	Commercial Areas: Commercial Corridor - Building Form and Character (p. 68)	

North Avenue Transportation		Add to Area-Specific Policies?	Current Comp Plan or Area-Specific Policy Reference and Text	Potential Next Steps
a	There are 25 intersections within the North Avenue study area, not including driveways to abutting property. Sixty percent are T-intersections (a three-leg intersection) and 40 percent are cross-intersections (a four-leg intersection). Intersections, by their very nature, create numerous conflicts between vehicles, bicycles, and pedestrians. For instance, there are 32 vehicle/vehicle conflict points and 16 vehicle/pedestrian conflict points at every four-leg intersection.	No	Commercial Areas: Commercial Corridor - Access and Streetscape (p. 68)	
b	The design of an intersection requires a balance between the needs of vehicles, pedestrians, bicyclists, freight and transit. The following are attributes of good intersection design for pedestrians, as documented in AASHTO’s Guide for the Planning, Design and Operation of Pedestrian Facilities (2004). (1) Clarity – Making it clear to drivers that pedestrians use the intersections and indicating to pedestrians where the best place is to cross; (2) Predictability – Drivers know where to expect pedestrians; (3) Visibility – Good sight distance and lighting so that pedestrians can clearly view oncoming traffic and be seen by approaching motorists; (4) Short Wait – Providing reasonable wait times to cross the street at both unsignalized and signalized intersections; (5) Adequate Crossing Time – The appropriate signal timing for all types of users to cross the street; (6) Limited Exposure – Reducing conflict points where possible, reducing crossing distance and providing refuge islands when necessary; and (7) Clear Crossing – Eliminating barriers and ensuring accessibility for all users.	No	Pedestrian and Bicycle Plan (See Bicycle Crossing Guidance, p. 54 and Pedestrian Crossing Guidance, p. 66)	
c	Signalized intersections typically have marked crosswalks, Walk/Don’t Walk indications, and regulatory signing. Motorists expect pedestrians and pedestrians are reasonably well protected when crossing at these locations. The study corridor currently has nine signalized intersections. Most of these signals are located at one-quarter-mile intervals. The most notable exception is between 12th Street and 23rd Street, where there is no signalized intersection for approximately three-quarters of a mile. In urban areas, pedestrians must be able to cross streets at regular intervals. They rarely will go more than 300 to 400 feet out of their way to take advantage of a controlled intersection. Consequently, the crossings at uncontrolled locations deserve serious consideration as urban arterial corridors redevelop. Treatments commonly used on higher-volume multilane streets throughout the United States include: high-visibility markings with double-posted pedestrian crossing signs; refuge islands; flags; and flashing beacons. Signals that are used just for pedestrian crossings are used primarily at midblock crossings.	No	Pedestrian and Bicycle Plan (See Safe Objectives p.20 that includes safety at intersections and along high injury areas such as North Avenue, and connectivity objectives)	Call for additional protected crossings

North Avenue Streetscape Kit-of-parts	Add to Area-Specific Policies?	Current Comp Plan or Area-Specific Policy Reference and Text	Potential Next Steps/Notes
The following illustration and images serve as a guide of how the streetscape character using amenities can create a cohesive and unified streetscape.	No	Commercial Areas: Commercial Corridor - Access and Streetscape (p. 68)	Consider adding different amenities/treatments to p.68 Commercial Corridor - Access and Streetscape (2025 refresh?)

North Avenue Streetscape		Add to Area-Specific Policies?	Current Comp Plan or Area-Specific Policy Reference and Text	Potential Next Steps/Notes
The creation of a more unified street edge condition along North Avenue will improve the overall character of the corridor. This can be accomplished by consolidating existing curb cuts and parking lots, providing more definition to vehicular entry ways, and adding walks and plantings. The addition of pedestrian-scale street lights, trees, benches and other amenities will create a more pedestrian-friendly environment, and encourage local residents to walk or bike along the corridor. Most of these improvements can be made within the existing street right-of-way. Suggested improvements outside of the right-of-way can be made without the removal of existing structures.		Yes	Commercial Areas: Commercial Corridor - Transportation Modes, Access and Streetscape, Building Form and Character (p. 68); Mixed Use Areas: Mixed Use Corridor - Circulation and Access, Building Form and Character, Streetscape (p. 70)	Add language to Subareas p. 76 by adding the North Avenue Overlay boundary on the map and adding language on North Avenue Corridor including 1) Mix and Relationship of Uses, 2) Circulation and Access, 3) Building Form and Character, and 4) Streetscape and Signage
a	Drainage Way Adjacent to Street. In areas where there are open concrete channels adjacent to the street, suggested improvements include the creation of a generous vegetated swale to collect, filter, and distribute stormwater, as well as provide an irrigation source for planted trees. The addition of an attached walk, defined curb cut entrances, and pedestrian light fixtures will also enhance and define the street edge. An alternative would be to provide a covered pipe to convey this drainage. This proves less advantageous relative to cost, construction caused delays to traffic and the loss of utilizing this runoff for future plantings.	No		2024-2025 CIP sidewalk/streetscape installation will complete implementation
b	Building Adjacent to Street. In areas where buildings front the street, suggested improvements include sidewalks that connect building entrances to walks along North Avenue, defined curb cut entrances, and the relocation of parking between buildings. These improvements will enhance the street edge by reducing the number of parked cars in front of building entrances, provide space for street tree plantings, and reduce the expanse of asphalt. This makes the buildings easier to see and identify from North Avenue. Additional sidewalks provide a clear, safe pedestrian path between parking lots to building entrances, and provide pedestrian connections to North Avenue.	No	Mixed Use Areas: University District - Building Form and Character	Implemented in 2023 ZDC. Include description of overlay standards in write-up
c	Parking Lot Adjacent to Street. In areas where parking lots front the street, suggested improvements include defined curb cut entrances, added sidewalks, and additional plantings to define parking lot circulation and enhance way finding. Tree plantings can be added to existing parking lots by removing asphalt at the end of the parking bays, defining an entrance into the lot (Option B). Another option is to include planted islands between parking bays, which is especially effective in reducing the expanse of paved areas and urban heat islands often found in large parking lots (Option A). Attached or detached sidewalks can be added adjacent to North Avenue, depending on the specific condition and width of the right-of-way. Fencing, 42 inches tall, can also add positive streetscape character. The fencing should be steel and open, and may be an opportunity to incorporate art, static or kinetic, and color.	No		Include description of overlay standards in write-up

North Avenue Streetscape		Add to Area-Specific Policies?	Current Comp Plan or Area-Specific Policy Reference and Text	Potential Next Steps/Notes
d	<p>Curb Cut Consolidation. Reducing the quantity of curb cuts along North Avenue will greatly enhance the overall character of the corridor, and improve vehicular and pedestrian circulation. Curb cuts can be consolidated in areas where parking lots can be linked and shared by adjacent uses. Consolidating curb cuts also provides additional space for plantings and sidewalks adjacent to North Avenue within the right-of-way and less discontinuity of the sidewalk.</p>	No	Commercial Areas: Commercial Corridor - Access and Streetscape (p. 68)	Implemented in TEDS and Access Control Plan; much consolidation already completed

North Avenue Streetscape New Development		Add to Area-Specific Policies?	Current Comp Plan or Area-Specific Policy Reference and Text	Potential Next Steps/Notes
Areas identified for redevelopment along the corridor provide an opportunity to greatly enhance the character of the corridor, by creating pockets of redevelopment that will stimulate further improvements. As shown on the land use plans, these areas of redevelopment will consist primarily of additional residential, office, retail/commercial areas, or new regional store developments. In all cases, each undertaken project should provide connectivity to the pedestrian network and include public open space.		Yes		Add language to Subareas p. 76 by adding the North Avenue Overlay boundary on the map and adding language on North Avenue Corridor including 1) Mix and Relationship of Uses, 2) Circulation and Access, 3) Building Form and Character, and 4) Streetscape and Signage
a	Residential. Clustered townhome developments adjacent to North Avenue will provide a distinct living option for residents along the corridor that is currently unavailable in the region. Located within walking distance of shopping, restaurants, bus service, and employment opportunities, townhomes offer a very desirable alternative to single-family housing. A green corridor in a walking environment creates a pedestrian connection between the existing neighborhoods and North Avenue, providing convenient access to the street for existing residents. On-street parking provides parking for visitors.	No	Mixed Use Areas: Neighborhood Center - Mix and Relationship of Uses, Circulation and Access, Building Form and Character, Parking and Services, Streetscape (p. 70); Mixed Use Areas: University District - Mix and Relationships of Uses, Circulation and Access, Building Form and Character, Parking and Services, Streetscape (p. 71)	"Green corridor" concept has been improved through implementation of the North Avenue street section and the ZDC update. Previous C-1 and current MU-2 zoning allows and promotes higher-density residential than townhomes. "Green corridor" to be implemented through adoption of North Avenue street section in TEDS.
b	Commercial/Retail. New commercial/retail uses along North Avenue are located close to the street, which helps to activate the street edge. Generous sidewalks with spaces for outdoor seating, active open space, pedestrian lights, and street trees enhance the character of the corridor. Building signage can be located directly on the face of the buildings, which can be readily identified from passersby on North Avenue. Front door access to retail/commercial uses is provided on the interior side, providing easy access from nearby parking, with back door service access provided along North Avenue. Civic spaces such as plazas, corridors, or pocket parks create a permeable frontage along North Avenue that provides easy pedestrian flow between the street and commercial or retail uses.	No	Commercial Areas: Commercial Corridor - Mix and Relationship of Uses, Access and Streetscape, Building Form and Character (p. 68); also, new ZDC	Infrastructure implemented through adoption of North Avenue street section in TEDS. Include "Civic spaces such as plazas, corridors, or pocket parks create a permeable frontage along North Avenue that provides easy pedestrian flow between the street and commercial or retail uses" in write-up.

North Avenue Streetscape New Development		Add to Area-Specific Policies?	Current Comp Plan or Area-Specific Policy Reference and Text	Potential Next Steps/Notes
	Regional Store. New regional store developments, proposed as infill projects, along North Avenue can be designed to minimize the impacts of large expanses of asphalt parking. This can be accomplished by locating retail stores at the edge of North Avenue, which activates the street edge, and screens the expansive parking lots from view from North Avenue. Interior parking lots can be shared between the super store and retail uses. Architectural elements such as canopies, columns, and articulated roof forms help to define the building entries of the super store, and reduce the scale of large facades. This approach will provide a more pedestrian-scaled environment for merchants, and create a more inviting facade. Entrances to the super store should be located from a side street, which will prevent excessive traffic build up along North Avenue.	No	Commercial Areas: Commercial Corridor - Mix and Relationship of Uses, Access and Streetscape, Building Form and Character (p. 68); also, new ZDC	Addressed in ZDC for big box development

North Avenue Signage			
	Add to Area-Specific Policies?	Current Comp Plan or Area-Specific Policy Reference and Text	Potential Next Steps/Notes
Improvements to signage along North Avenue can be accomplished by minimizing the quantity of pole-mounted signs and replacing them with monument signs. This will aid in reducing the visual clutter of the pole-mounted signs, and also provide an opportunity for street trees to be planted. Because monument signs are low, they are not in conflict with the larger canopy of the street trees, and can be readily seen from passing traffic. Further detail on signage and recommended adjustments to the Grand Junction sign code are provided. This philosophy supports the neighborhood or district concept, in that distinctions are to areas and not individual pad development. This encourages the walking environment and provides for a group of retailers to engage the property as a whole.	Yes	North Avenue Corridor Zoning Overlay District	Add language to Subareas p. 76 by adding the North Avenue Overlay boundary on the map and adding language on North Avenue Corridor including 1) Mix and Relationship of Uses, 2) Circulation and Access, 3) Building Form and Character, and 4) Streetscape and Signage

North Avenue Sign Code Strategies and Recommended Improvements			Add to Area-Specific Policies?	Current Comp Plan or Area-Specific Policy Reference and Text	Potential Next Steps/Notes
Summaries of particular sections from the Grand Junction sign code are provided below, indicating where the current Grand Junction sign regulations could be strengthened or amended to improve the overall visual quality and pedestrian scale of North Avenue. Currently, the majority of signs along North Avenue fall within these existing regulations:					
a Sign Location (GJMC 21.36.070(c)(3)).					
	1	Code Summary. Grand Junction's current sign regulation allows signs most anywhere on the property being advertised, and the total square footage allowed for a single sign is not to exceed 300 square feet.			
	2	Recommended Improvements.			
		i No signs permitted at the right-of-way.			
		ii Establish setbacks from the right-of-way.			
		iii Limit square footage of signs based upon to-be-established criteria.			
b Building Mounted Signage (GJMC 21.36.070(c)(5)).					
	1	Code Summary. There is no overall maximum square footage for wall-mounted signs, although the formula is similar to other sign code regulations. Each building will have its own maximum square footage, rather than a universal not-to-exceed size. The language also allows for wall-mounted signs on any side of the building facing a street, thus a corner lot could have two signs, each with a different sign allowance. The regulation allows for roof signs, up to 40 feet above grade in height. In addition, the regulation permits more than one building-mounted sign (wall-mounted, roof sign, or projecting) per structure as long as the maximum square footage is not exceeded.			
	2	Recommended Improvements.			
		i Limit square footage for building-mounted signs based upon to-be-established criteria.			
		ii Limit number of signs to one per building or tenant.			
		iii Prohibit roof signs.			
c Projecting Signage (GJMC 21.36.070(c)(6)).					
	1	Code Summary. A minimum size (12 square feet) is given for projecting signs rather than a maximum, if the projecting sign is the only sign on the building.			
	2	Recommended Improvement.			
		i Limit square footage of projecting signs based upon to-be-established criteria.			
d Freestanding Signage (GJMC 21.36.070(c)(7)).					
	1	Code Summary. The square footage and height limitations are calculated similar to other sign code regulations, but allow for greater height (25 feet for two traffic lanes and 40 feet for four traffic lanes) and maximum square footage than other regulations. The sign square footage is also calculated based upon property frontage rather than building face frontage. Signs are allowed to be installed at the right-of-way, with no setback requirements.			
	2	Recommended Improvements.			
		i Limit square footages and heights of signs based upon to-be-established criteria based upon building linear feet rather than property linear feet.			
		ii Lower heights of signs from what is now permitted.			
		iii No signs permitted at the right-of-way.			
		iv Sign design reflects the associated building architecture.			
		v Signs shall be attached to the ground or with minimal space between the sign and the ground.			
e Off-Premises Signage (GJMC 21.36.070(d)).					
	1	Code Summary. Off-premises signage (building-mounted, freestanding, or roof signs) is allowed in C-2 (General Commercial) and I-1 and I-2 (Industrial) zones. Maximum height is 40 feet, and square footage must be between 15 square feet and 300 square feet. Some municipalities are not allowing any off-premises signs within the City limits.			
	2	Recommended Improvement.			
		i Prohibit off-premises signage.			
f Recommended Sign Implementation Measures from Other Municipal Sign Codes. The following are representative regulations found within the sign codes from Denver, Fort Collins, Longmont, Greeley, and Loveland. Typically, it was found that heights and square footages were regulated, often based on a formula determined by the setback distance from the face of curb or edge of pavement and/or the linear feet of building facing the street. This summary does not provide that level of detail, but rather an overview of suggested strategies.					
	1	General			
		i Existing on-premises signs which have become nonconforming because of subsequent Code amendments shall be brought into conformance within 15 years of the date of amendment.			
		ii All features of the sign, including illumination, sign cabinet, base, color, lettering, and materials shall match the primary finishes and colors of the associated building(s) which it serves.			
		iii All signs which are greater than four square feet in area, except ground signs or signs that replicate a business logo, must be comprised only of individual letters or cabinets where only the letters are illuminated.			
					The City has updated the Zoning and Development Code twice since this plan was adopted, once in 2010 following the adoption of the 2010 Comprehensive Plan and

North Avenue Sign Code Strategies and Recommended Improvements				Add to Area-Specific Policies?	Current Comp Plan or Area-Specific Policy Reference and Text	Potential Next Steps/Notes
		iv	Signs shall be professionally designed and fabricated from quality, durable materials.	No.		in 2023 following the adoption of the 2020 Comprehensive Plan. In both updates the sign code was not updated due to community input. For North Avenue the Zoning Overlay has encouraged monument signs over pole signs. City Incentive Grant program was successful in reducing the number of pole signs and replacing them with monument signs.
		v	No roof signs.			
		vi	Lighting shall be indirect.			
	2		Maximum Size of Letters.			
	3		Sign Location.			
		i	Signs will be set back from the right-of-way based upon the sight distance triangle requirements, zoning district, or size of sign.			
		ii	No signs allowed within the right-of-way, with the exception of projecting signs which meet all requirements set forth in the regulations.			
		iii	No off-premises signs.			
		iv	All freestanding signs shall be located to be compatible with required landscaping, including street trees at maturity, so that the public views of the sign will not be obstructed.			
	4		Freestanding/Ground Signage.			
		i	Freestanding or ground signs comply with the following requirements:			
		ii	Freestanding signs only permitted with a supporting sign structure, the width of which exceeds 70 percent of the width of the sign face.			
		iii	Freestanding or ground signs shall have no more than two faces.			
		iv	Maximum height for freestanding signs shall be 18 feet above grade; and for ground signs shall be 12 feet above grade. (Another code said maximum six-foot height for freestanding.)			
		v	All freestanding signs shall be of a monument design including a monument base attached to the ground with no or minimal space between the sign cabinet and the monument base (no pole signs).			
		vi	Monument bases shall be equal or greater (up to 20 percent greater) in width and length than the sign cabinet. Sign cabinets for freestanding signs shall not exceed 24 inches in width.			
		vii	Multi-tenant buildings or developments are allowed one sign per lot or development parcel (no individual freestanding or ground signs).			
	5		Projecting Signage.			
		i	Not allowed to be located on the same street frontage as a freestanding sign.			
		ii	Shall not exceed 30 square feet for all faces.			
		iii	Shall not extend more than five feet from the building face nor extend beyond the curbline of any street or parking area.			
		iv	Minimum of eight feet of clearance from the ground, and a maximum of 25 feet, and shall not extend beyond the roofline or parapet wall.			
		v	Only one per building or tenant.			
		vi	Not allowed on a wall that already has a wall-mounted sign.			
	6		Building-Mounted Signage.			
		i	No building-mounted sign shall exceed 100 square feet.			
		ii	Multi-tenant buildings allowed one wall sign per street frontage of business.			
		iii	Only one building mounted sign per building (single tenant). Shall not project above the roofline.			
	7		Awning Signage.			
		i	Signs on canopies or awnings are limited to the name of the building, business and/or address of the premises.			
		ii	Shall not exceed 50 square feet per face in area.			
		iii	Only allowed on first story of a building.			
		iv	Minimum clearance shall be eight feet.			
	g		Suggested Steps to Implementing New Signage Approach Along North Avenue. The following steps are suggestions for implementing a new signage strategy along the North Avenue Corridor. This strategy will require amending the existing sign code to incorporate the appropriate level of restrictions within the corridor.			

North Avenue Sign Code Strategies and Recommended Improvements			Add to Area-Specific Policies?	Current Comp Plan or Area-Specific Policy Reference and Text	Potential Next Steps/Notes
	1	Determine Desired Signage Aesthetic. Determine the appropriate level of regulations for signage based upon other similar Colorado communities, as well as the desired aesthetic and scale of the signs within the corridor.			
	2	Determine How to Approach Existing Nonconforming Signs. Decide whether to require existing nonconforming signs to be improved within an appropriate time frame, and/or require compliance to new sign code amendments at time of business transition or sale. For example, Fort Collins requires all existing signs to be in compliance with the updated sign code within 15 years.			
	3	Amend the Sign Code Appropriately. Determine if this is a City-wide amendment, or limited to a special zoning district for North Avenue. Amend the City sign code with the desired adjustments, and begin transitioning out the old signs.			

North Avenue Implementation Strategies			Add to Area-Specific Policies?	Current Comp Plan or Area-Specific Policy Reference and Text	Potential Next Steps/Notes
MARKET	a	Capitalize on proximity to college and downtown through marketing efforts, product mix and positioning (West)	No	Plan Principle 4: Downtown and University Districts - A Maturing University (p. 22) and How We Will Get There 1.v. University District (p. 23)	
	b	Educate Council about this market reality and prepare them for filling the gap of catalyst projects resulting from inflated land prices and other factors (West)	No		No identified catalyst projects to fill funding gaps.
	c	Consider an urban renewal district to assist with on-site and off-site costs for qualifying elements (West)	No		City approached North Ave. businesses to form a taxing district to help revitalize North Avenue and it was not receptive.
	d	Capitalize on proximity to hospital and park through marketing, product mix and positioning (including actual physical connectivity) (Central)	No		Will be incorporated in write-up
	e	Encourage hospital, or unrelated clinics, to explore partnership opportunities (Central)	No		Existing services within and beyond corridor have built upon such partnerships
	f	Encourage mixed uses including affordable residential and multifamily (Central)	No		Commercial Corridor does not account for this. See also "Streetscape New Development" tab. Will be incorporated in write-up; already implemented by MU-2 zoning.
	g	Capitalize on substantial infrastructure investment in north-south connection (along with presumably softer land assembly costs) to pursue regional retail opportunity (East)	No		29 Road intersection already completed
FINANCIAL	a	Work with private and nonprofit interests to assemble and hold opportunity sites (West/Central)	No		North Avenue Owners Association, formed for these purposes, no longer exists, along with no creation of taxing district, make these difficult to accomplish
	b	Share market opportunities with various audiences to encourage investment (West/Central)	No		
	c	Use range of mechanisms – land swaps, low-interest loans, acquisition/write-down (West/Central)	No		
	d	Consider establishing appropriate districts as holding entities (West/Central)	No		

North Avenue Implementation Strategies			Add to Area-Specific Policies?	Current Comp Plan or Area-Specific Policy Reference and Text	Potential Next Steps/Notes
	e	Problem is less severe near logical development node – probably can use traditional land assembly (East)	No		
PHYSICAL	a	Maintain (City) flexible position on the use and width of alleys (if relevant) in select locations	No		Citywide policy, context-sensitive
	b	Create street standards for building setbacks, sidewalk width, signage, awnings, etc. sensitive to the feasibility of the remaining building envelope	No	Commercial Areas: Commercial Corridor - Building Form and Character (p. 68); ZDC and adopted North Avenue Overlay	
	c	For stores that require surface parking in front, require heightened landscaping or pad site development with interior parking – remaining sensitive to sight-lines	No	Mixed Use Areas: Neighborhood Center - Parking and Services (p. 70); Mixed Use Areas: University District - Parking and Services (p. 71); ZDC and adopted North Avenue Overlay	
	d	Establish street standards – define the street’s role by district based on the vision and desired character	No		Completed by adoption of North Avenue street section and Overlay
	e	Update any transportation plans relative to desired district functions, including encouraging public transportation through increased comfort and security of stops	No		GVT responsible for stops; City secures location with development
	f	Link parking lots, minimize curb cuts and consolidate driveways – selectively – where feasible	No	Commercial Areas: Commercial Corridor - Access and Streetscape (p. 68)	Included in Adopted Zoning Overlay District; City secures cross-access easements with development
	g	Redesign intersections and access points within the nodes to simplify and coordinate signal sequences	No		Ongoing coordination between City and CDOT

North Avenue Implementation Strategies			Add to Area-Specific Policies?	Current Comp Plan or Area-Specific Policy Reference and Text	Potential Next Steps/Notes
REGULATORY	a	Consider overlay district that is more supportive/encouraging of mixed use	No	Commercial Areas: Commercial Corridor - Mix and Relationship of Uses (p. 68); Mixed Use Areas: Neighborhood Center and University District - Mix and Relationship of Uses (p. 70-71)	Completed by adoption of North Avenue Overlay
	b	Revise regulations to eliminate barriers to investment in neighborhoods on the corridor; promote density and greater variety of residential products (i.e., density bonuses)	No	Commercial Areas: Commercial Corridor - Mix and Relationship of Uses (p. 68); Mixed Use Areas: Neighborhood Center and University District - Mix and Relationship of Uses (p. 70-71)	Commercial Zoning Allows high density
	c	Encourage the introduction of residential development within nodes and at key locations; fill financial gaps, and support demonstration projects (look for institutional partners)	No	Commercial Areas: Commercial Corridor - Mix and Relationship of Uses (p. 68); Mixed Use Areas: Neighborhood Center and University District - Mix and Relationship of Uses (p. 70-71)	Also, not feasible without taxing district
	d	Where necessary, assist with assembling properties to accommodate a range of product types (workforce, student, senior, etc.)	No		
	e	Eliminate regulatory barriers which preclude or delay redevelopment buildings for residential use (building codes, limits on adjacency among uses, etc.)	No		Completed; ZDC nonconforming and compatibility standards
	f	Work with private and nonprofit interests to provide opportunity sites for residential development	No	Plan Principle 5: Strong Neighborhoods and Housing Choices - How We Will Get There c. Regional Housing Initiatives (p. 28)	

North Avenue Implementation Strategies			Add to Area-Specific Policies?	Current Comp Plan or Area-Specific Policy Reference and Text	Potential Next Steps/Notes
POLITICAL	a	Request Council support for participation in at least two demonstration projects – enlisting the participation of existing property owners (if feasible), identifying partners, soliciting lender support, and committing a range of tools to assist with filling the gap	No		Completed - Council established and funded matching grant program, resulting in sign modifications and façade renovations
	b	Once a “tool box” of incentives has been assembled, publish a request for development, nonparcel specific to encourage investment	No		
	c	Through this corridor plan process, identify individuals and organizations which have the desire and resources to continue this effort and ensure the implementation strategy is implemented	No		Completed - North Avenue Owners Association, formed for these purposes, no longer exists
	d	Develop a range of “stories” which speak to the interests of different audiences – physical, fiscal, market, financial, etc. – educating about the opportunities and commitment to overcome barriers	No		
	e	Encourage transitional programs (e.g., job training, adult education) that provide positive daytime activities for homeless shelter population	No		Completed by HomewardBound

Title 27. Horizon Drive District – Overlay Zone District Standards

Chapter 27.04. Purpose

§ 27.04.010. Purpose.

The purpose of the Horizon Drive Zone District Overlay is to provide a consistent level of architectural character, quality and aesthetics of the Horizon Drive area as well as to improve and enhance pedestrian access, vehicular access, parking and circulation within the designated Horizon Drive Zone District Overlay.

(Ord. 4911, 4-15-20)

Chapter 27.08. Applicability

§ 27.08.010. Applicability.

The Horizon Drive Zone District Overlay includes all commercial properties within the general geographic area of Horizon Drive between G Road and H Road as identified on the zone district overlay map.

(Ord. 4911, 4-15-20)

Chapter 27.12. Standards and Guidelines

§ 27.12.010. Architectural features and materials.

Architectural features are intended to provide variations in massing, be at a human scale, and provide variety in design that work to reinforce the importance of the civic, public and open spaces.

- (a) All buildings facing Horizon Drive shall use materials that are durable, including but not limited to stone, brick, precast concrete and architectural metals.
- (b) Materials prohibited for any building facing Horizon Drive include metal-clad prefabricated buildings and building made from pre-cast concrete and tilt-up wall systems that are structural in appearance.
- (c) Building entries shall have a strong visual and pedestrian relationship to the street.
- (d) Buildings shall provide the following architectural design elements:
 - (1) Buildings shall provide their main entry facing Horizon Drive. When not feasible, a side or rear of a building may face Horizon Drive. However, the facade fronting Horizon Drive shall give the appearance of a front facade in terms of quality of architecture, materials and detailing.

- (2) Building form shall provide recessed or projecting elements to provide facade articulation. This can be accomplished through the design of entryways, awnings, rooflines, projecting bays, pilasters, columns or other features. Articulation shall occur a minimum of every 30 feet for all sides of the building.
 - (3) The first floor of a building designed to accommodate a restaurant or retail use shall have windows facing the public right-of-way that create visual interest to pedestrians and provide views from inside of buildings to the street.
- (e) In addition, the site shall exhibit a minimum of three of the following seven architectural design elements:
- (1) Variation in materials, material modules, expressed joints and details, surface relief and texture to break up building forms and wall surfaces. Such detailing may include sills, headers, belt courses, reveals, pilasters, window bays or similar features for all sides of the building.
 - (2) Variation in roof lines/roof materials in order to add interest to and reduce the scale of buildings or expanses of blank wall. This can be accomplished through design elements such as overhangs, eaves, recesses, projections, raised cornice parapets over doors or bays and peaked roof forms.
 - (3) Establishing the main building entrance on the street with facade features that emphasize the primary building entrance through projecting or recessed forms, detail, color and/or material.
 - (4) Outdoor patio in combination with or without outdoor seating located between the building and the primary street.
 - (5) Ground story transparency of at least 50 percent in the form of windows and/or door(s) for facades facing all public street frontages.
 - (6) Public art, as approved by the Director.
 - (7) Other architectural or site features that achieve the goals of the overall Horizon Drive District Overlay vision or concept, as determined by the Director.

(Ord. 4911, 4-15-20)

§ 27.12.020. Site design.

Elements required or encouraged for site design are intended to minimize vehicular orientation and emphasize pedestrian activities such as ease of access from the public way and safe access to parking areas, increase walkability of the district especially between the public way, transit facilities and other buildings. They are also intended to provide safe access to businesses from the street and sidewalks, as well as parcel interconnectivity to minimize multiple access points to Horizon Drive.

- (a) Clearly visible and direct pedestrian paths with adequate lighting should be established between neighboring buildings, between buildings and outlying parking areas, and between buildings and transit facilities.
- (b) A six-foot-wide concrete sidewalk is required from the street to the front of the primary building main entrance.
- (c) New development shall be required to close redundant or multiple accesses to Horizon Drive from a single site. Access location and turning movements shall be limited to those which can be safely accommodated as determined by City of Grand Junction traffic engineers.
- (d) New development shall be required to consolidate accesses with neighboring properties to the extent practicable.
- (e) Bicycle parking shall be provided at locations that do not obstruct the flow of pedestrians, are easily identifiable and visible and convenient to customer entrances.

- (f) Where pedestrian circulation paths cross vehicular routes, a change in paving materials, textures, or colors shall be provided to emphasize the potential conflict point, improve visibility, enhance safety and enhance aesthetics.
 - (g) On-site signage and traffic markings necessary to facilitate circulation and improve public safety and awareness are required.
 - (h) Drive-up windows such as those used for banks, restaurants, groceries and pharmacies or drive-through facilities such as gasoline service islands and car wash or vacuum bays shall be oriented as to not be visible from the public right-of-way.
- (Ord. 4911, 4-15-20)

§ 27.12.030. Landscaping and public amenities.

This section is meant to ensure appropriate landscaping improvements and the creation of public amenities that enhance the character of the district, and to utilize xeric principles in landscaping design encouraging the planning of low water plants that are appropriate for Grand Junction's arid climate. The intent is also to encourage future development to include civic areas and open space (parks).

- (a) Landscaping shall be designed to provide drought tolerant plant species that are native to the region or otherwise suitable to the climate.
 - (b) For all development, street trees planted every 40 feet shall be planted along all public rights-of-way.
 - (c) For all development with landscaped areas fronting Horizon Drive, any groundcover provided shall be visually similar to existing landscape rock (e.g., crushed red granite) in place on Horizon Drive.
 - (d) Art, sculpture, transit shelters, benches, planters, bike racks, trashcans and other hardscape feature, plazas, landscaping and other amenities shall be included where appropriate.
- (Ord. 4911, 4-15-20)

§ 27.12.040. Signage.

Signage is intended to communicate information and reduce existing visual clutter as well as prohibit new visual clutter. Signage shall provide visual continuity within a single project.

- (a) Signs shall be consistent in design, color, typeface, materials and construction details with each project.
- (b) Freestanding signs shall comply with the following requirements:
 - (1) Shall be placed perpendicular to the right-of-way.
 - (2) Shall be constructed with a stone or veneer base. The sign may be single- or double-faced. If single the back side of the sign shall be painted the same color as the cabinet and poles.
 - (3) Shall be located no closer than 10 feet from a property line and no closer than six feet from the curb of a street or drive.
 - (4) The height shall be measured from finished grade.
 - (5) One freestanding sign shall be allowed per street frontage.
 - (6) Up to two small, freestanding directional signs may be allowed that are three square feet or fewer and no more than 30 inches in height.
 - (7) The sign allowance per frontage can only be used on that frontage and shall not be transferred to any other frontage.

(8) Maximum sign dimensions shall not exceed the following:

(i) For properties fronting Horizon Drive between G Road and 27 1/2 Road and H Road:

- (A) Forty feet in height;
- (B) One hundred square feet for properties with up to 150 feet of linear frontage;
- (C) One hundred twenty square feet for properties with 150 to 200 feet of linear frontage;
- (D) One hundred sixty square feet for properties with 201 to 300 linear feet of frontage;
- (E) Two hundred square feet for properties with greater than 300 linear feet of frontage.

(ii) For properties with frontage on Horizon Drive between 7th Street and G Road (27 1/2 Road), the maximum sign dimensions shall not exceed the following:

- (A) Twelve feet in height;
- (B) One hundred square feet.

(iii) For properties with Interstate 70 frontage maximum sign dimensions are subject to GJMC § 21.10.070.

(iv) For any regulated cannabis business, the maximum sign dimensions shall not exceed the following:

- (A) Twenty feet in height;
- (B) Seventy-five square feet.

(c) Flush wall signs may be either nonilluminated or internally illuminated.

(d) No off-premises signs or outdoor advertising shall be permitted, except where existing at the time of adoption of the ordinance codified in this title, provided such signs are appropriately permitted through the City of Grand Junction.

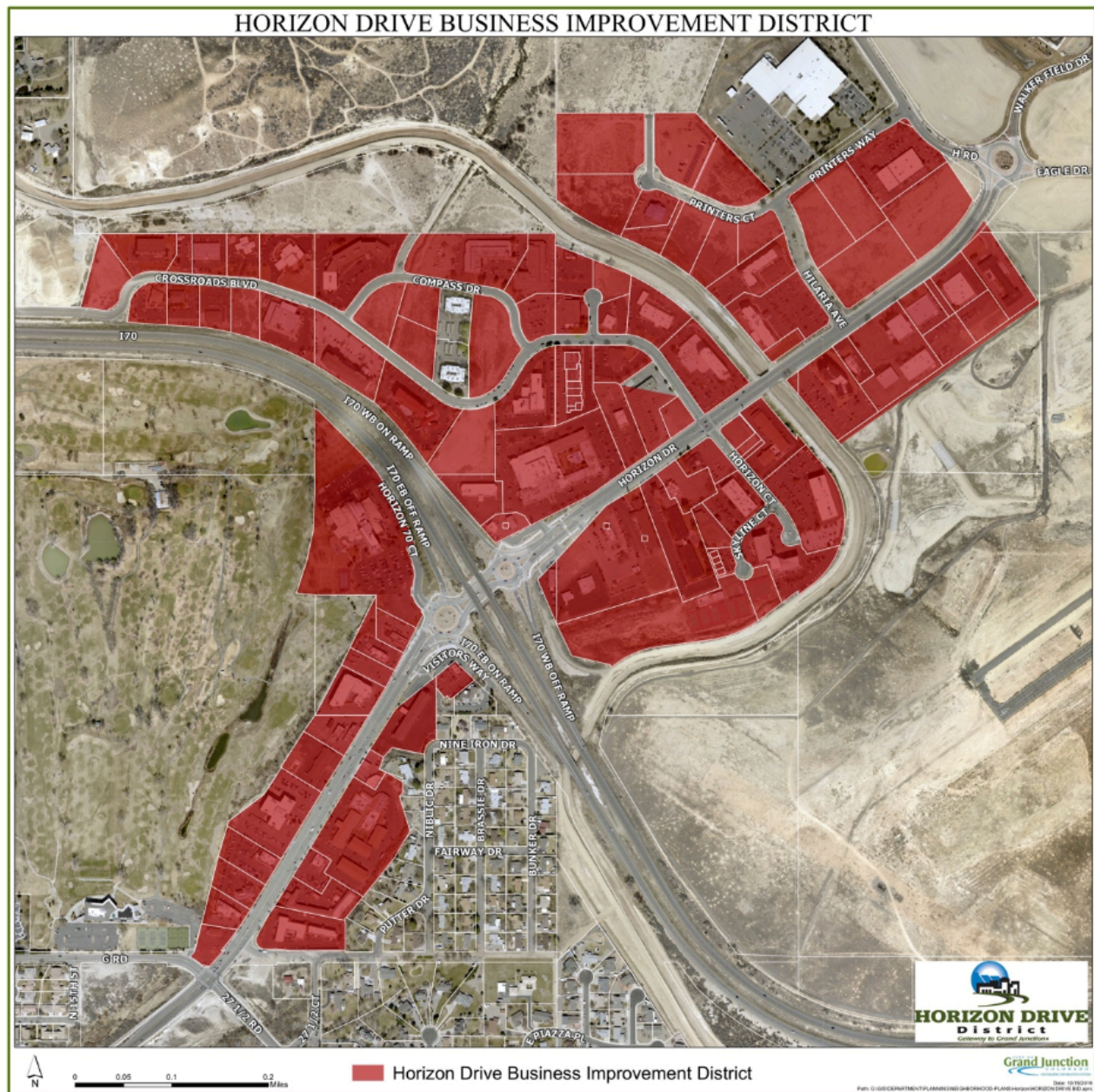
(e) Projecting signs shall be permitted as per GJMC § 21.10.080.

(f) Roof signs are not allowed.

(Ord. 5070, 5-4-22; Ord. 4911, 4-15-20)

Chapter 27.16. Map of Horizon Drive Overlay District

§ 27.16.010. Map of Horizon Drive Overlay District.



MAP OF HORIZON DRIVE OVERLAY DISTRICT

(Ord. 4911, 4-15-20)

Title 22. H Road/Northwest Area Plan

Chapter 22.04. Background Information

§ 22.04.010. Background information.

The H Road/Northwest Area Plan is located in the 21 1/2 Road and H Road vicinity. The plan area was added to the Persigo 201 sewer service area (which is the urban growth boundary) in March 2006. This area plan establishes appropriate future urban land uses and policies for ensuring compatible future development options for the study area. In September 2006 the plan (study) area contained:

- (a) Approximately 250 acres;
- (b) Thirty-seven parcels;
- (c) Thirty ownerships;
- (d) Fourteen residences on 13 parcels;
- (e) Fifteen businesses on 16 parcels;
- (f) Eight vacant parcels.



(Ord. 4066, 4-18-07; (City) GPA 2007-025, (County) 2007-027 MP1)

§ 22.04.020. Planning process.

The planning process began in the fall of 2006 with initial meetings among City, County and Colorado Department of Transportation staff. Focus groups were held to discuss traffic/transportation needs and commercial/industrial needs for vacant land.

Planning staffs conducted baseline inventories and met with in-house and external service providers to help identify key issues prior to meeting with the public. Focus group meetings were held with Grand Junction economic development representatives, oil and gas representatives and property owners along the 22 Road and H 1/2 Road corridors. A public open house was held in November 2006. The entire schedule is outlined below:

September 20, 2006	City/County Meeting #1
October 3, 2006	Base Mapping Completed
October 4, 2006	City/County Meeting #2
October 17, 2006	Focus Group Meeting – Economic Development Needs
October 30, 2006	Focus Group Meeting – Transportation Needs
November 1, 2006	City Council's Strategic Planning Team #1 Meeting
November 17, 2006	Sent Notice for Neighborhood Meeting
November 29, 2006	Neighborhood Meeting/Open House
December 8, 2006	City/County Meeting #3
January 16, 2007	City/County Meeting #4
February 6, 2007	Sent Invitation Letter to 22 Road and H 1/2 Road Residents
February 15, 2007	Focus Group Meeting – 22 Road and H 1/2 Road Residents
February 21, 2007	City/County Meeting #5
March 8, 2007	Joint City/County Planning Commission Workshop
March 12, 2007	Newsletter/Notice for Public Hearing Mailed
March 27, 2007	City/County Meeting #6
March 27, 2007	Joint City/County Planning Commission Hearing
April 18, 2007	City Council Hearing

Public input was solicited at the open house held November 29, 2006, and the February 15th focus group. Staff also received numerous written comments, phone calls, letters and comments by e-mail, and other personal communications throughout the planning period. The public meetings were well attended, more than 60 individuals signed in at the open house and 17 people attended the focus group meeting. Two newsletters were mailed to every property owner in the planning area and within one-half mile of the plan area. The City Internet website kept the public up-to-date on issues and progress of the plan.

(Ord. 4066, 4-18-07; (City) GPA 2007-025, (County) 2007-027 MP1)

§ 22.04.030. Public comments.

Public comments received at the open house were clearly divided between those that expressed a desire to:

- (a) Not allow commercial/industrial land uses east of Pritchard Wash and only allow residential densities no greater than residential estates (two to five acres per lot); and
- (b) Make the entire study area commercial/industrial including the areas east of Pritchard Wash.



22 Road (north of H Road) in foreground – looking west to Pritchard Wash

The comments from the adjacent residential neighborhood were in opposition to commercial/industrial land uses east of Pritchard Wash for such reasons as compatibility, quality of life, noise, feared loss of property values, etc.

The need for commercial/industrial land uses east of Pritchard Wash was strongly supported by many of the property owners as well as the Grand Junction economic development community, oil and gas representatives, the Grand Junction Area Chamber of Commerce, and the business incubator staff and board. These groups all stated the need for additional commercial/industrial land in the Grand Junction area, especially industrial parcels greater than 10 acres in size.

(Ord. 4066, 4-18-07; (City) GPA 2007-025, (County) 2007-027 MP1)

Chapter 22.08. Plan Adoption

§ 22.08.010. Plan adoption.

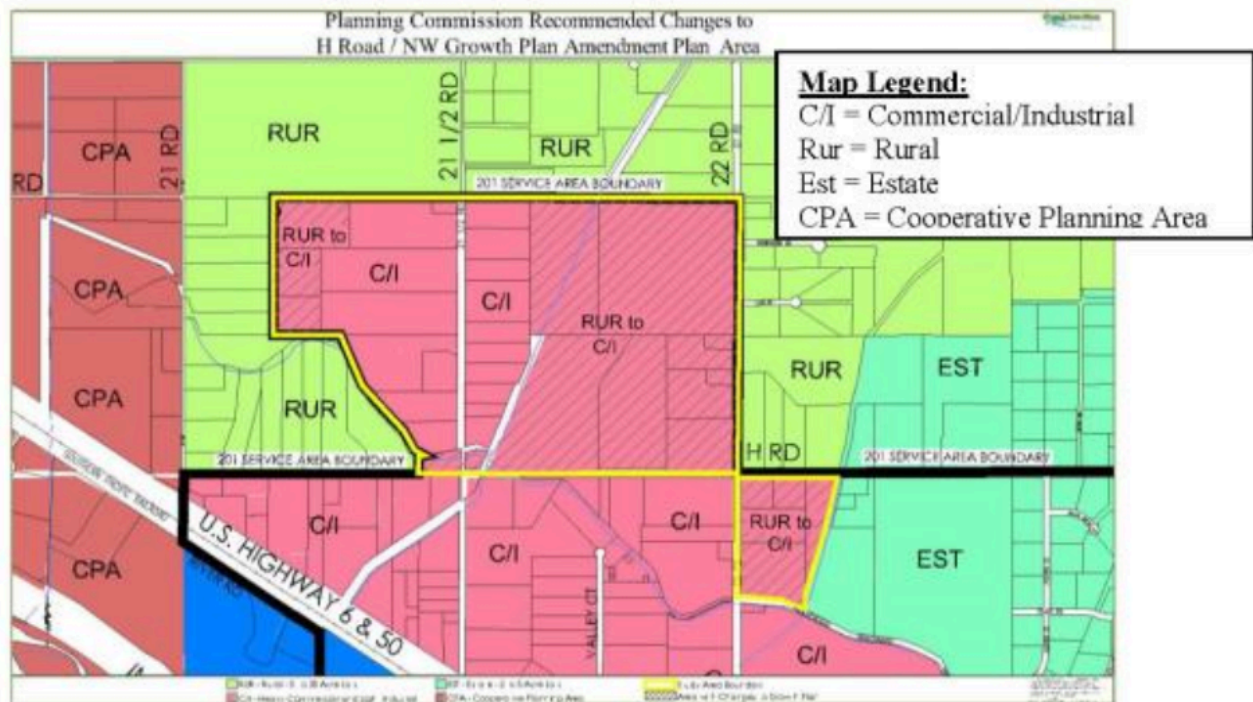
Amendment to the Joint Urban Area Future Land Use Plan, an element of the Mesa County Master Plan, and a part of the Grand Junction Growth Plan in recognition that the area was added to the Persigo Sewer Service Area in March 2006.

After six months of review, the City Planning Commission and the Mesa County Planning Commission held a public hearing on March 27, 2007, and approved the H Road/Northwest Area Plan. The City Council held a public hearing on April 18, 2007, and approved the plan. The H Road/Northwest Area Plan includes the following elements:

- (a) An amendment to the City and County's Future Land Use Map from "Rural" to "Commercial/Industrial" (C/I) for all properties located within the plan area that are currently designated as "Rural."

- (b) Adoption of policies and performance standards that will help mitigate impacts to the adjacent residential neighborhood(s) outside of the plan area.
 - (c) An amendment to the Grand Junction Circulation Plan to include the plan area and establish an appropriate street network that will accommodate future growth in the area.
- (Ord. 4066, 4-18-07; (City) GPA 2007-025, (County) 2007-027 MP1)

§ 22.08.020. Future Land Use Map.

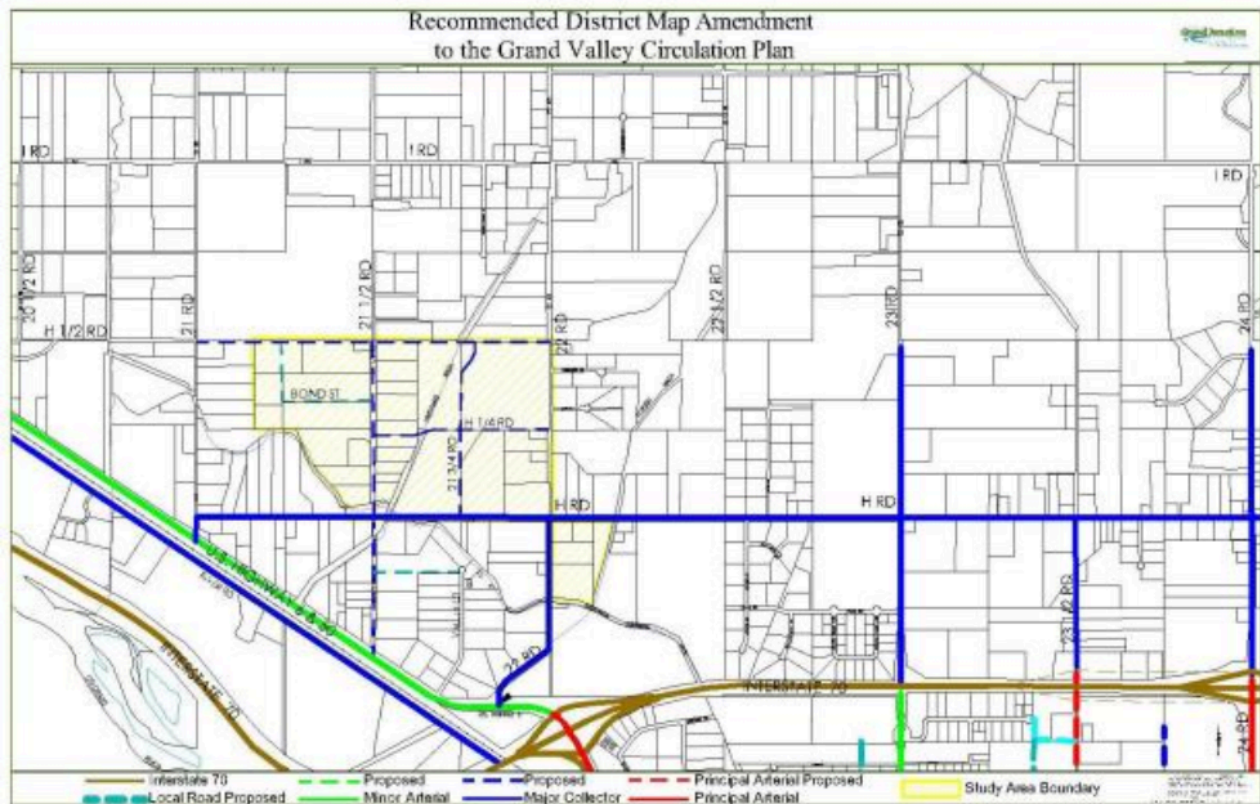


Cross-hatched area illustrates changes to the Future Land Use Map

(RUR to C/I = Rural to Commercial/Industrial)

(Ord. 4066, 4-18-07; (City) GPA 2007-025, (County) 2007-027 MP1)

§ 22.08.030. Grand Junction Circulation Plan Map amendment.



Changes to the Grand Junction Circulation plan (formerly Grand Valley Circulation Plan) include adding major collector roads including 21 1/2 Road between U.S. Highway 6 and H 1/2 Road, H 1/4 Road between 21 1/2 Road and 22 Road, 21 3/4 Road between H Road and H 1/2 Road, and H 1/2 Road between 21 Road and 22 Road. The plan also identifies the need for a local street to be constructed between Valley Court and 21 1/2 Road and Bond Street and H 1/2 Road.

Potential intersection improvements were identified through the planning process for the intersections of 21 1/2 Road and U.S. Highway 6 where the potential exists for a proposed traffic control location such as a traffic signal or other traffic control improvements. The two intersections of Valley Court and 22 Road with U.S. Highway 6 where traffic movements may need to be restricted in the future were not included in the final Grand Valley Circulation Plan (now the Grand Junction Circulation Plan) amendment, although they were identified as needing additional study. (Ord. 4066, 4-18-07; (City) GPA 2007-025, (County) 2007-027 MP1)

Chapter 22.12. Policies and Performance Standards

Article I. Policies

§ 22.12.010. Affected area.

The following performance standards shall affect the entire H Road/Northwest Area Plan. (Ord. 4066, 4-18-07; (City) GPA 2007-025, (County) 2007-027 MP1)

§ 22.12.020. Truck traffic.

Site design shall direct truck (operations) traffic to the 21 1/2 Road Corridor. All other traffic including customer or light vehicle traffic may also use 22 Road and H 1/2 Road. (Ord. 4066, 4-18-07; (City) GPA 2007-025, (County) 2007-027 MP1)

§ 22.12.030. Billboards.

All signage as defined under the existing development codes and regulations of the City and County as off-premises signs are not allowed anywhere within the H Road/Northwest Area Plan boundaries.
(Ord. 4066, 4-18-07; (City) GPA 2007-025, (County) 2007-027 MP1)

Article II. Performance Standards

§ 22.12.040. Affected area.

Development on all parcels abutting the west side of 22 Road from H Road to H 1/2 Road and the south side of H 1/2 Road from 21 Road to 22 Road shall be required to meet the following performance standards.
(Ord. 4066, 4-18-07; (City) GPA 2007-025, (County) 2007-027 MP1)

§ 22.12.050. Corridor aesthetics/landscaping.

All property frontages along these corridors shall provide at a minimum:

- (a) A 25-foot-wide landscaping strip the entire length of the frontage (excluding driveways).
- (b) A berm the entire length of the frontage with a minimum of 36 inches in height.

Fencing shall not be allowed within the 25-foot landscape strip with the exception of split rail fences with up to three rails and not more than four feet in height.
(Ord. 4066, 4-18-07; (City) GPA 2007-025, (County) 2007-027 MP1)

§ 22.12.060. Loading docks and fleet parking.

All loading docks and fleet/equipment parking shall be located in the rear half of the lot or behind the principal structure (i.e., south side of buildings fronting on H 1/2 Road and west of buildings fronting on 22 Road).
(Ord. 4066, 4-18-07; (City) GPA 2007-025, (County) 2007-027 MP1)

§ 22.12.070. Outdoor storage and display.

Outdoor storage areas shall be:

- (a) Adequately screened so as not to be visible from adjacent public roads (i.e., H 1/2 Road and 22 Road);
 - (b) In the rear half of the lot or behind the principal structure (i.e., south of buildings fronting on H 1/2 Road and west of buildings fronting on 22 Road);
 - (c) Trash dumpsters shall be fully screened and located in the rear half of the lot or behind the principal structure.
- (Ord. 4066, 4-18-07; (City) GPA 2007-025, (County) 2007-027 MP1)

§ 22.12.080. Parking lots.

All parking lots located within the front half of the parcel or front of the principal structure (adjacent to 22 Road and H 1/2 Road rights-of-way) shall only be used for customer parking.
(Ord. 4066, 4-18-07; (City) GPA 2007-025, (County) 2007-027 MP1)

§ 22.12.090. Architectural standards.

Applies only to building facades facing the 22 Road and H 1/2 Road rights-of-way. Building form shall incorporate projected and recessed elements to provide architectural variety, such as entryways, special functional areas, rooflines, and other features, including the following requirements:

- (a) Blank, windowless walls are discouraged. Where the construction of a blank wall is necessary, the wall shall be articulated.
- (b) Large monolithic expanses of uninterrupted facades (greater than 50 feet) are not allowed. Pilasters, texture transitions, windows and stepping of the wall plane are required.
- (c) Buildings with flat roofs shall provide a parapet with an articulated cornice.
- (d) All primary buildings shall use materials that are durable, economically maintained, and of quality that will retain their appearance over time including but not limited to stone, brick, stucco, and pre-cast concretes.

(Ord. 4066, 4-18-07; (City) GPA 2007-025, (County) 2007-027 MP1)

§ 22.12.100. Signage standards.

Only monument style signs at a maximum of eight feet in height with a maximum total of 64 square feet per sign face shall be allowed. Signs shall not be internally illuminated. External illumination is allowed.

(Ord. 4066, 4-18-07; (City) GPA 2007-025, (County) 2007-027 MP1)

§ 22.12.110. Other standards.

The following are adequately addressed under existing development codes and City of Grand Junction and Mesa County regulations and therefore conformance must be met through the development process under then-existing code requirements:

- (a) Retail sales/wholesale sales area;
- (b) Odors;
- (c) Glare;
- (d) Parking lots;
- (e) Lighting standards;
- (f) Noise (regulated in § 25-12-103, C.R.S., maximum permissible noise levels, and GJMC § 8.16.010).

(Ord. 4066, 4-18-07; (City) GPA 2007-025, (County) 2007-027 MP1)

City of Grand Junction, CO
Thursday, August 15, 2024

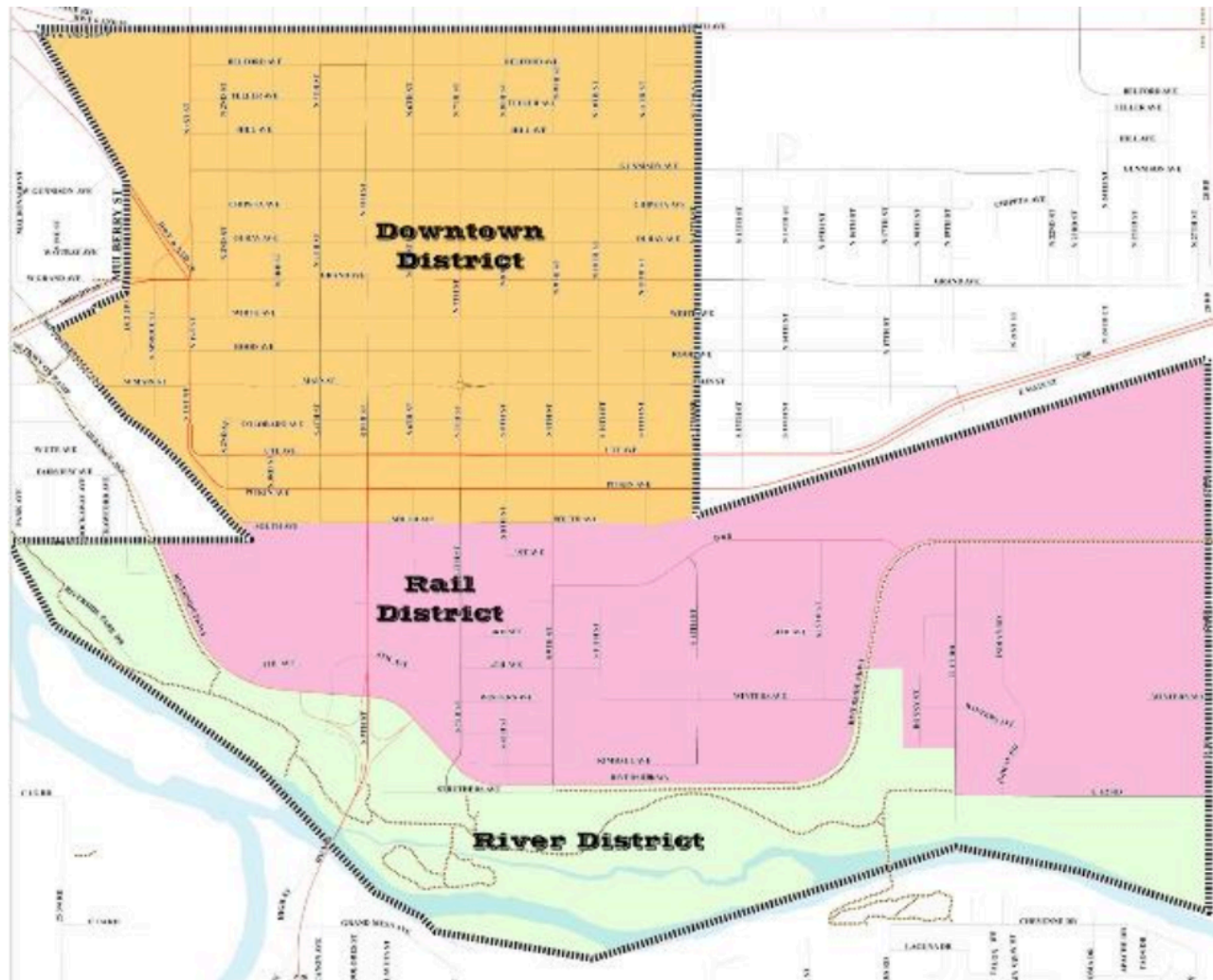
Title 36. Greater Downtown Plan

Chapter 36.04. General Provisions



§ 36.04.010. Study area context.

The Greater Downtown planning area is comprised of three subdistricts: the Downtown, Rail and River Districts. The areas are distinctly different due to their location, influences and historic development but complement each other to form a developed area that is central to the community with uses that range from single-family residential to rail-oriented heavy industrial.



(Ord. 4571, 3-20-13)

§ 36.04.020. Planning background.

- (a) A Strategic Downtown Master Plan (SDMP) that encompassed the original square mile was developed through the Downtown Development Authority (DDA) and the guidance of a steering committee of interested downtown merchants, property owners and policymakers during 2007-2008. The SDMP defined an overall vision and goals for downtown and included implementation strategies such as a zoning overlay.

The vision of the SDMP addressed the transportation network and other infrastructure, introduction of green treatments, creation of design standards and strategies for community marketing and promotion efforts. The goals of the SDMP were to:

- (1) Maintain and enhance the economic, cultural and social vitality of downtown;
- (2) Promote downtown living by providing a wide range of housing opportunities;
- (3) Enhance the transportation system to accommodate automobiles, bikes and pedestrians, and provide adequate, convenient parking;
- (4) Stabilize and enhance the historic residential neighborhoods;
- (5) Establish and promote a unique identity;
- (6) Jump-start the revitalization and reinvestment in the downtown area with strategic catalyst projects.

The Strategic Downtown Master Plan was considered by City Council on September 14, 2009, but, due to pending adoption of the Grand Junction Comprehensive Plan (Comprehensive Plan), Council voted to continue the SDMP to an unspecified future date.

- (b) A South Downtown Neighborhood Plan (South Downtown Plan) encompassed the area between the railroad tracks and the Colorado River and the Riverside neighborhood on the west to 28 Road on the east. A plan for the area was developed from 2006-2008 with 15 community focus group meetings, three public open houses with 80 to 100 people in attendance at each open house. The South Downtown Plan included an existing conditions analysis, goals and implementation including a circulation and trails plan, economic development strategies, rezoning some properties and zoning overlay. The goals of the South Downtown Plan were to:

- (1) Create, maintain and enhance a green waterfront;
- (2) Recognize existing heavy industry and rail service that supports it;
- (3) Recognize the distinction between “industrial” streets and “public” streets;
- (4) Promote higher quality, “cleaner” uses in the area generally between 7th and 9th Streets;
- (5) Improve entry points and connections to downtown;
- (6) Increase light industrial opportunities;
- (7) Create areas of mixed uses to screen and transition to the heavy industry;
- (8) Create and enhance redevelopment opportunities and partnerships.

The South Downtown Neighborhood Plan was considered by City Council on June 16, 2008, but was not adopted.

- (c) The planning process for the Greater Downtown Plan reanalyzed the two previous planning efforts and made revisions as conditions have changed, included areas that had not been covered by either of those plans, and integrated them into a single plan for the downtown area. In addition, the Greater Downtown Plan incorporates elements of the Downtown Development Authority’s potential projects in order to support the DDA’s Downtown Plan of Development, as well as incorporating elements of the Comprehensive Plan discussed below.

(Ord. 4571, 3-20-13)

§ 36.04.030. Comprehensive Plan.

- (a) The Grand Junction Comprehensive Plan is based on extensive public input that identifies what kind of community we want to have and identifies ways to achieve our vision. It charts the course to help us become the most livable community west of the Rockies. It establishes a vision that focuses the community on what it should do to sustain the quality of life that all residents desire and expect. The Comprehensive Plan establishes the following guiding principles that will shape growth, all of which apply to development of the Greater Downtown area:

- (1) Concentrated Centers. The plan calls for three types of centers: the City Center, Village Centers and Neighborhood Centers.
- (2) Sustainable Growth Patterns. Fiscal sustainability where we grow efficiently and cost-effectively. Encourage infill and redevelopment.
- (3) Housing Variety. Allow, encourage more variety in housing types that will better meet the needs of our diverse population.
- (4) A Grand Green System of Connected Recreational Opportunities. Take advantage of and tie together the exceptional open space assets of Grand Junction, including the Colorado River, our excellent park system, trails and our surrounding open spaces.

- (5) Balanced Transportation. Accommodate all modes of transportation including air, transit, freight, auto, bike and pedestrian.
 - (6) A Regional Center. Preserve Grand Junction as a provider of diverse goods and services and residential neighborhoods.
- (b) Specific policies within the Comprehensive Plan further support the concepts of the Greater Downtown Plan as outlined below.
- (1) Goal 4. Support the continued development of the downtown area of the City Center into a vibrant and growing area with jobs, housing and tourist attractions.
 - (2) Goal 5. To provide a broader mix of housing types in the community to meet the needs of a variety of incomes, family types and life stages.
 - (3) Goal 6. Land use decision will encourage preservation of historic buildings and their appropriate reuse. For purposes of the Greater Downtown Plan and overlay district, a historic building is defined as one that would be eligible for inclusion on the City Register of Historic Sites, Structures and Districts according to criteria outlined in the Zoning and Development Code.
 - (4) Goal 8. Create attractive public spaces and enhance the visual appeal of the community through quality development.
 - (5) Goal 9. Develop a well-balanced transportation system that supports automobile, local transit, pedestrian, bicycle, air and freight movement while protecting air, water and natural resources.
- (Ord. 4571, 3-20-13)

Chapter 36.08. Site Analysis

Article I. Downtown District

§ 36.08.010. Downtown District.

The Downtown District encompasses the original square mile, the area south of North Avenue between 1st Street and Highway 6 and 50, a small triangle of properties north of Grand Avenue and west of Highway 6 and 50, and the area south of Grand Avenue between 1st Street and the railroad tracks (see map).

The Downtown District should retain its role as the City's center and a regional destination. With a diverse mix of land uses, civic and public amenities, art and cultural facilities, the Downtown District offers a hub of activity that supports and reflects the regional demographics. Every effort should be made to keep and maintain existing public amenities and services in the Downtown District. These uses attract large numbers of residents and visitors to downtown which contributes to its economic vitality.



The Downtown District benefits from a number of characteristics that make it appropriate for development of retail, office, residential, institutional and community uses as described below.

- (a) On average, urban residents spend a greater percentage of household income on retail expenditures, particularly on items such as apparel and food away from home. This indicates an opportunity for additional specialty retail and entertainment space in the Downtown District.
- (b) Housing within the Downtown District has been and is expected to continue to increase in density with smaller households comprised of young and old, and moderate- and lower-income residents. However, with a growing concentration of middle-aged, moderate- to high-income households in the City as a whole, there is an opportunity for the Downtown District to attract more diverse, higher-income resident base. The entirety of Grand Junction (urban and fringe) faces a growing shortage of quality affordable housing for its very low- and moderate-income residents, as well as working-wage families. This planning process emphasized the need for a set of strategies tailored specifically to the housing challenges present within the Downtown District.
- (c) Downtown commercial vacancy and rental rates are approaching levels required to support new development and/or redevelopment. However, “seed” money will likely be necessary to leverage private investment in projects that will catalyze reinvestment activity throughout the Downtown District.
- (d) Among the higher growth employment sectors in Mesa County are service industries often consisting of small businesses. This represents an opportunity for the Downtown District to develop not only additional live/work units, but also to promote the adaptive re-use of historically significant buildings and less traditional spaces including former church facilities.
- (e) Forecasts indicate that more than one million square feet of employment space (office), more than 1,600,000 square feet of retail space and nearly 1,100 residential units could be absorbed in the market over the next 10 years, from which the Downtown District could benefit. The degree to which the Downtown District is able to capture new demand within the trade area and beyond will be a function of the redevelopment process itself. Redeveloping key catalyst areas as residential, retail, employment and community destinations will necessarily increase its ability to capture not only a greater share of trade area demand, but also to reach beyond those boundaries.
- (f) The Downtown District is comprised of many uses that result in a number of diverse neighborhoods. While some areas within downtown are comprised of one single use, such as residential or business/commercial, there are many areas that include a range of uses, such as

along Grand Avenue. In order to more fully understand specific issues facing the many, diverse neighborhoods within downtown, the Downtown District was divided into seven subareas: Central Business District (CBD), Central Business District Core Area (Downtown Core), Transitional, Residential, North 1st Street Neighborhood Center (Neighborhood Center), Existing Commercial, and the North Seventh Street Historic Residential District.



(Ord. 4571, 3-20-13)

§ 36.08.020. Central Business District (CBD).



Main Street Character

The CBD contains primarily commercial uses and is located generally south of Ouray Avenue. The CBD also contains a significant number of public and civic facilities. While much of the CBD is zoned to permit a wide range of uses, there are several new projects that have integrated uses into one cohesive project. Pedestrian accessibility and amenities, public facilities, traffic patterns, building heights and commercial development densities combine to create an urban character that attracts local businesses, employees, residents and visitors to the CBD. Within the CBD, the Downtown Core includes the streets and development patterns that most depend on and are defined by the heaviest pedestrian activity. (Ord. 4571, 3-20-13)

§ 36.08.030. Transitional subarea.

The Transitional areas of downtown contain a mix of residential, commercial and institutional uses. Development densities and building heights are not as extreme as in the CBD and residential uses, which includes single-family homes adapted to multifamily uses, are prevalent throughout the Transitional areas. With the exception of the main arterial Grand Avenue, traffic is less congested and parking is available, both on-street and in publicly and privately owned surface parking lots.

Existing homeowners in the Transitional areas, especially along Grand Avenue, expressed concern over the rapidly expanding commercial character of the area. Traffic congestion, parking issues, vehicle accessibility, lighting and signage were each cited as an adverse impact that existing single-family residential homeowners were grappling with as more homes are converted to business uses.



New Infill Development Building on Right



Residential Subarea Character

(Ord. 4571, 3-20-13)

§ 36.08.040. Residential subarea.

A significant amount of the Downtown Core contains single-family residential uses. The large number of older, historic structures results in a definitive architectural character that local residents want preserved. The North Seventh Street Historic Residential District contains 34 historic homes with a high concentration of early Twentieth Century architectural styles and construction methods. The district covers five blocks of North Seventh Street and is the most intact historical residential area in Grand Junction with a variety of Queen Anne, Colonial Revival and Mission style homes. A detailed analysis of, and specific guidelines and standards applicable only to, properties within the North Seventh Street Historic Residential District were adopted in 2012 and thus, the District is not further addressed in the Greater Downtown Plan.

Along the edges of the downtown residential areas, commercial activity is encroaching on established single-family residential neighborhoods. Although existing residents expressed their satisfaction with existing development and uses along North 1st Street and North Avenue, many participants expressed concern over future expansion and potential rezoning along these edges and wanted to see the Greater Downtown Plan respect existing zone districts and development patterns and prohibit any further rezoning or variances.

(Ord. 4571, 3-20-13)

§ 36.08.050. North 1st Street neighborhood center.

This subarea of the Downtown District is an extension of the neighborhood center defined along the North Avenue corridor. The intent of the neighborhood center is to provide for limited employment, residential, open space and limited retail uses that primarily provide convenience to the immediate neighborhood. While much of the area is already developed with commercial area, there is a need to create a better transition between the commercial uses facing North 1st Street and the side streets to the residential areas on the east side of North 2nd Street.

(Ord. 4571, 3-20-13)

§ 36.08.060. Existing commercial.

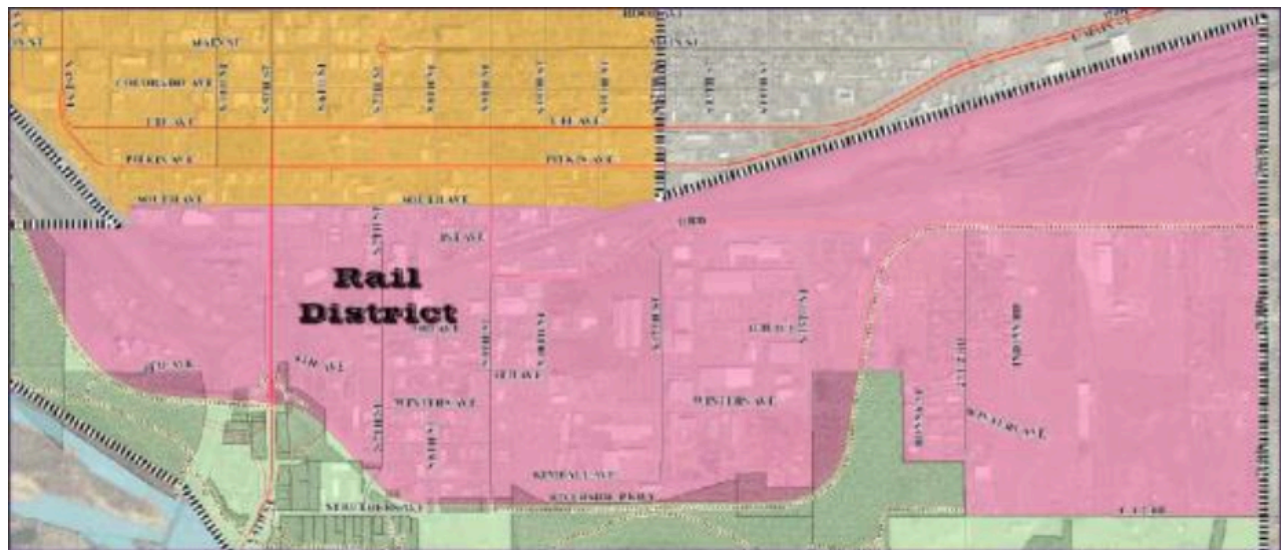
This subarea of the Downtown District corresponds with developed commercial corridors on the north/northwest and south sides of the district that have a different character than the type of uses that exist in the other subareas.

(Ord. 4571, 3-20-13)

Article II. Rail District

§ 36.08.070. Rail District.

The Rail District is the area bounded by South Avenue and the railroad tracks to the north, Riverside Parkway and C-1/2 Road along the west and south and 28 Road on the east. The man-made framework of the area consists of the buildings and infrastructure that already exist, some of which is very old, and some of which was recently constructed. The railroad tracks along the northern portion of the Rail District as well as the sidings that come into the area are very important considerations. Several spurs continue to be heavily used by the industry in the area, while others have been abandoned but may be available for future use. The former Grand Junction Steel site, located in the center of the Rail District, historically received three to four rail cars per week of raw material and finished products were then trucked from the site. Castings, Inc. handles approximately 480,000 pounds of materials and products in containers by rail each week. Thus, as in the past and as long as it continues to be used as a means of transport, the railroad is a valuable asset to this area and to the community as a whole.



Existing Sidings in the Rail District

The existing street network in the Rail District is incomplete. The plan for the area addresses how the existing streets should be used and how new streets should be planned in the eastern and western ends of the Rail District to continue to provide and enhance access for many modes of transportation to, from and through the area. Presently, 9th, 12th and 15th Streets are the primary north-south streets utilized by the heavy commercial and industrial uses in the area. 7th Street is generally perceived by the

community as the “public” access to and from the Rail District and points south. Certainly, the completion of the Riverside Parkway through and along the southern edge of the Rail District has had a major impact on the area with a positive influence.

Part of the existing conditions of the built environment is the pattern of land ownership and use. In the central part of the Rail District there are numerous small parcels. Some have been aggregated into large holdings such as for the larger industries in the area including the former Grand Junction Steel site, Whitewater Building Materials Castings, Inc. and Munroe Pump. The railroad has large landholdings in the area as do various public entities. City-owned properties primarily include remnant parcels that were acquired for construction of the Parkway. Mesa County recently disposed of several small parcels and the State of Colorado has the Department of Transportation complex on the east side of 9th Street and D Road. To the east, the parcels are larger but some are not configured very conducive to development. There are also still remnants of the early days of this area. There are some remaining pockets of occupied single-family residential homes.

There are also a few isolated commercial structures with historic significance, most notably the one pictured below which is a remnant of a sugar beet factory complex. The building was previously most visible from the riverfront trail and Orchard Mesa but it is now very visible from the Riverside Parkway. As this area becomes more familiar to people passing through on the trail and on the Parkway, perhaps some of the buildings like this one can become a more integral part of redevelopment.



Existing Single-Family Residence

It is estimated that there is a daytime population of over 1,000 employees in the Rail District that have very little available in the way of goods, services, restaurants and other commercial uses in close proximity. There appears to be a need and a desire to promote and develop uses that could not only service the daytime working population but also support the recreational and park users in evenings and on weekends.



Remnant Building from Sugar Beet Factory



Existing Business on South 7th Street

There are surrounding influences that impact the Rail District such as the proximity of the area to the Downtown District. This plan considers the main entrances and connections to the other Districts within the Greater Downtown area and how they can be improved as well as determine if or where there are barriers such as the railroad crossing and how those can be mitigated.

An inventory of existing land uses within the Rail District was completed as part of the planning process, identifying patterns of development and architectural character. Existing land use in the more developed central area of the Rail District is and has historically been primarily heavy commercial and industrial with remnant pockets of residential. The area between 5th Street and 9th Street is characterized by smaller parcels with older structures. A few businesses have aggregated parcels into larger parcels which are more conducive to future improvement and/or redevelopment. This area presents the best opportunities within the Rail District for redevelopment that could be accomplished through creative incentives and partnerships.

The heavy industries are primarily located between 9th and 15th Streets, clustered around the existing railroad spurs on larger parcels. Since the rail and its users are valuable assets to the area and the community as a whole, the core of this area is unlikely to change. However, there appears to be some opportunity and community support to create transitional areas of varied land uses in a tier surrounding the core industrial area. These transitional areas can be used to create compatibility between adjacent uses such as the park and the heavy industrial as well as help visually screen the industrial areas.



Existing Heavy Industrial Uses

On the eastern side of the Rail District, much of the property is held in larger ownerships and is vacant or underutilized. There is also a small pocket of existing residential use along 27-1/2 Road just south of the Riverside Parkway but which has been zoned for future commercial/industrial uses for many years. Much of the eastern area presents the greatest opportunity for increasing heavy commercial and industrial use within the Rail District and the Greater Downtown area as a whole.

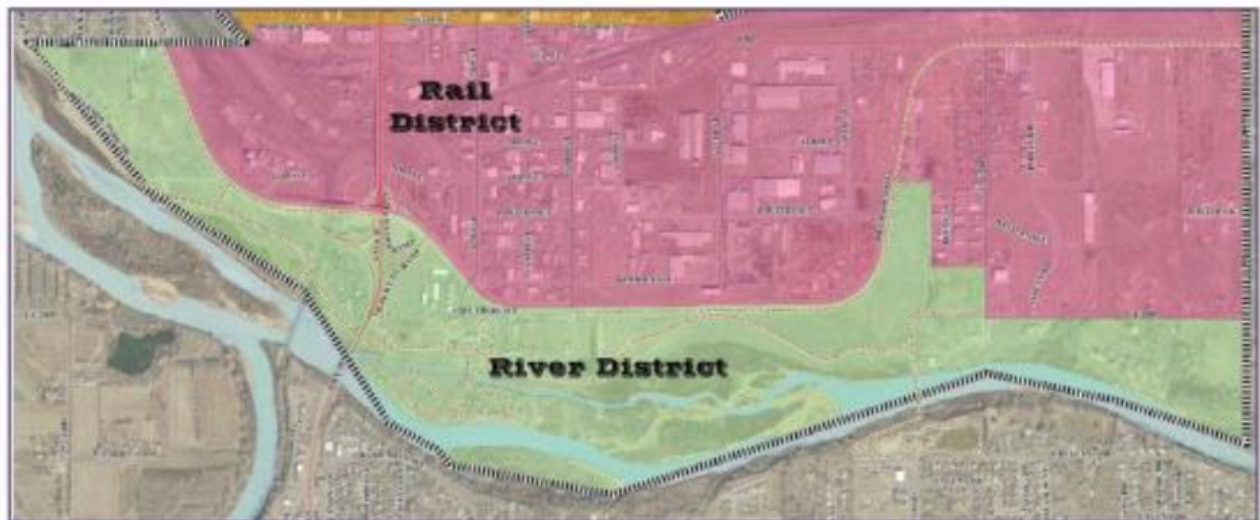
(Ord. 4571, 3-20-13)

Article III. River District

§ 36.08.080. River District.

The River District is located on either side of the confluence of the Gunnison and Colorado Rivers, roughly between the Riverside neighborhood to the northwest to 28 Road on the east and the Riverside Parkway to the Colorado River. At this “grand junction,” the area transformed from an agricultural based community into the commercial and industrial area it is today. Over time, the area has been used to store mill tailings along the river, process sugar beets in the historic beet packing complex, and more recently the community has rediscovered its natural value for green space, trails and public uses such as the Botanical Gardens. The majority of the property in the River District is publicly owned including the Las Colonias Park site, the Botanical Gardens property, the Jarvis property, some remnants of land that were acquired for construction of the Parkway and State Parks properties on the east end of the district.

There is an existing trail through the area that must be recognized as the area develops and redevelops. The plan considers that the trail will eventually be extended to the east along the river and that improved public access to the trail system throughout the area is a necessity.



Located within walking distance from the other downtown areas, the River District offers both easy access to recreational amenities along the river, as well as convenient access to shopping and businesses in the adjacent Central Business District. It also functions as a gateway into downtown from Highway 50. Its location and context establishes it as one of the most important places in the City.



Botanical Gardens

The River District is a critical area of the community. It virtually is THE Grand Junction. The Colorado River has a big influence on the area which presents both constraints and opportunities. The floodplain associated with the Colorado River has been altered by construction of a levee that protects a large part of the River District from flood inundation but there are some areas that are still impacted by potential flooding of the Colorado River.

On the other hand, the Colorado River does present excellent opportunities to maintain and enhance amenities that have already been placed along the Colorado River including the Botanical Gardens, the riverfront trail system, the Old Mill pedestrian bridge and the community investment of the Riverside Parkway.

The proximity of the area to Orchard Mesa influences the plan both physically and visually. There are also recreation and open space uses within and nearby – Eagle Rim Park on Orchard Mesa and the Botanical Gardens along the Colorado River. The Greater Downtown Plan addresses how these spaces should be connected, enhanced and integrated into redevelopment of the area.



The topography of the site is also an important consideration. While the River District area itself is flat, it is significantly lower than Orchard Mesa to the south. This makes it a very visible area as well as presents some unique opportunities for views and vistas.

In addition, the plan considers the planning efforts that have been completed for areas within the River District including the Botanical Gardens, Las Colonias Park and the City-owned Jarvis property. The Greater Downtown Plan integrates with the adjacent uses to the east that were included in the Pear Park Neighborhood Plan.

Several design concepts have been developed for the Las Colonias Park site which lies south of the Riverside Parkway (Parkway) from 9th Street to 27-1/2 Road. A trail has been constructed through the property and now that the Parkway has been in use, there is renewed interest in developing a more specific plan. Some amenities being considered are parking, restrooms, shelters, play area, open space, expanded walk/bike trails, open space for festivals, music, and educational presentations, various wetlands and arbor education opportunities, a kayak park, tree walk and a disc golf course.

The Jarvis property was historically used as a salvage yard until purchased and cleaned up by the City in the late 1980s. Since then, the property has been vacant with the exception of the recent trail construction through the site. Other uses on the west end of the River District are industrial along the south side of the railroad tracks. A mixed use conceptual plan has been developed for the Jarvis property which was considered through the development of the Greater Downtown Plan.



Views of the Rail and River District from Eagle Rim Park

There are areas of low density residential uses along the south side of C-1/2 Road just west of 28 Road. These uses are intended to remain, along with future development of the riverfront trail system from 27-1/2 Road to 28 Road. The Colorado State Parks has acquired several properties in this area in anticipation of continued trail development.

(Ord. 4571, 3-20-13)

Chapter 36.12. Greater Downtown Goals and Policies

§ 36.12.010. Background.

City staff held a series of meetings with property owners, including elected City officials, representatives of large industries, economic redevelopment interests and owners of small businesses and properties. The meetings were conducted as informal brainstorming sessions in order to define more specific issues, constraints and opportunities and continue to discuss the community's vision for the Greater Downtown area.

In addition to these meetings, two public open houses were held (December 2011 and February 2012) and questionnaires and information were available on the City's website for several weeks to invite similar input from other property owners, tenants and citizens at large. Participants were asked to evaluate various design and planning concepts to determine which of these are most important to the community and should be addressed in greater detail in the plan. The concepts addressed four major topics relative to an area plan: land use, circulation, economic (re)development and visual character. The results, along with previous information for the CBD, show strong community support for ideas that are translated to the goals and policies listed below for the Greater Downtown Plan.

(Ord. 4571, 3-20-13)

§ 36.12.020. Area-wide goals and policies.

(a) Goal 1. Enhance the transportation system to accommodate automobiles, bikes and pedestrians, and provide adequate, convenient parking.

(1) Policy 1a. Street design will accommodate travel lanes, parking, bike lanes, medians, sidewalks, and street trees, appropriate to and complementary of the adjacent land use.

- (2) Policy 1b. Street design will achieve a balance between travel mobility, land use access and livability and improve connections to the Greater Downtown area and the connections between subdistricts.
 - (3) Policy 1c. Emphasize “walkability” of the downtown area through street design that is pedestrian friendly to provide a foundation for a safe, active and livable area, including sidewalks, accessibility improvements, bicycle facilities, off-street trail connections and safe crossings, where appropriate.
 - (b) Goal 2. Establish and improve entry points into the Greater Downtown area (refer to the wayfinding and signage map in GJMC § **36.16.070**).
 - (1) Policy 2a. Street and streetscape design will include signage, landscaping and other design elements to delineate appropriate entry points into Greater Downtown.
 - (c) Goal 3. Promote downtown living by providing a wide range of housing opportunities, primarily in the Downtown District.
 - (1) Policy 3a. Support a regional housing strategy with an emphasis on infill, downtown housing.
 - (2) Policy 3b. Promote development patterns and regulations that accommodate vertical mixed-use development, primarily in the Central Business District.
 - (d) Goal 4. Redefine the land use along key corridors to provide a mix that will offer the most opportunities for redevelopment and revitalization.
 - (1) Policy 4a. Define subareas and corridor areas for groupings of land uses that are complementary to the rest of the Greater Downtown area.
 - (2) Policy 4b. Mixed uses, including residential, will be encouraged in appropriate subareas and corridors.
- (Ord. 4571, 3-20-13)

§ 36.12.030. Downtown District goals and policies.

- (a) Goal 1. Maintain and enhance the economic, cultural and social vitality of the Downtown District.
- (1) Policy 1a. Define subareas and corridor areas for groupings of land uses that are complementary to the rest of the Greater Downtown area.
- (2) Policy 1b. Implement infill and redevelopment policies that support downtown.
- (3) Policy 1c. Encourage a wide mix of uses, offering retail and commercial services at ground level and business/office/residential on upper floors in all but residential areas.
- (4) Policy 1d. Maintain and expand public amenities and services in the Downtown District.
- (5) Policy 1e. Enhance and preserve Whitman and Emerson Parks to integrate the space into the downtown fabric and encourage use by the community.
- (6) Policy 1f. The City with assistance from the Downtown Development Authority will explore the alternative street configuration to relocate the one-way couplet of streets that are currently Ute and Pitkin Avenues to utilize Pitkin and South Avenues for this purpose.
- (7) Policy 1g. Study alternatives for 4th and 5th Streets including returning these streets to the two-way grid system between Ute Avenue and North Avenue.
- (8) Policy 1h. Within the CBD, encourage shared parking, discourage single-use, surface parking and develop new means of paying for shared parking (e.g., develop a fee in lieu of required on-site parking that will be used to fund shared parking structures).

- (b) Goal 2. Require density/intensity in the Downtown District as prescribed by the Comprehensive Plan, primarily within the Central Business District.
 - (1) Policy 2a. Strengthen means of implementation that promote vertically mixed-use structures, primarily within the CBD.
 - (2) Policy 2b. Require minimums in height and density/intensity for new development in the CBD.
 - (3) Policy 2c. Require minimal or no building setbacks within the Downtown Core to maximize site intensity/density.
- (c) Goal 3. Develop a pedestrian-oriented, walkable Downtown Core.
 - (1) Policy 3a. Discourage uses on ground level that do not support pedestrian activity.
 - (2) Policy 3b. Require building facade details that activate the ground floor, particularly on corner buildings to activate north-south streets.
- (d) Goal 4. Stabilize and enhance the residential subareas.
 - (1) Policy 4a. Discourage further encroachment of nonresidential uses into the established residential neighborhoods.
 - (2) Policy 4b. Establish design standards to address conservation and enhancement of the residential development patterns and streetscape.
 - (3) Policy 4c. Establish design standards for the transitional subareas to emphasize use and development compatibility with adjacent residential areas.
 - (4) Policy 4d. Promote the establishment of neighborhood watch and neighborhood organizations.
- (e) Goal 5. Recognize and promote opportunities to build subareas/neighborhoods, each with a unique identity.
 - (1) Policy 5a. Develop a set of guidelines within each subarea to address building and facade design, streetscape, landscape and other elements of site development.
- (f) Goal 6. Jump-start the revitalization and reinvestment in the Downtown District with strategic catalyst projects.
 - (1) Policy 6a. Plan and budget for strategic property acquisition for future development.
 - (2) Policy 6b. Identify locations for and promote concepts of catalyst projects, including public building/housing/mixed use, live/work units, mixed-use retail/residential and mixed-use retail/office.

(Ord. 4571, 3-20-13)

§ 36.12.040. Rail District goals and policies.

- (a) Goal 1. Preserve the opportunity for heavy industry and rail service that supports it.
 - (1) Policy 1a. The City will maintain industrial zoning in those areas served by rail lines and sidings.
- (b) Goal 2. Recognize distinction between “industrial” streets such as 9th and 12th Streets and “public” streets 7th Street and Riverside Parkway.
 - (1) Policy 2a. Develop street sections that reflect the differences in development patterns along and the use of the street.
 - (2) Policy 2b. Discourage industrial and commercial traffic from using C-1/2 Road and direct this traffic to travel north and east using the Riverside Parkway.

- (c) Goal 3. Promote higher quality, customer and pedestrian friendly development along 7th Street and Riverside Parkway.
 - (1) Policy 3a. Implement design guidelines and standards along corridors that will result in site and building design improvements along the corridors.
 - (d) Goal 4. Re-establish and improve a street grid in the Rail District.
 - (1) Policy 4a. The City will implement the Grand Junction Circulation Plan in the Rail District, including construction of new streets as development occurs in the industrial and commercial/industrial areas.
- (Ord. 4571, 3-20-13)

§ 36.12.050. River District goals and policies.

- (a) Goal 1. Create/maintain/enhance a green waterfront.
 - (1) Policy 1a. Take advantage of and create opportunities and partnerships to enhance the riverfront trail system.
 - (2) Policy 1b. Take advantage of and create opportunities and partnerships to develop Las Colonias Park and open space areas within the Jarvis property.
 - (b) Goal 2. Create retail, general commercial and mixed use opportunities that complement the uses along the riverfront.
 - (1) Policy 2a. Utilize zoning, overlay districts and incentives for development and redevelopment of complementary uses.
 - (c) Goal 3. Create/enhance redevelopment opportunities and partnerships.
 - (1) Policy 3a. The City will work with the Downtown Development Authority to expand its boundaries.
 - (2) Policy 3b. The City will consider implementation of incentive strategies for redevelopment.
 - (3) Policy 3c. The City will consider redevelopment opportunities for the Jarvis property including the potential for public-private partnerships.
- (Ord. 4571, 3-20-13)

Chapter 36.16. Implementation Strategies

§ 36.16.010. Implementation strategies.

The City has a variety of tools available through which these goals can be implemented so that the vision for Greater Downtown can materialize and eventually be realized. This plan represents the first phase of implementation and includes the basic implementation strategies of designating future land use designations, zoning properties accordingly as needed, amending development standards of the zoning districts through a zoning overlay and establishing goals and policies for future phases of plan implementation such as economic (re)development strategies.

(Ord. 4571, 3-20-13)

§ 36.16.020. Downtown District future land use and zoning.

- (a) Future Land Use. The Downtown District is formulated around seven general land use designations: Commercial, Downtown Mixed Use, Neighborhood Center Mixed Use, Urban

Residential Mixed Use, Residential Medium High Density, Residential Medium Density and Parks and Open Space. These designations correspond with those established in the City-County Comprehensive Plan and are illustrated in Figure 1. A more detailed description of the land use designations may be found in the Comprehensive Plan. The Blended Land Use Map that is included in the Comprehensive Plan and allows for increases in residential density in compatible areas does not apply to properties within the Downtown District. This is due to the strong public sentiment in the Downtown District residential neighborhoods that the existing historic character of the single-family development be protected.

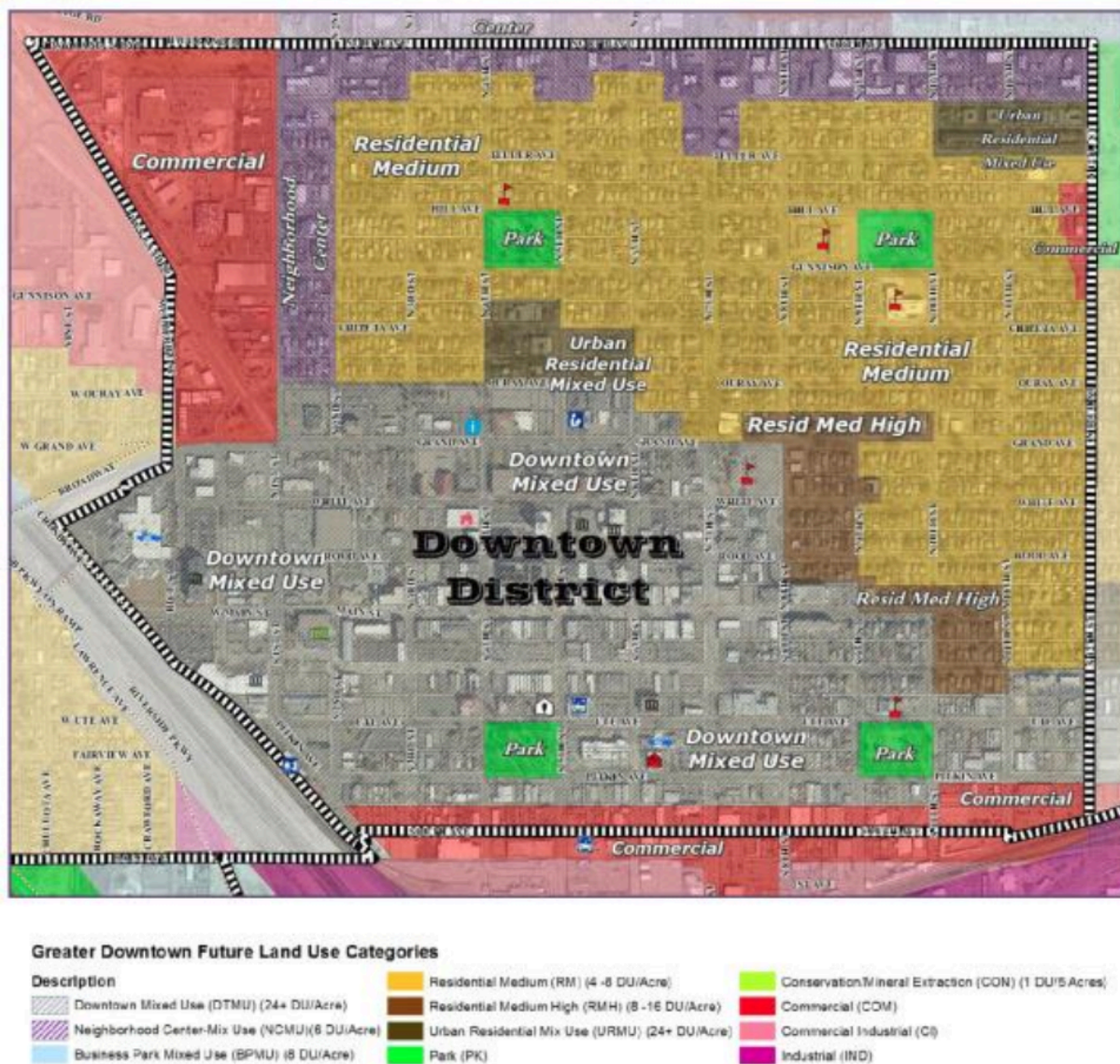


Figure 1 Downtown District Future Land Use Plan

- (b) Zoning. Proposed zoning within the Downtown District is shown in Figure 2 and includes the following zone districts: Residential 8 units per acre (R-8), Residential 24 units per acre (R-24), Residential Office (RO), Neighborhood Business (B-1), Downtown Business (B-2), Light Commercial (C-1), General Commercial (C-2), and Community Services and Recreation (CSR). Generally, the zoning is proposed to remain the same as currently exists. A more detailed description of the zone districts within the Downtown District may be found in the Zoning and Development Code.

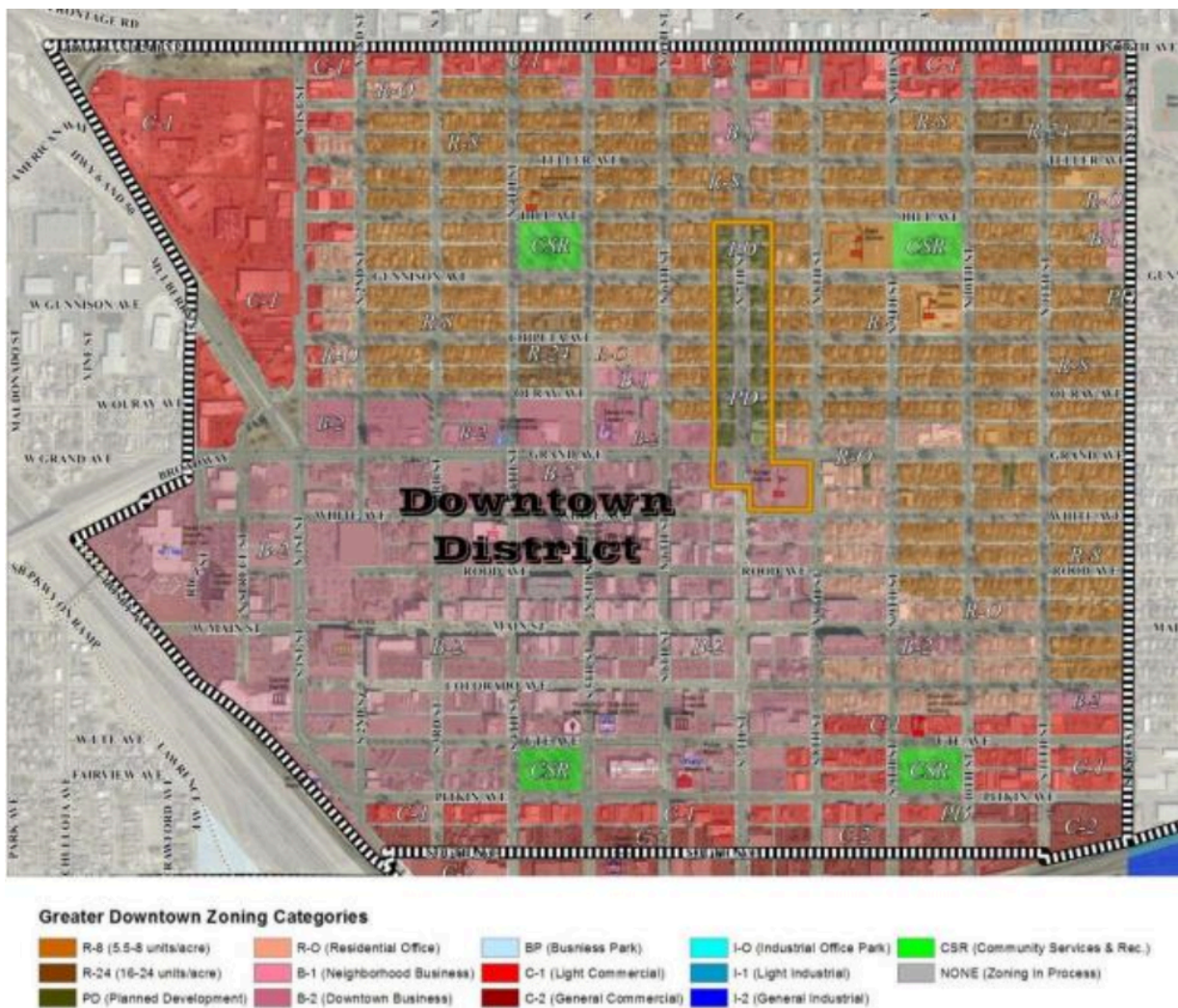


Figure 2 Downtown District Zoning Map

(Ord. 4571, 3-20-13)

§ 36.16.030. Rail District future land use and zoning.

- (a) Future Land Use. The Rail District is shown in Figure 3 and is formulated around five general land use designations: Business Park Mixed Use, Commercial, Commercial Industrial, Industrial and Parks and Open Space. These designations correspond with those established in the City-County Comprehensive Plan. A more detailed description of the land use designations may be found in the Comprehensive Plan.



Figure 3 Rail District Future Land Use Plan

- (b) Zoning. Proposed zoning within the Rail District is shown in Figure 4 and includes the Light Commercial (C-1), General Commercial (C-2), Community Services and Recreation (CSR), Business Park Mixed Use (BP), Industrial/Office Park (I-O), Light Industrial, and General Industrial (I-2) zone districts. The majority of the zoning is proposed to remain the same as currently exists. A more detailed description of the zone districts within the Rail District may be found in the Zoning and Development Code.

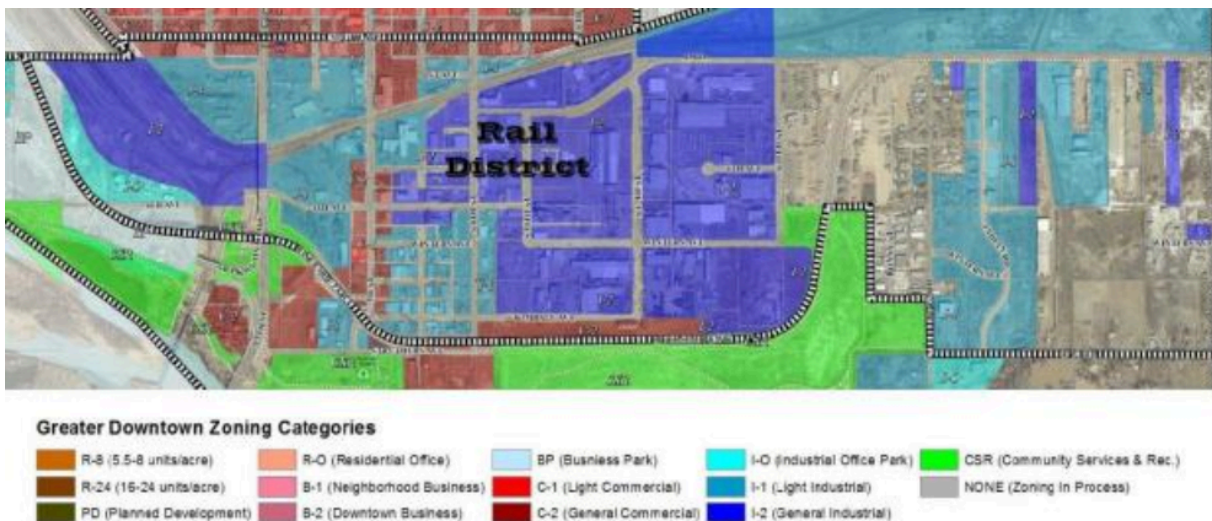


Figure 4 Rail District Zoning Map

(Ord. 4571, 3-20-13)

§ 36.16.040. River District future land use and zoning.

- (a) Future Land Use. The River District as shown in Figure 5 is formulated around six general land use designations: Commercial, Commercial Industrial, Parks and Open Space, Conservation, Estate and Business Park Mixed Use. These designations correspond with those established in the City-County Comprehensive Plan. A more detailed description of the land use designations may be found in the Comprehensive Plan.

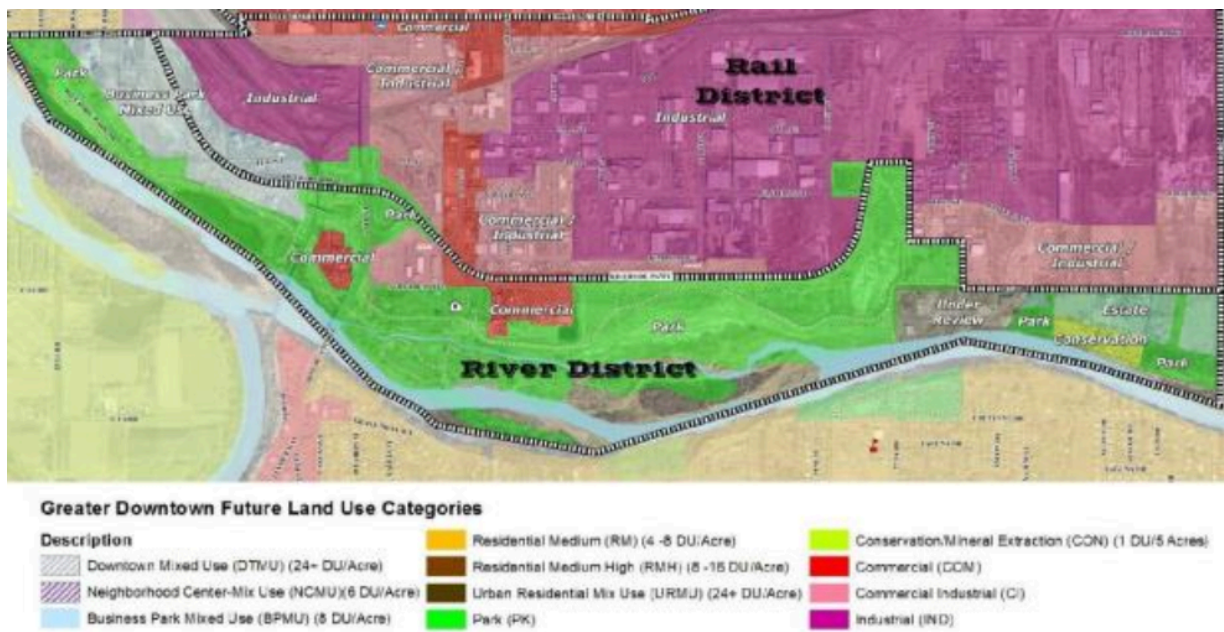


Figure 5 River District Future Land Use

- (b) Zoning. Proposed zoning within the River District is shown in Figure 6 and includes the following zone districts: Light Commercial (C-1), General Commercial (C-2), Community Services and Recreation (CSR), Business Park Mixed Use (BP), Industrial/Office Park (I-O), and Light Industrial (I-1). The majority of the zoning is proposed to remain the same as currently exists. A more detailed description of the zone districts within the River District may be found in the Zoning and Development Code.

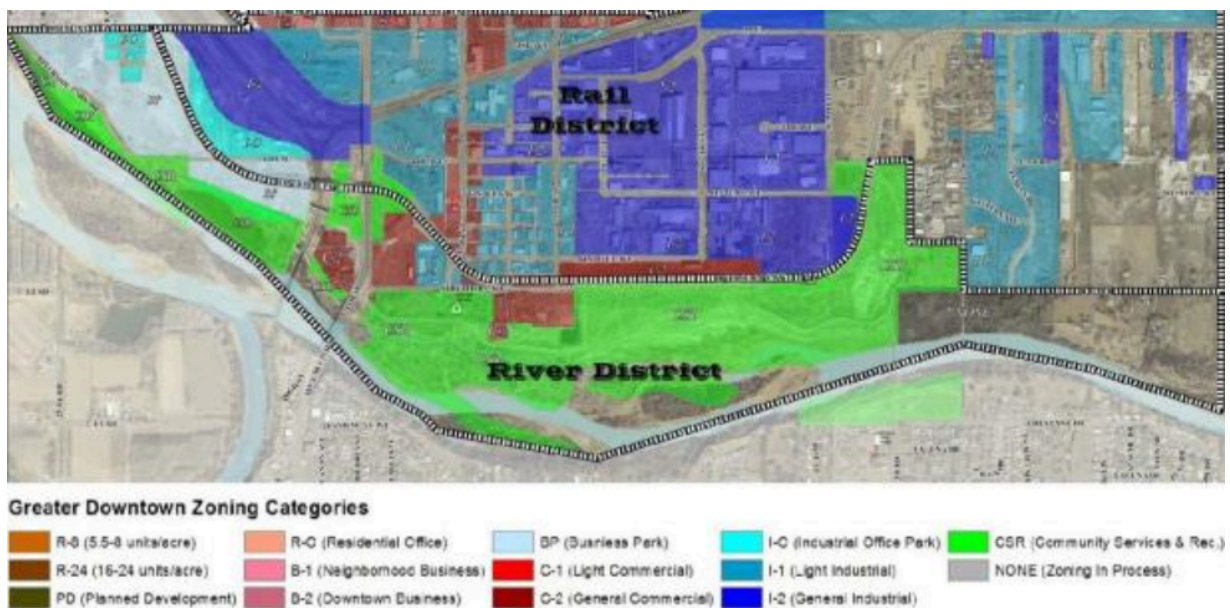


Figure 6 River District Zoning Map

(Ord. 4571, 3-20-13)

§ 36.16.050. Development standards.

The community desires to improve the visual character of the Greater Downtown areas that are most visible along major public corridors or from the major public spaces. Consequently, the plan promotes a higher quality built environment through improved architectural character, reduced visual clutter and

enhanced streetscape. These elements are addressed through the Greater Downtown zoning overlay detailed in a companion document to this plan. The elements of the overlay are intended to augment the zoning district standards in the Zoning and Development Code.

(Ord. 4571, 3-20-13)

§ 36.16.060. Conceptual plan for traffic circulation in Greater Downtown.

- (a) The Grand Junction Circulation Plan adopted by the City of Grand Junction and Mesa County serves as the traffic circulation plan for the Greater Downtown area. The following conceptual plan for traffic circulation in Greater Downtown describes the policies for streets and corridors in the Greater Downtown, depicts preferred conceptual designs and proposes additional unclassified streets which support those policies. This conceptual plan for circulation does not modify the Grand Junction Circulation Plan, but provides preferred concepts and designs for future consideration and development of public rights-of-way.
- (b) Policies.
 - (1) Conduct a more detailed traffic analysis of the area to determine needed intersection control, street cross-sections, pedestrian and vehicle facilities, additional streets and access control, and amend the Grand Junction Circulation Plan accordingly.
 - (2) Continue the review process for the Grand Junction Circulation Plan to determine needed pedestrian and bicycle transportation facilities such as trails, pedestrian paths and bike lanes, and amend the Grand Junction Circulation Plan accordingly.
- (c) In the Grand Junction Circulation Plan, urban streets are classified according to their function in the transportation network. The two components of function of the Grand Junction Circulation Plan are (1) to provide access to properties and (2) to carry traffic safely and efficiently from point to point. In order to preserve safety and capacity and to enhance the quality of living, these two components should be inversely related, such that busier streets have limited access (resulting in more efficient flow) and quieter streets provide access to properties.
- (d) Major Street Corridors. The components of the major street system are identified on the Grand Junction Circulation Plan. These corridors primarily function as carriers of traffic from point to point. The conceptual circulation plan in the Greater Downtown Plan includes proposed unclassified roadways shown in yellow on Appendix C, GJMC § 36.20.030.

As properties develop and redevelop within the greater downtown area, the Grand Junction Circulation Plan will be implemented through construction or improvement of streets warranted by the proposed development, based on adopted street cross sections in the Transportation Engineering Design Standards (**TEDS**). The conceptual circulation plan in the Greater Downtown Plan provides preferred alternatives for specific design and layout of certain streets, corridors and intersections in the planning area. For example, in commercially-zoned areas, the Commercial Street cross-sections of **TEDS** will apply, but modifications may be made according to the conceptual circulation plan illustrated and described here (street cross-sections, illustrations and text).

- (1) North Avenue – Arterial Street. A potential layout for North Avenue between 1st and 12th Streets was adopted with the North Avenue West Corridor Plan. The layout is incorporated into the Greater Downtown Plan as depicted in Figure 7.



Figure 7 North Avenue Cross-Section

- (2) Grand Avenue – Arterial Street. While Grand Avenue is an established street with improved streetscape elements the length of the segment within Greater Downtown (1st to 12th Streets), there could be improvements made in some blocks that would enhance traffic flow, pedestrian safety and visual aesthetics. A typical, potential median improvement to Grand Avenue is shown in Figure 8. The segment of Grand Avenue between 1st Street and 7th Street also has the potential to be used as a “sharrow.” A sharrow is marked as in Figure 9 so that the travel lane may be used by both bicycle and vehicular traffic.

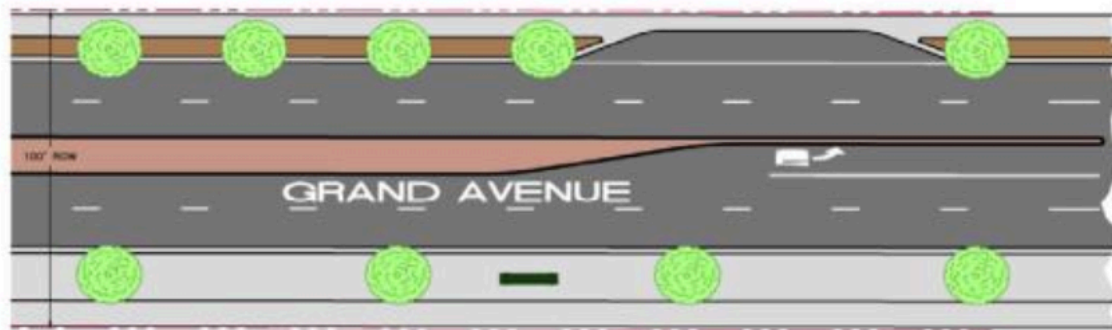


Figure 8 Grand Avenue Cross-Section



Figure 9 Example of a Sharrow

- (3) North 1st Street – Arterial Street. The segment of North 1st Street from Grand Avenue to North Avenue is deficient in street design and pedestrian amenities. The Greater Downtown Plan envisions eventual redevelopment of this street to improve safety, drainage, pedestrian circulation, access and improve on-street parking. A potential layout is shown in Figure 10.

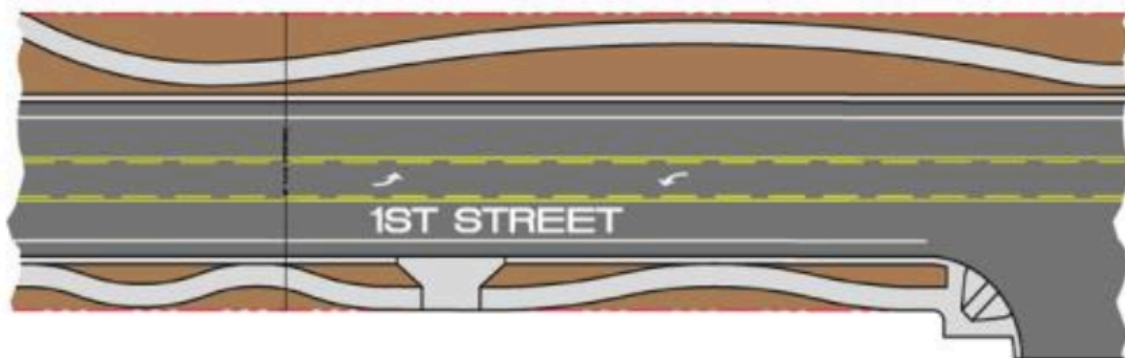


Figure 10 1st Street Cross-Section

- (4) 1st Street and Grand Avenue Intersection. The Colorado Department of Transportation (CDOT) completed an environmental assessment for the I-70B/6 and 50 corridor which recognized circulation needs at the 1st and Grand intersection. A potential design is shown in Figure 11. This design is incorporated into the Greater Downtown Plan as are any potential improvements CDOT may include in this project on 1st Street from Grand Avenue to Pitkin Avenue.



Figure 11 Proposed Design 1st Street and Grand Avenue

- (5) 7th Street and Grand Avenue Intersection. A design for a roundabout was included with the plans for 7th Street improvements that were completed in 2007-2008. The roundabout was not constructed at that time, but the intersection still warrants vehicular and pedestrian improvements. The plan as originally proposed is shown in Figure 12.

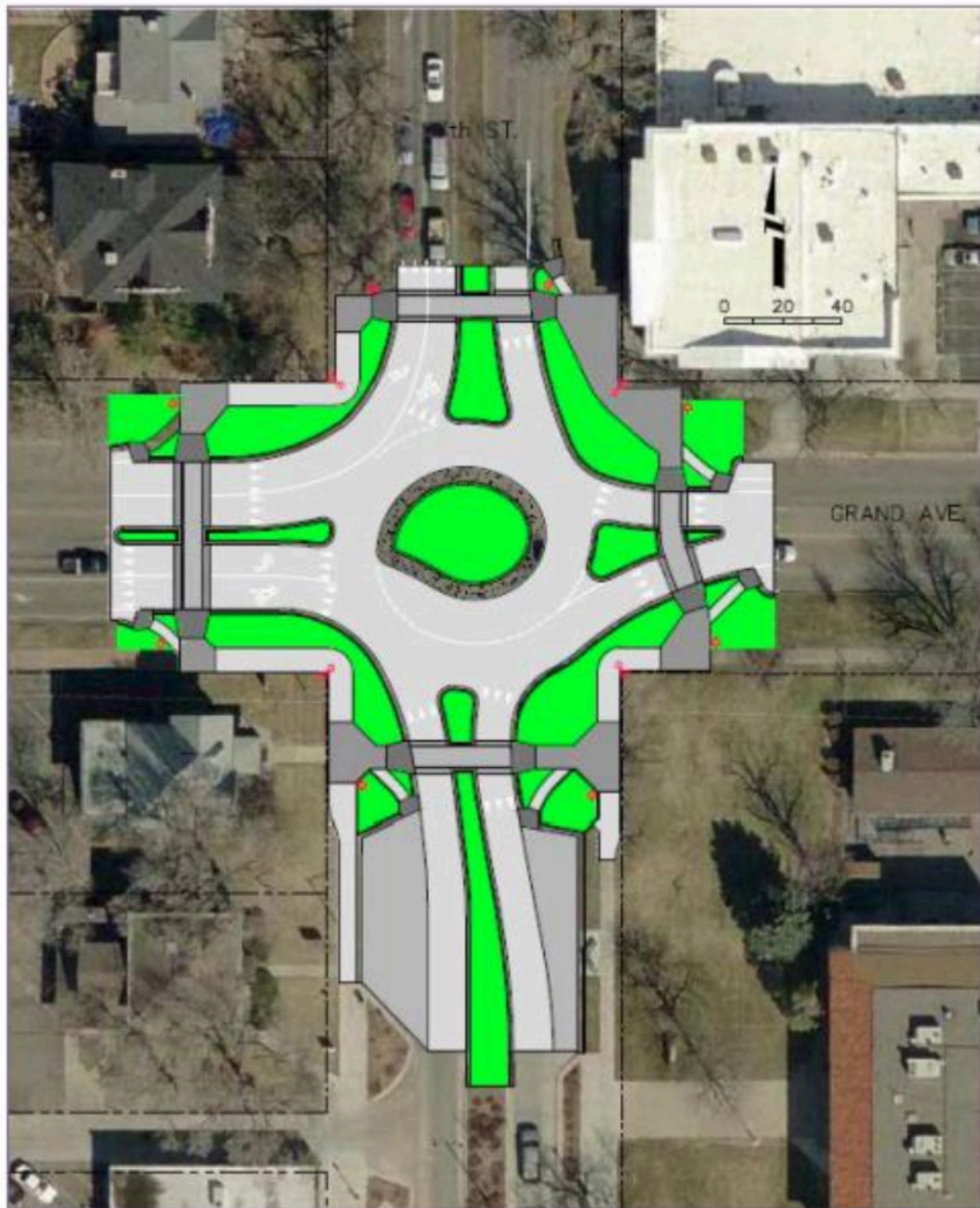


Figure 12 Proposed Design 7th Street and Grand Avenue

- (6) West Main Street and Spruce Street Intersection. Vehicular and pedestrian traffic has increased on the West Main Street corridor due to activity at the County Justice Center and the Mesa County Central Services building. To ease traffic flow through the area and improve pedestrian safety, this potential project for a roundabout at Spruce and West Main Streets was originally conceived during the visioning of Main Street in 2008 (Figure 13). The roundabout would serve as a visual anchor to the west end of Main Street just as the one at 7th Street serves the east end. The roundabout would also allow for a circulator bus to serve the Main Street corridor from Spruce Street to 7th Street.

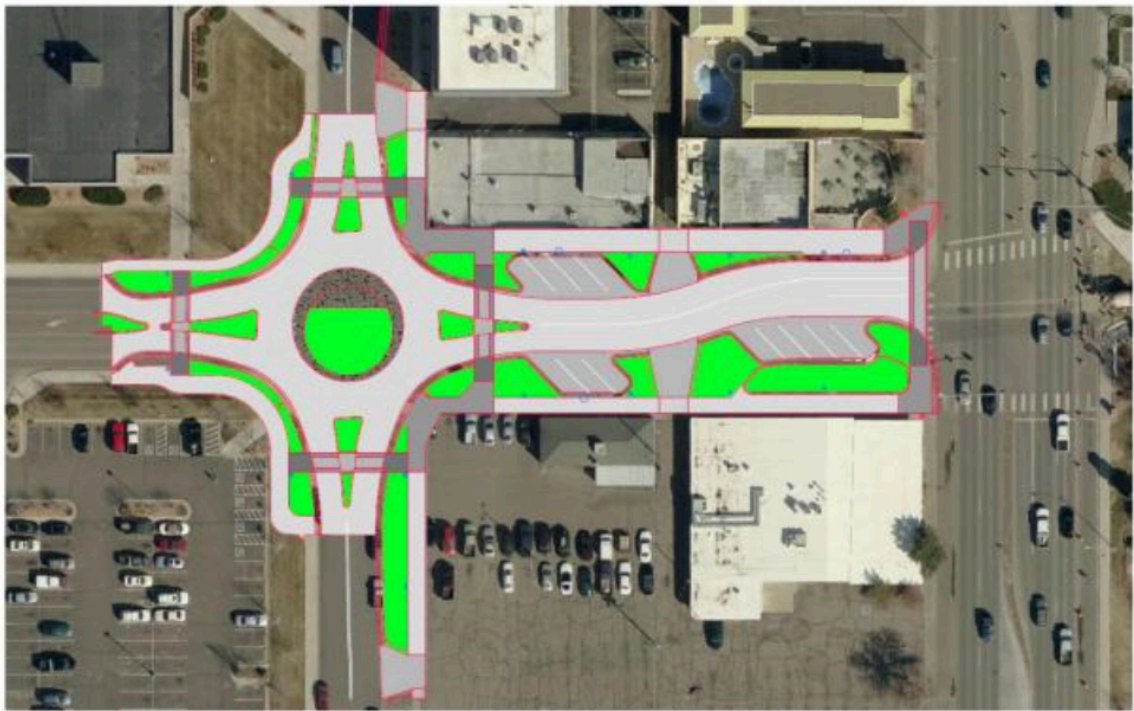


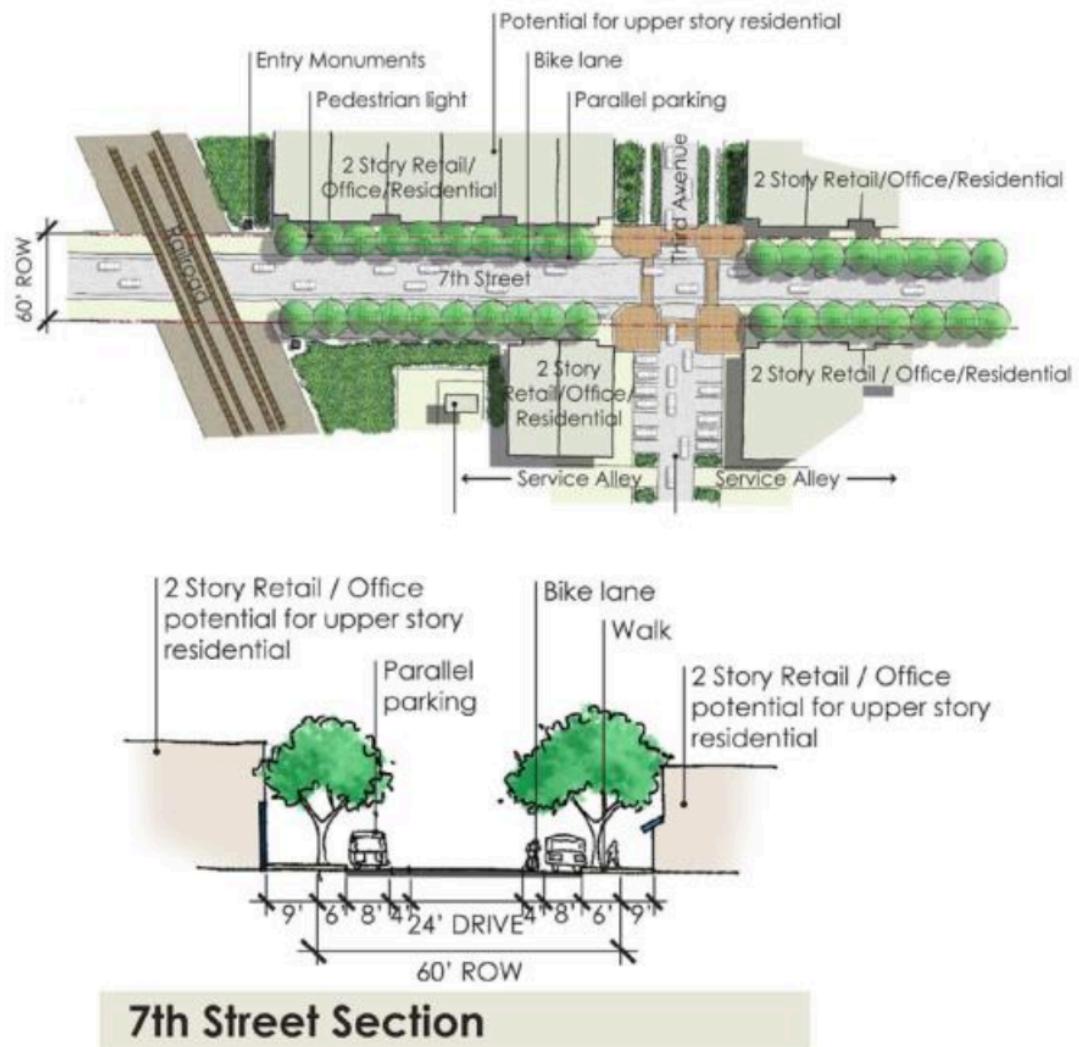
Figure 13 Proposed Design West main Street and Spruce Street

- (7) Riverside Parkway – Arterial Street. The right-of-way width varies; multi-lane; bike lanes; detached walk on the south side; no on-street parking. Completed in 2007.

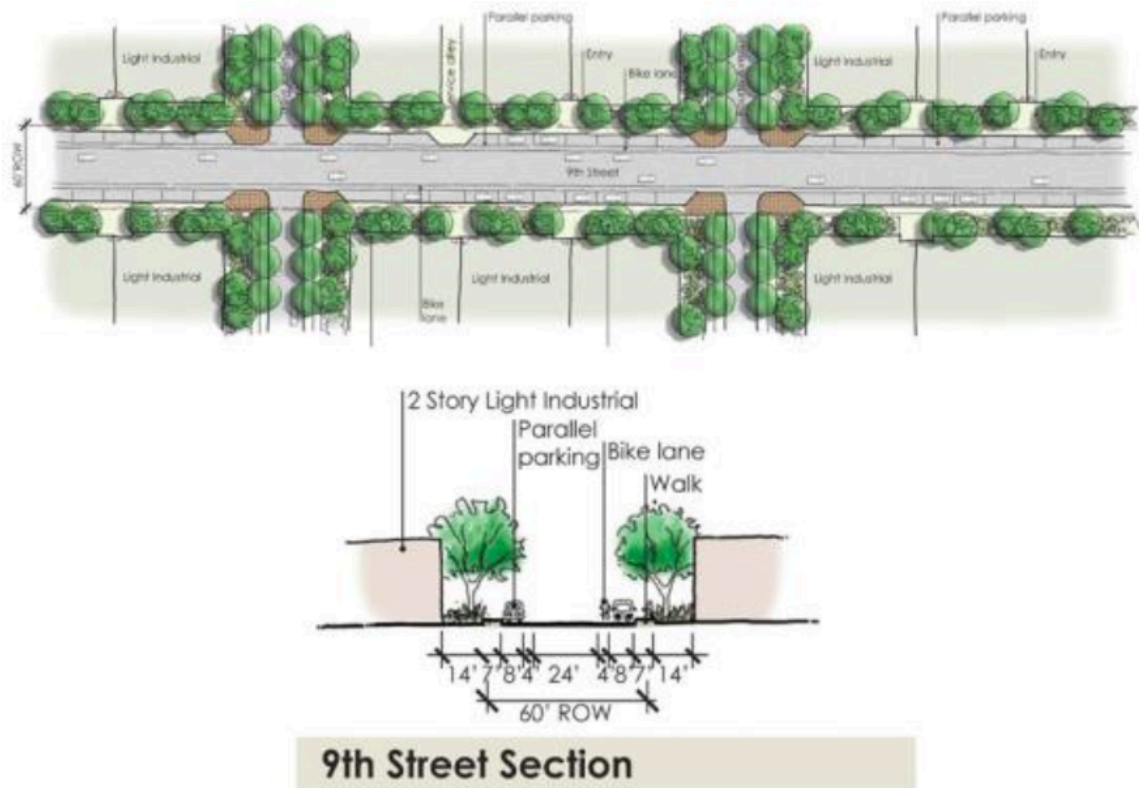


- (8) South 7th Street. Suggest two lanes; bike lanes; on-street parking both sides; detached walks with landscaping. The streetscape plan for South 7th Street should be enhanced with a similar design as was constructed on 7th Street south of Grand Avenue. Generally, the design would continue the 7th Street boulevard treatments from downtown, Ute and Pitkin to the Botanical Gardens and riverfront area with additional street trees, historic street lights, street furniture and public art. This concept would create a more consistent visual character to connect the River and Rail Districts to the traditional downtown area and improve the visual design of the

corridor and emphasize its use as the primary public north-south corridor through the neighborhood. The concept features enhanced pedestrian facilities with colored concrete, pedestrian safe zones at the “bulb-outs” for easier crossing and additional landscaping.



- (9) South 9th Street and 27-1/2 and C-1/2 Roads. Suggest two lanes; on-street parking both sides; detached sidewalk preferred where possible. The streetscape plan for South 9th Street is to develop similar to what presently exists along South 7th Street with a more defined hardscape of curb and gutter, enhanced pedestrian facilities and street trees. This concept improves the visual quality of the corridor without requiring improvements on private property or compromising adjacent uses. The concept also allows the street to function for the commercial/light industrial traffic that it carries as well as provides for a more comfortable pedestrian or bicycle environment since South 9th Street may be used by the public to access business/commercial areas and Las Colonias Park.



(10) Kimball Avenue. Suggest two lanes; on-street parking; detached walks with landscaping.



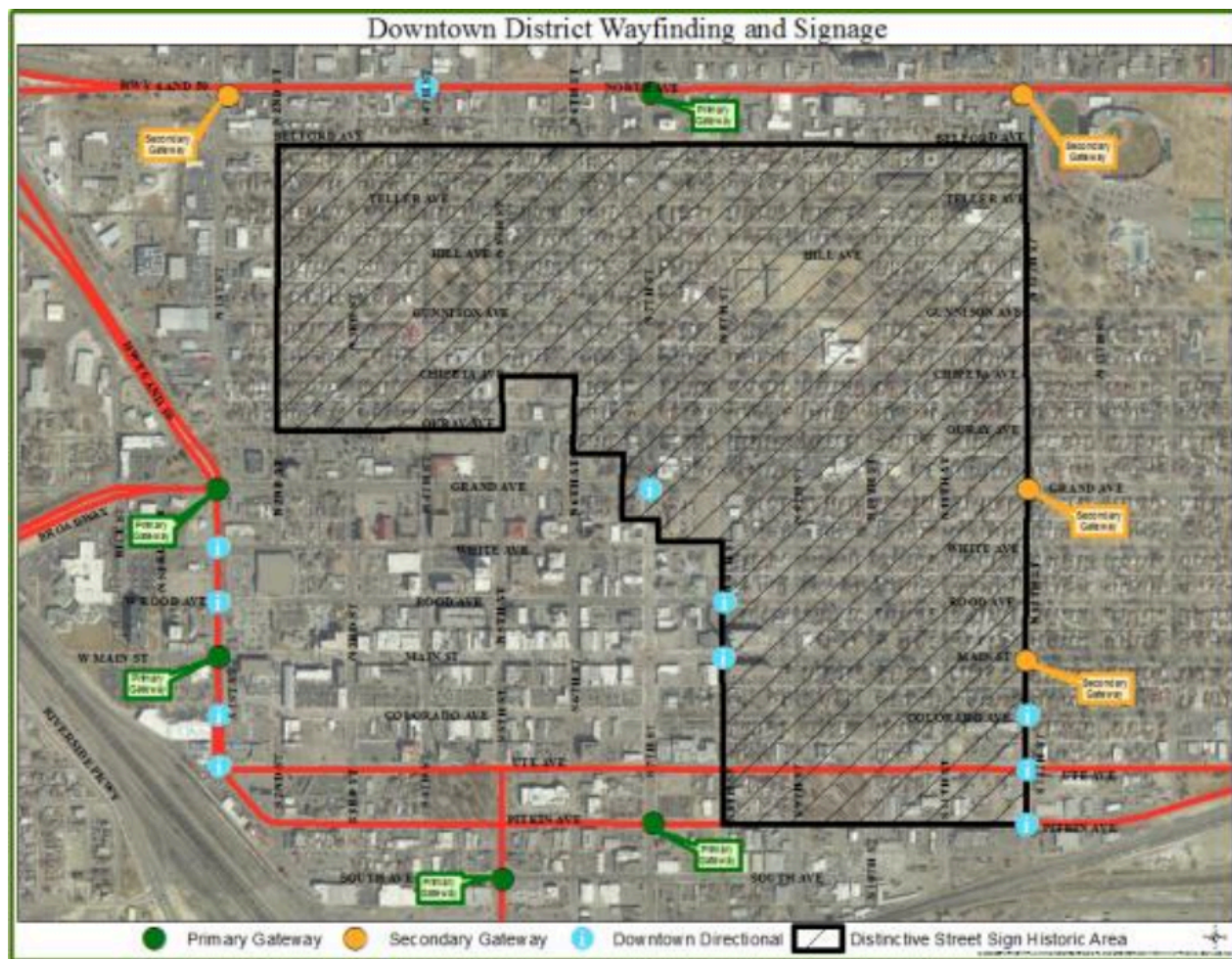
(11) D Road (from 9th Street east to the Riverside Parkway). Section yet to be determined.

- (e) Local Streets. The components of the local street system are identified on the Grand Junction Circulation Plan. The primary function of the local street network is to provide access to and connections among individual parcels and subdivisions, provide a safe network for movement of pedestrians and bicycles, and serve short length trips to and from collector and higher order streets. Trip lengths on local streets should be short with a lower volume of traffic moving at slower speeds. As properties develop and redevelop within the greater downtown area, the Grand Junction Circulation Plan will be implemented through construction or improvement of streets warranted by

the proposed development, based on adopted street cross sections in the Transportation Engineering Design Standards (**TEDS**), GJMC Title **29**.

- (f) Trails. The Grand Junction Circulation Plan shows the location of future bicycle facilities, trails and pedestrian paths. As development or redevelopment occurs, trails, paths, bike lanes and pedestrian facilities will be constructed in accordance with the adopted Grand Junction Circulation Plan. Also the City may construct these facilities using transportation capacity payments (TCP) funds as part of one or more capital improvement project(s). The Grand Junction Circulation Plan is currently undergoing review by the City of Grand Junction and Mesa County. The conceptual circulation plan of the Greater Downtown Plan includes, in Appendix C, GJMC § **36.20.030**, the a preferred alternative for trails, paths, bike lanes and pedestrian facilities in the Greater Downtown planning area. The preferred alternative shown comes from a 2012 draft of proposed amendments to the Grand Valley Trails Plan (now Grand Junction Circulation Plan), and it includes the following:
- (1) Extension of Riverfront Trail from Las Colonias Park east to 28 Road (and beyond).
 - (2) Bike routes (signs but no separate lane) on Grand Avenue from 7th Street east and 28 Road between the Riverside Parkway and C-1/2 Road.
 - (3) Sharrow bike routes (bikes share slightly wider lane with cars as depicted in Figure 9) on Grand Avenue between 1st and 7th Streets.
- (g) Riverside Parkway Pedestrian Overpass. This conceptual circulation plan envisions one or more pedestrian overpasses from the commercial areas of the Rail District to the riverfront areas and Las Colonias Park. Activities and uses in the future park, types of development along the north side of the Parkway will determine where such facilities may be needed. An overpass on the western end in the vicinity of 7th or 9th Streets could also serve as an entrance feature to the neighborhood as further discussed in GJMC § **36.16.070**.
- (h) Public Transportation. Grand Valley Transit (GVT) presently serves the Downtown District but does not provide service in the Rail and River Districts. Future transit needs within the Rail and River Districts should be monitored as development or redevelopment occurs in these areas and as Las Colonias Park sees more activity.
- (Ord. 4571, 3-20-13)

§ 36.16.070. Entryways and signage.



Important intersections in the street network offer opportunities to develop a unique theme and identity for the Greater Downtown area. The primary intersections vary in scale and include entries to the Downtown, Rail and River Districts. Each of these should be developed according to general concepts and criteria that are appropriate for their scale, function and importance.

- (a) Downtown District Entryways and Signage. While the Downtown District has a strong base of local and regional users, approaches to the District offer no distinguishing features or directional signage that orient first time visitors. When approaching or traversing perimeter streets, visitors have no sense that they are within eyesight of the historic town site or even that they are headed in the right direction toward downtown. Creating gateways at key entry points, as well as simple perimeter identifiers, will orient visitors, as well as strengthen and celebrate the heritage of the Downtown Districts. Because of the diversity of uses and neighborhoods within the Downtown District, the design of entryways should include a range of sizes, poles and ornamentation of signage and varied landscape elements, lighting and other features that are integrated with the signage. The primary gateways to the Downtown District and between downtown and the Rail District include the following intersections:

- (1) 7th Street and North Avenue.
- (2) 1st Street and Grand Avenue.
- (3) 1st Street and Main Street.
- (4) 5th Street and South Avenue.
- (5) 7th Street and Pitkin Avenue.

In addition, the distinctive public sign palette already started in the Downtown District should be continued and expanded to include street signs and directional signs that have recognizable poles, ornamentation, colors, fonts and logos. The City, together with the DDA, will further develop sign standards and guidelines for private signage placed on buildings or as freestanding signs.

(b) Rail and River District Entryways.

- (1) 5th Street/Riverside Parkway. There are opportunities to celebrate the entry into Grand Junction and the Rail and River Districts at the 5th Street bridge and Struthers Avenue area in conjunction with the Western Colorado Botanical Gardens with attractive low scale signage and sculpture. In addition, there are smaller monuments at various points along the Riverside Parkway that indicate to motorists that they are approaching or traveling on the Riverside Parkway. Due to the scale of the 5th Street/Riverside Parkway intersection and the publicly-owned area around it, this intersection affords the opportunity to create a monument/sculpture of a much larger scale to mark the entrance and give identity to the Rail and River Districts and/or to this "Grand Junction."

Another possibility in this vicinity is if a pedestrian overpass is desired/needed near the 7th Street/Riverside Parkway intersection it could serve several purposes: pedestrian access across the Parkway, include design elements that give a distinct character to the Rail and River Districts and integrate with surrounding open space, pocket parks and/or water features at the landings on each side of the Parkway.

- (2) South 7th Street/Railroad Tracks. There is an existing silo on the Mesa Feed property that identifies the Rail District as you travel south on 7th Street. It is of a scale that is visible from the southern perimeter of downtown and represents the historical agriculture and industrial base upon which the Rail and River Districts have developed. Such a structure could be enhanced and/or replicated to become an even stronger element at this major entrance to the area.
- (3) 28 Road/Riverside Parkway. This intersection is a smaller scale than the others, but a neighborhood entry could be created, particularly along the north side. The sense of arrival at this location could be created through a water feature, public art, an architectural feature with signage paving patterns and/or landscaping. The design of the entry feature should be of the same character of those that might be created at the other major entry points.

(Ord. 4571, 3-20-13)

§ 36.16.080. Economic redevelopment.

- (a) Downtown District Economic Redevelopment. While the Downtown District is the heart of the community, it is but one subset of a larger market and has strengths which can be capitalized on and limitations which should be overcome. The Downtown District has a tremendous influence on the economic well-being of the entire region. Therefore, it is widely accepted that early projects in any revitalization effort could be publicly assisted until market conditions reach levels where new construction can support itself.

The Grand Junction Strategic Downtown Master Plan presented guiding principles which, while general in nature, were considered responsive to prevailing conditions, market opportunities, framework elements and stakeholder input. These guiding principles are still relevant to the Greater Downtown Plan and are listed and described below.

- (1) The Downtown District is one submarket that competes with other submarkets in Grand Junction. The downtown environment, while presenting tremendous opportunity for investment in a setting uniquely positioned to offer both heart and history, carries with it certain limitations, particularly for land-intensive non-destination-oriented land uses. Several market sectors, however, not only survive, but also thrive in a downtown setting. Recognize the obstacles associated with downtown development and encourage regulatory and financial solutions including public subsidies and creative financing mechanisms.

- (2) The Downtown District must be market-responsive to changing conditions, with implementation tools and mechanisms in place to both offset competitive disadvantages and capitalize on competitive assets. Market conditions should be continually monitored and information distributed to a broad audience including developers, business and property owners, lenders, city staff and elected and appointed officials.
- (3) The Downtown District infrastructure must be protected and retained including physical features, service organizations, mix of employers, historic residential neighborhoods and community attitudes toward the Downtown District. Unlike many communities across the country, Grand Junction's Downtown District existing infrastructure is more than sufficient to promote itself as a downtown neighborhood. These assets, which provide the impetus for investment, need to be protected and promoted.
- (4) The Downtown District's "tool bag" must contain a variety of strategies and mechanisms to attract investment. These tools can be financial, physical, market, or organizational in nature and can be used independently or in various combinations. Given the obstacles associated with downtown development, it is imperative that whatever mix of tools is put in place it be comprehensive, flexible and creative.
- (5) Public investment must leverage private investment. Historically, the planning, financing and implementation of projects in the downtown market were the primary responsibility of public sector entities such as the City and the DDA. However, while the public sector continues to play a significant role in most downtown efforts, a critical component to the success of any revitalization strategy today is participation by both the public and private sectors. Leveraging of resources is key, as no one entity, public or private, has sufficient resources alone to sustain a long-term downtown improvement effort.
- (6) Public policy must support development in the Downtown District. Experience has proven that downtown development will best succeed if regional growth management programs reward efficient development patterns. If growth is allowed to occur in a land extensive, inefficient way that effectively subsidizes lower densities, downtown development will operate at a competitive disadvantage. Given Grand Junction's existing land use patterns, the Downtown District is susceptible to continued dilution of its role as the community's central business and shopping district.
- (7) Public-private partnerships are essential. Under any investment strategy, local government needs to have strong involvement, a visible presence, perhaps be the entity that provides continuing leadership, regulatory incentives, and seed capital for early projects. Not only does government have the legal responsibility to address many of the implementation components, but it is also the logical conduit to local, regional, state and federal funding sources.

(b) Rail and River District Economic Redevelopment.

- (1) The changes that have occurred in portions of Greater Downtown such as completion of the Riverside Parkway and planning for the future development of Las Colonias Park have already had a positive influence on the River and Rail Districts. Many properties have been renovated or redeveloped, new uses are relocating to the area and property values are generally on the rise. The Greater Downtown Plan envisions this trend continuing and being enhanced by the following redevelopment concepts:
 - (i) Allow existing heavy industry to remain, taking advantage of rail spurs within the area.
 - (ii) Intensified commercial edge along the north side of the Riverside Parkway with opportunities for mixed use development.
 - (iii) New general commercial, retail and residential uses will provide activity at the edge of the park after business hours to create a safe park environment that gives "ownership" of the park to the adjacent local business owners and residents.

- (iv) New retail and commercial uses such as restaurants, shops and services along South 7th Street to serve the employees, recreational users and residents of the neighborhood.
 - (v) Commercial industrial uses bridge the existing industrial and the commercial corridors.
- (2) In addition, discussions with the Grand Junction Economic Partnership, Business Incubator, Manufacturers' Council and Chamber of Commerce during development of the Greater Downtown Plan brought to light many opportunities for the area, the majority of which is within the established Mesa County Enterprise Zone. The Greater Downtown Plan outlines goals, policies and strategies that can be used to further the economic (re)development of the Rail and River Districts.
- (i) Need for flex space for different types of small business – new to area or graduating from the business incubator. Opportunity for these businesses to serve employee base, residents and recreational users in the area.
 - (ii) Opportunity to develop additional incentives for redevelopment that has taken advantage of partnerships and/or assembled parcels of land totaling a minimum of one-half acre or more.
 - (iii) Allow for live-work opportunities.
 - (iv) Opportunity to develop partnerships.

(Ord. 4571, 3-20-13)

§ 36.16.090. Greater Downtown area parks.

- (a) The four downtown parks were included on the original town plat created by the Grand Junction Town Company in 1882. All four parks still exist as downtown open space. The City acquired Las Colonias Park from the State Parks Department in 1997. It encompasses the mill tailings clean-up site.
- (1) Hawthorne Park – 400 Gunnison Avenue.
- (i) New restroom/shelter constructed 2010;
 - (ii) South picnic shelter requires new roof; scheduled 2013;
 - (iii) Playground surfacing replaced in 2012;
 - (iv) Playground equipment on a five- to seven-year replacement schedule;
 - (v) Irrigation is good, 10- to 15-year replacement.



(2) Washington Park – 10th Street and Gunnison Avenue.

- (i) No restrooms or shelter at this park;
- (ii) Playground equipment scheduled for replacement in 2013;
- (iii) Playground surface scheduled for replacement every two years;
- (iv) Irrigation will be replaced every five to seven years.

(3) Whitman Park – 5th Street and Pitkin Avenue.

- (i) Restroom scheduled for replacement in 2013;
- (ii) Development of a new master plan for the park recommended (see potential concepts in subsection **(b)** of this section).

(4) Emerson Park – 9th Street and Pitkin Avenue.

- (i) Restroom scheduled for replacement in 2013;
- (ii) Playground area for ages two to five is good;
- (iii) Playground for ages five to 15 scheduled for replacement in five to seven years;

- (iv) Playground surfacing replaced in 2012;
 - (v) Development of a new master plan for the park recommended (see potential concepts in subsection **(b)** of this section).
- (5) Las Colonias Park – Struthers Avenue and 7th Street.
- (i) A master planning process for the park is anticipated to occur in 2013;
 - (ii) Some amenities being considered are parking, restrooms, shelters, play area, open space, expanded walk/bike trails, open space for festivals, music, and educational presentations, various wetlands and arbor education opportunities, kayak park, tree walk and a disc golf course.



(b) Potential Concepts for Whitman and Emerson Parks.

- (1) Provide for a diversity of uses to create a higher level of utilization.
 - (2) Minimize the impact of adjacent streets and automobile traffic.
 - (3) Activate the edges of the parks with mixed use.
 - (4) Contemplate programmed, active use.
 - (5) Redevelop as more active, flexible urban open space rather than as passive, green parks.
- (Ord. 4571, 3-20-13)

§ 36.16.100. City-owned Jarvis property.

The City completed an initial planning analysis for the City-owned property on the west side of 5th Street between the Colorado River and the Riverside Parkway known as the Jarvis Property. The property is constrained by natural features and the encroachment of the Parkway, but does have approximately 43 acres of developable land.

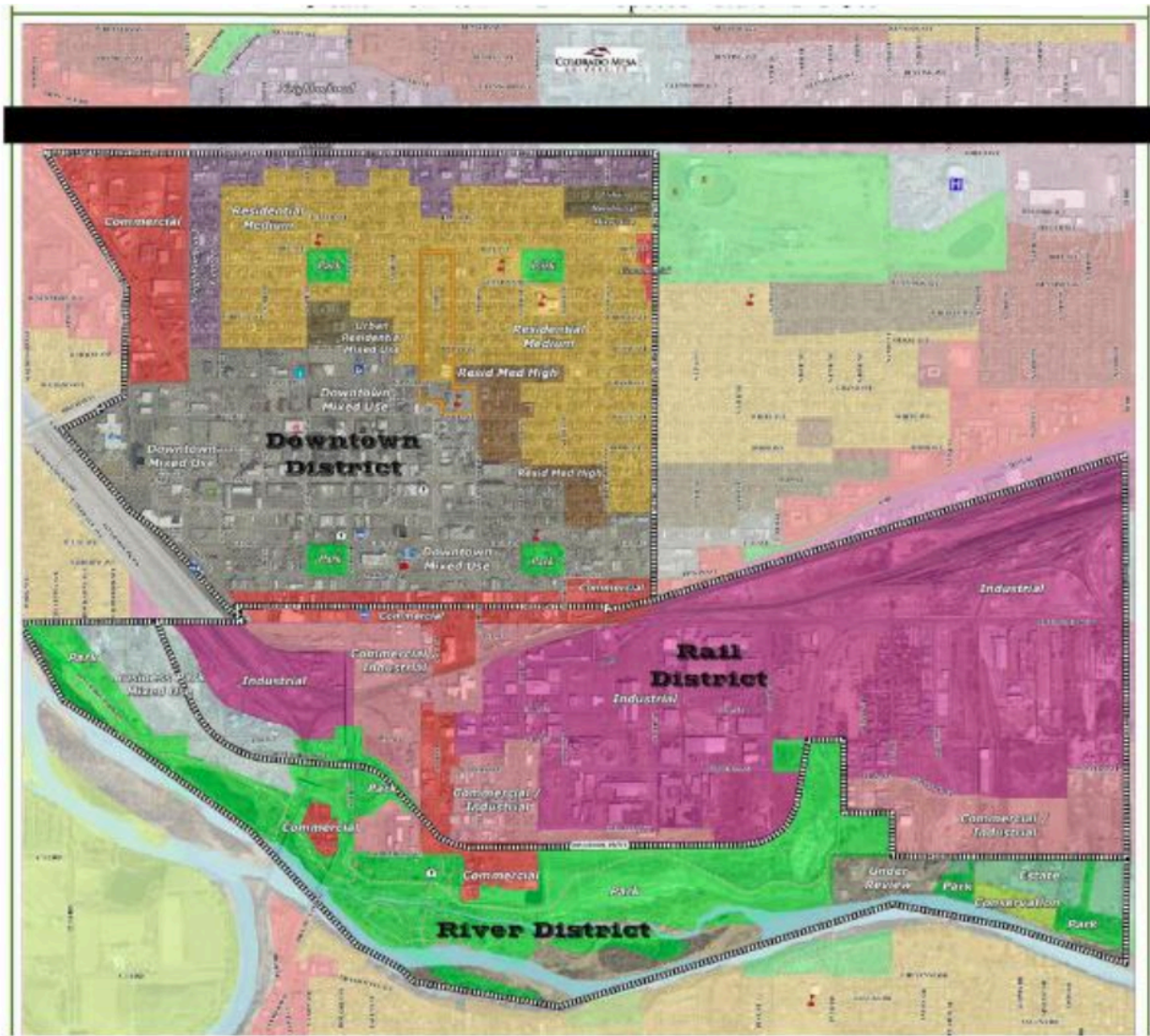
The initial study was to chart a direction for revitalization of the property. It summarized the key assets, identified some important issues and potential impediments to development, analyzed current market conditions and outlined a concept for organizing potential development of the property. The property and potential project are viewed as a unique opportunity for the City to chart the future of a rare property type, a place where it may be possible to provide a mix of uses, including residences, along the bank of the Colorado River. The major opportunities and constraints identified for potential development of the Jarvis Property were to: continue the riverfront greenway through the property, restore habitat, relocate the high voltage power facilities that run through the site, and provide flood protection for the property.

(Ord. 4571, 3-20-13)

Chapter 36.20. Appendices

§ 36.20.010. Future Land Use Map.

Appendix A – Future Land Use Map



Greater Downtown Future Land Use Categories

Description

- Downtown Mixed Use (DTMU) (24+ DU/Acre)
- Neighborhood Center-Mix Use (NCMU) (8 DU/Acre)
- Business Park Mixed Use (BPMU) (8 DU/Acre)

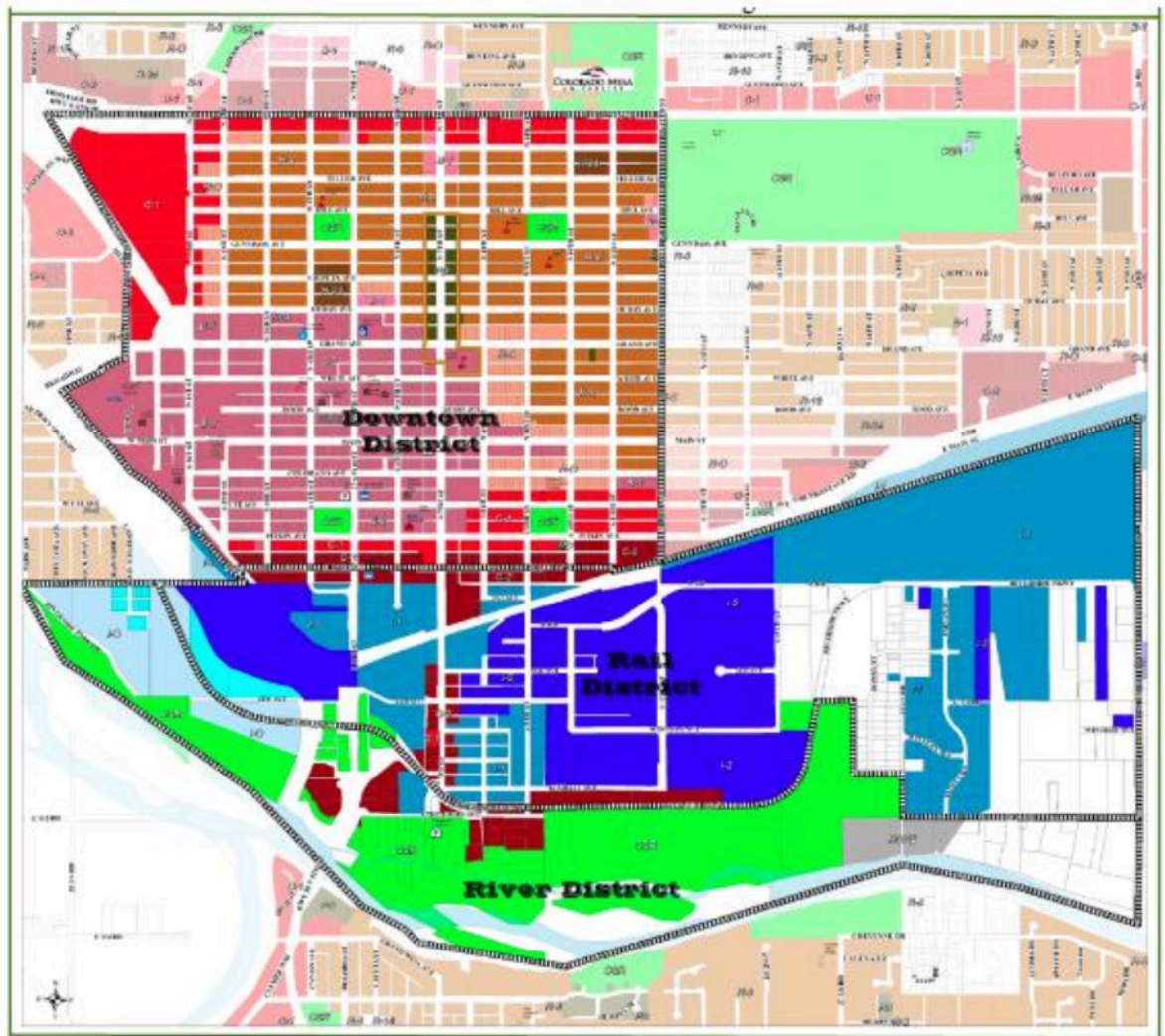
- Residential Medium (RM) (4 - 8 DU/Acre)
- Residential Medium High (RMH) (8 - 16 DU/Acre)
- Urban Residential Mix Use (URMU) (24+ DU/Acre)
- Park (PK)

- Conservation/Mineral Extraction (CDN) (1 DU/5 Acres)
- Commercial (COM)
- Commercial Industrial (CI)
- Industrial (IND)

(Ord. 4571, 3-20-13)

§ 36.20.020. Zoning Map.

Appendix B – Zoning Map



Greater Downtown Zoning Categories

R-8 (8-8 units/acre)	R-O (Residential Office)	DP (Business Park)	I-O (Industrial Office Park)	CGR (Community Services & Rec.)
R-24 (16-24 units/acre)	B-1 (Neighborhood Business)	C-1 (Light Commercial)	I-1 (Light Industrial)	NONE (Zoning In Process)
PD (Planned Development)	B-2 (Downtown Business)	C-2 (General Commercial)	I-2 (General Industrial)	

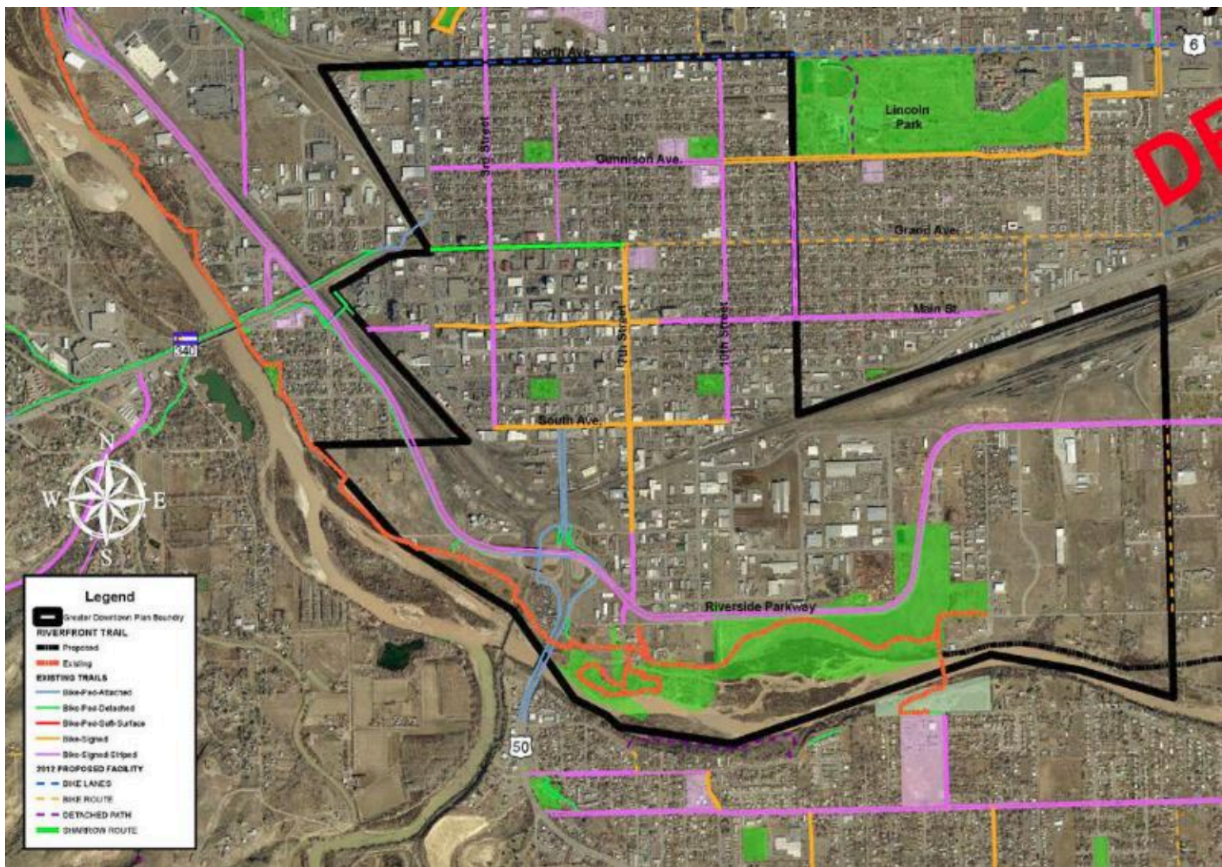
(Ord. 4571, 3-20-13)

§ 36.20.030. Conceptual plans for traffic circulation and trails.

(a) Conceptual Traffic Circulation Plan.



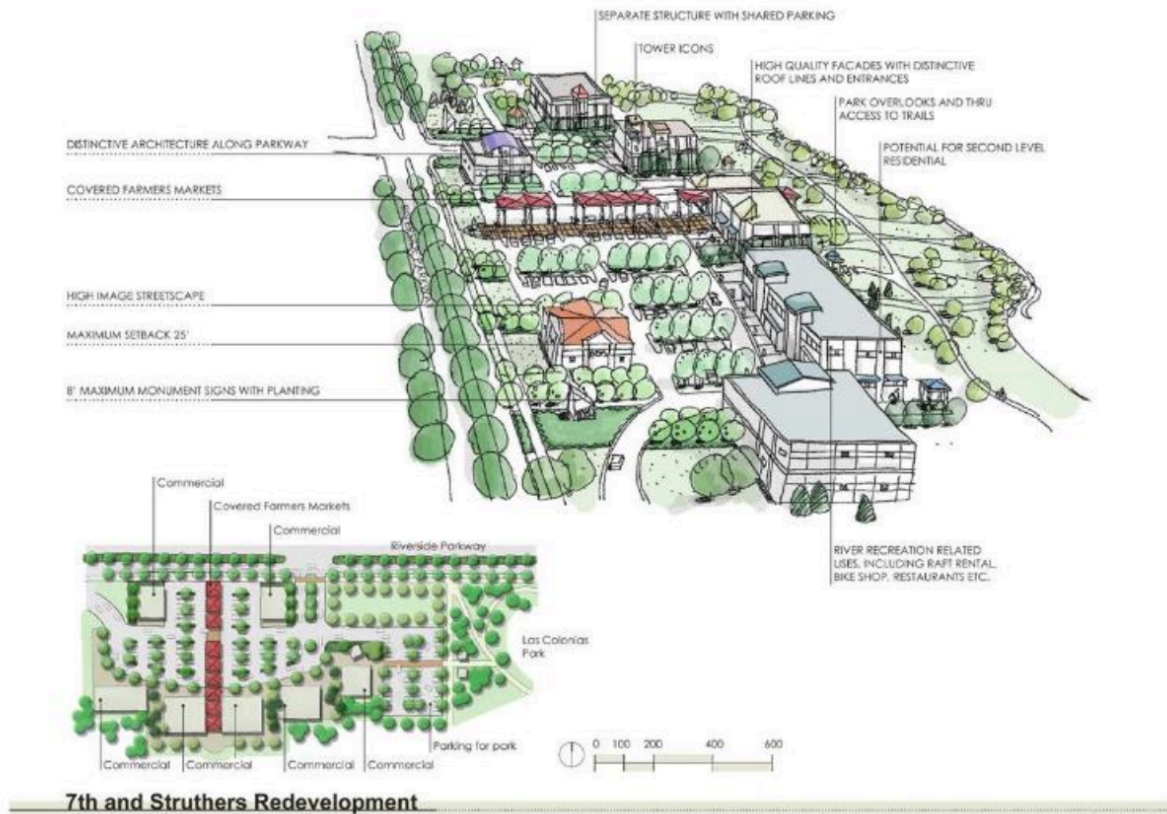
(b) Conceptual Trails Plan.



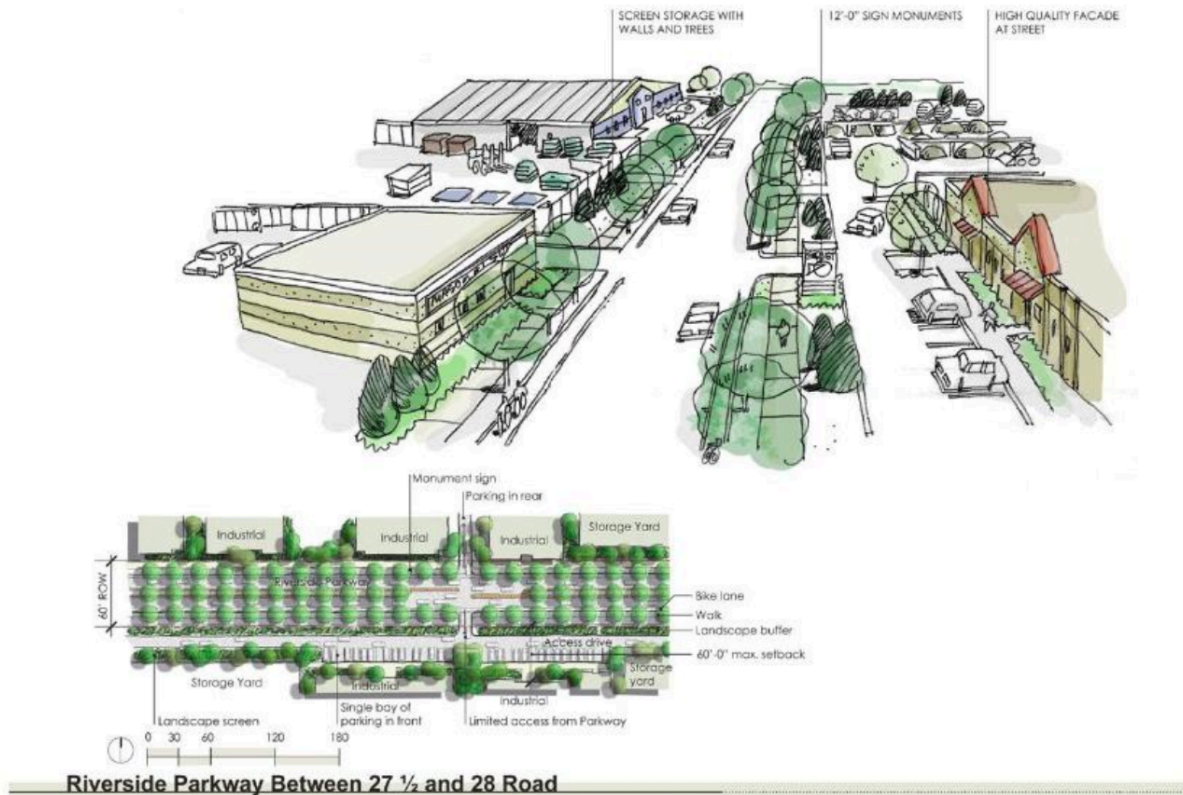
(Ord. 4571, 3-20-13)

§ 36.20.040. Development concepts.

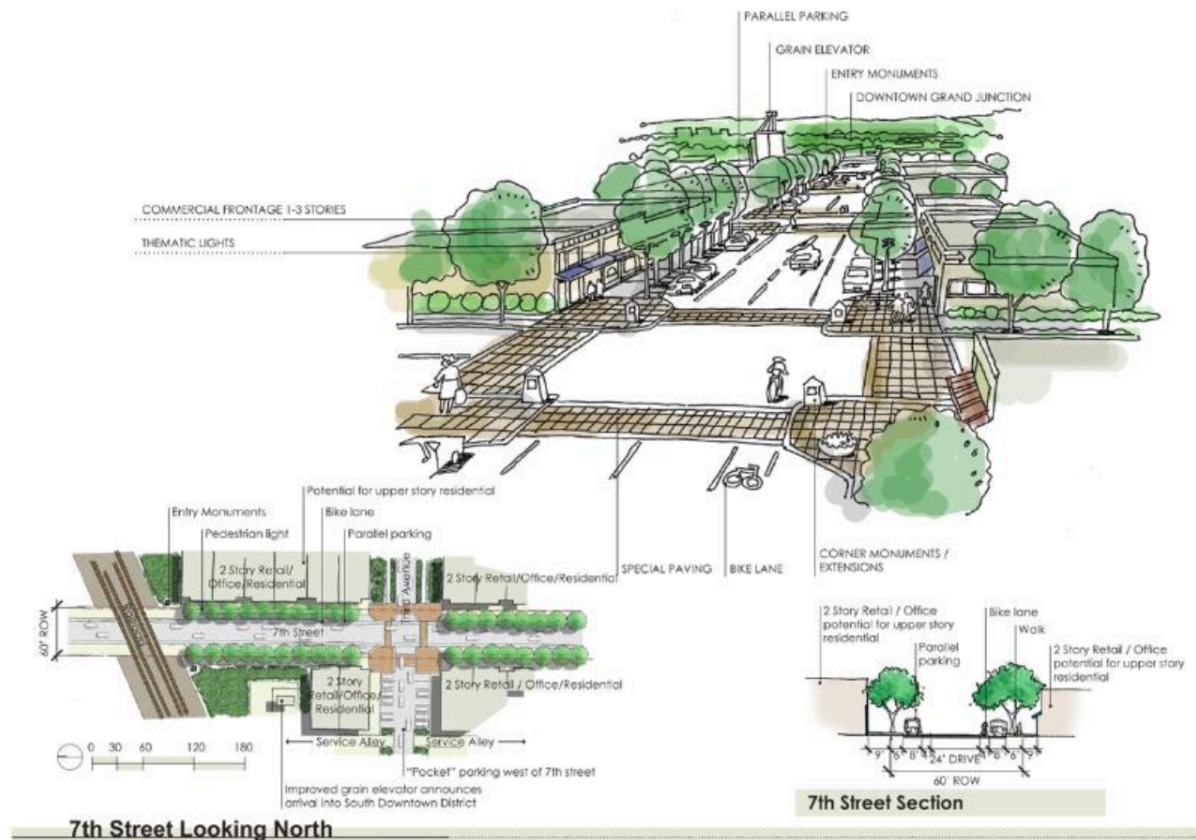
(a) 7th and Struthers Redevelopment.

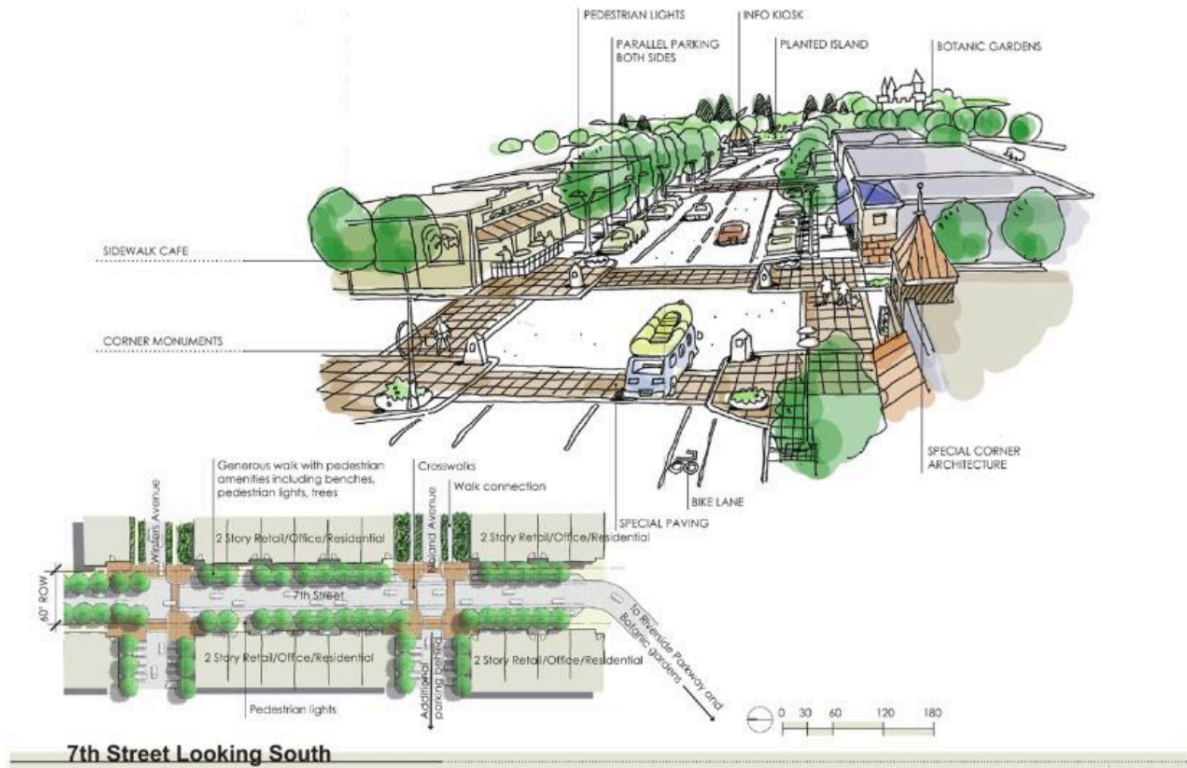


(b) Riverside Parkway Between 27-1/2 and 28 Road.

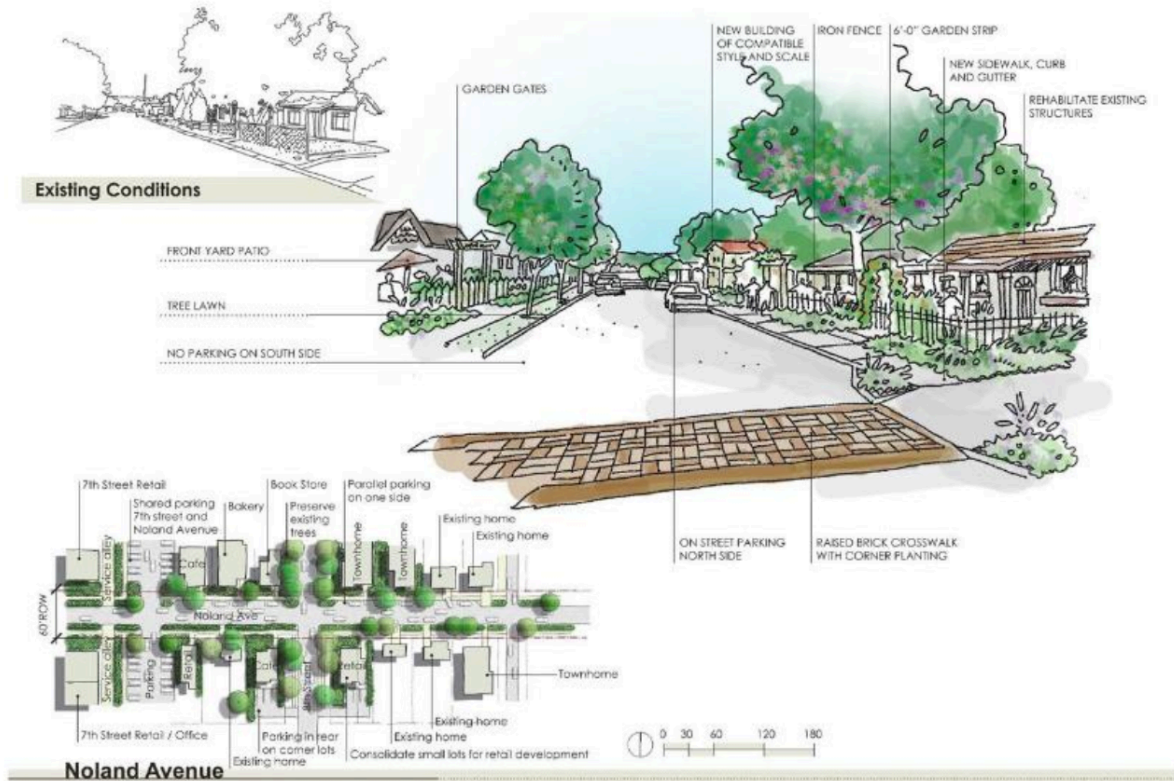


(c) Riverside Parkway Between 5th Street East and 12th Street.

(d) 7th Street Looking North.(e) 7th Street Looking South.



(f) Noland Avenue.



(Ord. 4571, 3-20-13)

Title 24. Greater Downtown Overlay

Chapter 24.04. Introduction

§ 24.04.010. Implementation – Standards and guidelines.

One of the first steps in implementing the Greater Downtown Plan is a zoning overlay. The overlay is intended to provide guidance and criteria for the planning, design and implementation of public and private improvements in the Greater Downtown area and is set forth in this document to be known as the Greater Downtown overlay district (zoning overlay). If properly administered and adhered to, the standards and guidelines should result in public and private development improvements (or a combination thereof) that achieve, as a minimum, a common level of quality in terms of site design, architectural design, landscaping and other site improvements.

The general purposes of the standards and guidelines are to support the overall goals of the Greater Downtown Plan.

- (a) Maintain and enhance the economic, cultural and social vitality of Greater Downtown. Promote downtown living by providing a wide range of housing opportunities in appropriate areas within Greater Downtown.
- (b) Enhance the transportation system to accommodate automobiles, bikes and pedestrians, and provide adequate, convenient parking.
- (c) Stabilize, preserve, protect and enhance the downtown residential neighborhoods.
- (d) Promote and protect the unique identity of the Greater Downtown area.

The standards and guidelines were developed upon an analysis of the existing character of the Greater Downtown area. The area was divided into subdistricts and the Downtown District was further divided into subareas based on existing zoning, character of existing development and potential for redevelopment opportunities. In addition, primary corridors were identified for which overlay guidance is created with the adoption of the document. The subdistricts, primary corridors and subareas are shown on Figures 1, 2 and 3.

These standards supplement other development regulations such as the City of Grand Junction Zoning and Development Code, which includes detailed criteria by zone district, planned development regulations, design and improvement standards, supplemental use regulations, sign regulations and the City Transportation and Engineering Design Standards (**TEDS**). In the instance the following standards are silent on a development concern, the existing regulations shall apply. None of the guidelines and standards within the zoning overlay is intended to apply to properties within the North Seventh Street Historic Residential District or the North Avenue Corridor Overlay since separate guidelines and standards have been established through overlays for those areas.

The standards identify design alternatives and specific design criteria for the visual character and physical treatment of private development and public improvements within Greater Downtown. They are adopted through an overlay zoning district, which will establish the means by which the standards are administered and enforced. The Director will make all decisions, and appeals and variance requests will be heard by the City of Grand Junction Planning Commission. The Downtown Development Authority

(DDA) will be a review agency for all applications and will make recommendations for proposals in the Central Business District.

Figure 1: Greater Downtown Plan Subdistricts

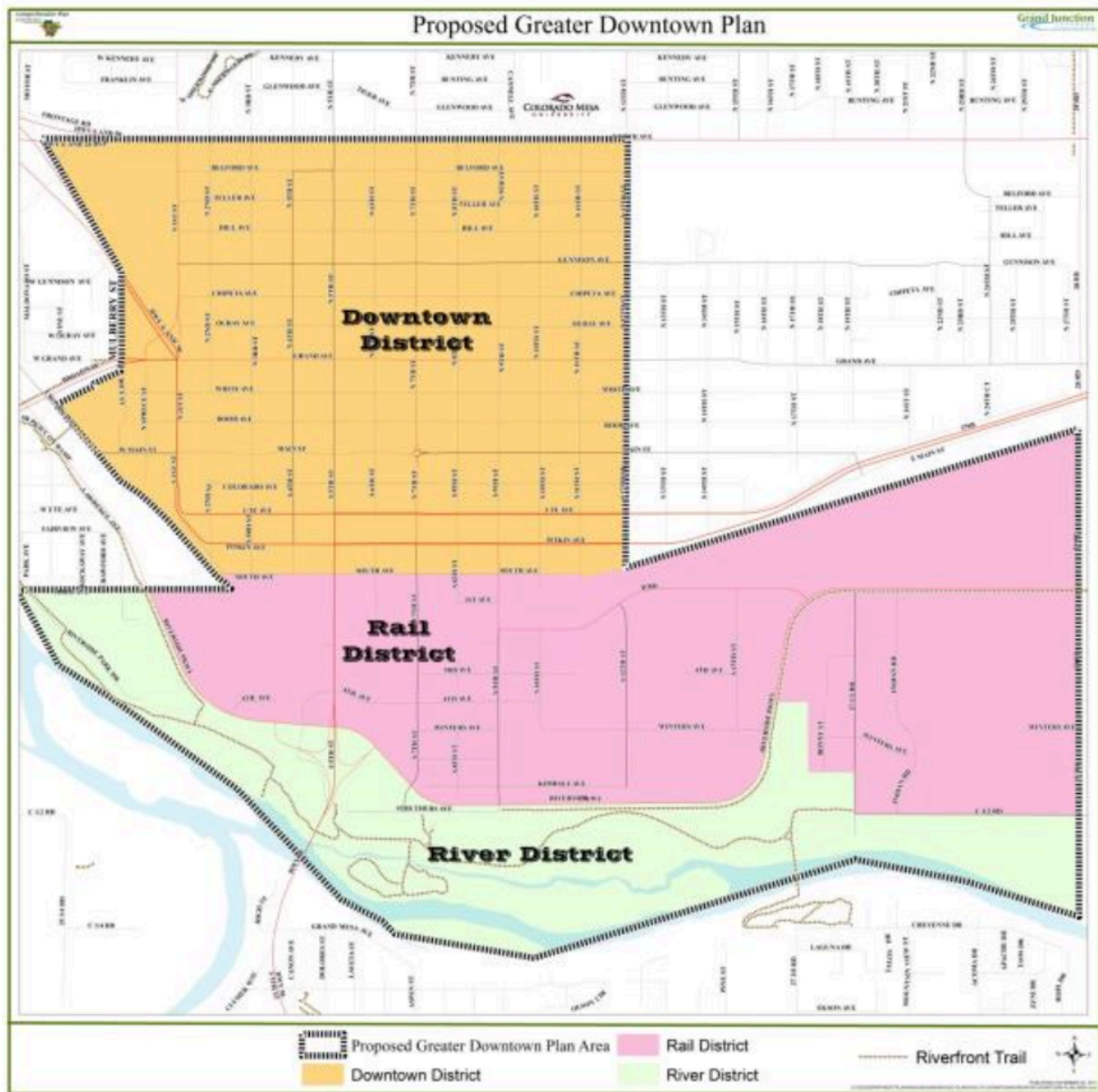


Figure 2

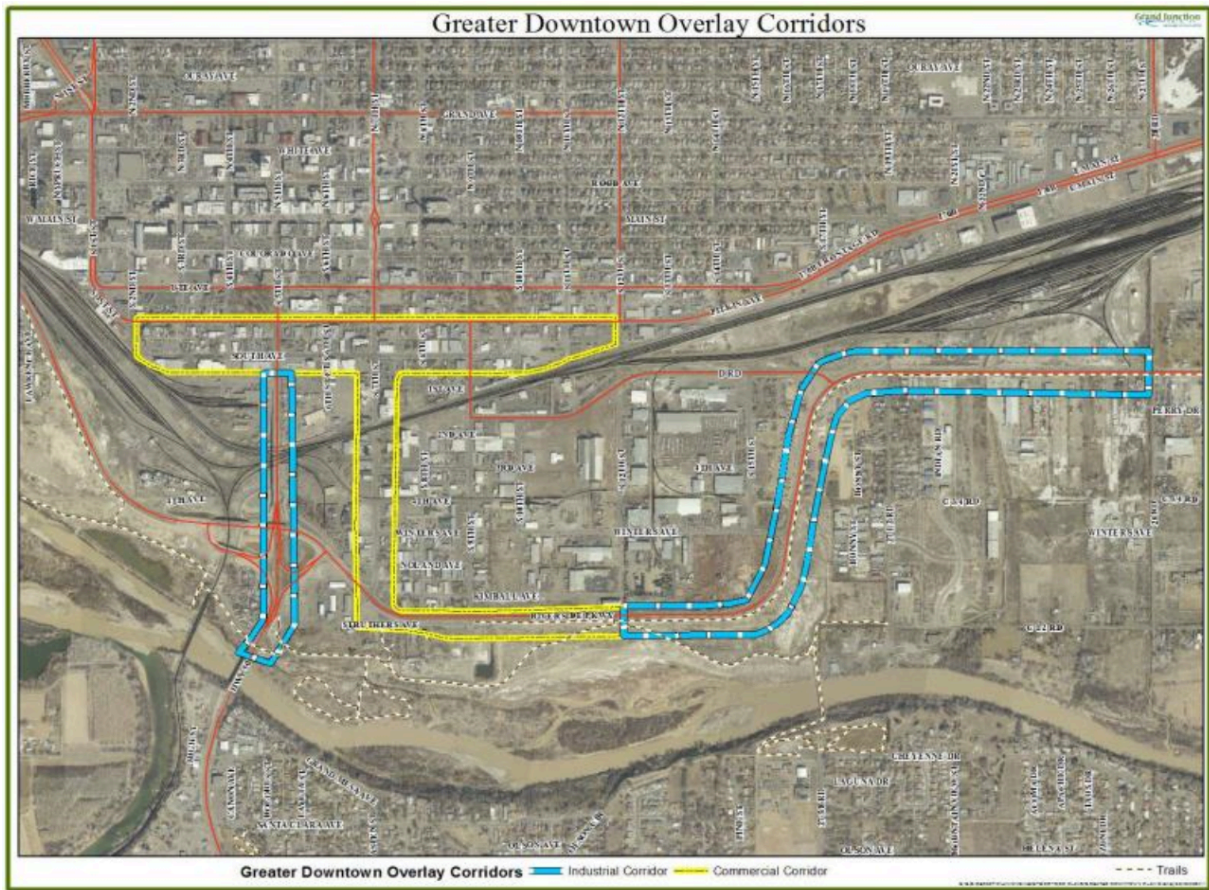
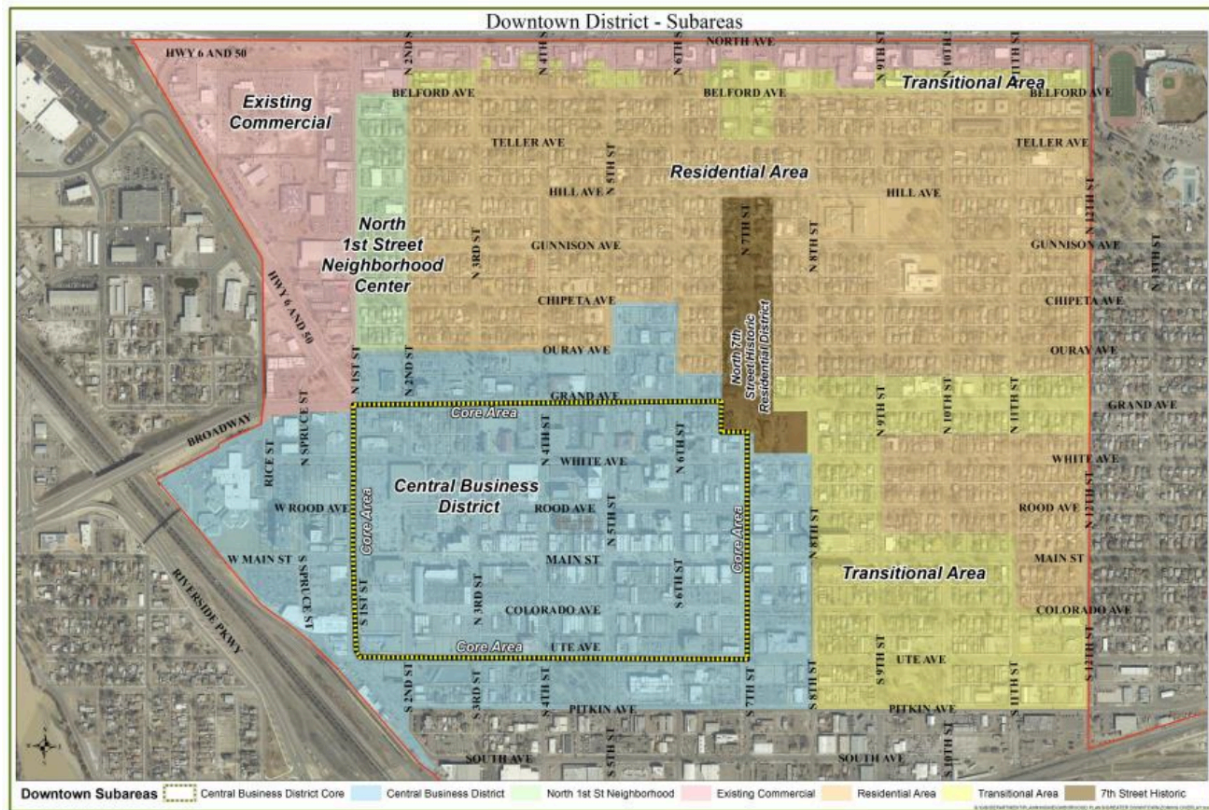


Figure 3



(Ord. 4572, 3-20-13)

Chapter 24.08. Corridor Standards and Guidelines

Article I. Overall Corridor Vision/Concepts

§ 24.08.010. Purpose.

The Greater Downtown overlay district standards for the Greater Downtown commercial and industrial corridors in the Rail and River Districts begin to implement goals of the Greater Downtown Plan to (i) improve the visual impact of development along the corridors; and (ii) promote higher quality architectural treatment and site design as new development and redevelopment occurs along the corridors. The visual features identified below define the concepts proposed along the corridors and, as implemented as new development and redevelopment occurs, will shape the desired character of the built environment. The features are grouped into three categories: architectural features; building materials; and streetscape and site design.

(Ord. 4572, 3-20-13)

§ 24.08.020. Architectural features.

- (a) Facade Modulation and Roofline Variations. The corridors can be improved through use of visually interesting architectural features that are designed to reduce mass and scale, including variation in the building form with recessed or projecting bays and variation in the rooflines.



- (b) Facade Design. Design details can be used to emphasize architectural features such as the modulation and roof line changes discussed above or other features such as entryways and windows. Facade design details that break up a facade and add visual interest include:
- (1) Columns and pilasters that help break up a horizontal plane of a building or other site feature.
 - (2) Change of material such as on an exterior wainscoting panel.
 - (3) Accent colors that help define and/or accentuate architectural features.
- (c) Defined Entry. Many of the other architectural features above can be used to accentuate and define the primary entrance to a building, add architectural interest, as well as make the building more pedestrian- or customer-friendly.
- (d) Window Sizes and Shapes. The shape and sizes of windows used on a building break up the facade, provide visual variety and provide a pedestrian- and customer-friendly character.
- (e) Awnings and Porticos. Awnings and porticos are encouraged to help provide architectural interest.

(Ord. 4572, 3-20-13)

§ 24.08.030. Building materials.

- (a) Mix of Materials. Variations of materials used on exterior facades break up large building forms and walls.



- (b) Different Textures, Colors and Tones. Use of different textures, colors and tones provide visual interest and can be used to accentuate architectural features.



(Ord. 4572, 3-20-13)

§ 24.08.040. Streetscape and site design.

Streetscape features along the primary corridors within the Rail and River Districts in Greater Downtown also help establish the visual character of the corridors. Examples of the desired characteristics are

illustrated below.

- (a) Building Placement. Buildings are encouraged to be brought forward on a site to the street or sidewalk.
- (b) Landscaping. Many of the streets within the Rail and River Districts do not have, nor are planned to have, a sidewalk. However, streetscape interest can be created through landscaping, encouraging use of xeriscape concepts.



Example Existing Streetscape Landscaping

- (c) Streetscape Design. An urban streetscape is desired that includes hard surfaces, tree grates and street trees. This design concept exists along some of the corridors in the Rail and River District. The urban streetscape along the street can be blended with the site design of the adjacent parcel so that on-site landscaping complements and enhances the existing streetscape.



Existing 7th Street Landscaping

- (d) Detached Sidewalks. Where possible, detached sidewalks are desired along the commercial and industrial overlay corridors and already exist along some streets. The park strip between the curb and sidewalk can provide the landscape relief for an abutting developed property.



Existing Detached Sidewalk on Riverside Parkway (left) and South Avenue (right)

- (e) Outdoor Storage, Display and Operations. Minimize the visual impact of outdoor storage, display and operations areas through placement on a site behind a building, to the rear of the property or otherwise screened.



*Examples of Outdoor Storage and Operations
Located Behind Buildings on 7th Street (left) and
Riverside Parkway (right)*



Screening Example on Riverside Parkway

(Ord. 4572, 3-20-13)

Article II. Commercial Corridor Standards and Guidelines

§ 24.08.050. Application of standards and guidelines.

The following Commercial Corridor standards and guidelines apply to those parcels on the south side of Pitkin Avenue between 2nd Street and 12th Street as shown in Figure 4: all parcels, or aggregation of parcels to be developed that have frontage on: (i) the east side of 2nd street between Pitkin Avenue and South Avenue; (ii) South Avenue between 2nd Street and 12th Street; (iii) South 7th Street between Pitkin Avenue and the south side of the Riverside Parkway; (iv) Struthers Avenue and Riverside Parkway area from just west of South 7th Street to the alignment of 12th Street; and all parcels between South 7th Street and 12th Street within 100 feet immediately north and south of the centerline of the Riverside Parkway. This area shall be referred to hereafter as the Commercial Corridor.



Figure 4

(Ord. 4572, 3-20-13)

§ 24.08.060. New site development or redevelopment – Standards.

- (a) Outdoor storage and permanent display areas shall only be allowed in the rear half of the lot, beside and/or behind the principal structure. For properties with more than one street frontage, the front of the lot shall be considered, for purposes of this standard, to be the side abutting the higher or highest order street. Portable display of retail merchandise may be permitted subject to the provisions of the Zoning and Development Code.
- (b) In order to prevent parking from dominating the visual setting of the Commercial Corridor, front yards shall allow only principal structures, landscaping, sidewalks, driveway access to parking areas and signage. No parking shall be allowed in the front yard. All parking that is accessory to a principal use shall be located behind or to the side of the building.
- (c) Front yard setbacks for principal structures on parcels within the Commercial Corridor are reduced to zero feet.
- (d) Residential uses are allowed as allowed in the Mixed-Use Light Commercial (MU-2) Zone District in the Zoning and Development Code, regardless of how the property is zoned.

(Ord. No. 5207, 3/20/2024; Ord. 4572, 3-20-13)

§ 24.08.070. Architectural design of new buildings/additions or substantial exterior remodel.

- (a) Policy. For construction of new buildings and additions, or substantial exterior remodel (see definition under subsection **(b)(2)** of this section), any facade(s) on all buildings that face a street within the Commercial Corridor shall be designed to relate directly to and reinforce the pedestrian scale and quality of the abutting streets, civic and open spaces.

(b) Standards.

- (1) Any facade(s) of a new building, addition or substantial exterior remodel (see definition under subsection **(b)(2)** of this section) that face a street within the Commercial Corridor or that face the public property containing the Riverfront Trail, shall have visually interesting architectural features and patterns that are designed to reduce mass and scale and reflect the desired vision for the corridors as described in GJMC § **24.08.010** through **24.08.040**. In order to do so, the facade(s) of a new building, addition or substantial remodel that face a street within the Commercial Corridor shall exhibit a minimum of three of the following nine architectural design elements:
 - (i) Variation in materials, material modules, expressed joints and details, surface relief and texture to break up building forms and wall surfaces. Such detailing may include sills, headers, belt courses, reveals, pilasters, window bays or similar features.
 - (ii) Facade articulation/variation such as recessed or projecting bays or pilaster/column projections a minimum of every 30 feet.
 - (iii) Variation in color.
 - (iv) Facade feature that emphasizes the primary building entrance through projecting or recessed forms, detail, color and/or materials.
 - (v) Variation in roof lines/roof materials in order to add interest to and reduce the scale of buildings or expanses of blank wall. This can be accomplished through design elements such as overhangs/eaves, recesses/projections, raised cornice parapets over doors or bays and peaked roof forms.
 - (vi) Screening of mechanical equipment either located on the roof or on the ground.
 - (vii) Windows.
 - (viii) Window or entry awning.
 - (ix) Other architectural features that achieve the goals of the overall corridor vision/concept as outlined in GJMC § **24.08.010** through **24.08.040** as determined by the Director.
- (2) Definition of Substantial Exterior Remodel. Exterior building alteration that is greater than or equal to 65 percent of the value of the existing site and building. This increase shall trigger 100 percent site development compliance and 100 percent architectural standards compliance. Value shall be the greater of: total actual value per the Mesa County Assessor; or a current appraisal.

(c) Guidelines.

- (1) Exterior building materials should be durable, economically maintained, and of a quality that will retain their appearance over time.

(Ord. 4572, 3-20-13)

§ 24.08.080. Landscaping for new site development or redevelopment – Standards.

On-site landscaping shall be required per the Zoning and Development Code. The total amount of required landscaping may be reduced by the Director if a minimum of two of the following five conditions exist or are proposed:

- (a) A higher quality of architectural design is achieved with greater than three of the architectural design elements listed in GJMC § **24.08.070**.
 - (b) Fifty percent or more of the building facade is set back no more than 15 feet.
 - (c) Street trees and other finished landscaping or hardscape exists within the public street immediately in front of the property.
 - (d) All parking is located to the side and/or rear of the building.
 - (e) All outdoor storage and operations are located behind the building.
- (Ord. 4572, 3-20-13)

§ 24.08.090. Signage – Standards.

- (a) Only flush wall and monument style signs are allowed on the parcels that have frontage on 7th Street, Struthers Avenue, and/or Riverside Parkway within the Commercial Corridor. Flush wall signs are allowed in accordance with the Zoning and Development Code. Monument signs shall be a maximum of 12 feet in height with a maximum total of 100 square feet per sign face allowed per parcel. Illumination shall comply with the Zoning and Development Code.
 - (b) Off-premises signs and billboards as defined by the Zoning and Development Code that did not exist as of April 21, 2013, are not allowed on properties within the Commercial Corridor.
- (Ord. 4572, 3-20-13)

Article III. Industrial Corridor Standards and Guidelines

§ 24.08.100. Application of standards and guidelines.

The following Industrial Corridor standards and guidelines apply to all properties or aggregation of parcels to be developed with frontage on 5th Street or the 5th Street/Riverside Parkway intersection right-of-way, south of South Avenue to the river, referred to hereafter as the 5th Street Industrial Corridor (Figure 5), and on Riverside Parkway from the east edge of 12th Street, extended along the Riverside Parkway, east to 28 Road, referred to hereafter as the Riverside Parkway Industrial Corridor (Figure 6). Please see the maps below. Collectively, the two shall be referred to as the Industrial Corridors.

(Ord. 4572, 3-20-13)

§ 24.08.110. 5th Street industrial corridor standards.



Figure 5

- (a) No new outdoor uses and operations shall be allowed on parcels that have frontage on 5th Street or on the right-of-way for the 5th Street/Riverside Parkway interchange. Maximum expansion and construction of any outdoor uses and operations (such as outdoor operation of rail yards, salvage yards, etc.) on properties that abut the 5th Street right-of-way shall be 25 percent of the total square footage as it existed on April 21, 2013.
- (b) For uses that require screening per the Zoning and Development Code and will be visible from the elevated portions of the 5th Street viaduct, screening shall be provided on the viaduct that limits views to adjacent uses but still provides longer vistas to the east (Grand Mesa) and west (Colorado National Monument and Uncompahgre Plateau), in accordance with CDOT specifications as follows: 36 inches by three-eighths inches by 11 ga Core, 10 ga finish vinyl coated chain link. The City may accept payment in lieu of screening; the amount of the payment-in-lieu may be established by resolution of the City Council.
- (c) Off-premises signs and billboards as defined by the Zoning and Development Code that did not exist as of April 21, 2013, are not allowed on properties within the 5th Street Industrial Corridor. (Ord. 4572, 3-20-13)

§ 24.08.120. Riverside Parkway industrial corridor standards.



Figure 6

(a) New Site Development or Redevelopment.

- (1) Service entrances, service yards and loading areas shall be located only in the rear or side yard. For properties with more than one street frontage, the front yard shall be considered, for purposes of this standard, to be the side abutting the higher or highest order street. The rear and side yards are any other sides that do not meet the definition of a front yard. Along the Riverside Parkway right-of-way, a six-foot-high solid fence or wall of stone, wood or masonry shall screen each service yard or area from adjoining single-family residential zones and uses which are not separated by a street (not counting an alley or any easement).
- (2) Outdoor storage and permanent display areas shall be allowed per the zone district of the property. Any storage in the front yard adjacent to the Riverside Parkway right-of-way shall be screened with a six-foot-high solid architectural wall constructed of stone, masonry or combination thereof with a minimum 14-foot landscape buffer provided outside of the wall unless modified per the landscaping in subsection (c) of this section.
- (3) The front yard setback for principal structures on parcels within the Riverside Parkway Industrial Corridor is reduced to zero feet.

(b) Architectural Design of New Buildings or Substantial Exterior Remodel.

- (1) Any side of a new building, addition or substantial remodel that faces the Riverside Parkway or the public property containing the Riverfront Trail shall exhibit a minimum of three of the following eight architectural design elements:
 - (i) Variation in materials.
 - (ii) Facade modulation/articulation a minimum of every 30 feet.
 - (iii) Variation in color.
 - (iv) Facade feature that emphasizes the primary building entrance.

(v) Variation in roofline.

(vi) Windows.

(vii) Window or entry awning.

(viii) Other architectural features that achieve the goals of the overall corridor vision/concept as outlined in GJMC § **24.08.010** through **24.08.040** as determined by the Director.

(c) Landscaping for New Site Development or Redevelopment.

(1) On-site landscaping shall be required per the Zoning and Development Code. The total amount of required landscaping may be reduced by the Director if a minimum of two of the following five conditions exist or are proposed:

(i) A higher quality of architectural design is achieved with greater than three of the architectural design elements listed in subsection **(b)** of this section.

(ii) Fifty percent or more of the building facade has no more than a 20-foot setback.

(iii) Street trees exist within the abutting public street.

(iv) All parking is located to the side and/or rear of the building.

(v) All outdoor storage and operations are located behind the building.

(d) Signage.

(1) Only flush wall and monument style signs are allowed on the Riverside Parkway Industrial Corridor properties. Flush wall signs are allowed per the Zoning and Development Code. Monument signs shall be a maximum of 12 feet in height with a maximum total of 100 square feet per sign face allowed per parcel.

(2) Off-premises signs and billboards as defined by the Zoning and Development Code that did not exist as of April 21, 2013, are not allowed on properties within the Riverside Parkway Industrial Corridor.

(Ord. 4572, 3-20-13)

Chapter 24.12. Downtown District Standards and Guidelines

Article I. Overall Downtown District Vision/Concepts

§ 24.12.010. Purpose.

In order to implement the Greater Downtown Plan, the following zoning overlay standards have been developed for the subareas of the Downtown District. Application of the standards and guidelines will begin to implement goals of the plan to:

(a) Maintain and enhance the economic, cultural and social vitality of the Downtown District.

(b) Promote downtown living by providing a wide range of housing opportunities.

(c) Enhance the transportation system to accommodate automobiles, transit, bikes and pedestrians, and provide adequate, convenient parking.

(d) Stabilize and enhance the historic residential neighborhoods.

(e) Establish and promote a unique identity for each of the subareas of the Downtown District.

(f) Preserve and restore significant historic structures.

- (g) Activate the edges of the downtown parks with mixed use and programmed/active use of the parks as urban open space rather than passive green parks.

(Ord. 4572, 3-20-13)

Article II. Downtown District Area-Wide Policies, Standards and Guidelines

§ 24.12.020. Policies.

- (a) Maintain and enhance the economic, cultural and social vitality of downtown.
- (b) Promote downtown living by providing a wide range housing opportunities.
- (c) Enhance the transportation system to accommodate automobiles, bikes and pedestrians and provide adequate, convenient parking.
- (d) Stabilize and enhance the historic residential neighborhoods.
- (e) Establish and promote a unique identity.
- (f) Preserve and restore significant historic structures.
- (g) Activate the edges of the downtown parks with mixed use and programmed/active use of the park as urban open space rather than passive green parks.

(Ord. 4572, 3-20-13)

§ 24.12.030. Standards.

Due to the constraints of many downtown properties and the City's desire to promote improvement and redevelopment in the Downtown District, the Director may make reasonable exceptions to the provisions of the Zoning and Development Code and the Greater Downtown Overlay District to bulk standards (except for building height), landscaping, parking or other use-specific special regulations for properties that are within the Central Business District (CBD), Transitional or Residential subareas. The following criteria shall be used to consider exceptions from the bulk standards, landscaping, parking or other use-specific special regulations:

- (a) Hardship Unique to Property, Not Self-Inflicted. There are exceptional conditions creating an undue hardship, applicable only to the property involved or the intended use thereof, which do not apply generally to the other land areas or uses within similar zone districts, and such exceptional conditions or undue hardship was not created by the action or inaction of the applicant or owner of the property;
- (b) Special Privilege. The exception shall not confer on the applicant any special privilege that is denied to other lands or structures within similar zone districts;
- (c) Literal Interpretation. The literal interpretation of the provisions of the regulations would deprive the applicant of rights commonly enjoyed by other properties in similar zoning districts and would work unnecessary and undue hardship on the applicant;
- (d) Greater Downtown Plan Goals. The proposal actually meets overall goals of the plan better than if standards are followed;
- (e) Conformance with the Purposes of the Zoning Overlay and the Zoning and Development Code. The granting of an exception shall not conflict with the purposes and intents expressed or implied in this Zoning Overlay District or the Zoning and Development Code; and

- (f) Conformance with Comprehensive Plan. The granting of an exception shall not conflict with the goals and principles in the City's Comprehensive Plan.

(Ord. 4572, 3-20-13)

§ 24.12.040. Guidelines.

- (a) Projects will include good, interconnected transportation choices for better access and better health.
- (b) Traffic calming measures will be provided where appropriate, including pedestrian refuge areas, medians, landscaping and corner bulb-outs.

(Ord. 4572, 3-20-13)

Article III. Central Business District (CBD) Standards and Guidelines

§ 24.12.050. Applicability.

The following standards and guidelines apply to the CBD area shown in Figure 7 (blue area). Further development and implementation of these concepts will be done in coordination with the DDA. The standards and guidelines are intended to apply to new development or redevelopment within the area.

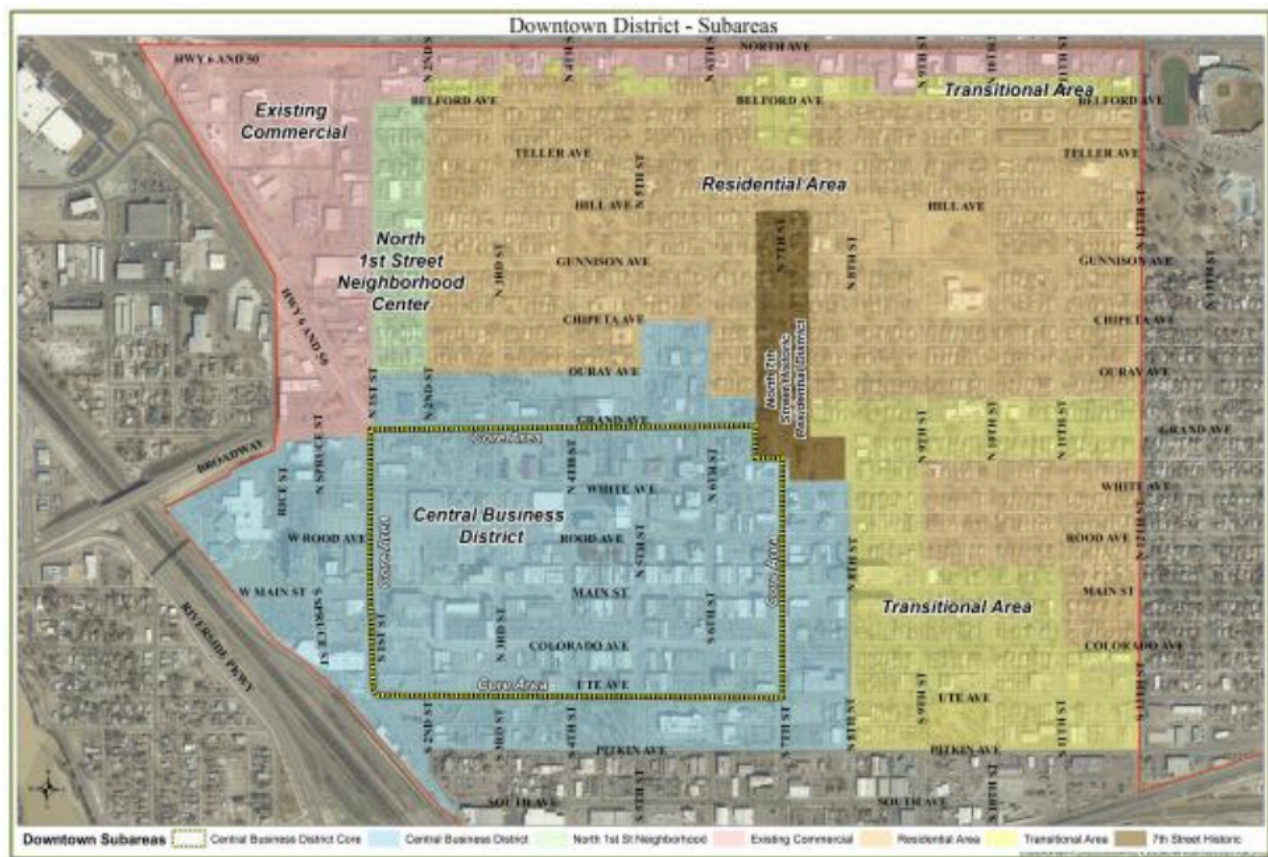


Figure 7

(Ord. 4572, 3-20-13)

§ 24.12.060. Application of standards and guidelines.

(a) Unless otherwise noted below, the standards and guidelines shall apply under the following conditions:

- (1) Construction of a new building.
- (2) Addition. Construction of an addition to an existing building if the addition increases total building square footage by 100 percent or greater (baseline is building square footage of existing building on April 21, 2013). This expansion standard is cumulative after this date in 2013. Once square footage has exceeded 100 percent of the 2013 square footage, the standards and guidelines shall apply to all further expansions.
- (3) Substantial Exterior Remodel. Exterior building alteration that is greater than or equal to 65 percent of the value of the existing site and building. This increase shall trigger 100 percent site development compliance and 100 percent architectural standards compliance. This shall not trigger the requirement of minimum height. Value shall be the total actual value per the Mesa County Assessor or the appraised value based on an appraisal completed by a certified appraiser licensed to do business in the State of Colorado utilizing the "cost" approach.

(b) The standards and guidelines do not apply to:

- (1) Construction of an addition to an existing building if the addition will increase the total building square footage by less than 100 percent (baseline is building square footage of existing building on April 21, 2013). This expansion standard is cumulative after this date in 2013. Once square footage has exceeded 100 percent of the 2013 square footage, the standards and guidelines shall apply to all further expansions.
- (2) Exterior building alteration that does not exceed 65 percent of the value of the existing site and building (value determined as set forth above).
- (3) Interior remodel of an existing building.
- (4) This zoning overlay does not prohibit rezoning to a form-based zone within the Downtown District and this zoning overlay shall not apply to properties within a form-based zone within the Downtown District.

(Ord. 4572, 3-20-13)

§ 24.12.070. Overall Central Business District (CBD) vision/concepts/policies.

- (a) Activate the Downtown Core area streets through emphasis on higher pedestrian traffic, businesses on the ground level that are oriented towards attracting higher pedestrian volumes, and buildings that "turn the corner" (invite activity on both the primary and cross streets). Refer to the CBD Core Area map in Figure 8.



Existing Downtown Street Activity

- (b) Maintain the prominence of buildings along the streets by minimizing building setbacks.
- (c) Encourage high quality, compatible design for all new buildings and establish a cohesive architectural character/theme that harmonizes new structures with the existing buildings through common materials, scale and basic architectural details as outlined in greater detail in the following standards and guidelines.
- (d) Typical building materials found in the CBD materials are traditional and weather well, allow a broad variety in appearance and ensure buildings are of high quality. To facilitate the creation of a cohesive architectural character/theme for new buildings, additions or exterior remodels in the Downtown Core, the following exterior finishes are most appropriate: brick, sandstone, stucco, metal cladding, tiles, wood, glazing and decorative concrete masonry units (CMU).



Example Downtown Building Materials

- (e) Encourage high density, mixed-use development and structures (e.g., retail at street level and residential or office above).



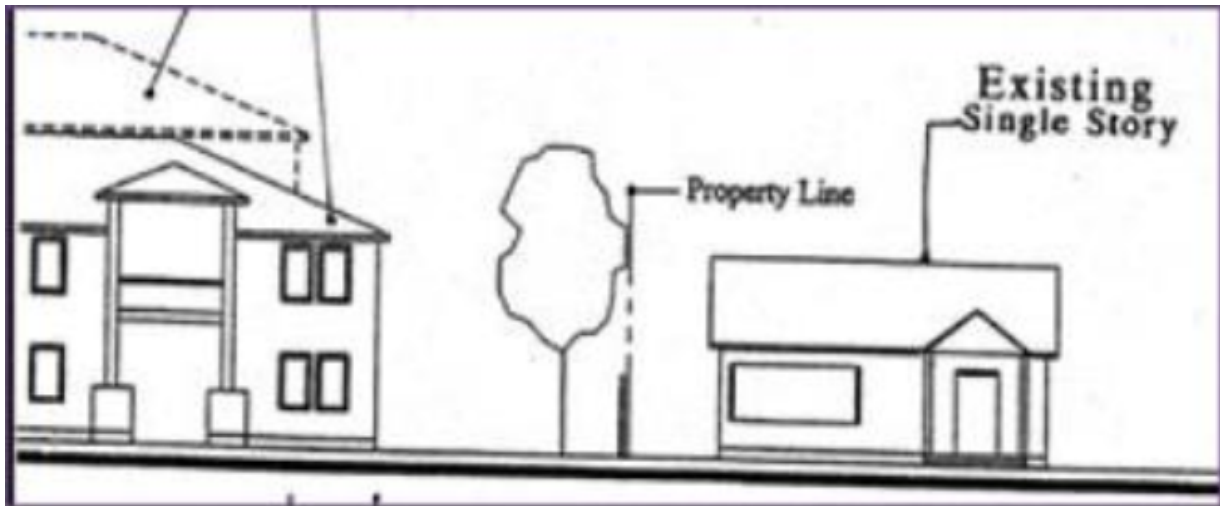
Existing Downtown Mixed Use

- (f) Encourage gradual scale transitions between the CBD and adjacent neighborhoods. Taller buildings will be located in the center and southern and western perimeter of the CBD, with shorter buildings on the northern and eastern edges of the CBD.
- (g) Encourage maximizing building scale and intensity/density by offering incentives to build above the required minimum height.
- (h) Minimize single use, surface parking throughout the CBD.
- (i) Maintain streetscapes dominated by buildings with parking located behind. Consider elimination of existing curb cuts as a performance benefit.
- (j) Encourage shared parking.
- (k) Encourage new infill development on existing, under-utilized surface parking lots.
- (l) Provide streetscape details and landscaping that compliment the architectural character of the CBD and exhibit urban character.
- (m) Create entries to the CBD at strategic locations as shown on the Wayfinding and Signage Map in the Greater Downtown Plan report. Enhancements may include landscaped medians, corner bulbs and special signs.
- (n) The DDA will assist in developing sign standards and guidelines for private signage placed on buildings or as freestanding signs for consideration and adoption by the City.
(Ord. 4572, 3-20-13)

§ 24.12.080. Overall Central Business District (CBD) standards.

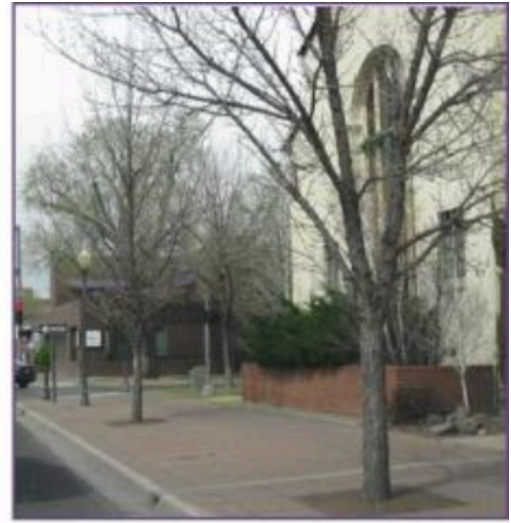
- (a) Maximum building height in the CBD shall be 90 feet. A one-time increase of up to 25 percent per property may be considered by the Grand Junction Planning Commission.

- (b) Buildings shall be set back a minimum of 20 feet from the street on Chipeta and Ouray Avenues.
- (c) The buildings will step down so that the front facade of the buildings that are directly across Ouray and Chipeta Avenues from residential buildings or uses are no taller than 40 feet. Minimum depth of the step shall be 10 feet.



Example of Building Stepping Down to Single-Family Residential Scale

- (d) Scale and massing of buildings or portions of buildings along Ouray and Chipeta Avenues will be compatible with residential scale.
- (e) If off-street parking is provided, it shall be located behind buildings on private property. If the property abuts an alley, the parking area shall take access from the alley. If the property has more than one street frontage, "behind the building" shall mean on the opposite side of the building from the front door or the main public door entrance to the building.
- (f) Maximize opportunities for on-street parking by minimizing curb cuts along the street.
- (g) All pedestrian level lighting shall be downlit and, if on poles, shall be in a historical style light standard.
- (h) The streetscape along Grand Avenue and 4th and 5th Streets north to Ouray Avenue within the CBD will continue in a design compatible with the existing improvements along Grand Avenue (e.g., decorative pavement and street trees).
- (i) Landscaping is expected to comply with the Zoning and Development Code per the requirements of the zone district. The Director may approve variations for new development or redevelopment in the CBD if: (a) street trees exist within the abutting public street; and (b) streetscape elements (plantings, low walls and/or street furniture) are proposed with the development consistent with the urban design character of the CBD.
- (j) The streetscape along 5th and 6th Streets north of Ouray Avenue to Chipeta Avenue within the CBD shall transition between the urban hardscape and a more residential streetscape character (e.g., detached sidewalk, landscaping in park strip between curb and sidewalk and street trees).

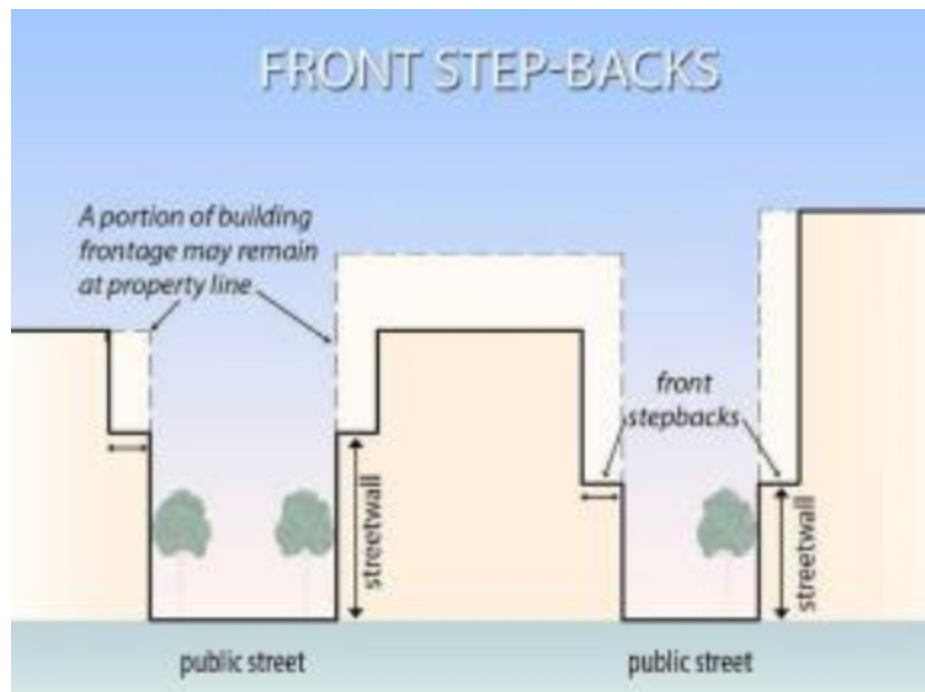


Existing Grand Avenue Streetscape

(Ord. 4572, 3-20-13)

§ 24.12.090. Central Business District Core Area (Downtown Core or Core Area) guidelines.

- (a) Facade detailing should be compatible with, but not be identical to, that of a neighboring historic building. New facades should have their own, unique design. To create continuity, horizontal lines should be in alignment with neighboring buildings.
- (b) Entrances are often the primary focal point of a building and, as such, should be designed to fit with the overall character of the area.
- (c) Doorways may be finished with paints, stains, metal and aluminum cladding set to match the existing trim colors.



- (d) Single, double, revolving and corner doorways are acceptable in new construction or substantial remodels.

- (e) On upper floors of street-facing facades on buildings taller than the traditional three stories in the CBD, consider stepping back the upper floors a minimum depth of 10 feet in order to enhance the traditional scale of the CBD and ensure adequate air and light at the sidewalk level.

(Ord. 4572, 3-20-13)

§ 24.12.100. Central Business District Core Area (Downtown Core or Core Area) standards.

The following standards apply only to the Downtown Core shown in Figure 8 (within the yellow-black dashed line).



Figure 8



Existing Downtown Core Scale

- (a) Building Height.

(1) Minimum building height in the Downtown Core shall be two stories.

The following uses as allowed in the zone district and as defined by the Zoning and Development Code shall be exempt from the minimum two-story requirement:

- (i) Schools, colleges and universities;
- (ii) Vocational, technical and trade schools;
- (iii) Community activity building;
- (iv) All other community service;
- (v) Museums, art galleries, opera houses, libraries;
- (vi) Day care;
- (vii) Detention facilities;
- (viii) Hospital/clinic;
- (ix) Parks and open space;
- (x) Religious assembly;
- (xi) Funeral homes/mortuaries/crematories;
- (xii) Safety services;
- (xiii) Utility infrastructure and corridors;
- (xiv) Car wash, gasoline service station, quick lube;
- (xv) Industrial services, contractors and trade shops with indoor operations and storage.

Exemptions to this requirement for other uses of land or occupancies of a building not listed and that is not conducive to a vertical organization of operational space may be considered and approved by the Grand Junction Planning Commission with input from the DDA upon a review of a conceptual level development proposal.

(2) The following criteria shall be used by the Planning Commission to consider exemptions from the bulk standards, landscaping, parking or other use-specific special regulations:

- (i) Hardship Unique to Property, Not Self-Inflicted. There are exceptional conditions creating an undue hardship, applicable only to the property involved or the intended use thereof, which do not apply generally to the other land areas or uses within similar zone districts, and such exceptional conditions or undue hardship was not created by the action or inaction of the applicant or owner of the property;
- (ii) Special Privilege. The exception shall not confer on the applicant any special privilege that is denied to other lands or structures within similar zone districts;
- (iii) Literal Interpretation. The literal interpretation of the provisions of the regulations would deprive the applicant of rights commonly enjoyed by other properties in similar zoning districts and would work unnecessary and undue hardship on the applicant;
- (iv) Greater Downtown Plan Goals. The proposal actually meets overall goals of the plan better than if standards are followed;
- (v) Conformance with the Purposes of the Zoning Overlay and the Zoning and Development Code. The granting of an exception shall not conflict with the purposes and intents expressed or implied in this zoning overlay or the Zoning and Development Code; and

(vi) Conformance with Comprehensive Plan. The granting of an exception shall not conflict with the goals and principles in the City's Comprehensive Plan.

(b) Building Setbacks/Site Placement, Scale, Massing and Street Encroachment.

- (1) Maximum building setback from the abutting street shall be two feet or compatible with the mean setback of the immediately adjoining lots on both sides of the subject lot but in no case greater than 20 feet.
- (2) When building setbacks are not determined as in subsection **(b)(1)** of this section, setbacks of up to 10 feet from the abutting street may be allowed if there is a designed function for the space such as limited outdoor display, seating, outdoor dining areas or a small street park, whether for private or public use.
- (3) Architectural features on stories above street level may encroach on the public right-of-way, provided all safety considerations have been met. A revocable permit for such encroachments shall be reviewed and may be approved by the Director or a permanent easement for such encroachments may be reviewed and considered for approval by City Council.
- (4) Awnings that overhang windows or entries on street level facades are encouraged and shall be constructed of canvas or heavy cloth or metal (no plastic), utilizing primarily neutral colors.



*Existing Canopy on
Main Street Building*

- (c) Architectural Character. The facade(s) of a new building, addition or substantial exterior remodel that abut the streets within the Downtown Core shall have articulated architectural features and patterns that are designed to reflect the desired vision for the CBD described in GJMC § **24.12.070**. In order to do so, the facade(s) of a new building, addition or substantial exterior remodel within the Downtown Core shall exhibit a minimum of four of the following nine architectural design elements:



*Example Ornamentation for
Visual Interest*

- (1) On corner parcels, facade design of ground floors “turn the corner” to induce activity and interest in the streetscape on the north-south streets within the Downtown Core.
- (2) Facades are articulated and have ornamentation such as varied brick patterns, change in material or color accents and window headers or columns that create shadow lines are examples of acceptable ornamentation.
- (3) The street level front facade of the building is “active” with at least 50 percent of the facade in windows, with doors spaced no more than 50 feet apart. Side facades shall meet a minimum of 50 percent of this requirement.
- (4) Facade features that emphasize the primary building entrance through projecting or recessed forms, detail, color or materials.
- (5) Building facades are articulated on any street-facing side. Larger buildings are articulated in a hierarchy of smaller volumes and masses that better relate to other buildings and the scale of streets. This is accomplished through establishment of building bays that are distinguished by recessed or protruding elements or a variation in materials or color to break up the facade and reduce the overall scale of large buildings.



Example Facade Articulation to Define Smaller Scale Bays

- (6) Higher levels of fenestration are required for buildings along both sides of the streets within the CBD Core Area. At a minimum, fenestration shall be concentrated on the street level facade and diminish on upper floors (e.g., window size decreases as the floor level increases).



Existing facade with Diminishing Fenestration on Upper Floors

- (7) Buildings include a facade cap. The cap is be defined by a distinct roof line or parapet. The design uses ornamentation of these features to enhance the building's identity and support the

architectural character of the CBD. The facade cap is in three dimensions that projects and casts a shadow and relates proportionately to the overall building design.

- (8) Building design minimizes the visual impact of mechanical equipment located on the roof as viewed from the adjacent streets.



Example Three Dimensional Facade Cap

- (9) Other architectural features that achieve the goals of the overall Central Business District (CBD) vision/concept as outlined in GJMC § **24.12.070** as determined by the Director.
- (d) On-street parking shall be located and designed to maintain and support a safe pedestrian environment on streets located within the Downtown Core (yellow-black outline in Figure 9). This includes coordinating crosswalks with parking location and eliminating visual and physical obstructions to the pedestrian travel way.

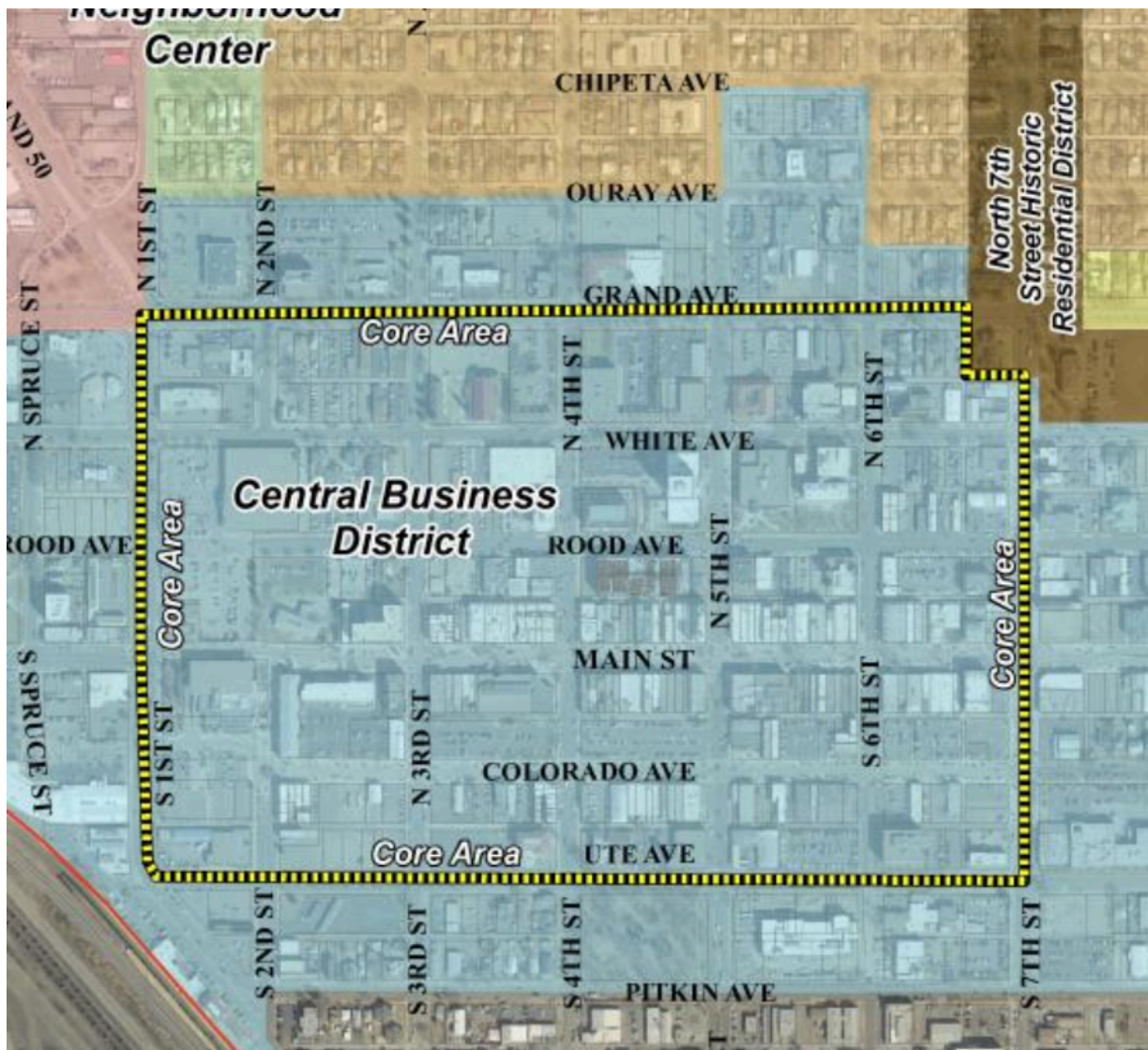


Figure 9

(Ord. 4572, 3-20-13)

Article IV. Residential Areas Standards and Guidelines

§ 24.12.110. Applicability.

The following standards and guidelines apply to the residential areas shown in Figure 10 (orange areas). The standards and guidelines are intended to apply to new development or substantial redevelopment within the area. Substantial redevelopment is any reconstruction, rehabilitation, addition or other improvements to the existing structure(s) on a site where the value of the improvement exceeds 50 percent of the fair market value of the building(s) before the start of construction.

(Ord. 4572, 3-20-13)

§ 24.12.120. Policies.

- (a) The existing historic residential neighborhoods within the Downtown District will be stabilized and enhanced.
- (b) The existing historic residential neighborhoods within the Downtown District will be preserved for residential uses, with no further encroachment by nonresidential uses.

- (c) Where existing residential zoning allows, provide a diversity of housing types through development of multifamily housing that is in keeping with the character of the neighborhood (refer to multifamily development, GJMC § **24.12.130(c)**).
- (d) Enhance access to and improvements within existing public open spaces (e.g., parks and school grounds) within the downtown residential core.

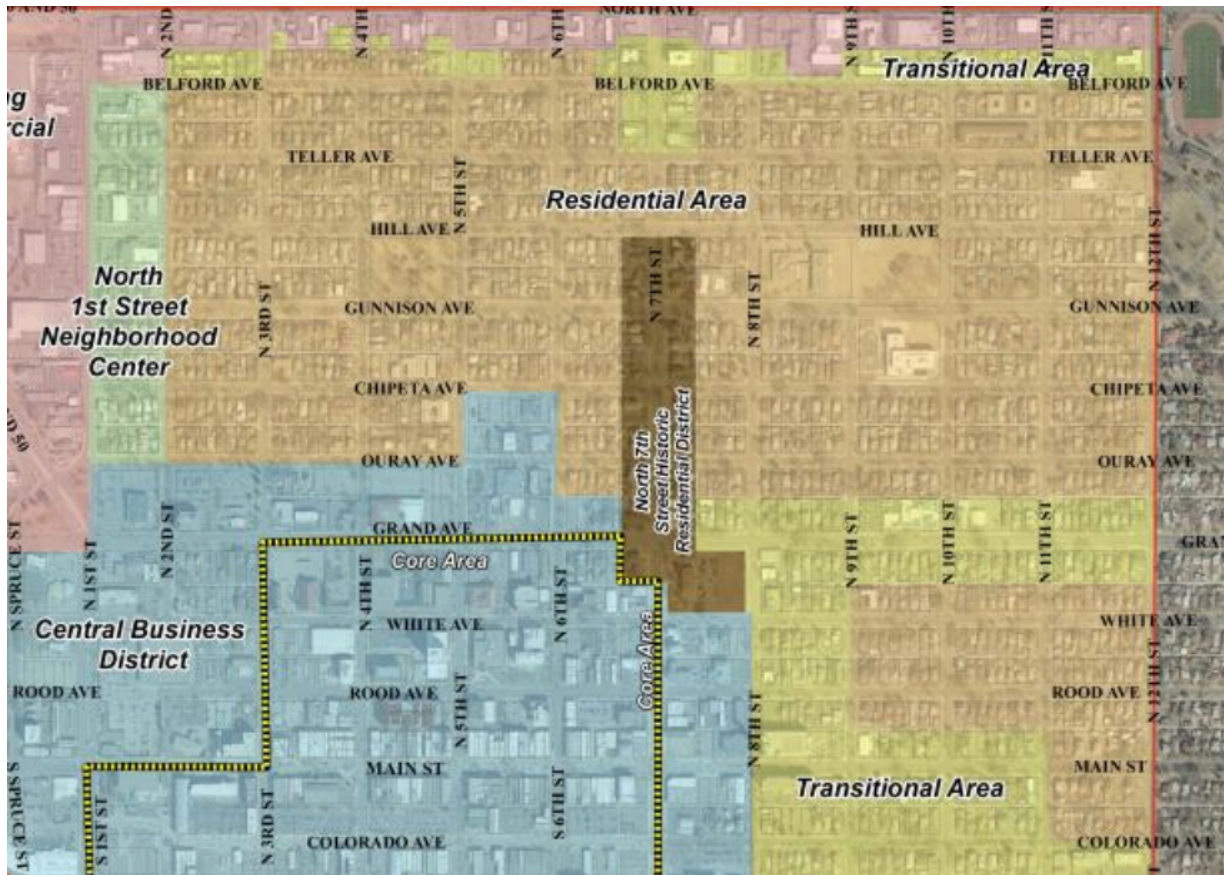


Figure 10

- (e) Maintain and enhance the historic character of the streetscape with emphasis on the following elements: street trees, landscaping rather than parking or other uses in the park strip between sidewalk and curb, street signs that identify the neighborhoods, lighting and detached sidewalks.



Existing Residential Subarea Streetscape Character

(Ord. 4572, 3-20-13)

§ 24.12.130. Standards.

(a) Architectural Considerations.

- (1) Building Style and Character. Maintain the existing character of the house styles within the residential neighborhoods in the Downtown District. New construction and alterations shall be compatible with key architectural characteristics and site elements of the area.



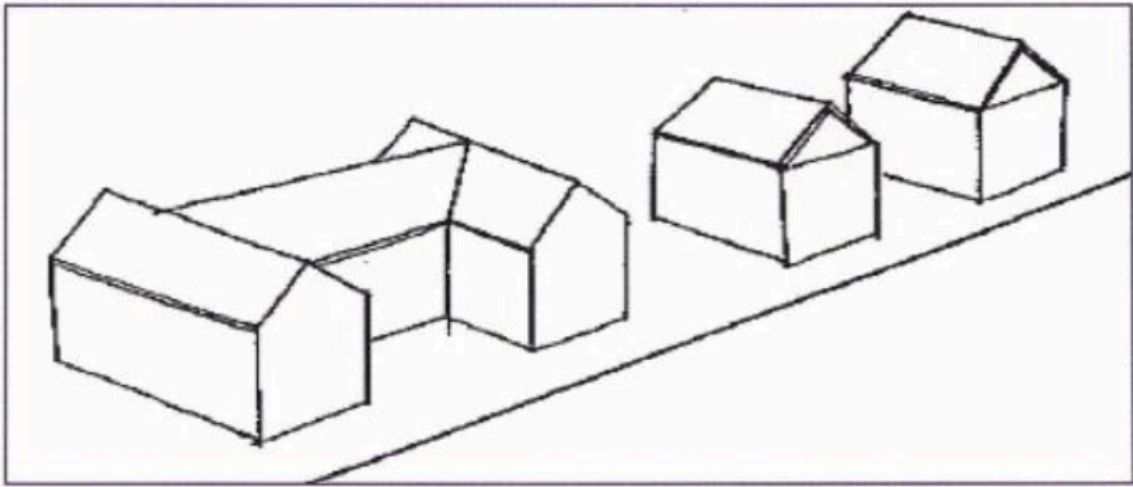
Existing Residential Building Alignment

- (2) Accessory Structure Setbacks. The setback for accessory structures is a zero-foot setback from the alley and three feet from neighboring property line(s). Garages with overhead doors facing the alley must be set back a minimum of 25 feet from the far edge of the alley or the zoning setback, whichever is greater.
- (3) Building Mass/Scale and Proportion. New buildings or additions to existing buildings shall be visually compatible with the area. Visually compatible means compatible with adjacent and neighboring buildings including mass and scale, shape, windows, doors, openings, roof shape, roof pitch and orientation.
- (4) Roof Shape. The roofs of new buildings shall be visually compatible with adjacent buildings.
- (5) Fenestration. The pattern of windows and doors on structures shall be visually compatible with buildings in the area. Visually compatible includes the relationship of width to height, and the spacing of windows and doors. For example, tall evenly spaced rectangular windows are typical of many of the residential styles in the downtown area.



Example Existing Architectural Character

- (6) Materials. The exterior materials of all new buildings, additions and alterations shall be similar in size and appearance to area buildings.
- (7) Setbacks. On a corner lot, the front yard setback for that part of the yard that extends to and along the side property line on the street side may be reduced to 10 feet on properties within the Downtown District residential subareas. On corner lots where an existing parkway strip exceeds 10 feet in width between a sidewalk and the curb, the front yard setback on a side street may be reduced to five feet.
- (b) Repealed by Ord. 4723.
- (c) Multifamily Development. Infill of new multifamily buildings may occur where zoning allows within the residential neighborhoods of the Downtown District. However, the site design and structures for this type of development must maintain a scale and character compatible with the residential neighborhoods in the Downtown District. In addition to the architectural considerations listed in subsection **(a)** of this section, multifamily development shall follow the standards below.
 - (1) Incorporate forms typical of the single-family residential architecture of the Downtown District including sloping roofs, porches, roof dormers and other architectural details.
 - (2) Break up the mass of larger buildings into forms that are similar in scale to the residential character.
 - (3) Facades must be composed of smaller sections, similar in scale and material finish to residential structures.



Example – Break Up Facade of Larger Structure to be Compatible with Single-Family Scale

- (4) Off-street parking for multifamily development shall not be located in the front yard setback. Parking shall be in the rear or side yards. If the property abuts an alley, the parking area shall take access from the alley. If the property has more than one street frontage, “behind the building” shall mean on the opposite side of the building from the front door or the main public door entrance to the building.

- (5) Develop pedestrian links between the front sidewalk and building entrances and between parking and rear or side entrances.

(Ord. 4890, 11-20-19; Ord. 4812, 9-19-18; Ord. 4723, 10-21-16; Ord. 4572, 3-20-13)

§ 24.12.140. Guidelines.

- (a) Demolition of existing historic homes in order to construct new residential structures is strongly discouraged.
- (b) Maintain and enhance the pattern of landscaped front yards that gives the residential neighborhoods within the Downtown District a distinctive, friendly appearance.
- (c) Each new building and addition should be located so that it aligns with existing buildings in the area. “Aligns” means elevation (e.g., horizontal lines of peaks of roofs, cornices and window sills) and plan (e.g., setbacks from the street and rear property lines and spacing between structures/setbacks from side property lines).
- (d) Main entrances should open onto a street and should align with those of adjacent residential buildings. For example, on many of the downtown homes, raised foundations and steps that define the main entrance are prevailing characteristics. Door styles should be similar to those found on residential buildings within the area.
- (e) New buildings and additions should have the same number of stories and a height which is compatible with buildings in the area.
- (f) Park strips will be landscaped in a traditional style, including street trees, grass, and low plantings or a combination thereof. Park strip landscaping should include some live material – use of all nonliving material such as rock is discouraged. Use of drought-tolerant plants is encouraged.



Existing Character of Front Yards and Park Strips

(Ord. 4812, 9-19-18; Ord. 4572, 3-20-13)

Article V. Transitional Areas Standards and Guidelines

§ 24.12.150. Applicability.

The following standards and guidelines apply to the Transitional areas shown in Figure 11 (yellow areas). The standards and guidelines are intended to apply to new development or substantial redevelopment within the area. Substantial redevelopment is any reconstruction, rehabilitation, addition or other improvements to the existing structure(s) on site where the value of the improvement exceeds 50 percent of the fair market value of the building(s) before the start of construction.

(Ord. 4572, 3-20-13)

§ 24.12.160. Policy.

The peripheral areas of the CBD provide a mix of established residential uses and low intensity, nonretail, neighborhood service and office uses that are compatible with adjacent residential uses and neighborhoods. New development or reuse of existing structures will maintain compatibility with building scale and appearance in the area.

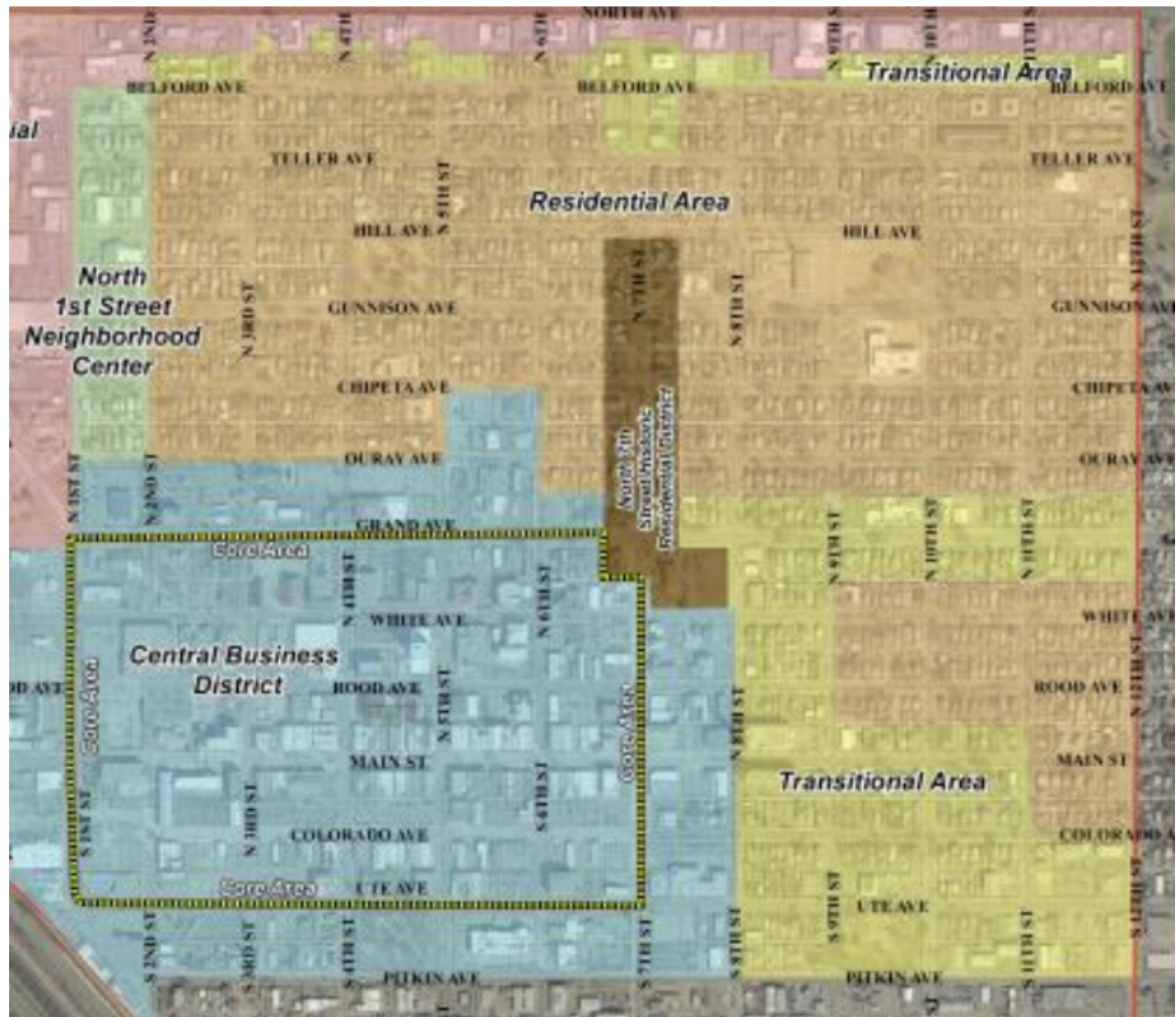


Figure 11

(Ord. 4812, 9-19-18; Ord. 4572, 3-20-13)

§ 24.12.170. Standards.

- (a) Land Use and Development Intensity. Nonresidential uses within the transitional subareas shall not open earlier than 7:30 a.m. and shall close no later than 8:00 p.m.
- (b) Architectural Considerations. New construction, including additions and rehabilitations, in the transitional subareas shall be designed to have a residential character consistent with existing buildings in the area. "Consistent" means the operational, site design and layout, and architectural considerations described below.
 - (1) New principal buildings shall be located to align with existing buildings in the area. "Aligns" means elevation (e.g., horizontal lines of peaks of roofs, cornices, window sills) and plan (e.g., setbacks from the street and rear property lines and spacing between structures/setbacks from side property lines).



*Example Infill Development in Transitional
Area - 9th Street and Colorado Avenue*

- (2) Building entrances shall face a street or architectural features shall be provided that visually suggest an entrance. Door styles shall be similar to those found on residential buildings.
- (3) Each new principal building, its mass in relation to open spaces and its windows, doors, and openings shall be visually compatible. "Visually compatible" means compatible with buildings in the area, including mass, shape, window, doors, openings, roof shape, roof pitch and orientation. For example, a large building shall be compatible with surrounding smaller dwellings by dividing its mass into smaller components to create a building elevation that is more like the size and proportion of the buildings in the area.
- (4) The roofs of new principal buildings or additions to principal buildings shall be visually compatible with buildings in the area.
- (5) Window and door spacing on structures shall be visually compatible with structures in the area. Visually compatible includes the relationship of width to height, and the spacing of windows and doors. For example, tall evenly spaced rectangular windows are typical of certain residential styles near the transitional subareas.

(c) Signs.

- (1) Flush wall signs and monument signs shall be the only sign type allowed.
- (2) Signs shall be located at least 10 feet behind the front property line. Total sign area shall not exceed 25 square feet per street frontage. The sign allowance for one street frontage may be transferred to a side of a building that has no street frontage but cannot be transferred to another street frontage. Monument signs shall not exceed eight feet in height.



Example Signs within Transitional Subarea

(d) Parking and Site Development.

- (1) Non-single-family uses in the transitional subareas shall be designed and utilized not to increase on-street parking in front of single-family dwellings in the neighborhood.
- (2) Service entrances, loading areas and dumpster areas shall be located only in the rear or side yard. If the property has more than one street frontage, the rear or side shall mean on the opposite side of the building from the front door or the main public door entrance to the building; and each loading area shall be screened from each abutting residential use.

(Ord. 4812, 9-19-18; Ord. 4572, 3-20-13)

§ 24.12.180. Guidelines.

- (a) New buildings should have the same number of stories and a height which is compatible with those of buildings in the area.
- (b) The exterior of all new buildings, additions and alterations should be similar in size and appearance to buildings in the area.
- (c) Sign materials should be visually compatible with materials used on the building facade.

(Ord. 4812, 9-19-18; Ord. 4572, 3-20-13)

CITY OF GRAND JUNCTION, COLORADO

RESOLUTION NO. 174-07

**A RESOLUTION ADOPTING THE NORTH AVENUE CORRIDOR PLAN AS A PART
OF THE GRAND JUNCTION GROWTH PLAN**

Recitals:

Elements such as age, dilapidated structures, and high turnover in area businesses along North Avenue have contributed to its deterioration. Being primarily zoned for commercial use has resulted in sporadic disinvestment, underutilized buildings, old strip malls, and vacant property. Phase One of the proposed plan includes North Avenue from 12th Street east to I-70B.

A plan has been completed based on input received from two public open houses, both of which were well attended, as well as focus group meetings to address specific issues. The plan includes an overall strategy to revitalize the corridor, primarily focusing on sustaining and increasing vitality. The proposed plan includes specific key projects and strategies for the implementation of improvements and future investment opportunities that will stabilize the corridor and provide land uses to:

- Fill a 'market gap' in the region
- Create services at the neighborhood level
- Restore the regional destination desire
- Improve mobility for pedestrians, bicyclists and transit riders
- Create a significant 'neighborhood' of residential, retail, commercial and public activity areas

**NOW, THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF
GRAND JUNCTION THAT:**

The North Avenue Corridor Plan, included as [attached Exhibit A](#), is hereby adopted and made a part of the Grand Junction Growth Plan.

PASSED on this 3rd day of December, 2007.

ATTEST:

/s/: Stephanie Tuin
City Clerk

/s/: Bonnie Beckstein
President of the Council Pro Tem

North Avenue Corridor Plan

September 2007

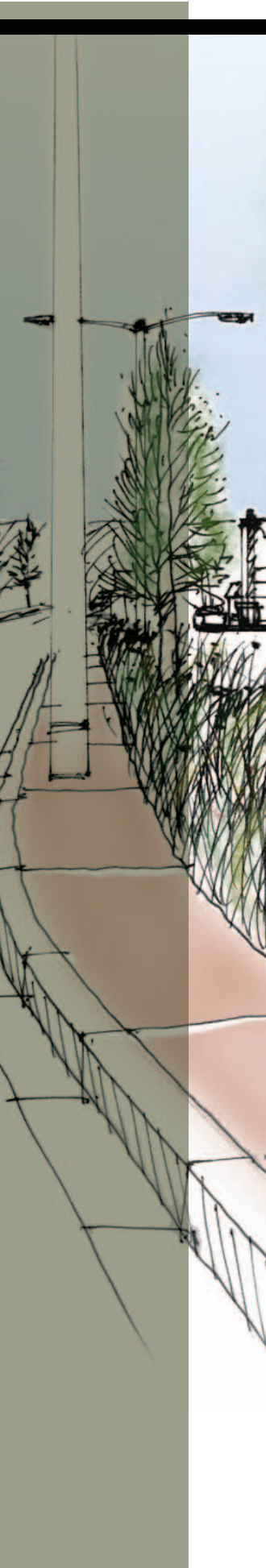




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EDAW was retained by the City of Grand Junction to complete a conceptual Master Corridor Plan for the future development and redevelopment of the North Avenue Corridor. North Avenue is a major east-west commercial thoroughfare, and has experienced significant decline over the last years. The corridor study area includes the intersection at 12th Street, eastbound to the I-70 B business loop intersection, and an approximate one block radius on either side of North Avenue.

This plan includes an overall strategy to revitalize the corridor in order to promote the future development of retail, commercial, office and residential opportunities to sustain and increase the vitality of this corridor. Specific key projects and strategies for the implementation of improvements have been identified including:

- Full-fill a 'market gap' in the region
- Create services at the neighborhood level
- Restore the regional destination desire
- Improve mobility for pedestrians, bicyclists and transit riders
- Create a significant 'neighborhood' of residential, retail, commercial and public activity areas

Success in filling the market gap is dependent in 3 areas; financial, physical and regulatory. The Plan recognizes existing barriers and provides strategies to overcome these:

Financial -

- Work with private and non-profit interests to assemble and hold opportunity sites
- Share market opportunities with various audiences to encourage investment
- Use range of mechanisms – land swaps, low-interest loans, acquisition / write-down
- Consider establishing appropriate districts as holding entities
- Problem is less severe near logical development node – probably can use traditionally land assembly

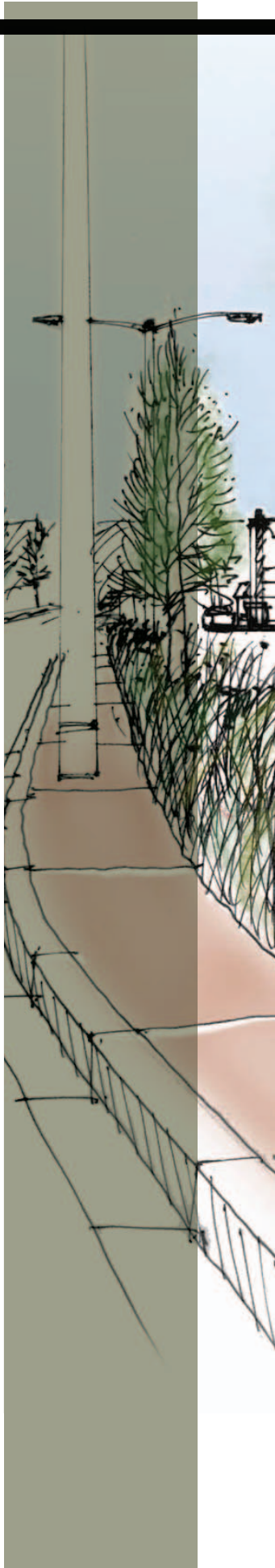
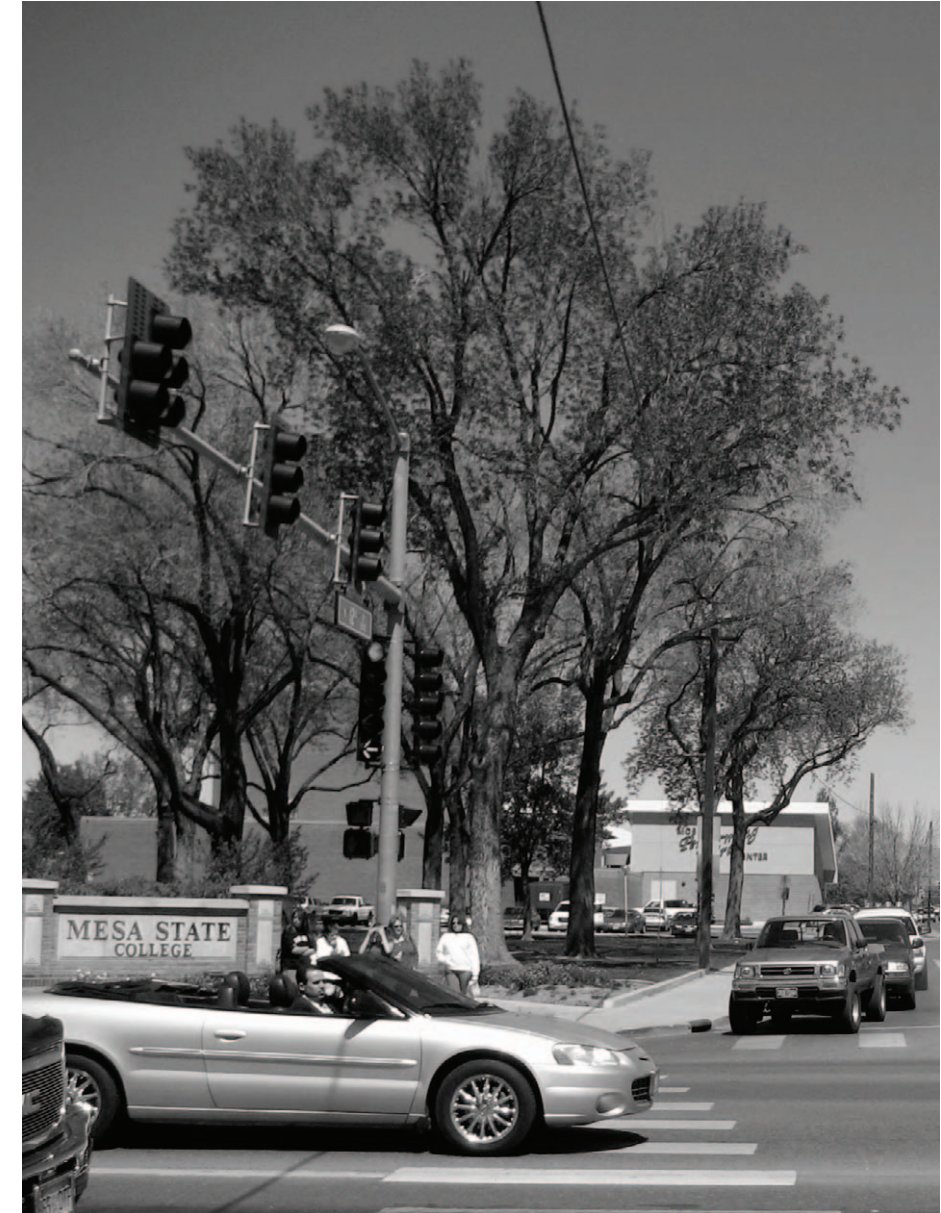


Regulatory -

- Consider overlay district that is more supportive/encouraging of mixed-use
- Revise regulations to eliminate barriers to investment in neighborhoods on the corridor; promote density and greater variety of residential products (i.e., density bonuses)
- Encourage the introduction of residential development within nodes and at key locations; fill financial gaps, and support demonstration projects (look for institutional partners)
- Where necessary, assist with assembling properties to accommodate a range of product types (workforce, student, senior, etc.)
- Eliminate regulatory barriers which preclude or delay redevelopment buildings for residential use (building codes, limits on adjacency among uses, etc.)
- Work with private and non-profit interests to provide opportunity sites for residential development

Physical -

- Maintain (City) flexible position on the use and width of alleys (if relevant) in select locations
- Create street standards for building setbacks, sidewalk width, signage, awnings, etc. sensitive to the feasibility of the remaining building envelope
- For stores that require surface parking in front, require heightened landscaping or pad site development with interior parking – remaining sensitive to sight-lines
- Establish street standards – define the street's role by district based on the vision and desired character
- Update any transportation plans relative to desired district functions, including encouraging public transportation through increased comfort and security of stops
- Link parking lots, minimize curb cuts and consolidate driveways – selectively – where feasible
- Redesign intersections and access points within the nodes to simplify and coordinate signal sequences



The key to achieving new development must be initiated on a theme of:

- Capitalize on proximity to college and downtown through marketing efforts, product mix and positioning
- Educate Council about this market reality and prepare them for “filling the gap” of catalyst projects resulting from inflated land prices and other factors
- Consider an urban renewal district to assist with on-site and off-site costs for qualifying elements
- Capitalize on proximity to hospital and park through marketing, product mix and positioning (including actual physical connectivity)
- Encourage hospital, or unrelated clinics, to explore partnership opportunities
- Encourage mixed uses including affordable residential and multi-family
- Capitalize on substantial infrastructure investment in north-south connection (along with presumably softer land assembly costs) to pursue regional retail opportunity

Creating services at the neighborhood level requires an assessment of existing resources for project area workers and residents. Accommodating these uses and supplemental regional, county wide services, will ‘round-out’ the corridor. Key physical projects have been identified for the revitalization of North Avenue and are more clearly identified and discussed on page 52. The three key projects identified are:

- Community / Development Nodes at the intersections of 12th Street, 28 and 29 Road
- Corridor - ROW and Streetscape Improvements
- Land Assembly Areas for Potential Large / High Impact - Public / Private Partnerships at intersection nodes. (An example massing and phasing concept is presented for such larger development opportunities on page 54.)

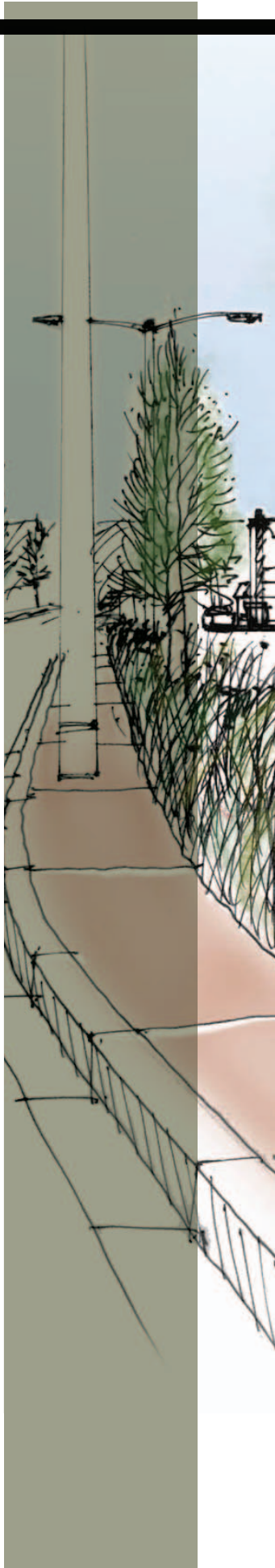


This philosophy will address the goal of restoring the corridor for regional destinations. The project area is disabled in providing for pedestrian and bicyclist mobility. Addition of sidewalks, curb-cuts consolidation and streetscape improvements will achieve a number of goals including creating a unified street edge and overall character of the corridor. The following examples are taken from the in-depth study of the streetscape improvements that would further improve North Avenue:

- Create a generous vegetated swale to collect, filter and distribute stormwater to replace the drainage adjacent to street
- Where buildings are adjacent to the street, sidewalks improvements include connecting the sidewalks to building entrances, defined curb cut entrances, and relocation of parking between buildings. This allow for an improved streetscape, greater visibility of storefronts, clear and safe pedestrian connections.
- Parking lots adjacent to streets should have a defined curb cut entrance, added sidewalks and additional plantings to define parking lot circulation and enhance way finding. Additionally, planting islands within these expansive asphalt areas will allow for a reduction in the urban heat islands typically found in large parking lots. Logical ped/bike connections from North Avenue, to destinations, through parking lots should be explored.
- Curb cuts should be reduced and consolidated in order to enhance the overall character of the corridor and improve vehicular and pedestrian circulation.
- Signage along North Avenue can be improved by minimizing pole mounted signs and replacing them with ground mounted monument signs.
- A streetscape kit-of-parts will further unify the streetscape by implementing amenities that maintain the character throughout the corridor.
- Modifications and recommended adjustments to the Grand Junction Sign Code are provided in detail on page 64.

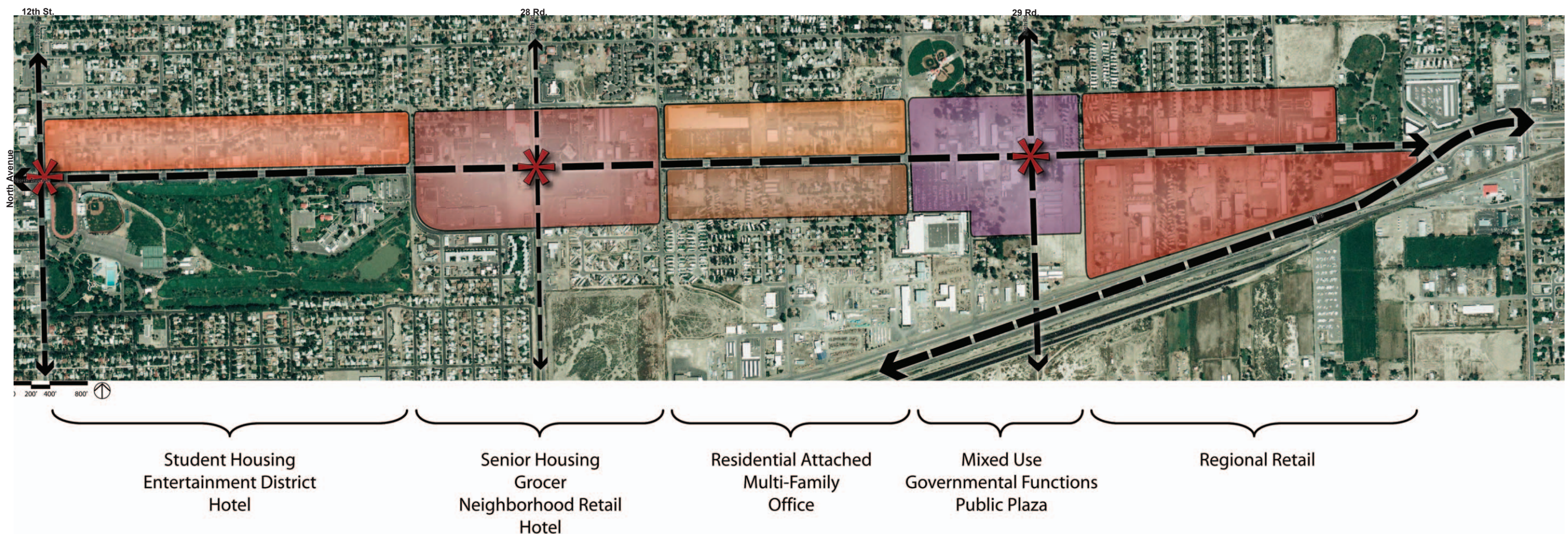
Additional streetscape improvements and guidelines recommendations are provided in order to help guide new and future development along North Avenue. These guidelines will assist in creating a cohesive and unified streetscape. These recommendations are located on page 60 of this report.

These projects, at a reasonable scale and value, will enlist future developer interest and act as catalyst projects to meet the greater need of private investment.



The preferred alternative was developed from the demand and market study as well as the public comment obtained from the open houses. The predominant elements of this preferred alternative are:

- A Student and Entertainment District in proximity to Mesa College would also provide student housing, after 5 activities and the opportunity for a quality Hotel to serve the area
- A Neighborhood Center where Senior Housing located in proximity to a quality Grocer, Neighborhood Retail, and the potential for a Hotel to serve the hospital and surrounding neighborhood
- A Multi-Family and Attached Residential district connecting to the neighborhoods to the north, and potential for Office space to the south
- A Mixed Use area with residential over retail incorporated with the Governmental Functions and Public Plazas to create a gateway for the 29 Rd. intersection
- Regional retail to anchor the east end of the corridor



Roles and Responsibilities

City Leadership

New Initiatives

Implementing Entities (i.e., City Staff, Advocacy Entities, etc.)

Investors / Private Sector

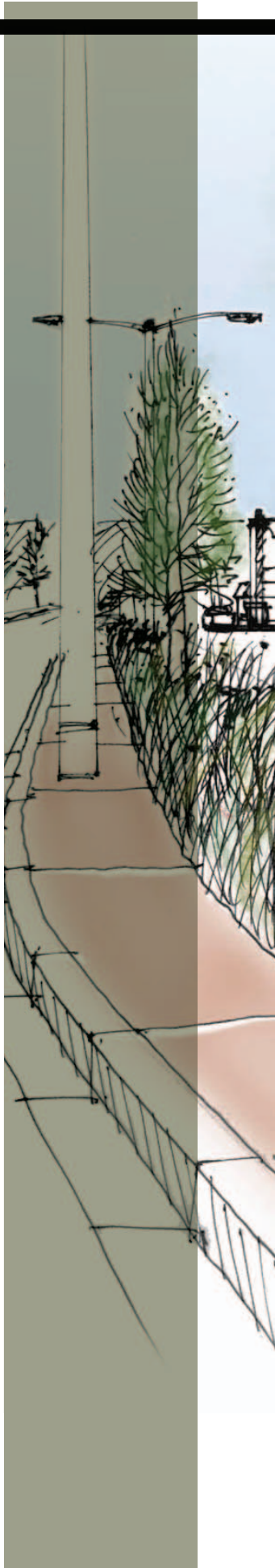
Community

- Support at least two demonstration projects (catalyst projects) – commit support for the use of a range of “gap filling” tools
- Once potential incentives are identified – publish a request for development
- Consider the range of district mechanisms which could potentially fund improvements in the corridor

- Continue to monitor the market and development climate – establish benchmark database
- Develop targeted marketing materials – solicit regional retail opportunity
- Establish street standards
- Link parking lots, minimize curb cuts and consolidate driveways – within nodes
- Complete regulatory review – eliminate “gaps and barriers”

- Host developer / lender (“delivery system”) information meetings – progress updates
- Share development challenges – open book and open dialogue

- Continue to participate – get educated
- Support leadership decisions which advance the vision



Strategy Layers

Guiding Principles



Study Area

Catalyst Projects



Tool Kit (3 Ps – Projects, Programs and Policies)

Tool Kit (3 Ps – Projects, Programs and Policies)



Realized Vision

Realized Vision





The following report provides an overall view of the current situation of the North Ave. Corridor. Barriers and constraints have been identified and studied to understand their relevance and contribution to the decline of this commercial district and thoroughfare.

The 3 miles of the corridor expanse has been divided into three distinct proposed districts as a practical way of addressing the corridor. These three areas are logically divided by north / south major streets; 12th Street, 28 and 29 Roads and relate to specific neighborhoods and land uses. The report further defines the key projects that will provide the greatest synergy from which development can further progress and build upon.

Implementation tools and specific strategies have been developed to address the barriers pertaining to the districts. An example is presented and used to illustrate how such strategies and tools would be employed for the phasing of such a key project.

Other strategies that encompass the entire corridor study area are also defined with recommendations. Both, private and public leadership is necessary to achieve these goals and partnerships are encouraged for policy and physical planning efforts.

All of these recommendations will serve the City of Grand Junction to have a defined document by which to solicit and guide new development in an effort to revive and improve the current situations facing the North Avenue Corridor.

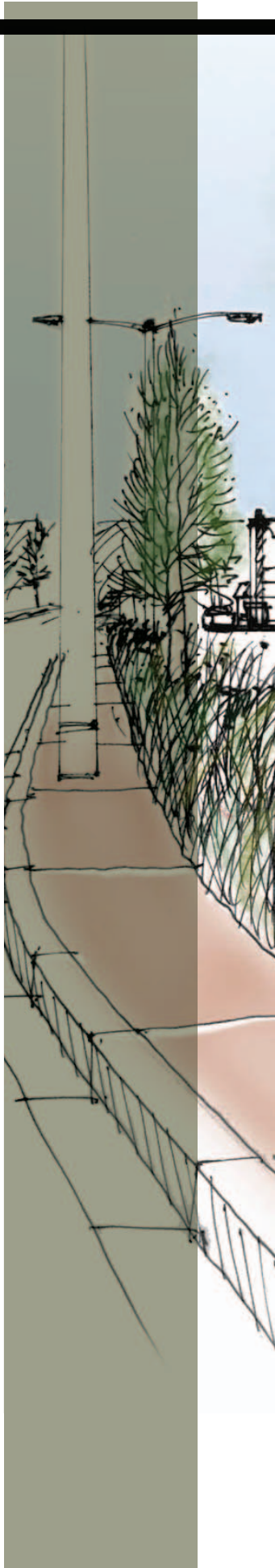


NORTH AVENUE CORRIDOR BACKGROUND

Historically, North Avenue was the northern most boundary of Grand Junction. As development continued to grow to the north, the North Avenue corridor now finds itself at the center of the city and in great location and proximity to many great assets and amenities such as the Stocker Stadium and Lincoln Park and Golf field, Mesa College and a wide range of community services, hospitals, downtown, access, etc.

Until the 1990's North Avenue was the primary retail tax generator for the City of Grand Junction. However, over the last few years it has experienced a dramatic loss in revenue in great part associated with the extensive development of new and large commercial and retail centers along the western edge of the City.

As business is pulled to these new commercial centers, North Avenue has the opportunity to re-establish itself with improvements and developments that support a community environment and will make it uniquely different. Rather than mimic the developments occurring on the western edge, The North Avenue Corridor can reclaim it's identity by promoting developments that combine retail, office and residential with civic components to establish a distinctive sense of place and character. This will provide, as supplement to downtown, an historically rich active neighborhood with a focus on entertainment and mixed use.



A Kick-Off Meeting with the EDAW project team and City of Grand Junction internal staff served to obtain information from the Planning Department on the conditions of the corridor and local insights and issues to keep in mind during the planning process. The following meeting notes show the items discussed and important information relating to the history and conditions of the corridor study area. These items will help guide implementation plans and will be maintained in further evaluation and development of the plan.

Primary key issues that were discussed in the Kick-Off meeting and have directed the progress of the corridor study and plan are as follows:

- The 29th Road intersection will become a key transportation corridor with the new bridge planned continuing south over the railroad tracks, connecting to the greater business loop. Daily trips are expected to increase and this development area could serve as an identifiable community node.
- North Avenue has historically been a cruising street and is currently still used as such. While the cruising of the corridor in itself is not a problem, associated problems such as the congregation and loitering in vacant parking lots is a safety and security problem.
- No major traffic problems were identified in regards to traffic lights, accidents, or problem intersections.
- Bus transportation and usage is well used within the corridor and potential “far-side” pull-outs after intersections might be explored in future design scenarios to allow for buses to not impede with the vehicular flow of traffic on North Avenue.
- There is a lack of consistency in sidewalks and street design that would create an identifying character to make the pedestrian environment more appealing and usable.



- The consolidation of curb-cuts would greatly improve access, safety and the pedestrian streetscape environment.
- Utilities should be consistently located and buried when ever possible to further improve the visual view of North Avenue.
- There is a great need and opportunity to provide good quality and affordable housing; multi-family and attached residential units along the corridor.
- Alternative and possible methods to mitigate the issues surrounding the existing homeless shelter would further improve the perceived environment of North Avenue.
- Consolidation of commercial uses into core districts would provide areas where the community could access neighborhood services with associated active public spaces while providing opportunities for large lot configurations for future development.
- An overall re-evaluation and improvement of the signage standards along the entire corridor would further improve the visual character of the streetscape and promote a pleasant pedestrian and vehicular environment.

MEETING NOTES

Present: Kathy Portner, Angela Harness, Jody Kliska, Scott Peterson, David Thornton, (City of Grand Junction), Lee Kellar (Matrix), Bill Cunningham (Leland), Steve Wilensky, Silvia Kjolseth (EDAW)

Date: February 7, 2007 Time: 1:00 PM

Project: Grand Junction, North Avenue Corridor Plan Project No.: 06180105.01

Subject: Initial Meeting with City of Grand Junction

Traffic Engineering

- North Avenue is a state highway, Highway 6, until 2009.
- Near 2008 the traffic is expected to change around 29 ½ St. as a bridge is planned over the rail tracks to serve as a business loop.
- General peak hours for the corridor are around noon and increasing again in the evening hours.
- North Avenue has historically been, and is still, a cruising street.
- Traffic signals are working well and are synchronized, although they are not evenly spaced.
- There is no specific traffic problems identified within the corridor other than the use of large vacant parcels as part of cruising for parking / loitering.
- Police issues are primarily around the large vacant parking lots that are used for loitering.
- The medians are maintained by the Parks department.
- Street improvements are made by the City through a fee from new developments, rather than expecting developments to be responsible for street improvements. This fund appears to be under funded and not working appropriately.
- There are no specific design street section unique to North Ave..
- The Transportation Design Standards can be explored.
- Street access consolidation of turns can also be explored.
- The busses do not have pull-out lanes and currently stop in the traffic lanes.
- Pull-outs for busses could be explored, and preferably placed after signals to ease merging back into traffic lanes.
- Busses are used primarily to access jobs; riders appear to be primarily by necessity.
- Connections to adjacent neighborhoods should be explored.
- North Ave. has a history of flooding as storm sewer is lacking.
- Sidewalks are discontinuous and bike lanes are not provided.

Utilities

- Utilities need to be more closely looked at and generally located.
- The overhead electrical is located in the street around 29th Street and further east; otherwise it is located in the alley.
- Lateral ditches along the corridor are primarily privately owned; some have easements, and there is likely little documentation and may be difficult to approach.
- The sewers may not be completely separated out, specifically around Mesa College.

Economic / Market

- Exclusive retail opportunities do not need to be maintained on corridor.
- Various job employments centers are found along corridor including: Veterans Hospital, Mesa College, Workforce Center, Career Center, Mental Health, etc..
- There appears to be a lack of multi-family residential product and rental product generally throughout the city.
- There is a general assumption of a low residential vacancy rate and high rent percentage in the area, especially around the college.
- There appears to be a need for good quality, affordable housing product.

Current Uses

- Distinction of districts along the corridor would be useful and likely would be well received.
- Wal-Mart is expected stay, although alternatives could be explored for site re-use.
- City Market is potentially going to close and move to 12th and Patterson. They currently lease their site.
- K-mart does appear to not be performing well.
- The old Amusement Park site is proposed to become new retail/office.
- The various Trailer Parks through the corridor may be difficult and sensitive issues to address, although alternative and improved housing might be explored and potentially well received.
- Smaller retailers and non-franchised business will also be sensitive issues.
- A homeless shelter is located on the corridor; however the soup kitchen is located separately and further away on Main Street in downtown.
- The current used car lots are not a favorable use of land, and potential consolidation and relocation of these should be explored.
- Motels appear to be primarily used as long-term rentals and well utilized.

Potential Uses

- There is a desire to have commercial concentrated in specific strategic areas to allow larger lots to be consolidated for future redevelopment that might be employment based.
- Mesa College students primarily commute into campus and appear to be lacking an area to congregate.
- There is a lack of after 5 uses for both residents and students.
- The oil and gas industry could service various uses in housing and service retail.

Programs

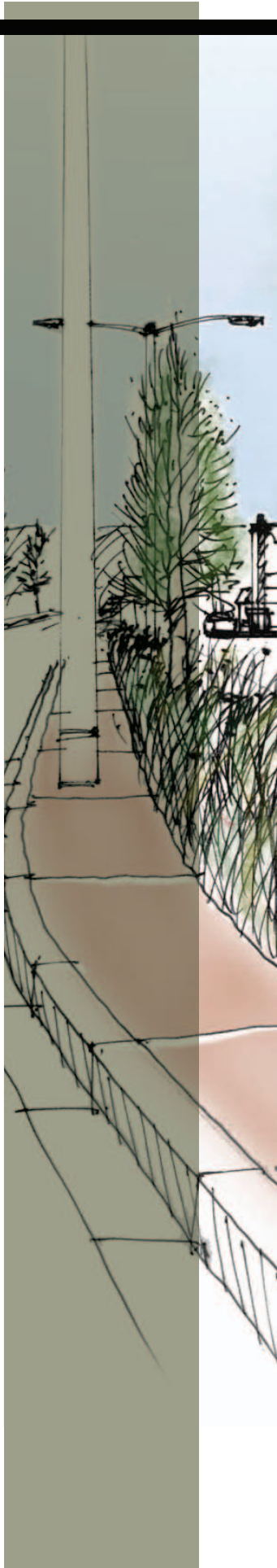
- City has program for infill redevelopment assistance for public infrastructure, to assist a new development.
- Design Standards and guidelines along with land assemblage of land and other assurances from the City could be used to promote development along North Ave..
- Urban renewal does not appear to be a popular option.
- Tax abatements are not typically used in Colorado, sales tax is also not a favorable option at this time.
- Business Improvement Districts have had greater success in other parts of the city and are more favorable as they show the support of surrounding businesses.
- Further investigation to City wide events will be noted, specifically for those that directly affect North Ave. and the surrounding area.
- The Comprehensive Plan for 20 year horizon will be starting soon and should be kept in mind.

Other

- Transition to areas beyond the North Ave. frontage should be explored.
- Sign use is a problem and there is a lack of consistency along North Ave. Currently it is cluttering the visual landscape.

Next Steps

- Current data obtained and observed will be documented.
- Stakeholders will be identified by the City and contacted for the Open House on March 8th
- The Open House is expected to allow stakeholders to share ideas, issues and concerns.
- Opportunities and constraints will be identified for the Open House.
- EDAW team will begin to explore potential retail and other uses that would service the community.
- The market overview will serve to address potential gaps that can be addressed and fulfilled (housing, retail, commercial, service).



The North Avenue study area is nearly 3 miles in length and contains the key intersections of 12th Street, 28 and 29 Roads that create ideal 5 minute walking districts. While the corridor is in general proximity to the downtown area, it is outside of a typical walking distance. The proposed bridge at 29 Road is also a key factor to North Avenue and will increase traffic circulation through this retail area. Crucially important assets to North Avenue are the adjacent Mesa College, Lincoln Park and Stocker stadium, the multiple government office and job employment centers, community centers, and the direct and easy access to I-70 to the North and I-70B to the South.

Legend

North Avenue Corridor Study Area

Park / Open Space

Schools

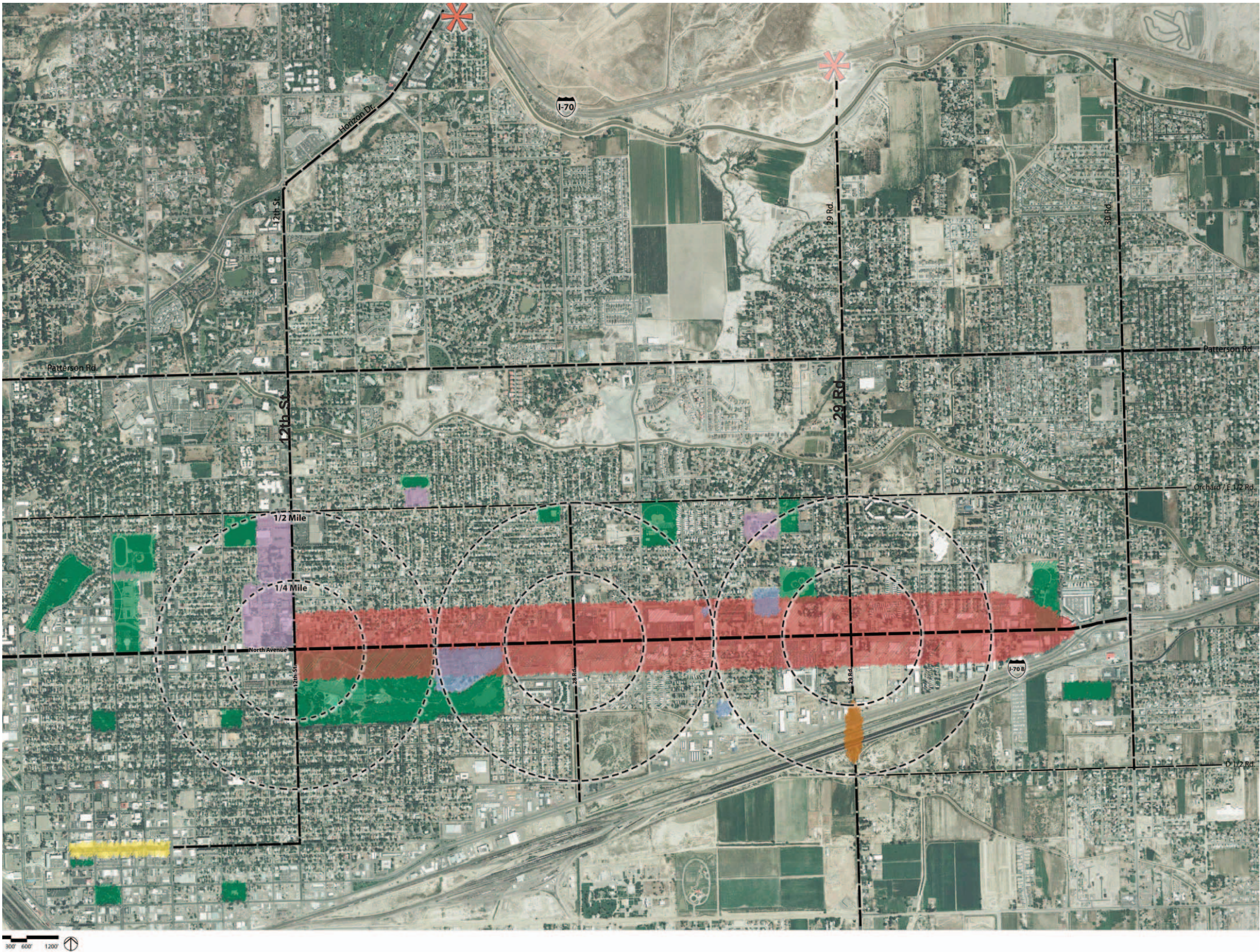
Community Services

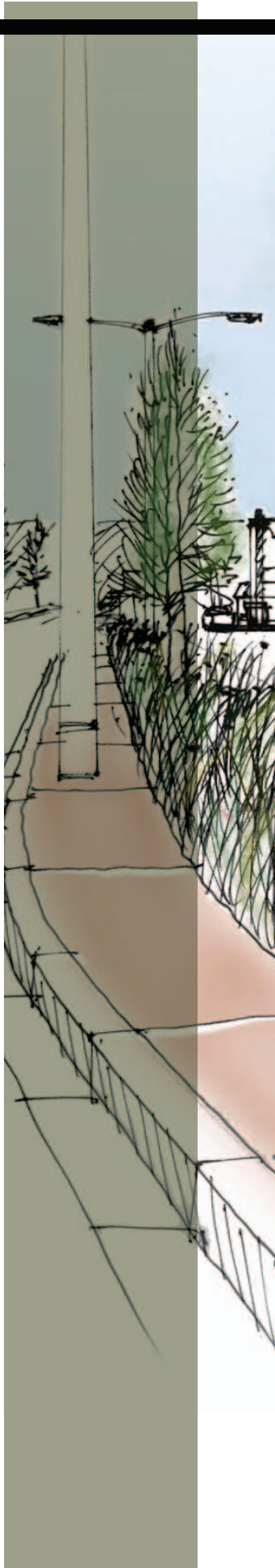
Main Street Downtown

Future Bridge / Business Loop

Current / Future I-10 Connections

Major Transportation Routes







An initial site visit included an inventory of the condition of sidewalks, streetscape amenities, streetscape character, building facades, types of uses, access, etc. Photo documentation and visual observation illustrated the ranging physical characteristics and conditions of North Avenue.

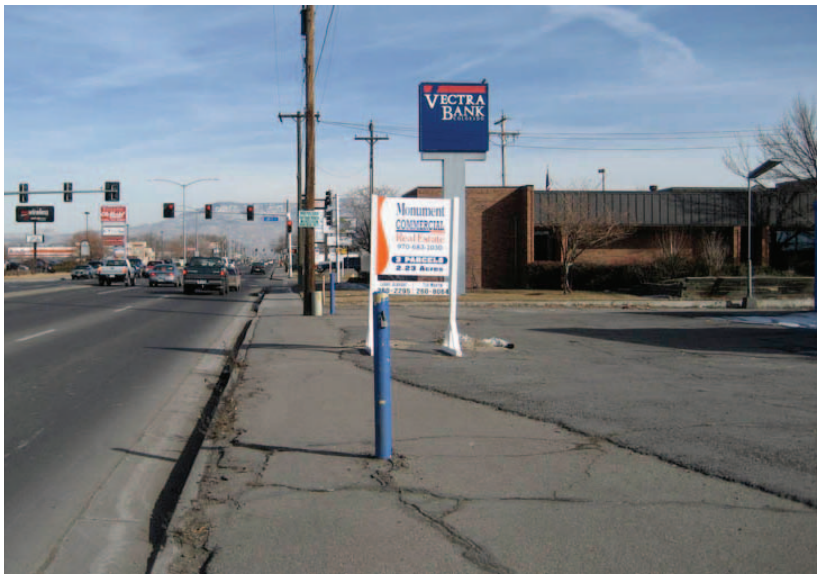
Sidewalks

The inventory of the existing conditions for the streetscape revealed that there is a significant discrepancy in consistent streetscape design or character.

On the western end of the corridor, sidewalks are in very poor condition. The sidewalks are irregular in dimensions, location, and character. There are many instances in which pedestrian are obligated to weave around and through curb-cuts in order to continue following the sidewalk. Accessibility is also a risk as many sidewalks have fallen to disrepair and pose great safety hazards with cracks, holes, and lack of ADA approved standards.

Progressing towards the eastern side, some sidewalks appear to be in better condition due to newer developments which have installed detached sidewalks with various buffers to the street and parking lots. In these situations the new sidewalks are more accessible to pedestrians, bicyclist and handicap individuals; however, there is still lacking continuation of such amenities from lot to lot. Unfortunately, the lack of accessible and continuous sidewalks creates difficulty in accessibility and an unusable streetscape environment.

On the eastern edge there are little to no sidewalks provided for the public and bus stop locations are haphazardly located over non accessible surfaces. There is simply no pedestrian right-of-way which one can access and pedestrians and bicyclists are forced to use the vehicular street lanes or private parking areas due to the lack of sidewalks or designated pedestrian areas in which to easily and safely traverse and access the corridor.



EXISTING CONDITIONS ANALYSIS - Streetscape

Bike Access

Although North Avenue is not designated as a bicycle access route, a fair number of bicyclists use the corridor. Access to the major corridor from surrounding neighborhoods and bike routes is fragmented due to the lack of cohesive sidewalks and designated bike routes.

Streetscape

The streetscape along the North Avenue Corridor study area lacks any cohesive character. Signage primarily consists of pole signs that dominate the visual environment. Along with overhead wires and billboards, the signage has become indistinguishable and illegible as it is lost in such consuming and excessive atmosphere.

Lighting and pedestrian amenities such as bus stops, seating, and bicycle racks are either completely lacking or lacking in character or consistency along the entire North Avenue corridor.

Building Facades

While a small percentage of the buildings along the North Avenue corridor have fallen to disrepair, many are simply outdated and in need of general building improvements and upgrades. Potential implementation tools can provide assistance and motivation for property owners to improve their buildings; however, the strongest influence will be the progression of new development and redevelopment in the surrounding area. For this reason it is important for the City to support larger new developments that implement the vision of this corridor.

Access



EXISTING CONDITIONS ANALYSIS - Streetscape

Studies using aerials and physical observations reveal a very high percentage of curb cuts through the entire corridor study area. These curb cuts presently serve the businesses along North Avenue and are creating an inefficient and unsafe access on and off from North Avenue. A strategic consolidation of such curb cuts would allow for the same access to businesses, greater safety by decreasing the areas in which traffic enters and exits the east-west movements and improved sidewalk amenities.

Street Designation

North Avenue is a four-lane roadway with a posted speed limit of 35 mph. Access to North Avenue is governed jointly by both the City of Grand Junction and the Colorado Department of Transportation (CDOT). CDOT is involved because North Avenue is designated as the US 6 By-pass. It should be noted CDOT's jurisdiction over this section of North Avenue ends in the Year 2009. At that time, the City will assume total control. Currently, North Avenue is classified as follows:

- Major Arterial (City) – These streets are intended to accommodate trips of shorter length and may also serve more access functions.
- Non-Rural Arterial (CDOT) – This category applies to North Avenue from 12th Street to Morning Glory Road (2.2 miles) It is intended to provide service to through traffic movements and also allow more direct access to occur.
- Non-Rural Principal Highway (CDOT) – This category applies to North Avenue from Morning Glory Road to the I-70 Business intersection (0.6 miles). This category is normally assigned to routes of regional significance. Direct access to abutting land is subordinate to providing service for through traffic movements.

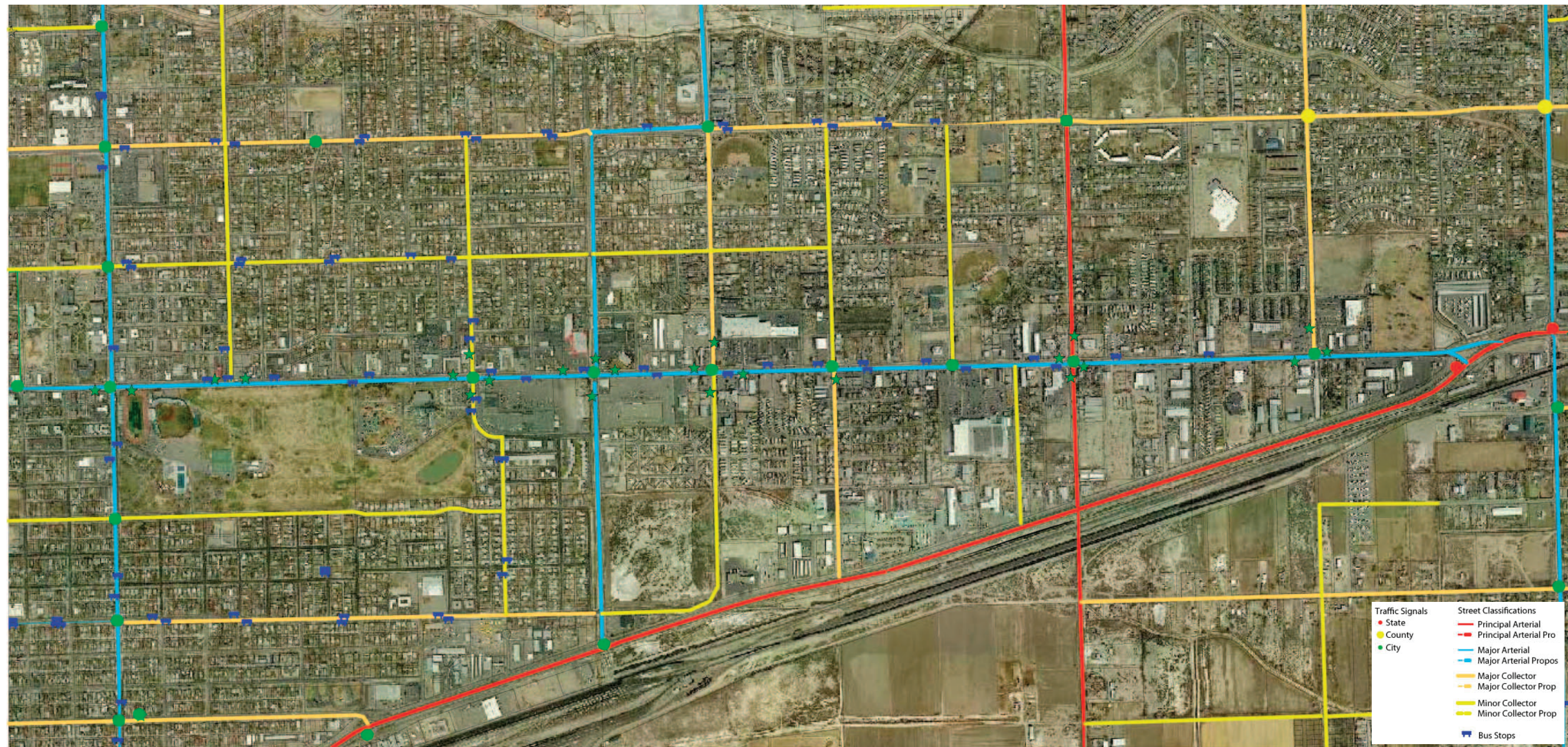
Many access points to abutting land within the Study Corridor are restricted to right in/right out driveways because of a raised median in the middle of North



Avenue. Median breaks are provided at all but one of the 25 intersecting streets to allow for full movement access. The lone exception is Pear Street. Nine of the full movement intersections are controlled with a traffic signal. The remaining intersections have stop signs to control the movements from the intersecting streets.

The existing access in the Study Corridor is consistent with both the City's Major Arterial and CDOT's Non-Rural Arterial classification. In some cases, however, the close proximity of curb cuts is creating an inefficient method of access. A strategic consolidation of these driveways would allow for the same level of business access, improved safety for traffic entering and exiting the abutting land uses, and better sidewalk continuity.

Primarily evident through the extent of the corridor are fast food restaurants, older and deteriorating strip commercial uses, automotive oriented retail, used car lots, light industrial and highway



oriented retailers. Building pads are scattered and lack any cohesive layout and design. The principle uses through the corridor appear to comply with the current and future land use code. Uses such as the mobile home parks may depend on older use reviews and may serve the community better if changed to higher quality, affordable multi-family homes. Such uses are further elaborated on in the recommendations.

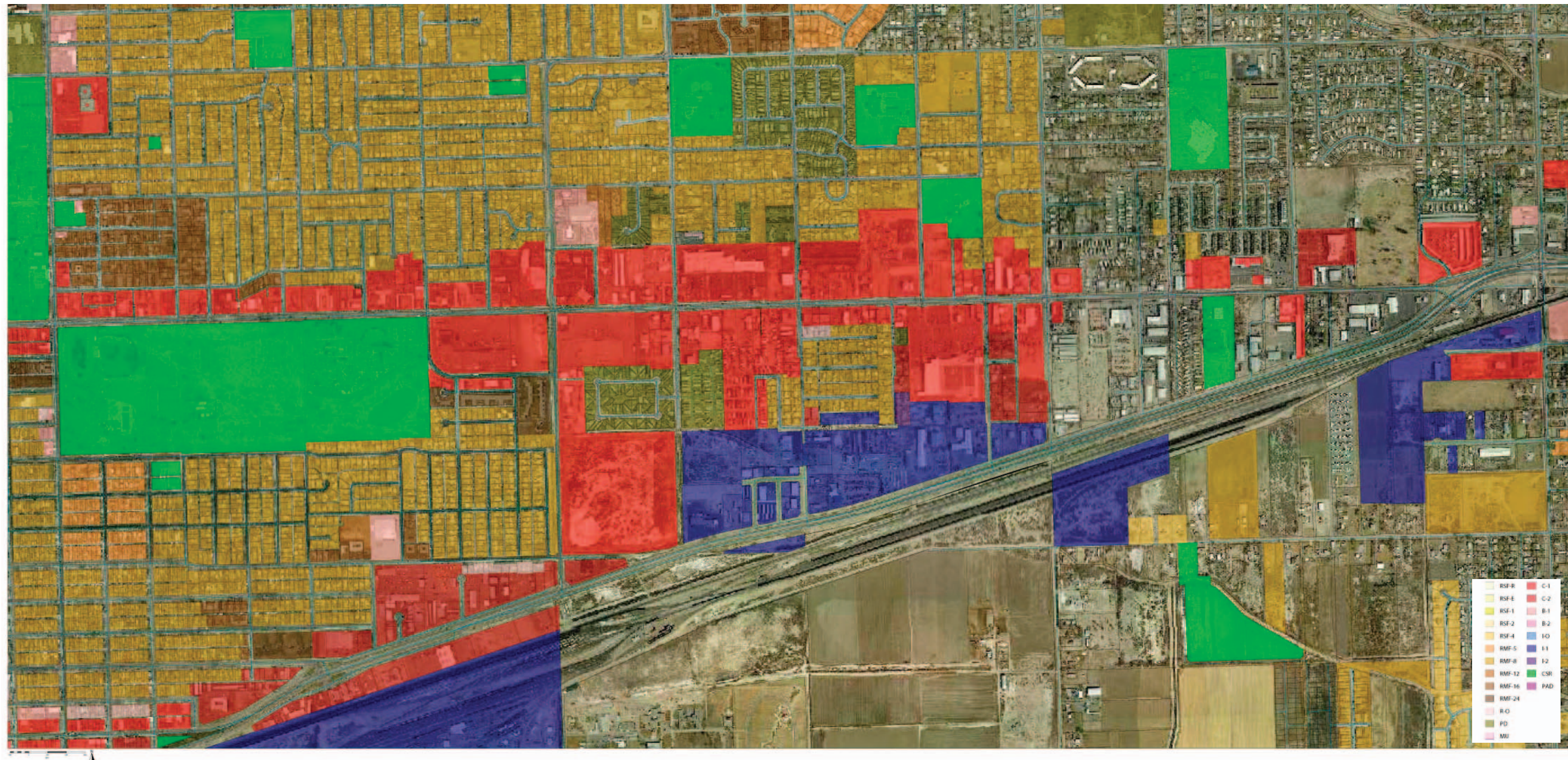
Non compatible uses observed in the corridor are primarily single family residential and trailer parks fronting directly North Avenue and surrounded commercial and retail uses as previously mentioned.



EXISTING CONDITIONS ANALYSIS - Zoning

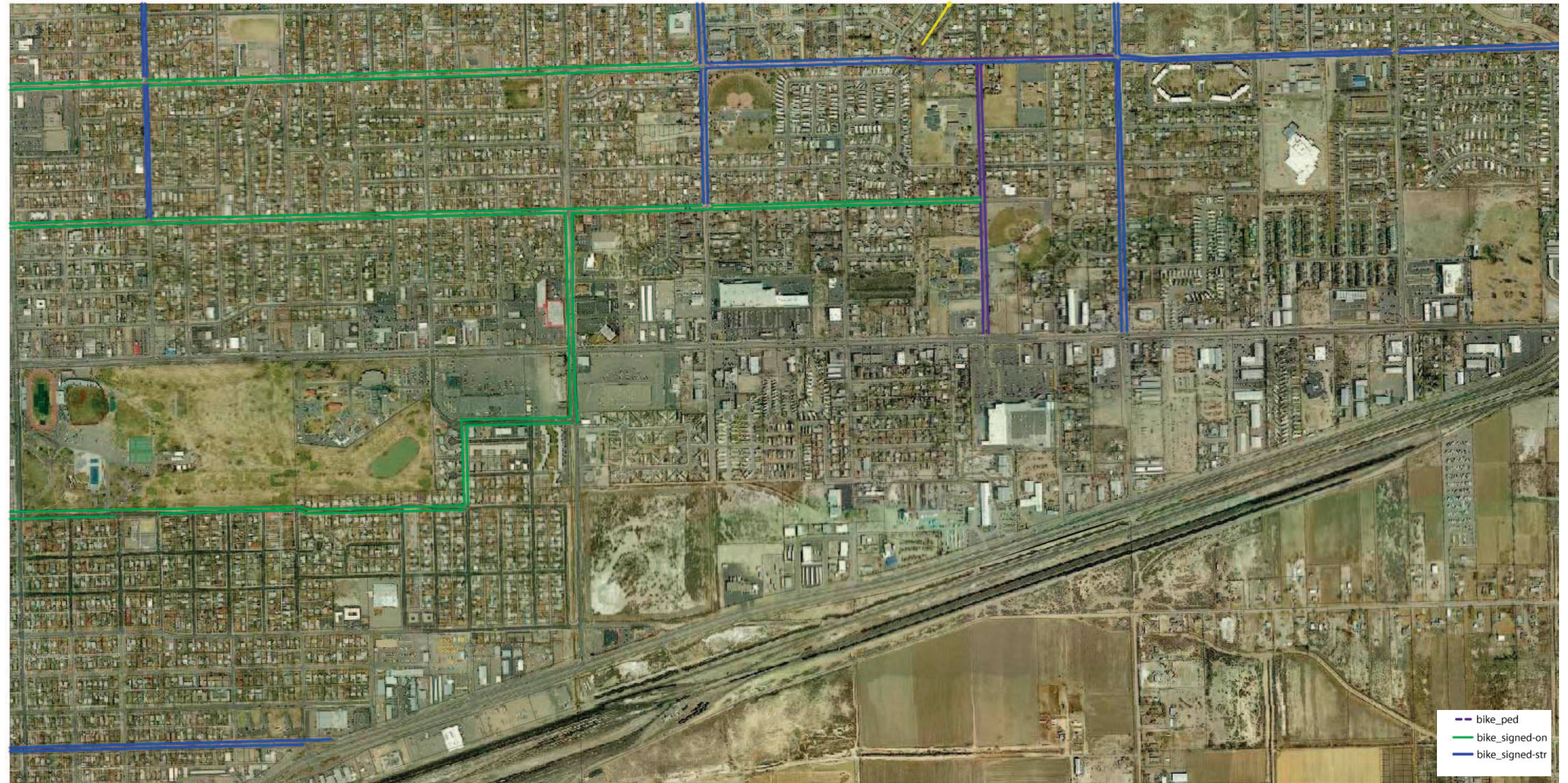
Current zoning fronting the North Avenue corridor is predominantly C-1: Light Commercial. Under this classification the primary uses allowed are retail, office and services, with a maximum building size of 80,000 square feet, and a maximum density of 24 dwelling units per acre. Under this zoning, outdoor storage and display are limited to the rear half of the lot, beside or behind principle structure, unless a CUP has been issued for such uses as auto dealers. The maximum building height is 40 feet with a minimum front setback of 15 to 25 feet. The C-1 zone district is consistent with the Growth Plan's future land use classification of commercial.

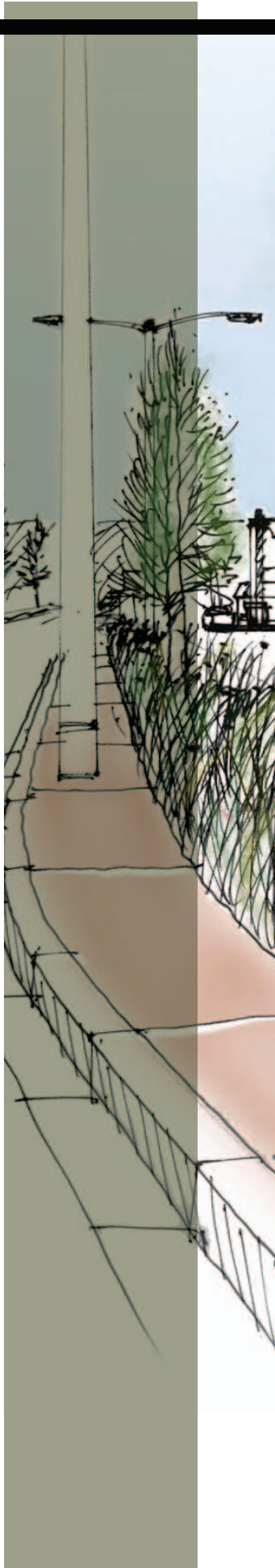
These standards and restrictions will be observed and reviewed for the optimal development and implementation strategy.



EXISTING CONDITIONS ANALYSIS - Bike Routes

The current bike routes designated in the study area appear to provide good access throughout the surrounding neighborhood, but are fragmented in relation to the North Avenue Corridor. Designated bike routes are lacking along North Avenue and the existing conditions of sidewalks also restrict the east - west access of this corridor. As the streetscape and north - south connections are improved, the designation of additional bike routes would enhance bicycle access and mobility along the corridor.





The first Open House was held March 8, 2007 and notification was sent out to the surrounding property owners and tenants of the corridor study area. The EDAW team presented the land use, districtization of the corridor area and character boards to assist the public in generating comments to guide the vision of the study area.

An overview analysis of traffic and circulation for the current conditions provided guidance as to the classification, peak hours, traffic volumes, and accident history. This data allowed the EDAW team to evaluate and understand the allowable increase of traffic that would potentially be produced from the redevelopment of commercial and residential uses to the corridor.

The following boards and information were presented to the public based on the initial site analysis:

- Barriers - photographic images of the current barrier conditions found throughout the North Avenue Corridor
- Opportunities - photographic images showing the potential vision of the corridor
- Transportation & Circulation
- Market Conditions



BARRIERS

Extensive Roadway Width



Multiple Curb Cuts



Regional Traffic



Lack of Physical Consistency



Streetscape Theme



Safety (Pedestrian / Bike)



Walkability



Tools to Solicit and Capture Private Investment

Lack of Central Attraction / Destination / Landmark

OPPORTUNITIES

Festivals / Events



Student Uses (after 5PM)
Functions and Uses



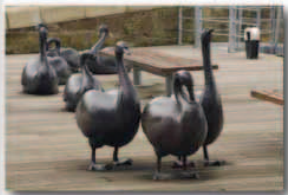
New Housing
- All Income Levels



Create Pedestrian / Bike Environment
- Connections

Scale
- Breakdown of Linear Corridor

Civic Spaces



Lifestyle Activities

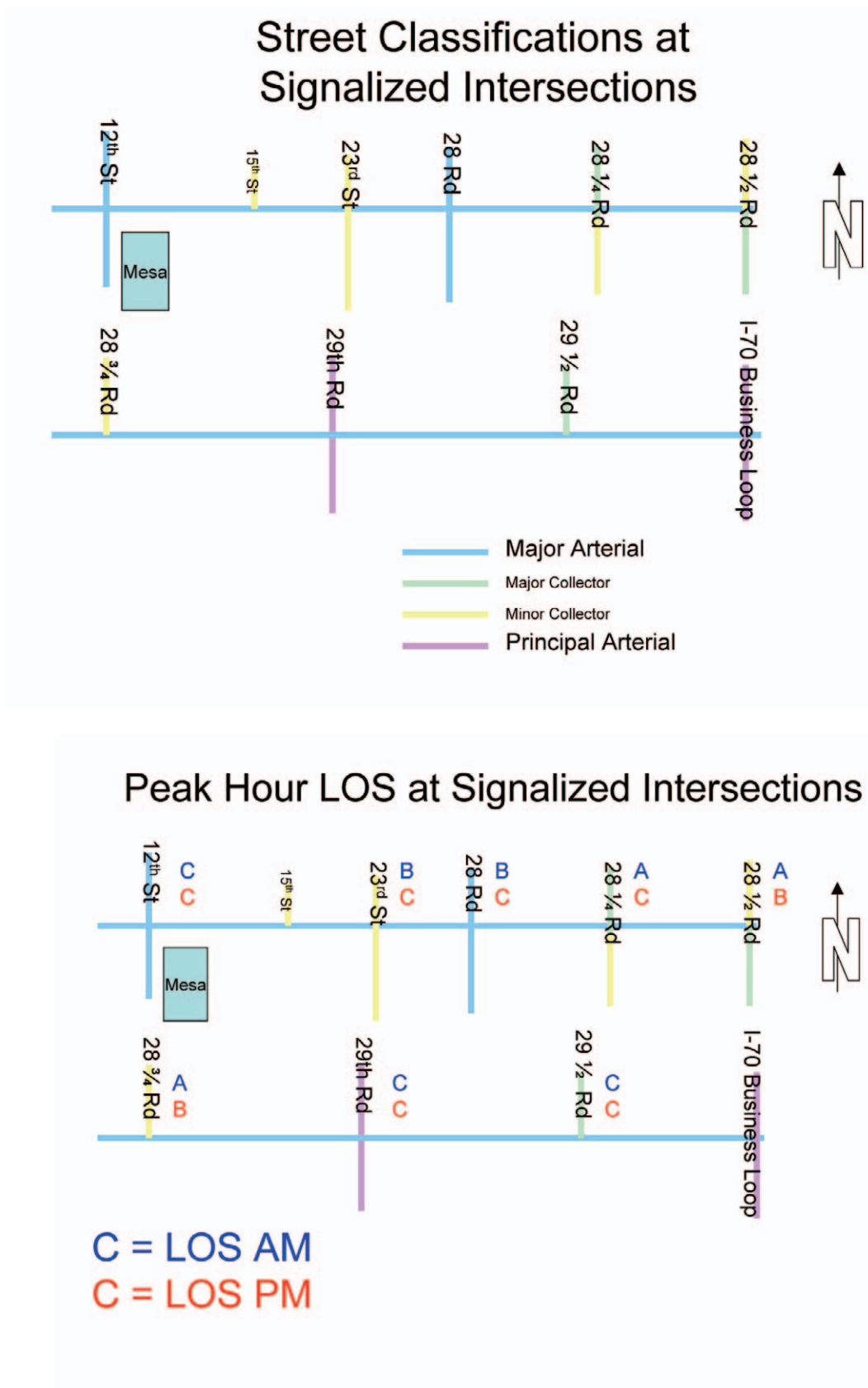


Infill
- Redevelopment to Meet Local Needs

Transportation Analysis of Conditions

Based on data collected by both the City and CDOT, average daily traffic volumes on North Avenue range from 33,400 in the vicinity of 15th Street to 21,200 in the vicinity of 29 ½ Street. On average, the peak hour of traffic represents approximately eight percent of the daily traffic. In the AM peak hour, 58 percent of the traffic is westbound and 42 percent is eastbound. In the PM peak hour, 46 percent of the traffic is westbound and 54 percent is eastbound.

To determine how efficiently and effectively the existing roadway network accommodates the existing traffic volumes, all of the signalized intersections were analyzed by City staff. The results are shown as Levels of Service (LOS). Letters designate each level, from A to F. LOS A, B and C represent the intersection status as under capacity, LOS D is near capacity, LOS E is at capacity, and LOS F is over capacity. LOS D or better is the desired operating performance at signalized intersections. In all cases, each signalized intersection is currently performing under capacity during each peak hour. In addition, an urban street LOS analysis was completed. This analysis is based on average travel speeds for through vehicles on North Avenue. The results indicate that traffic on North Avenue operates reasonably unimpeded at average travel speeds and delays at signalized intersections are not significant (LOS A/B).





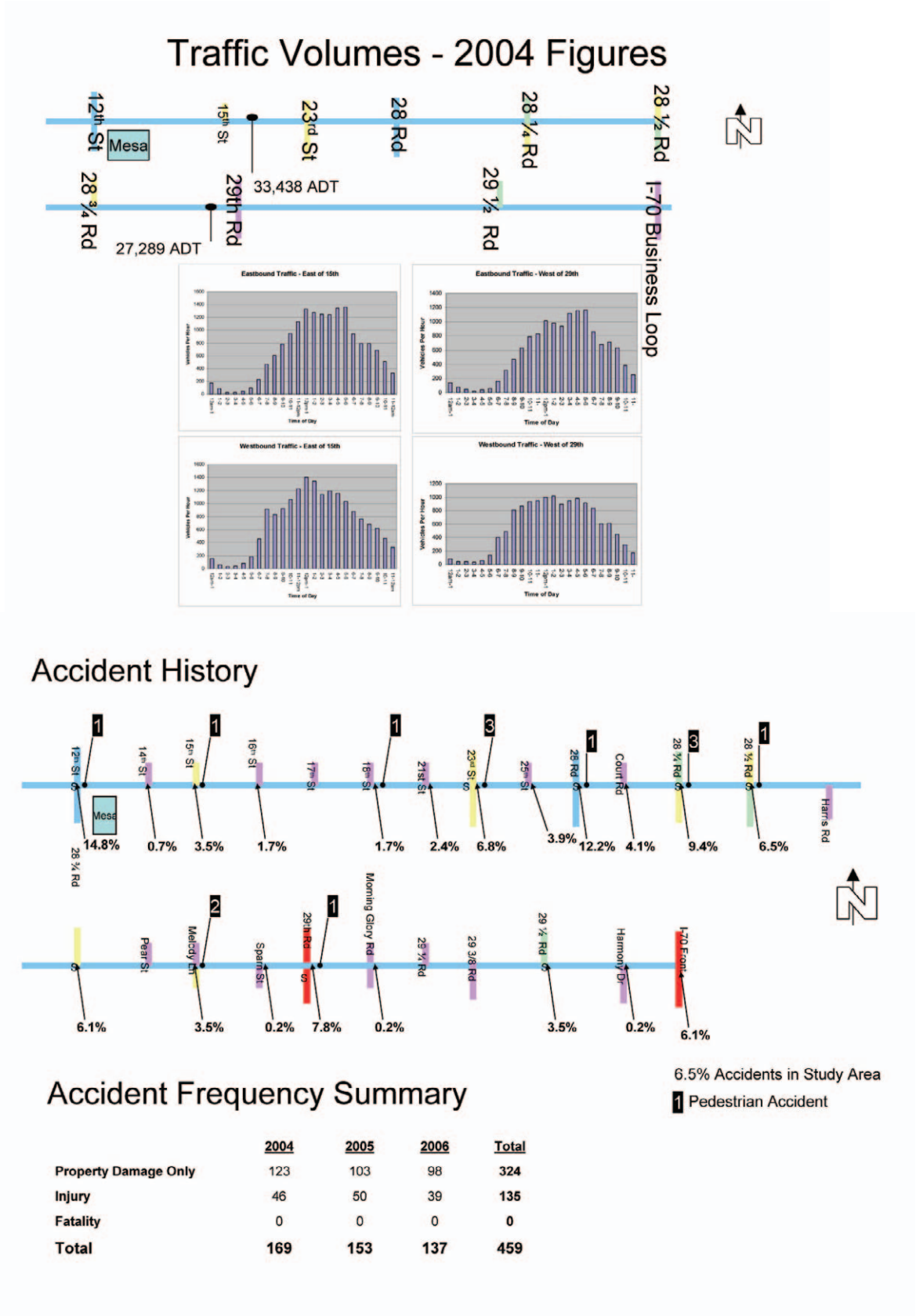
Over a three year period from 2004 to 2006, there was an average of 153 accidents per year on North Avenue. Seventy percent involved property damage only and the remaining 30 percent involved injury. There were no fatalities. Based on the daily usage of North Avenue, this number of accidents is relatively high when compared to comparable roadways throughout the state.

Approximately 70 percent of the accidents on North Avenue involve rear-end, broadside or approach-turn crashes. The rear-end crashes commonly indicate differences in vehicle speed or sudden starting and stopping. The numerous turning movement opportunities throughout the corridor provide the opportunity for broadside and approach-turn crashes.

From 2004 to 2006, there also were 14 pedestrian and 11 bicycle related accidents.

Based on the above analysis, the following can be concluded:

- North Avenue has the capacity to accommodate additional demand that may be generated from new projects in the corridor.
- As the corridor redevelops, access points should be consolidated and properly spaced to minimize conflicts, especially in close proximity to the signalized intersections.
- Pedestrian and bicycle related improvements should be provided to facilitate safer passage both along and across North Avenue.



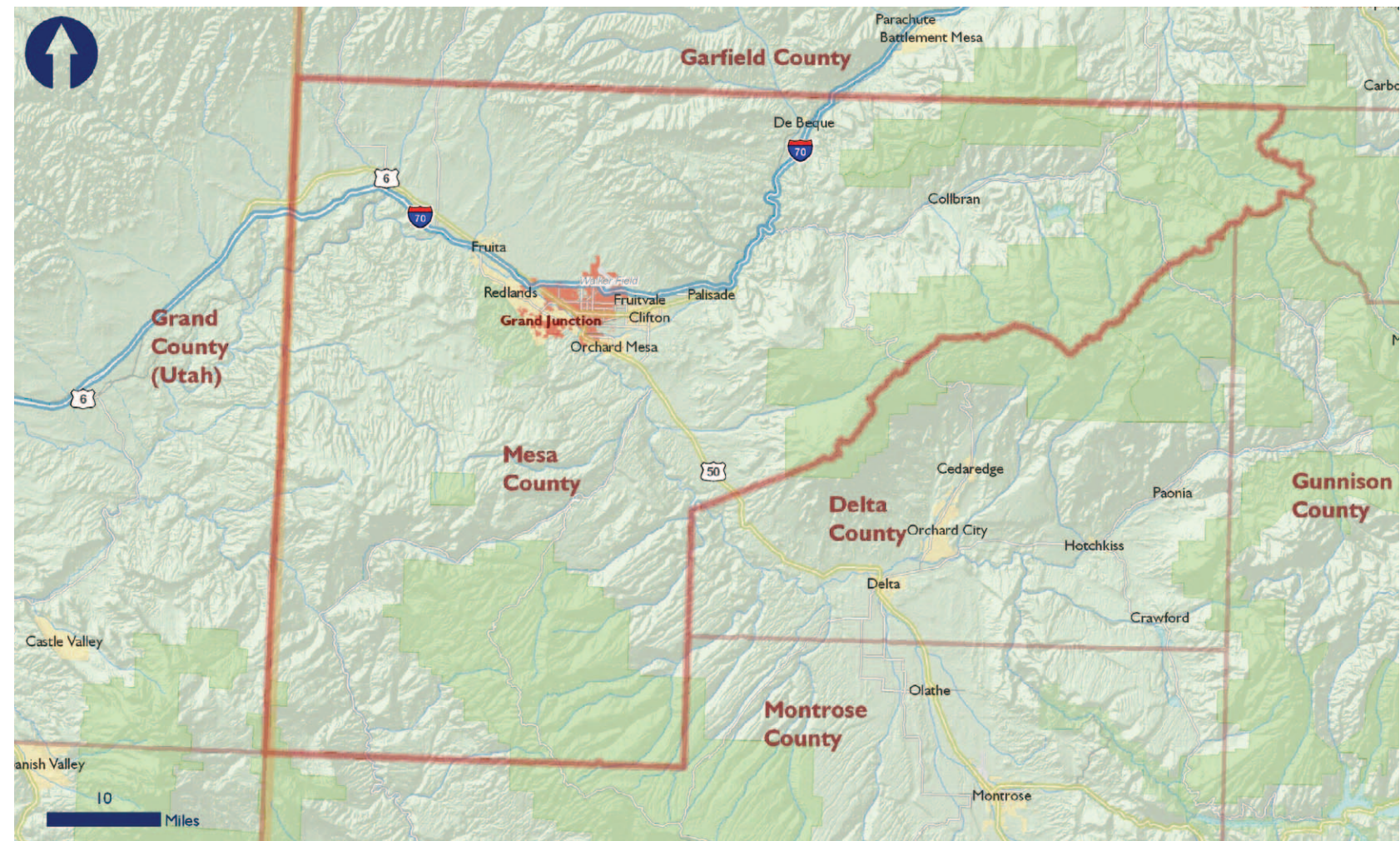
Market Analysis

Older commercial corridors are emerging as targets for revitalization efforts in cities throughout the nation. In virtually every story of success, these new redevelopments have been the result of a holistic approach which capitalizes on each segment of the project, yet positions the sum of the parts as a “place” in and for the community. Experience has proven that forming and advancing the development agenda within these corridors requires a keen understanding of the goals and aspirations of the community, the realities of the marketplace, peculiarities of the political landscape and constraints of local public / private resources. With this understanding, project advocates are then positioned to establish priorities for action that implement the vision.

Methodology

Work completed focused on investigating physical conditions related to the North Avenue Corridor area and its environment, contiguous uses, and access and visibility from the remainder of the community and the region; as well as, economic, financial, and market conditions in the influence area (trade area) of Grand Junction and Mesa County. This was accomplished through visual inspections of the North Avenue Corridor, competitive development areas within Grand Junction, and analyses of primary and secondary data sources.

The analysis that follows has several critical components. The first includes an overview of economic and demographic characteristics that will influence the type of development and/or redevelopment that occurs in the market and specifically within North Avenue Corridor. The second presents a review of those market indicators and trends that provide an indication of the health of the market and economy, including quantified demand by land use type. The third offers a discussion about corridor-wide and node-specific redevelopment elements that present a market opportunity for the study area.



Trade Area (Mesa County and Grand Junction)

The purpose of the market analysis is fourfold:

- Provides a reality check for the land use planning component
- Ensures that recommendations are grounded in market and economic reality
- Sets stage for implementation and policy reform
- Provides an accurate and independent story to tell potential developer / investor audiences

The market analysis attempts to answer the following questions:**For each land use, we ask...****What is the market and how will it evolve?**

- What is the environment today
- Trade area size & shape (should include most likely customers & competitors)
- Who's in the market (describe customer base demographics/economics)
- How will market be affected by "events" which occur over time

What is and will be the competition?

- Types of competition
- Size & performance of competitors
- Potential to leverage on-going investment and existing assets

How much demand is and will be in the market?

- Annual "absorption" (in units or square feet), projected forward

How much total market demand can the area "capture" and how?

- Given the market, the competition, & the nature of the project

How can market demand and capture be influenced over time?

- Impact of public and private events and actions



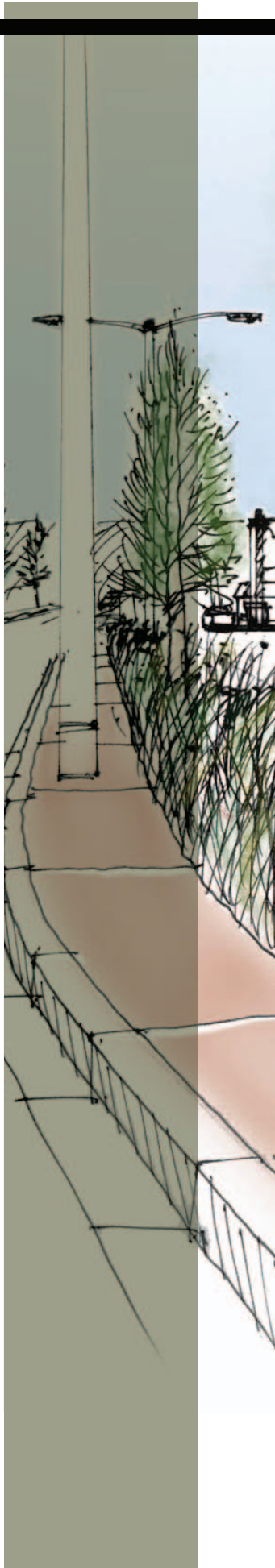
Planning city core redevelopment requires an understanding of the built environment and the people within it. The market analysis, conducted by Leland Consulting Group (LCG) and summarized herein, focused on identifying market opportunities within the region and representative influence or trade area. What the analysis showed was that ongoing and projected trade area growth provides healthy levels of market support for a range of different land use types. Characteristics of the North Avenue Corridor, in particular offer unique opportunities for targeted redevelopment. There is market demand and, with strategic public and private investment and continued policy support, North Avenue Corridor can be positioned to capitalize on niche and destination opportunities that can serve the community and region.

The information below presents an overview of current and future market conditions in Grand Junction and Mesa County. Since North Avenue Corridor represents a sub-market within the overall Grand Junction region (Mesa County), and, as such, will likely compete with projects from a broader influence (trade) area, indicators and conditions for both the City of Grand Junction (the City) and Mesa County (the County) were analyzed. A map of the Grand Junction Trade Area is presented on the following page.

The trade area is that area from which a project (s) or area will draw the majority of its patrons (retail), residents (housing) and employees (office) – that area which will likely be a source of competition and demand. Trade area boundaries for individual store types and specific residential uses will vary from one another and may have irregular shapes as they are influenced by the following conditions:

- Physical barriers
- Location of possible competition
- Proximity to population/employment concentrations
- Zoning
- Market factors
- Drive times, spending

Experience has proven that effective place-making (including revitalization of a key arterial corridor) can increase market shares and draw from larger than traditional trade areas, such as the greater Grand Junction region. For this reason, as well as for simplicity of interpretation and presentation, all market demand in this analysis is shown for either the City of Grand Junction or for Mesa County overall, with North Avenue's attainable market share or "capture" shown as a percentage of this total demand.



Economic & Demographic Indicators

Economic and demographic characteristics in the market are indicators of overall trends and economic health which may affect private and public sector development. The following tables and exhibits highlight those trends expected to affect development demand within the Grand Junction area over the next ten years.

Household & Employment Growth

Grand Junction is the only major metropolitan area on Colorado's Western Slope and as such is a regional trade and service center for much of western Colorado and eastern Utah. Over half of the area's employment base is concentrated in four industries – Retail Trade, Health Care, Leisure & Hospitality, and Government. The share of Mesa County jobs in the manufacturing sector is about equal to the state average but the local economy also draws substantial support from tourism and spending from relocated retirees. The mining sector's influence on the local economy has rebounded in recent years with renewed exploration for natural gas in the Piceance Basin. While mining now represents just 2.9 percent of local workers, it has accounted for 30 percent of the jobs added since 2002 in Mesa, Garfield and Rio Blanco counties.

Strong economic activity, led by the energy sector, together with climate, scenery and other quality of life advantages, is driving robust population and household growth in the Grand Junction region. ESRI (a census-based demographic data provider) projects 2.25 percent annual growth in households for Grand Junction and 2.47 percent growth for Mesa County over the next 5 years. The state demographer projects a 2.25 percent annual growth rate for the county from 2006 to 2016. Real growth in the area has exceeded these rates in recent years due to regional energy-related expansion. To reflect enhanced near term growth, leveling off somewhat in the longer-term, LCG forecasts a blended household growth rate of 2.5 percent annually over the next 10 years. This household growth is slightly higher than the projected population growth rate due to an assumption of gradually decreasing household sizes.

Western Colorado and the Grand Junction area in particular are experiencing robust employment growth driven by a strong energy sector. Statewide unemployment in May of 2007 was 3.3 percent, versus just 2.7 percent in Mesa County. In the past 12 months ending in May 2007, Mesa County's employment base grew by 4.0 percent to 74,393 jobs – almost double the healthy 2.1 percent growth rate statewide over the same period. The Colorado Department of Labor and Employment forecasts statewide annual job growth of calls for from 2004 to 2014 calls for just over 2.1 percent, versus just almost 2.8 percent for Mesa County. Based on these sources, LCG projected 2.8 percent annual employment growth through 2016 for the Grand Junction Trade Area.

Household & Population Characteristics

Demographic characteristics for Grand Junction Trade Area households are summarized in the tables below. Note that for this comparison, the "North Avenue Area" is defined as Grand Ave. to Orchard Ave, 7th Street to 30 Road.

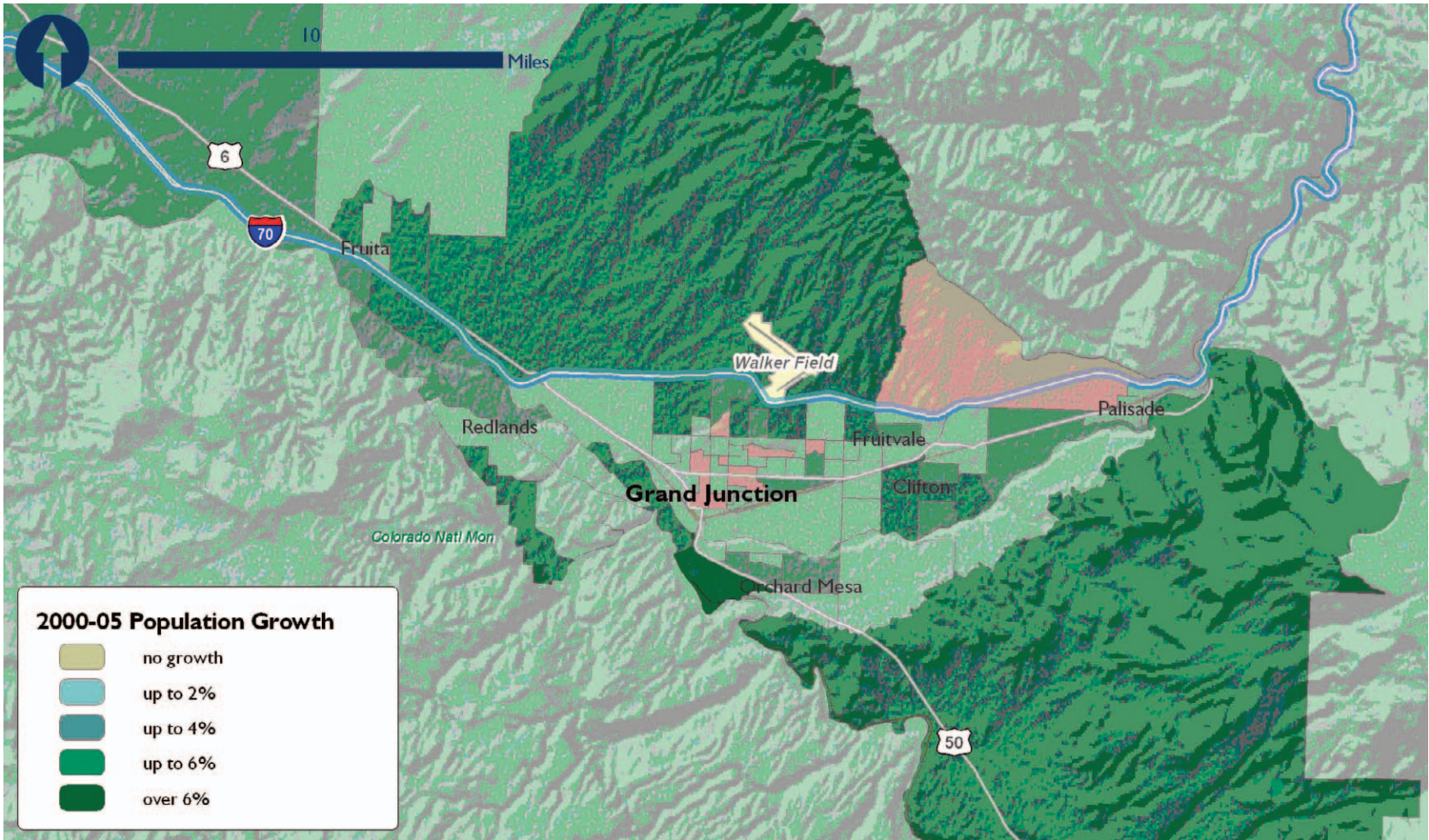


Household Growth

	North Ave Area	Grand Junction	Mesa County
2000 Households	5,405	17,865	45,823
2006 Households	5,548	19,999	53,065
2011 Households	6,082	22,538	60,161
2006 - 2011 CAGR	1.85%	2.42%	2.54%

While the North Avenue vicinity is growing more slowly than the City of Grand Junction and Mesa County – a phenomenon common to older central urban areas – it is still growing at a healthy rate. Grand Junction has 37 percent of Mesa County households, and the North Avenue area has just less than one-third of Grand Junctions households.

The map illustrates the distribution of recent household growth in the Grand Junction area, showing, as in many comparably sized cities, clusters of growth towards the fringe and relatively slow growth in the central city.





Household Characteristics

	North Ave Area	Grand Junction	Mesa County
Pct. Non-family	47%	41%	31%
Household size (2006)	2.20	2.25	2.47
% 1 & 2 Person Households (2000)	44%	31%	23%
2006 - 2011 CAGR	69%	70%	63%

Like the immediate North Avenue vicinity, the city of Grand Junction has households that are smaller, more likely to rent, and more likely to be non-family in nature, versus households in the county overall.

Ethnicity (2006)

	North Ave Area	Grand Junction	Mesa County
White Alone	87%	91%	91%
Black Alone	1%	1%	1%
Asian/Pacific Alone	1%	1%	1%
Other/Multiple	10%	8%	7%
Hispanic Origin	16%	13%	12%

The North Avenue area is slightly more ethnically diverse than the city and county as a whole, with 16 percent of the population of Hispanic origin, up from 14 percent in 2000.

Population by Age (2006)

	North Ave Area	Grand Junction	Mesa County
0 to 24	39%	31%	33%
25 to 34	17%	13%	12%
35 to 44	11%	12%	12%
45 to 54	12%	15%	16%
55 to 64	8%	11%	12%
65+	13	18%	15%
median	30.4	40.8	39.8

Grand Junction has a significant senior population, drawn to the area primarily for its climate. While the city and county have similar age distributions, the North Avenue vicinity has a significantly younger profile. This is due in part to the influence of Mesa State College and somewhat younger-skewing Hispanic households in the surrounding neighborhoods.

Educational Attainment (2000)

(age 25+)	North Ave Area	Grand Junction	Mesa County
Graduate Degree	5%	9%	7%
Bachelor's	14%	17%	15%
Some College	32%	32%	33%
High School Grad.	32%	27%	30%
No H.S. diploma	18%	15%	15%

Education attainment in the North Avenue vicinity is somewhat lower than in the city overall, but more similar to the county-wide profile. Most (58 percent) of Grand Junction adult residents have attended at least some college, versus 64 percent statewide.

Household Income (2006)

Ann. Household Income	North Ave Area	Grand Junction	Mesa County
\$0-25K	40%	32%	27%
\$25-35K	16%	13%	13%
\$35-50K	19%	19%	19%
\$50-75K	17%	18%	21%
\$75-100K	5%	7%	9%
\$100-150K	2	8	8
\$150K+	1	4	4
Per Capita	\$17,500	\$23,959	\$22,761
Med. Household	\$35,316	\$44,111	\$49,249

Household incomes in the county are higher than those in the City of Grand Junction, which in turn are higher than those for the North Avenue immediate vicinity. Because household sizes are smaller in Grand Junction than the county overall, the city has slightly higher per capita incomes, however.

The map illustrates the geographical distribution of households by income in the area. Note that higher incomes are generally found toward the outer edges of the city, especially towards the north side.





Psychographics

Psychographics is a term used to describe characteristics of people and neighborhoods which, instead of being purely demographic, which measure their attitudes, interests, opinions, and lifestyles. These more qualitative descriptions are increasingly used by marketers and planners to help tailor product offerings to suit the tastes of target market segments. Residential homebuilders and commercial retail developers, in particular, are interested in understanding a community’s psychographic profile, as this is an indication of its residents’ propensity to spend across select retail categories. Residential developers are also interested in understanding this profile as it tends to suggest preferences for certain housing product types. The most widely used source of psychographic profiling is the Tapestry - segmentation system offered by ESRI. In the Tapestry system, each neighborhood (census block group) in the United States is assigned to one of 64 distinct lifestyle/psychographic segments.

The top Tapestry segments in Grand Junction are shown in the table below, along with a count of households represented by each segment and an index indicating how prevalent that segment locally as compared to nationally (an index of 100 would be expected given the population, while an index of 400 would be four times the national level). The segments are described in the paragraphs that follow.

Segment	Trade Area Households	Index to U.S.
Midlife Junction	3,655	731%
Prosperous Empty Nesters	2,912	791%
Great Expectations	2,341	660%
Retirement Communities	1,663	557%
Simple Living	1,575	543%
Exurbanites	1,148	235%

Midlife Junction communities are found in suburbs across the country. Residents are phasing out of their child rearing years. Nearly half of the households are comprised of married-couple families and 31 percent are singles, living alone. The median age is 40.5 years and the median household income is \$43,600. A third of households receive Social Security benefits. Nearly two-thirds of households are single-family structures, and the remaining dwellings are mainly apartments in multiunit buildings. These residents live quiet, settled lives. They spend their money prudently and do not succumb to fads. They prefer to shop by mail or phone, from catalogs such as J.C. Penney, L.L. Bean, and Lands' End. They enjoy yoga, attending country music concerts and auto races, refinishing furniture, and reading romance novels.

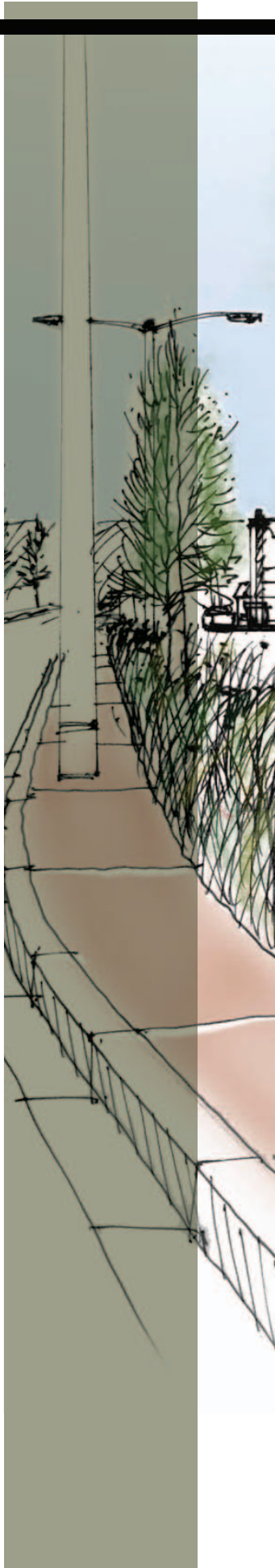
Prosperous Empty Nesters neighborhoods are well established, located throughout the United States; approximately one-third are on the eastern seaboard. The median age is 47.2 years. More than half of the householders are aged 55 or older. Forty percent of household types are married couples with no children living at home. Educated and experienced, residents are enjoying the life stage transition from child rearing to retirement. The median household income is \$66,200.

Residents place a high value on their physical and financial well-being and take an active interest in their homes and communities. They travel extensively, both at home and abroad. Leisure activities include refinishing furniture, playing golf, attending sports events, and reading mysteries. Civic participation includes joining civic clubs, engaging in fund-raising, and working as volunteers.

Great Expectations neighborhoods are located throughout the country, with higher proportions in the Midwest and South. Young singles and married-couple families dominate. The median age is 33.0 years. Labor force participation is high. Manufacturing, retail, and service industries are the primary employers. About half of the households are occupied by owners in single-family dwellings, with a median value of \$100,600, and the other half are occupied by renters, mainly living in apartments in low-rise or mid-rise buildings. Most of the housing units in these older suburban neighborhoods were built before 1960. Residents enjoy a young and active lifestyle. They go out to dinner, to the movies, to bars, and to night clubs. They enjoy roller skating, roller blading, playing Frisbee, chess, and pool, and attending auto races. They read music magazines and listen to rock music on the radio.

Retirement Communities neighborhoods are found mostly in cities scattered across the United States. The majority of households are multiunit dwellings. Congregate housing, which commonly includes meals and other services in the rent, is a trait of this segment dominated by singles who live alone. This educated, older market has a median age of 50.7 years. One-third of residents are aged 65 years or older. Although the median household income is a modest \$45,100, the median net worth is \$172,000.

Good health is a priority; residents visit their doctors regularly, diet and exercise, purchase lowsodium food, and take vitamins. They spend their leisure time working crossword puzzles, playing bingo, gardening indoors, canoeing, gambling, and traveling overseas. They like to spend time with their grandchildren and spoil them with toys. Home remodeling projects are usually in the works.





Simple Living neighborhoods are found in urban outskirts or suburban areas throughout the United States. Half of the households are singles who live alone or share housing, and 32 percent consist of married-couple families. The median age is 40.1 years. Approximately one-third of householders are aged 65 years or older; 19 percent are aged 75 years or older. Housing is a mix of single-family dwellings and multiunit buildings of varying stories. Some seniors live in congregate housing (assisted living). Fifty-five percent of households are occupied by renters. Approximately 40 percent of households receive Social Security benefits.

Younger residents enjoy going out dancing, whereas seniors prefer going to bingo night. To stay fit, residents play softball and volleyball. Many households do not own a computer, cell phone, or DVD player. Residents watch hours of TV per day, especially sitcoms and science fiction shows.

Open areas with affluence define these neighborhoods. Empty nesters comprise 40 percent of these households; married couples with children occupy 32 percent. Half of the householders are between the ages of 45 and 64 years. The median age is 43.6 years. Approximately half of those who work hold professional or managerial positions. The median home value is approximately \$255,900; the median household income is \$83,200.

Financial health is a priority for the **Exurbanites** market; they consult with financial planners and track their investments online. They own a diverse investment portfolio and hold long-term care and substantial life insurance policies. Residents work on their homes, lawns, and gardens. Leisure activities include boating, hiking, kayaking, playing Frisbee, photography, and bird-watching. Many are members of fraternal orders and participate in civic activities.

For North Avenue:

Of the segments described above, Prosperous Empty Nesters and Retirement Communities indicate a particular opportunity for senior-oriented residential development along the North Avenue Corridor. Such development would not need to be age-restricted, but could capitalize on pedestrian-friendly amenities, opportunities for outdoor activities and proximity to convenience-good shopping and medical facilities. The Great Expectations segment, with its relatively high proportion of renters and young couples, would be a promising target for more dense mixed-use residential and retail development along the corridor, particularly in the vicinity of Mesa State College.

Market Demand Estimates

Critical to interpreting The North Avenue Corridor’s competitive position within the region is an understanding of the characteristics of land uses within a defined trade area. In order to identify potential development opportunities among these uses (given the area’s competitive position and prevailing market conditions) demand estimates were prepared.

As stated earlier, because The North Avenue Corridor may have the ability to draw from a larger trade area, demand estimates by land use type were prepared for the broader region. Because of the urban nature of the corridor, residential demand is assumed to be a function of household growth in the City of Grand Junction itself, while retail and office demand is a function of growth in the county as a whole (and beyond, in the case of certain retail types). What follows is a discussion of recent and projected demand conditions for residential, retail, and office uses, along with estimates of attainable capture for redevelopment parcels within the corridor itself.

Residential

With strong residential growth over the past several years Mesa County is permitting an average of 1,430 units per year since 1999 and saw record construction of 1,589 units in 2005. There has been relatively little variation in level of construction in general, with the low point in 2001 seeing 1,253 permits issued. In part because of lower than average interest rates, multifamily construction has accounted for less than 10 percent of this total. Given that increases in energy-sector employment tends to be led by relatively itinerant single males, and that interest rates have been trending upwards, the share of rental units, especially in Grand Junction itself, should increase over time to closer to the existing overall rate of just over 30 percent.

Residential vacancy rates in Grand Junction and Mesa County are among the lowest in the state. Jim Coil Research & Consulting estimates January 2007 vacancy rates of 0.8 percent for ownership units and 2.5 percent for rental units – both well below equilibrium rates and indicative of housing shortages, especially at lower income points.

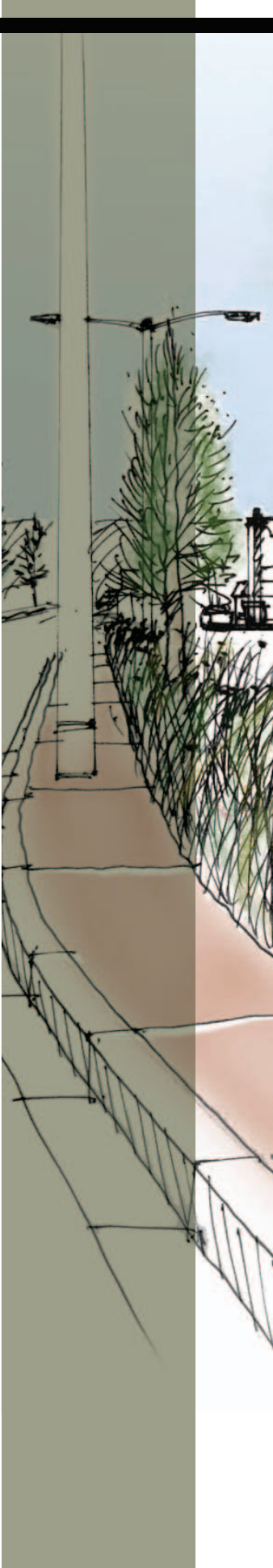
Demand for new residential units is primarily a factor of the growth in income-qualified households within a trade area. For parcels along North Avenue, this trade area is realistically restricted to growth within the municipal limits of Grand Junction. Projected trade area household growth was analyzed along with historical patterns of single- and multi-family development to arrive at an estimated average annual demand for housing in the City of Grand Junction of approximately 600 units per year over the next 10 years.

Residential Demand Analysis
Primary Residential Trade Area
Ten Year Demand Estimates

Households	2006	19,999		
	2011	22,627	Annual Growth Rate	2.50%
	2016	25,600		
	Household Growth (06-16)	5,601	Adjust for 2nd homes, demolition, vacancy	7.0%
	Adjusted Unit Requirement	5,994	Pct. Renters	31%

Annual Income Range (2006 dollars)	Approx. Rent Range	Approx. Home Price Range	Current HHs in Income Bracket	New HHs by Income Bracket	Trade Area Demand from New Households (10-yr)			
					Total Units	Est. Pct. Renters	Total Rental Units	Total Ownership Units
up to \$15K	up to \$375	up to \$75K	17%	15%	899	75%	674	225
\$15-25K	\$375 - \$625	\$75 to \$100K	15%	14%	839	65%	545	294
\$25-35K	\$625 - \$875	\$100 to \$150K	13%	13%	797	35%	279	518
\$35-50K	\$875 - \$1,000	\$150 to \$200K	18%	18%	1,049	20%	210	839
\$50-75K	\$1,000+	\$200 to \$250K	18%	19%	1,139	10%	114	1,025
\$75-100K	\$1,000+	\$250 to \$350K	7%	8%	479	5%	24	456
\$100-150K	\$1,000+	\$350 to \$500K	8%	9%	539	2%	11	529
\$150K and up	\$1,000+	\$500K and up	4%	4%	246	2%	5	241
Totals			100%	100%	5,994	31%	1,862	4,126

Residential Demand – Grand Junction Trade Area





Approximately 1,862 units (or approximately 30 percent) of the Trade Area’s 10-year demand could be in the form of rental units. Attached ownership housing (condominiums and townhomes) appears to be underrepresented in Grand Junction compared to other cities of its size. Of the 3,350 units of ownership demand for those making \$15,000 and above, up to 20% could be delivered in the form of an attached product (assuming appropriate marketing) based on demographic and consumer preferences.

Assuming quality design effort, support from the city in assemblage, regulatory improvements and development incentives, the North Avenue corridor could realistically absorb approximately 200 apartment units and 140 single family attached (condo, loft, townhome, rowhome) units over the next 10 years, as shown in the following tables. Note that this represents a capture rate of roughly 17 percent of market rate (income \$15,000 and up) units in the city as a whole.

Single Family Ownership Attached (townhome, condo, etc.) 10-yr. Demand and North Avenue Capture						
Annual Income Range	Approx. Home Price Range	Trade Area For-Sale Demand	Est. Pct. Townhome/ Condo	Townhome / Condo Demand	Attainable Capture Rate (within attached)	Attainable Subject Capture (units)
\$15-25K	\$75 to \$100K	294	65%	191	15%	29
\$25-35K	\$100 to \$150K	518	50%	259	20%	52
\$35-50K	\$150 to \$200K	839	20%	168	20%	34
\$50-75K	\$200 to \$250K	1,025	10%	102	15%	15
\$75-100K	\$250 to \$350K	456	10%	46	15%	7
\$100-150K	\$350 to \$500K	529	5%	26	10%	3
\$150K and up	\$500K and up	241	5%	12	0%	0
Totals		3,901	20.6%	804	17.3%	139

Multifamily Apartments 10-yr. Demand and North Avenue Capture				
Annual Income Range	Approx. Rent Range	Trade Area Rental Demand	Attainable Capture Rate (within apartments)	Attainable Subject Capture (units.)
\$15-25K	\$375 - \$625	545	15%	82
\$25-35K	\$625 - \$875	279	20%	56
\$35-50K	\$875 - \$1,000	210	20%	42
\$50-75K	\$1,000+	114	15%	17
\$75-100K	\$1,000+	24	15%	4
\$100-150K	\$1,000+	11	10%	1
\$150K and up	\$1,000+	5	0%	0
Totals		1,188	17.0%	201

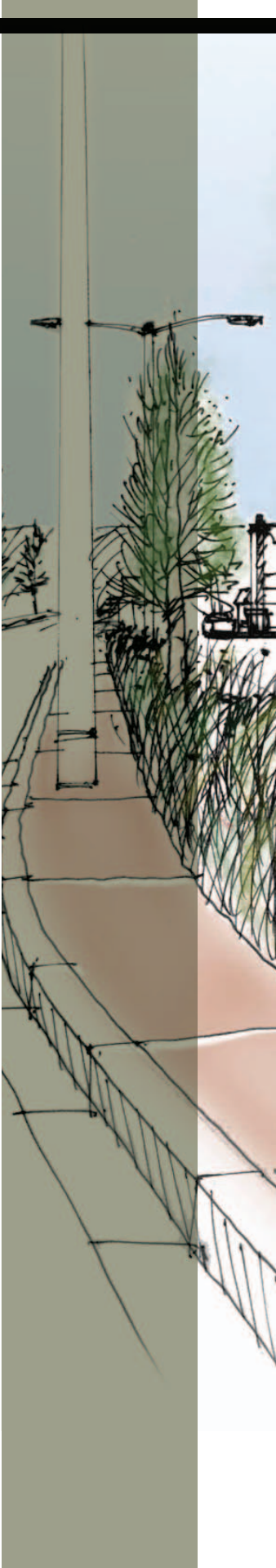
Retail

Demand for new retail space is determined by future retail expenditures by new households. This demand was determined by multiplying growth in households with that portion of household income spent on general retail purchases. An additional adjustment was made to allow for demand from space turnover and obsolescence. Because of Grand Junction’s role as a regional hub, its retail inventory is supported by rooftops well beyond Mesa County itself. We estimate that for 100 square feet of store space supported by Mesa County residents, an additional 80 square feet are supported by residents outside the county. As shown in the following table, Mesa County household growth alone would support approximately 900,000 square feet of new retail demand over ten years. Adding in demand from outside the county (at varying rates depending on the category) and replacement of obsolete space (at a rate of 1 percent annually), Mesa County could see demand for a total of 1.6 million square feet of new retail space by 2017.

Assuming a 15% to 20% capture rate of Trade Area retail demand, The North Avenue Corridor could be expected to add approximately 240,000 to 325,000 square feet of new retail space (again, assuming quality design, marketing, and city support). This demand would likely take the form of neighborhood serving dining and specialty retail in the key nodes around the college and hospital, but could have a significant region-serving component in the area around the improved 29 Road intersection.

Retail Trade Area (Mesa County) Ten Year Demand Estimates					
Category	Demand from Trade Area Household Growth (s.f.)	Regional Draw Factor (Existing Surplus Pct.)	Additional Regional Demand (s.f.)	Turnover/ Obsolescence Demand (s.f.)	Total New Trade Area Demand (s.f.)
Auto Parts, Accessories, and Tire Stores	26,126	55%	14,369	11,164	51,659
Furniture & Home Furnishings Stores	52,694	50%	26,347	18,337	97,378
Electronics & Appliance Stores	22,463	20%	4,493	4,931	31,887
Bldg Mater., Garden Equip. & Supply	43,601	55%	23,981	18,434	86,016
Food & Beverage Stores					
Grocery Stores	181,671	30%	54,501	47,431	283,603
Specialty Food Stores	4,579	25%	1,145	1,100	6,824
Beer, Wine, and Liquor Stores	18,090	65%	11,758	9,686	39,534
Health & Personal Care Stores	25,431	55%	13,987	10,611	50,030
Clothing and Clothing Accessories Stores	54,871	50%	27,435	21,119	103,424
Sporting Goods, Hobby, Book, & Music	31,726	75%	23,794	23,239	78,759
General Merchandise Stores	144,546	45%	65,046	48,184	257,777
Miscellaneous Store Retailers	32,328	75%	24,246	22,891	79,465
Food Services & Drinking Places					
Full-Service Restaurants	55,470	15%	8,321	11,760	75,551
Limited-Service Eating Places	56,506	5%	2,825	11,002	70,334
Special Food Services	4,598	10%	460	955	6,012
Drinking Places	3,521	65%	2,289	1,890	7,700
New Demand in Selected Categories	758,223		304,998	262,733	1,325,954
Demand From Excluded Categories* (20%)	151,645	50%	75,822	52,547	280,013
Total New Locally Supported Demand	909,868		380,820	315,280	1,605,967

*e.g. entertainment, banking, prof./medical office, etc.





Office

Demand for new office space is derived from two primary sources: expansion of existing industry and the relocation of new companies into the market. Employment projections by industry classification for the Trade Area were used to estimate demand over the next ten years. Assuming a 2.7 percent annual growth rate in overall employment, the analysis revealed demand for almost 1,000,000 square feet of new office space over this period. Assuming a 10 percent capture rate, the North Avenue corridor could absorb approximately 95,000 square feet of new office space over the next 10 years.

This demand could take the form of medical, educational and other primarily professional office space at key nodes along the western portions of the corridor, and could be in the form of flex office basic employment space nearer to the 29 Road junction.

Office Demand – Grand Junction Trade Area

Summary of Office Space Demand from Employment Growth							
Trade Area							
Ten Year Demand							
	Est. 2006 Jobs	Annual Job Growth Rate	10-yr. Job Growth	Est. Pct. Office	10-yr. Office Demand from Job Growth (s.f.)	Est. Office Demand From Turnover (10- year)	Total New 10- yr. Office Demand (s.f.)
Agriculture & Mining	733	5.0%	461	35%	36,312	3,208	39,520
Construction	2,377	1.0%	249	5%	2,798	1,486	4,284
Manufacturing	2,701	1.0%	283	5%	3,179	1,688	4,868
Transportation	1,584	2.5%	444	15%	14,973	2,970	17,943
Communication	505	2.5%	141	25%	7,959	1,579	9,538
Electric, Gas, Water, Sanitary	198	2.5%	55	20%	2,496	495	2,991
Wholesale Trade	1,928	1.0%	202	5%	2,270	1,205	3,475
Retail Trade	12,218	3.0%	4,202	5%	47,274	7,637	54,910
Finance, Insurance, Real Estate	3,839	2.5%	1,075	95%	229,822	45,586	275,407
Services (Non-Retail)							
Hotels & Lodging	862	3.0%	296	5%	3,334	539	3,872
Automotive Services	666	3.0%	229	5%	2,577	416	2,993
Entertainment & Recreation	664	3.0%	228	5%	2,568	415	2,982
Health Services	10,962	3.0%	3,770	25%	212,063	34,256	246,319
Legal Services	498	3.0%	171	95%	36,609	5,914	42,523
Educ. Institutions	1,894	3.0%	651	15%	21,979	3,551	25,530
Other Services	7,391	3.0%	2,542	20%	114,382	18,477	132,859
Government	3,774	3.0%	1,298	25%	73,009	11,794	84,803
Other	13	3.0%	5	30%	306	50	356
Totals	52,807	2.7%	16,302		813,909	141,263	955,173

Absorption Summary

The table below summarizes potential North Avenue Corridor absorption of land uses over the next 10 years.

Summary of Potential Absorption – North Avenue Corridor

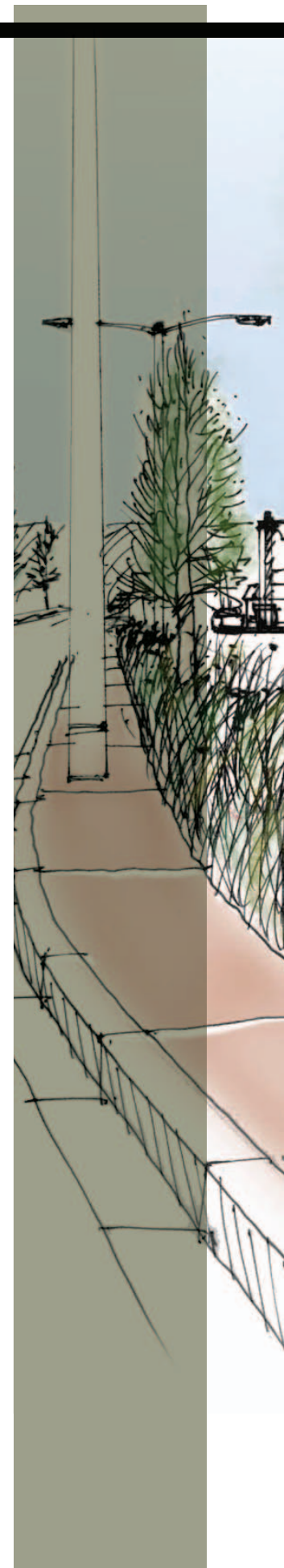
10 Year Demand Summary for North Avenue Corridor			
	Level of Public Sector Support/Investment		
	none	moderate	high
Residential (units)			
Single Family Attached	?	140	200
Multifamily Apartments	?	200	300
Retail (s.f.)	100,000	240,000	325,000
Office (s.f.)	25,000	95,000	150,000

Strategy Considerations

As market opportunities for residential, retail and office space in The North Avenue Corridor occur over the next 10 years, the following strategic implications should be considered:

Residential

- Trend towards town home / condo products in downtown neighborhoods
- Appeals to buyers seeking amenities without maintenance hassles
- Targets empty nesters, young professionals and single parents – the majority of downtown housing residents
- Some more affluent students (and recent students) may have interest in ownership housing near the college
- As interest rates climb, the trend may shift towards apartments -- less investment appeal, but greater flexibility and less commitment
- Conversion of vacant downtown office space into apartment and condominium residential units has been very successful in many urbanizing markets
- Open question as to whether Grand Junction has critical mass of residents with urban tastes to succeed with certain attached residential products
- Will depend heavily on design quality, experience of developer and education of community to overcome any negative attitudes about downtown living
- Investment in infrastructure, streetscape, and other pedestrian amenities also key – including investment in “soft spaces”
- College area near N. 12th Street represents a strong opportunity for vertical mixed use including both ownership and rental residential
- Veterans hospital area near 23rd Street has opportunity (with relocation of some big box retail) for mixed use including senior-oriented housing and even extended stay lodging targeting hospital patients and their families
- Workforce housing price points should be in strong demand throughout the corridor





Retail

- Levels of retail demand suggest support for a range retail product types
- Greatly enhanced connectivity around 29 Road will lend support to some regional retail – not mall scale, but two to three medium to large box tenants with pads and in-line complements
- Neighborhood-supporting services, dining (both limited and full-service) and specialty retail -- in a mixed-use urban environment – could make sense over time around nodes to the west
- Ground-floor retail with office and attached residential above would add overall pedestrian appeal and be mutually supporting

Office

- Major office development anywhere in Grand Junction carries the boom-bust risk
- Professional, medical, education and other small floor-plate office tenants could be attracted to coordinated developments on the west side of the corridor
- Sites further east along the corridor are more amenable to flex office and other employment-generating uses

Mixed-Use

- For new construction, three to four-story densities (in appropriate locations), at a human scale, with streetscaping and a mix of uses, would create a more user-friendly experience for:
 - Employees
 - Prospective residents
 - Visitors
 - Shoppers

1ST OPEN HOUSE - Public Comment Summary

In order to obtain the most information from the public during the open house, the following board and questionnaire were used. The EDAW team along with City staff collected commentary on the boards and concepts presented. The public was able to comment directly and were asked to also fill out the questionnaire in order to obtain the best guidance and direction for the project.

Community Goals / Vision

North Avenue Corridor

8 March 2007

What uses are lacking in North Avenue corridor ?

What are the best / worst qualities of the corridor ?

What is (or should be) the character(s) of North Avenue ?

What words best describe this area ?


What is (or should be) the theme of the corridor ?

What would you like the city to implement and invest in ?

How do you use the corridor ?

- Shopping
- Entertainment
- Access

EDAW / AECOM • LELAND CONSULTING GROUP • MATRIX DESIGN GROUP



NORTH AVENUE CORRIDOR QUESTIONNAIRE

Please take a moment and fill out this survey regarding the North Avenue Corridor from 12th Street to I-70-BI.
Your valued thoughts and ideas will help shape the vision for this corridor.

8 March 2007

What is your biggest concern regarding this section of North Avenue?

Are there currently any safety issues you would like see addressed?

What would you like to see improved or changed?

What uses are lacking in the corridor that you would use (retail, commercial, services, housing)?

Why and how frequently do you come to this corridor?

What type of activities are missing in the corridor that you would like to see?

How do you access the corridor area?



CITY OF
Grand Junction
 COLORADO



The Open House served as a workshop where the public was able to discuss with the EDAW team and City Staff items of concern as well as their desires for the improvement of the North Avenue corridor. The following list presents the primary items expressed by the public.

NEIGHBORHOOD CONCERNS:

- Aesthetics
- Safety
- Impact of Development on Neighborhood
- Homeless Shelter
- Current Lack of Identity
- Financial Impact to Business
- Incentives
- Loss of Neighborhood Identity
- Illicit Activity
- Dark Sky / Environmental Practices

ISSUES:

- Homeless Shelter
- Trailer Parks
- Undesirable Residential Homes / Lots
- Cruising of Large Empty Parking Lots
- Fence Height of Golf Park
- Bus Stop Locations
- North Access to Patterson
- Right Turns
- New Medians
- ADA Accessibility

DESIRED IMPROVEMENTS / CHANGES:

- Sidewalks
- Landscaping
- Traffic / Crossing at 12th Street
- Signage
- Ditches
- Overhead Utilities
- Access to Business (Vehicular and Pedestrian)
- Bike Paths
- Senior Affordable Housing
- Consolidate Curb Cuts

USES LACKING:

- Retail Anchor
- Neighborhood Serving Retail
- Quality Sit-Down Restaurants
- Affordable Quality Housing
- Mixed Housing Opportunities
- Quality Senior Housing Near Amenities
- Quality Grocer
- Quality Hotel
- Entertainment (After 5) Opportunities



The following document reflects the summary information attained at the first open house workshop. It includes the summary information from all of the public commentary received verbally and written. All of this information will help guide the approach scenarios of the potential districts and implementation strategies that the North Avenue corridor could take.



OPEN HOUSE

Initial Open Session at City Offices: Greg Palmer (Council Member), Kathy Portner, Scott Peterson, David Thornton, (City of Grand Junction), Ted Kamp (Leland), Steve Wilensky, Silvia Kjolseth (EDAW)

Date: March 8, 2007 Time: 1:00 PM
Project: Grand Junction, North Avenue Corridor Plan Project No.: 06180105.01
Subject: Open House

Council Session Comments

- Consistency in theme to consider for entire corridor.
- Maintain viable east – west traffic corridor use.
- Linkage of bike paths and sidewalks to the surrounding neighborhood to create a more walkable and bike friendly community.
- How is development initiated, what are the catalyst projects?
- Cultural aspect should be maintained along the corridor, ie: art, history of area, 1st – 12th original square mile.
- Identification of neighborhoods to the corridor and their presence and representation on the corridor needs to be strengthened.
- The 29th Street connection will change and affect the districts with a retail focus.
- Current zoning could lead to further heavy commercial uses unless identified and changed.
- Indian wash canal could potentially be a significant identity.

Public Open House 4:30pm - Compiled Comments

Public Attendance 44

Concern Regarding North Avenue

- 23 Aesthetics
- 20 Safety
- 16 Impact to Neighborhood (primarily related to the homeless shelter)
- 12 Lack of Identity
- 5 Financial Impact to Business (drive thru only – corridor not used for destination to local businesses)
- 5 Incentives
- 4 Illicit Activity
- 2 Loss of Identity
- 1 Dark Sky Compliance

Current Safety Issues to be addressed

- 15 Homeless Shelter
- 5 Trailer Parks
- 5 Infill
- 3 Large Assembly of Lots
- 3 Golf Fence Height (errant balls to R.O.W)
- 1 Undesirable Residential (mobile homes)
- 1 North Access to Patterson
- 1 Bus Stops
- 1 Right Turns
- 1 Pedestrian Crossings (primary concern at 12th intersection with students)
- 1 New Medians
- 1 ADA Access

Items to Improve or Change

- 30 Sidewalks (landscaping)
- 10 Traffic Congestion (specific to students crossing at 12th)
- 9 Signage
- 9 Bus Stops / Transportation
- 8 Traffic Lights
- 7 Ditch – Visual / Physical Hazard
- 6 Overhead Utilities
- 5 Violence
- 4 Access
- 4 Include Bike Paths
- 2 Remove Vacant Buildings
- 2 Drag / Cruise Street
- 1 Install Round-About
- 1 No Round-About
- 1 Senior Affordable Housing
- 1 Consolidate Curb Cuts
- Landscape
- Medians Improvements / Trees
- Remove Trailers / Mobile Homes

Uses Lacking

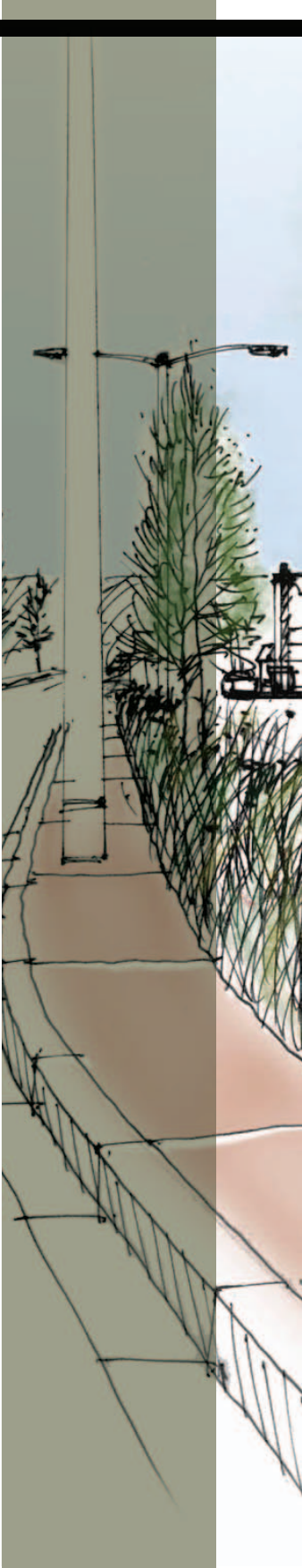
- 10 Retail Anchors
- 2 Commercial
- 10 Quality Sit-down Restaurants
- 2 Services
- 9 Affordable housing
- 2 Housing
- 1 Mixed Housing
- 5 Quality Hotels
- 4 Medical Office
- 4 Open Space / Civic Space
- 2 Car Wash
- 2 Entertainment
- 2 Quality Grocery
- 1 Human Services
- 1 Festivals
- 1 Water Feature
- 1 Parking

Current Use of Corridor by Open House Attendees

- 16 Shopping
- 14 Work
- 12 Services
- 5 Avoid Corridor
- 3 Transportation Only

Individual Access of Corridor by Open House Attendees

- 29 Private Vehicle
- 3 Walk
- 2 Bike
- 0 Bus



APPROACH

The first Open House provided the team with insight on the public priority goals and expectations for the project. (See Open House Notes) The Team proceeded with preparation of three alternative plans.

Common features carried through the alternative plans:

- Senior Housing
- Residential Multi-Family
- Grocer
- Big Box Retail
- Hotel
- Civic

The unique features within each alternative include:

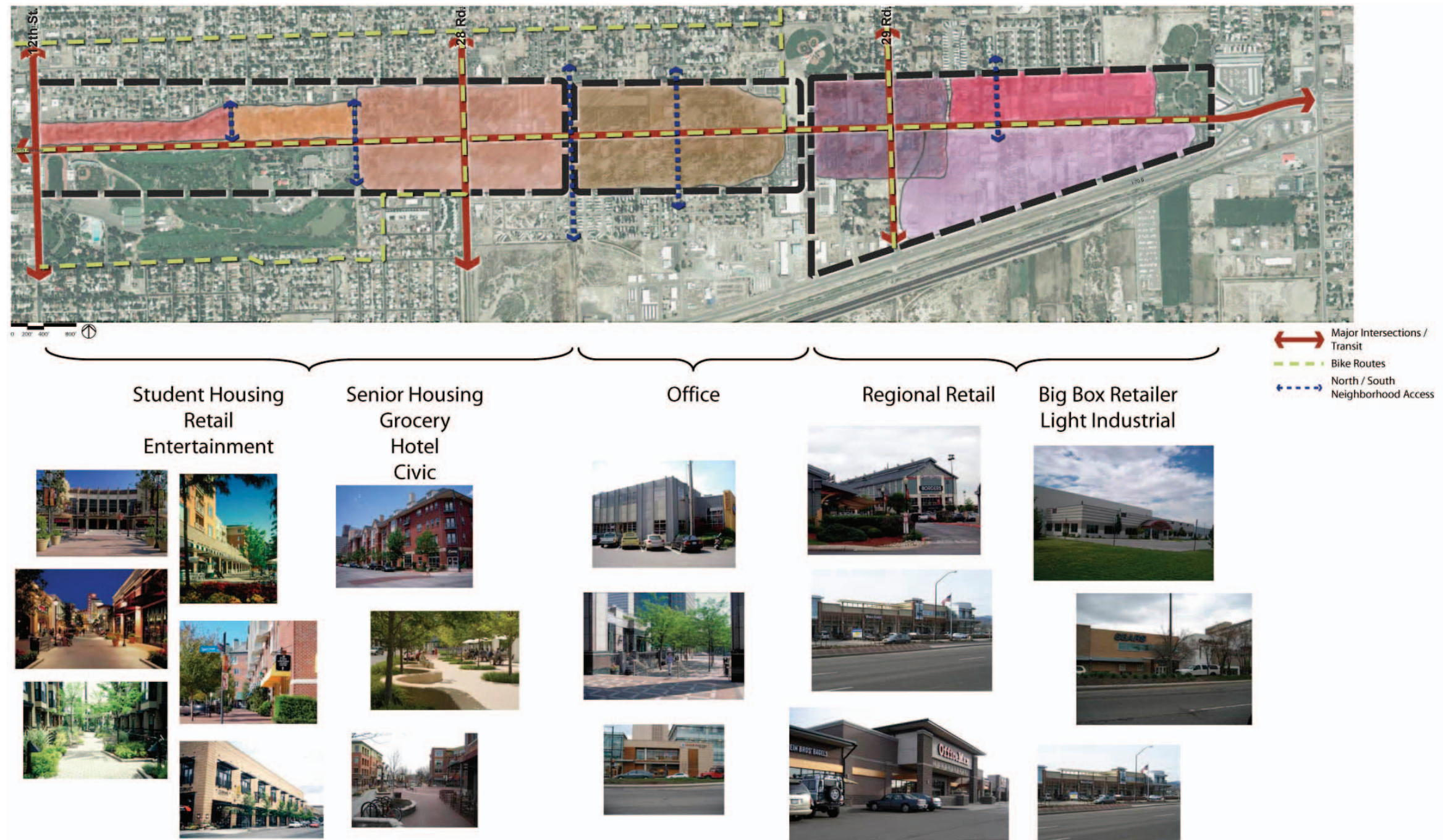
- Student District
- Entertainment District
- Residential Apartments
- Regional Retail
- Neighborhood Retail
- Lifestyle Center
- Light Industrial

Following are three alternatives presented illustrating the proposed districts and their connections to the surrounding neighborhood. These boards depict potential uses (commercial, residential, office, etc.) in three alternative layouts along with character images to depict that portrayed potential building mass and architectural character style.



The predominant elements of this alternative are:

- An Entertainment District in proximity to the students at Mesa College
- A Public Civic Open Space in correlation with Senior Housing, Grocer and a Hotel to create a focus at the intersection of 28th Rd.
- A high concentration of Office acts as a transition to the Regional Retail uses
- The Regional Retail would be a focus for the intersection of 29 Rd. serving as a retail gateway for higher intensity Big Box Retailer on the north and a Light Industrial Park to the south.



APPROACH - District Alternative 2

The predominant elements of alternative two are:

- A Neighborhood Retail Use in proximity to the students at Mesa College mixed with Residential as it continues to the east
- The 28 Rd. intersection focuses on combining Senior Housing with Retail and Grocer along with a Civic component
- Office and Residential transition the neighborhood services to the primary focus of Office and Hotel with some lighter retail uses along the 29 Rd. intersection
- The far eastern edge of the corridor places Residential uses to continue from the neighborhood on the north side and a Big Box retailer to take advantage of the southern large property area



The predominant elements of alternative three are:

- A Residential District near the Mesa College to transition to Neighborhood Retail uses
- The Neighborhood Retail uses would then be combined with Senior Housing, a Grocer and Hotel and minimal Office at the 29 Rd. intersection
- A Residential district would include a greater Regional Lifestyle Center component transitioning to Retail and Civic uses to establish the 29 Rd. intersection
- From the 29 Rd. intersection eastward a large Big Box district would transition to the bypass connection





During the 2nd Open House, the attending public was asked to place preferencing dots on those images which where preferred (green) and not preferred (red). These preferencing boards will help further guide the direction of one preferred alternative of districts to North Avenue.

Alternative 1 appeared to have greater preference of student housing, retail and entertainment district, as well as the senior housing and grocer. District not preferred in this alternative were the greater regional retail, big box and light industrial concepts.

Alternative 2 obtained greater preference for the more residential neighborhood and apartments rather than the larger big box and higher density apartments.

Alternative 3 again reiterates the desire from the public to have more neighborhood scale apartments rather than the larger scale apartment buildings. Again the desire for a retail and civic component that speaks to a neighborhood scale instead of a large retail component that is more regional in nature was evident.

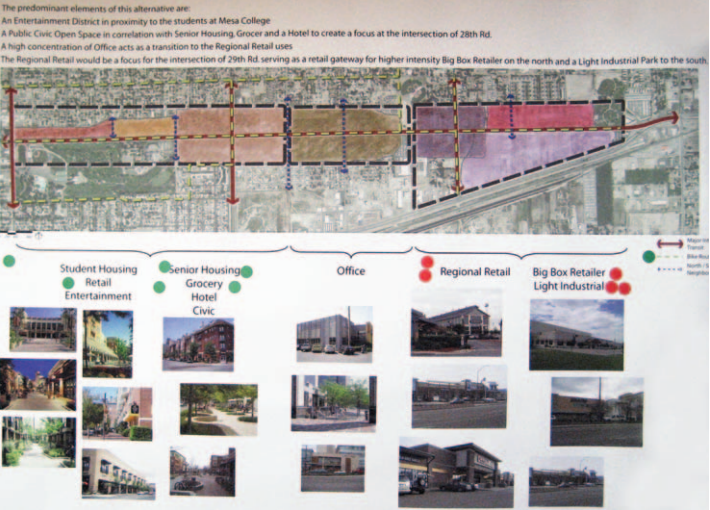
Based on this information obtained a preferred district alternative plan was created to further guide the appropriate revitalization on the North Avenue Corridor. This preferred alternative is presented in the Summary and Recommendations portion of this report.

Additional Public Comment

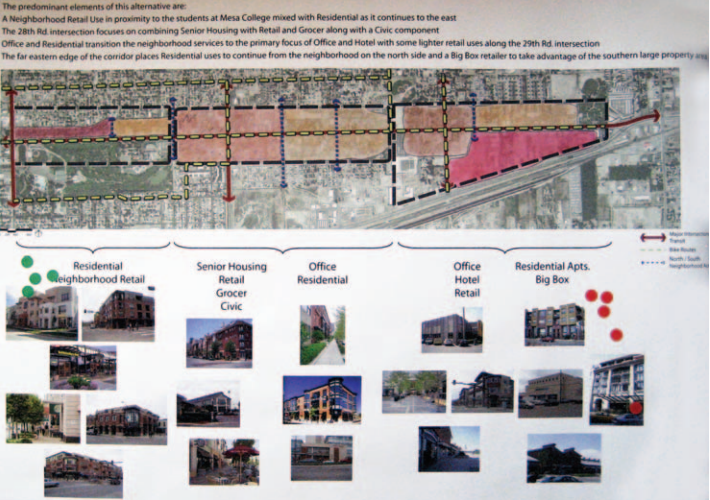
Public comment received during the open house was consistent with that heard at the first Open House. Additional comments received included; the need and desire for more high quality restaurants, the need for quality senior housing in proximity to grocery amenities, improved streetscape and pedestrian realm. Additionally the proposed student housing and entertainment district was well received along with greater mixed use development that would promote a neighborhood and community district where amenities and services are in proximity to housing.

Public Comments regarding the realization of improvements and redevelopment to North Avenue spoke of the need for the City to actively be involved in land assembly and incentives to propel any development. Incentives as corridor-wide enterprise zone, tax rebates and mixed use zoning, where voiced, The investigation of current disincentives that prohibit residential on commercial properties was also noted.

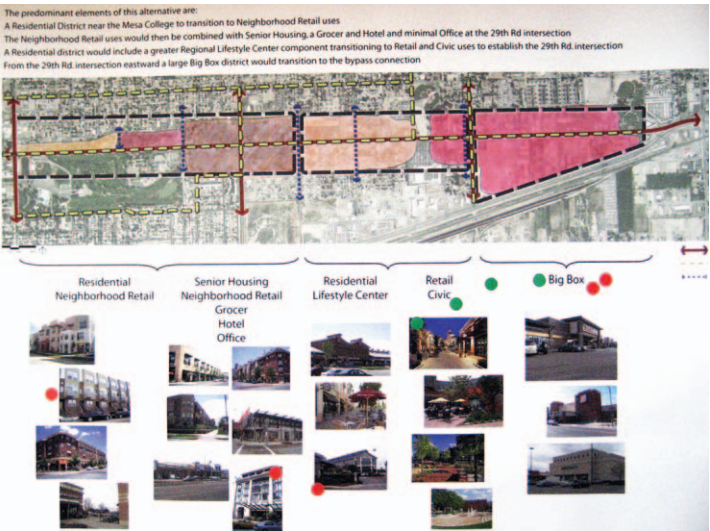
Alternative 1



Alternative 2



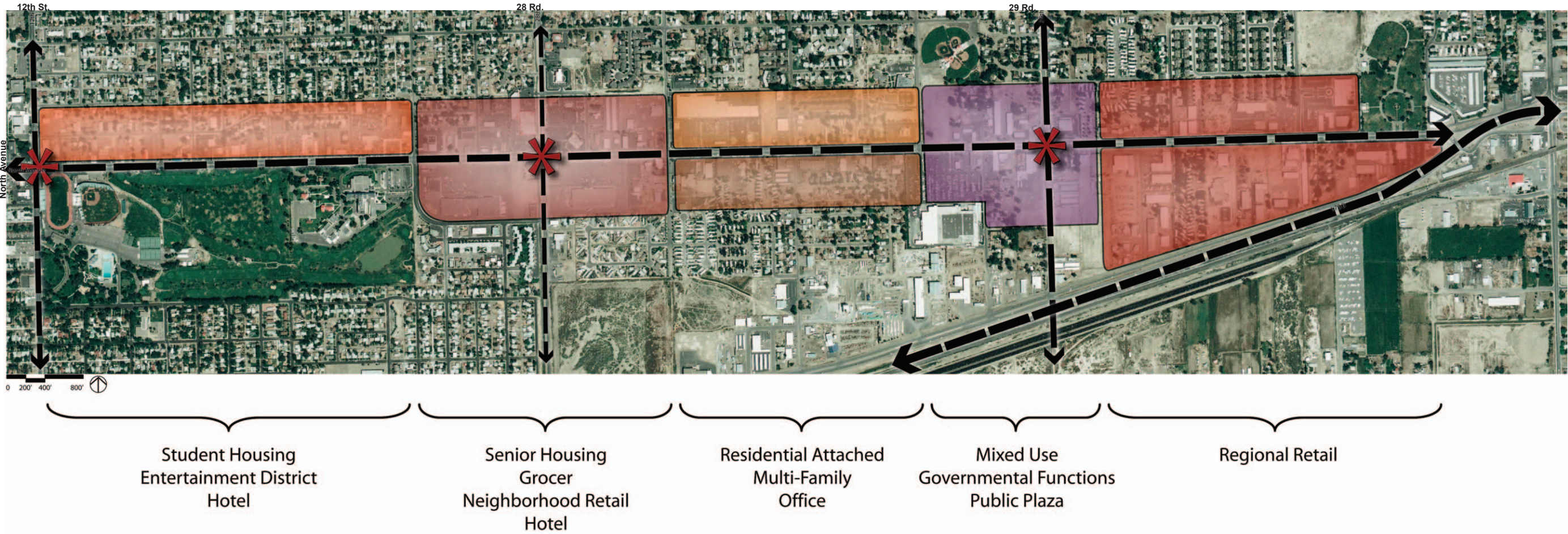
Alternative 3





The predominant elements of the preferred alternative are:

- A Student and Entertainment District in proximity to Mesa College would also provide student housing, after 5 activities and the opportunity for a quality Hotel to serve the area
- A Neighborhood Center where Senior Housing located in proximity to a quality Grocer, Neighborhood Retail, and the potential for a Hotel to serve the hospital and surrounding neighborhood
- A Multi-Family and Attached Residential district connecting to the neighborhoods to the north, and potential for Office space to the south
- A Mixed Use area with residential over retail incorporated with the Governmental Functions and Public Plazas to create a gateway for the 29 Rd. intersection
- Regional retail to anchor the east end of the corridor



Key Projects

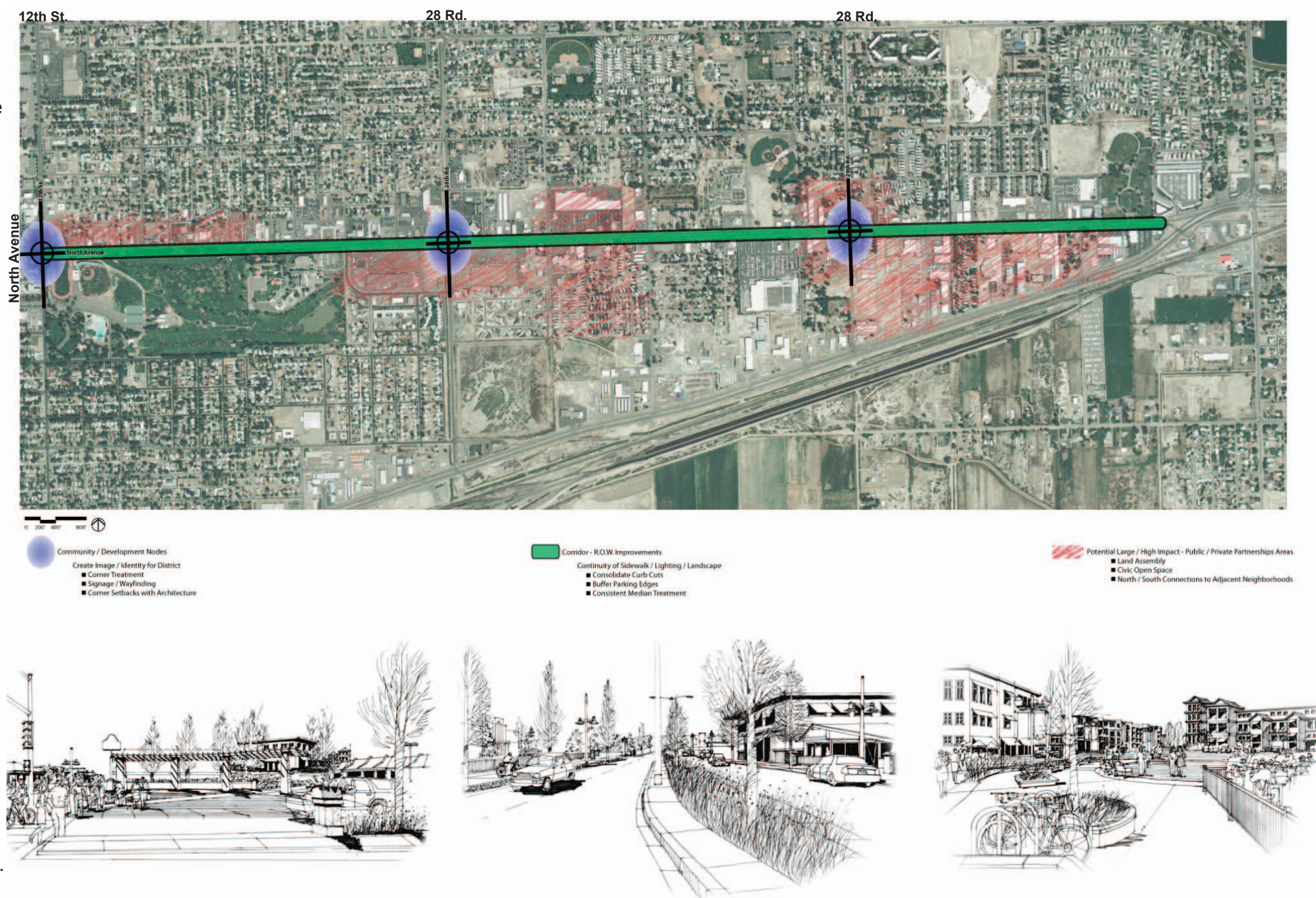
The Key Projects board presented the three catalytic areas identified that would provide the greatest results for improvement and attraction of the North Avenue corridor. These Key Projects are the primary areas in which investment can best empower the revitalization of North Avenue.

Concentrated efforts in streetscape treatment, community identity, way finding, signage and architecture at the key intersections of 12th Street, 28 and 29 Roads would launch the future development of the corridor area.

The overall improvements of the streetscape, primarily around the community / development nodes would set a foundation that is enticing for new development and improves the reputation and use of North Avenue.

Areas surrounding the Key Development Nodes provide the greatest opportunity for land consolidation and large development projects that would create the greatest impact and results.

Artist renderings provide a visual character and feel for these key development concepts proposed.



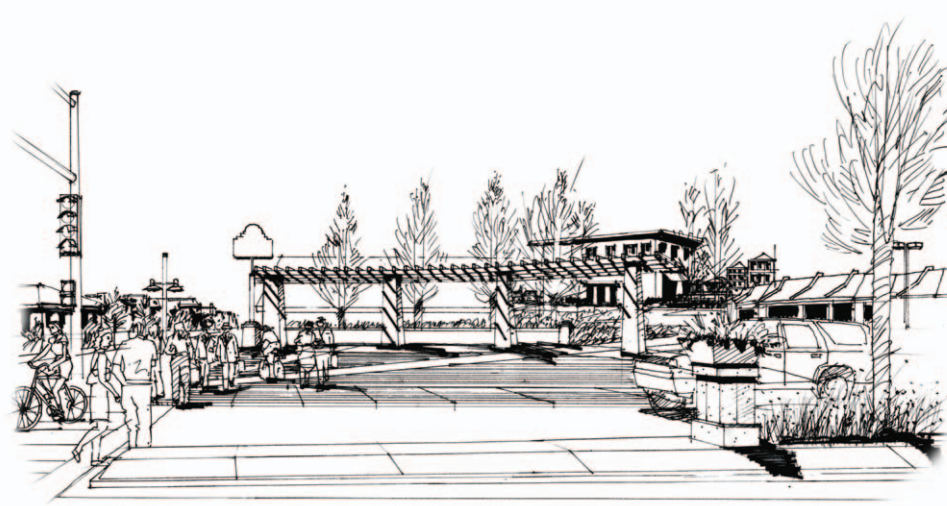
SUMMARY & RECOMMENDATIONS - Key Projects



The illustration above presents a vision of a civic plaza where the intersection treatments would provide for clear and safe pedestrian traffic movement. The civic space would serve as a gathering space and as an entry marker giving the North Avenue Corridor an identity. Existing restaurant spaces could be maintained while new residential development would be tucked around existing building pads.

Such projects could be realized at the intersection of North Avenue and 12th Street, where a student and entertainment district would create a marker and gathering space for students as well as the surrounding community. This potential project could serve the needs for higher quality restaurants, entertainment, and a student housing district.

This key project ultimately could serve as the catalyst to entice development and the further revitalization of the greater North Avenue Corridor.



Improvements to the streetscape are another key project that would further promote the revitalization of this corridor. A pedestrian friendly environment using native, xeric plant materials that are easily maintained while yet providing an appealing and beautiful streetscape are important to attract pedestrian traffic.

Median cuts and numerous parkinglot accesses will be consolidated. Traffic level of service and access will not be impacted.

Intersections and medians would be improved while a cohesive kit-of parts would bring character to the corridor. Improved sidewalks, lighting, street furniture and signage will improve the pedestrian, bike and vehicular access allowing for retail and commercial to thrive in a safe environment.



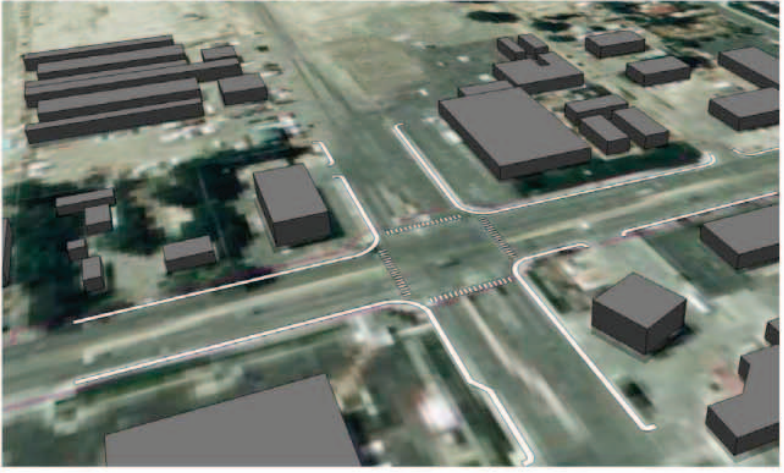
Finally, the assembly of parcels of land where a large development can build momentum and entice greater investment would bring the vitality back to North Avenue. The illustration presents retail and commercial uses with residential above combined with a plaza and patios. Senior housing, a grocer and neighborhood retail provide an environment to attract individuals interested in living within a vibrant community. Surrounding neighborhoods would further sustain the community node.

Infill development as this, will assist in a neighborhood center as well as a regional destination promoting activity beyond 5:00 pm and on weekends.

The phasing of development would allow for existing buildings and uses to slowly become integrated into the larger vision of the North Avenue Corridor. Illustrated below is an example of how such phasing could occur:

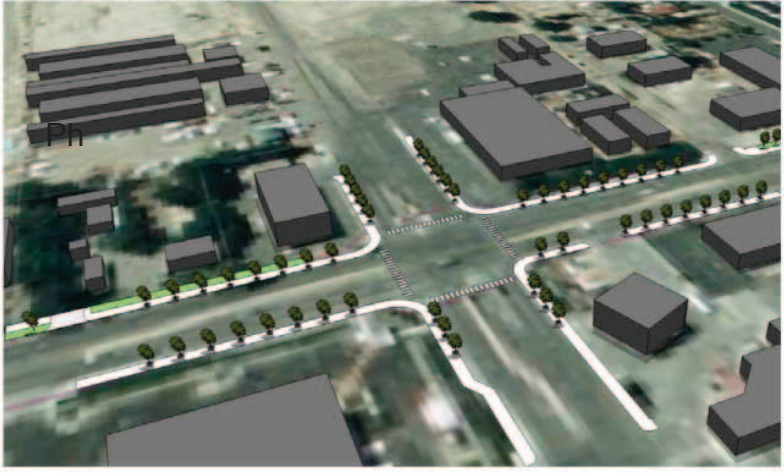
1

Existing Condition: Lacking a consistent streetscape and character to create a sense of place for pedestrians and vehicles.




2

Streetscape Improvements: Sidewalk and landscaping with greater emphasis along the intersections of 28th and 29th roads.




3

Phase 1: Corner developments that bring focus and attention to areas of higher impact. Potential for Civic spaces to activate the public realm.




4

Phase 2: Further continuation of development of vacant land with residential uses for added activity and support the commercial area.




5

Phase 3: Additional supporting commercial and mixed use development brought forward with civic space to create a gateway and sense of place.



6

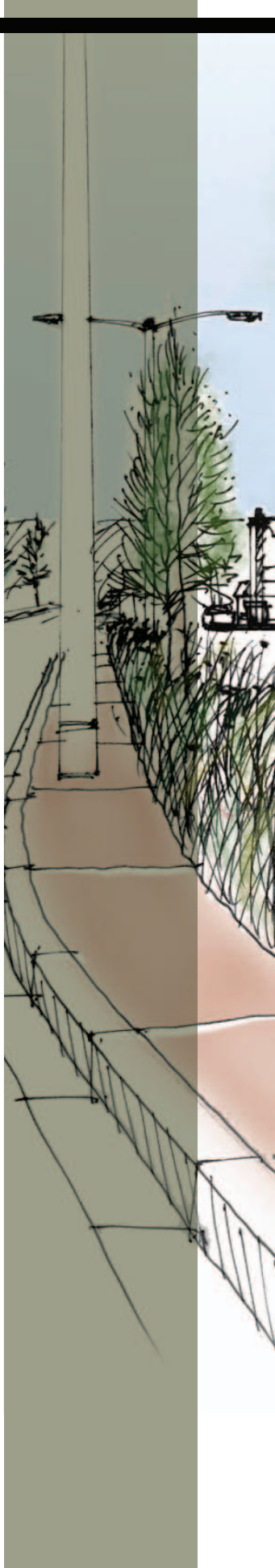
Phase 4: With further consolidation of parcels can provide opportunity for larger retail anchors (potential grocer).



Mixed Use (Residential or Office Over Retail)

Single Family Attached / Multi Family Residential

Mid Box / Anchor Retail



There are 25 intersections within the North Avenue study area, not including driveways to abutting property. Sixty percent are T-intersections (a three-leg intersection) and 40 percent are cross-intersections (a four-leg intersection). Intersections, by their very nature, create numerous conflicts between vehicles, bicycles, and pedestrians. For instance, there are 32 vehicle/vehicle conflict points and 16 vehicle/pedestrian conflict points at every four-leg intersection.

The design of an intersection requires a balance between the needs of vehicles, pedestrians, bicyclists, freight and transit. The following are attributes of good intersection design for pedestrians, as documented in AASHTO'S Guide for the Planning, Design and Operation of Pedestrian Facilities (2004).

- Clarity – Making it clear to drivers that pedestrians use the intersections and indicating to pedestrians where the best place is to cross;
- Predictability – Drivers know where to expect pedestrians;
- Visibility – Good sight distance and lighting so that pedestrians can clearly view oncoming traffic and be seen by approaching motorists;
- Short Wait – Providing reasonable wait times to cross the street at both unsignalized and signalized intersections;
- Adequate Crossing Time – The appropriate signal timing for all types of users to cross the street;
- Limited Exposure – Reducing conflict points where possible, reducing crossing distance and providing refuge islands when necessary; and
- Clear Crossing – Eliminating barriers and ensuring accessibility for all users.

Signalized intersections typically have marked crosswalks, Walk/Don't Walk indications, and regulatory signing. Motorists expect pedestrians and pedestrians are reasonably well protected when crossing at these locations. The study corridor currently has nine signalized intersections. Most of these signals are located at one quarter mile intervals. The most notable exception is between 12th Street and 23rd Street, where there is no signalized intersection for approximately three quarters of a mile. In urban areas, pedestrians must be able to cross streets at regular intervals. They rarely will go more than 300 to 400 feet out of their way to take advantage of a controlled intersection. Consequently, the crossings at uncontrolled locations deserve serious consideration as urban arterial corridors redevelop. Treatments commonly used on higher-volume multilane streets throughout the United States include: high-visibility markings with double-posted pedestrian crossing signs; refuge islands; flags; and flashing beacons. Signals that are used just for pedestrian crossings are used primarily at mid-block crossings.



Recommendation For Streetscape Kit-Of-Parts

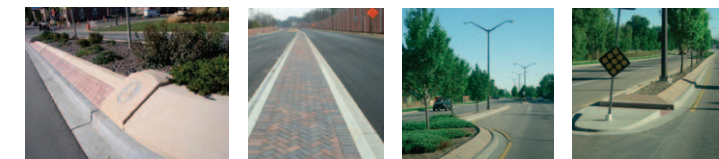
The following illustration and images serve as a guide of how the streetscape character using amenities can create a cohesive and unified streetscape.



① Benches



② Trash



③ Median



④ Plantings



⑤ Lighting



⑥ Bus Shelter



⑦ Crosswalks

The creation of a more unified street edge condition along North Avenue will improve the overall character of the corridor. This can be accomplished by consolidating existing curb cuts and parking lots, providing more definition to vehicular entry ways, and adding walks and plantings. The addition of pedestrian scale street lights, trees, benches and other amenities will create a more pedestrian friendly environment, and encourage local residents to walk or bike along the corridor. Most of these improvements can be made within the existing street ROW. Suggested improvements outside of the ROW can be made without the removal of existing structures.

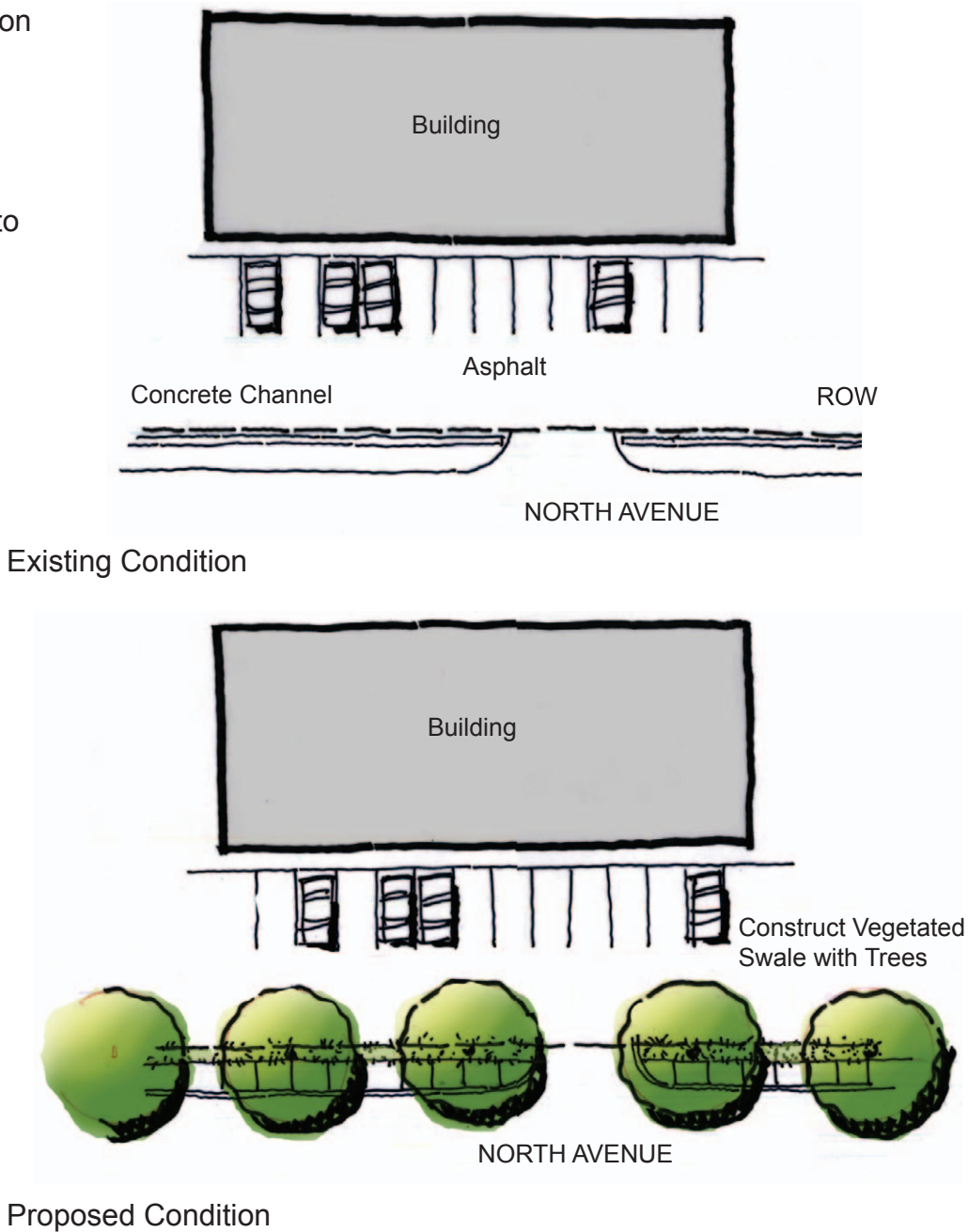
Drainage Way Adjacent To Street

In areas where there are open concrete channels adjacent to the street, suggested improvements include the creation of a generous vegetated swale to collect, filter, and distribute stormwater, as well as provide an irrigation source for planted trees. The addition of an attached walk, defined curb cut entrances, and pedestrian light fixtures will also enhance and define the street edge.

An alternative would be to provide a covered pipe to convey this drainage. This proves less advantageous relative to cost, construction caused delays to traffic and the loss of utilizing this run off for future plantings.



Existing Condition

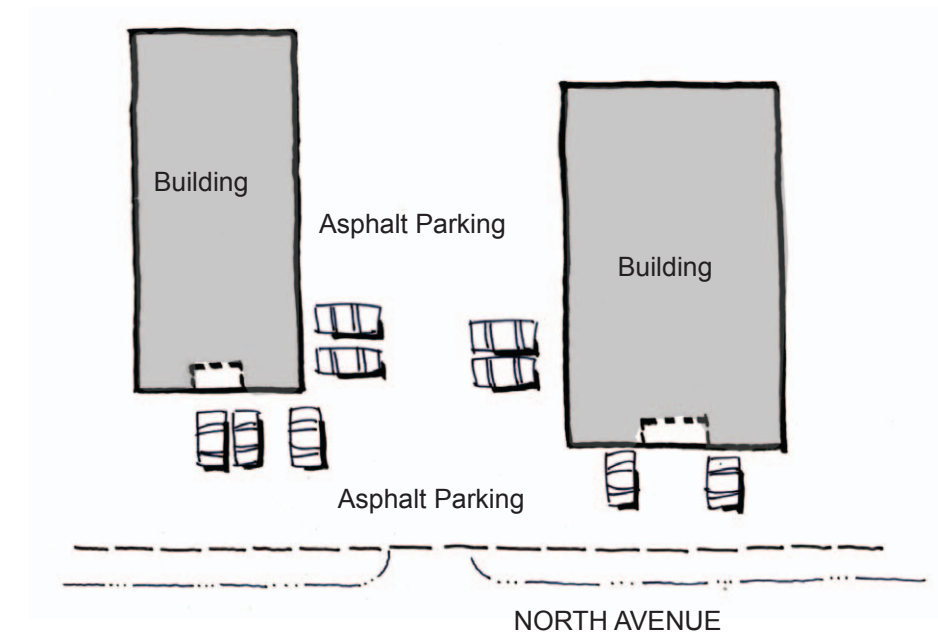


Building Adjacent To Street

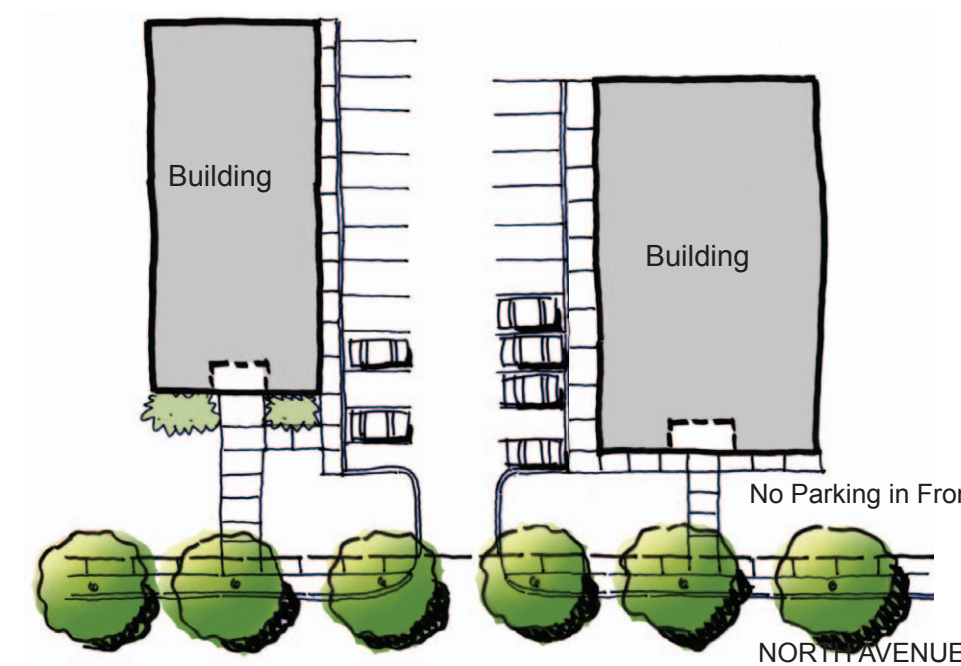
In areas where buildings front the street, suggested improvements include sidewalks that connect building entrances to walks along North Avenue, defined curb cut entrances, and the relocation of parking between buildings. These improvements will enhance the street edge by reducing the number of parked cars in front of building entrances, provide space for street tree plantings, and reduce the expanse of asphalt. This makes the buildings easier to see and identify from North Avenue. Additional sidewalks provide a clear, safe pedestrian path between parking lots to building entrances, and provide pedestrian connections to North Avenue.



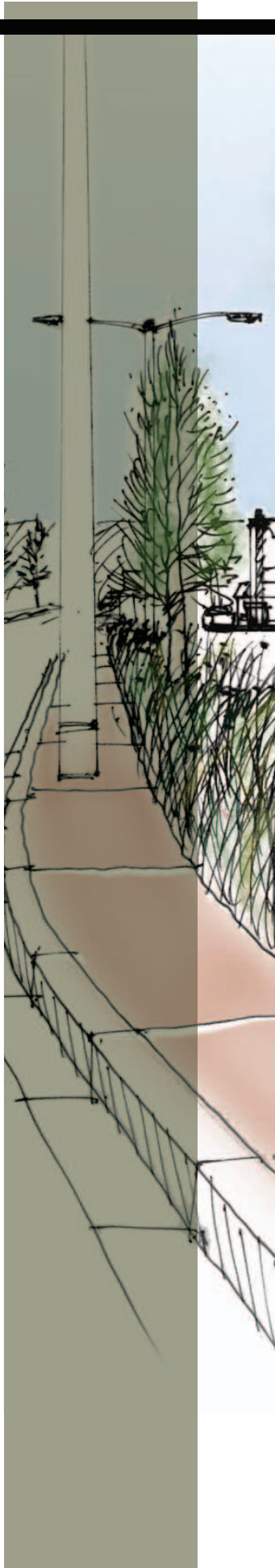
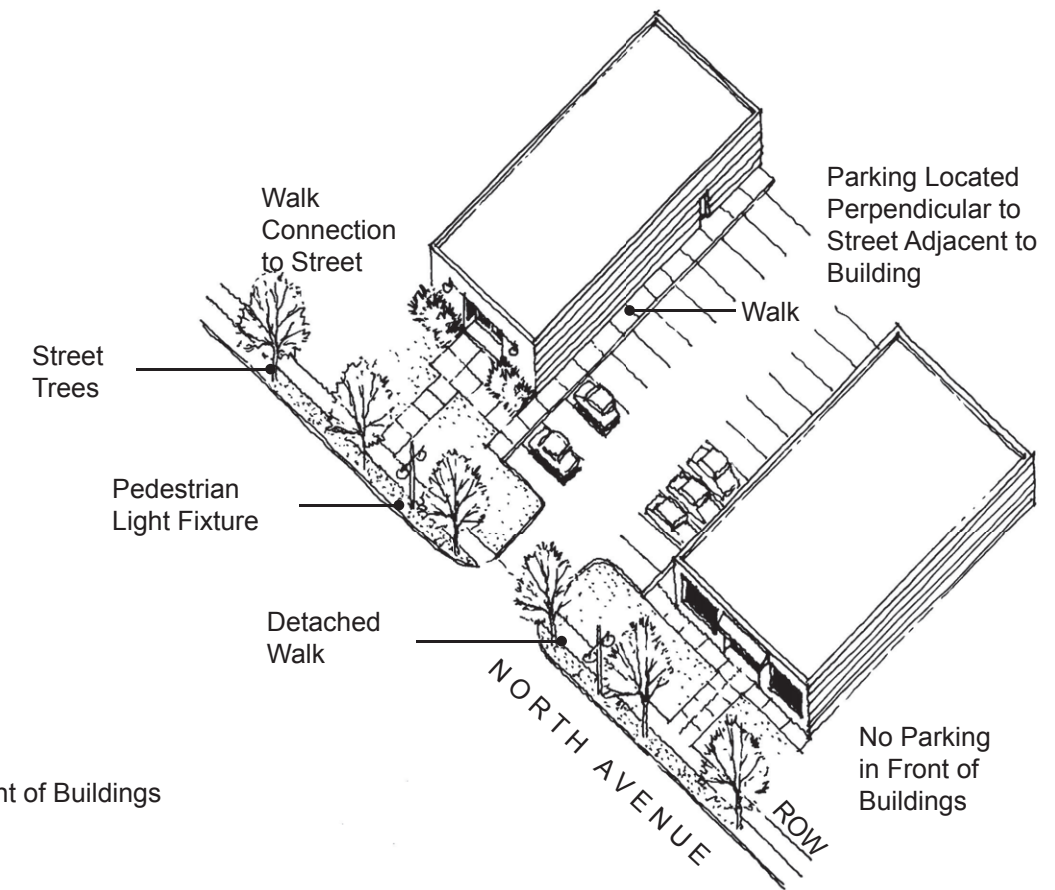
Existing Condition



Existing Condition



Proposed Condition

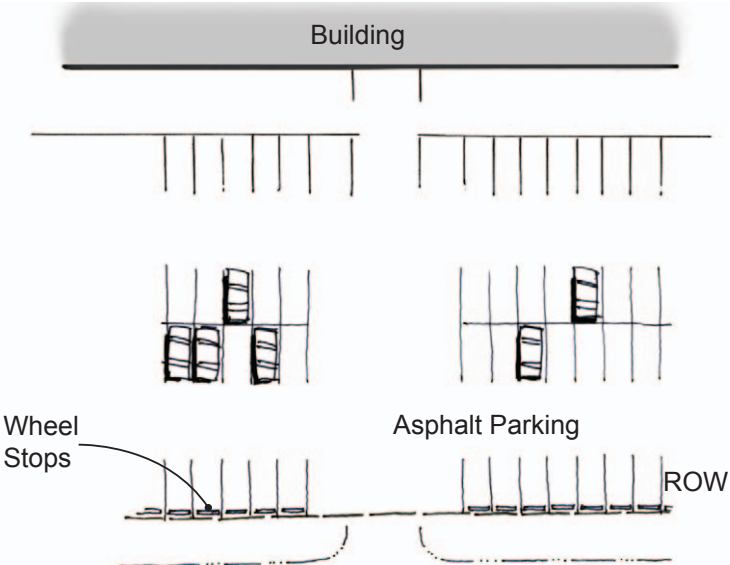


Parking Lot Adjacent To Street

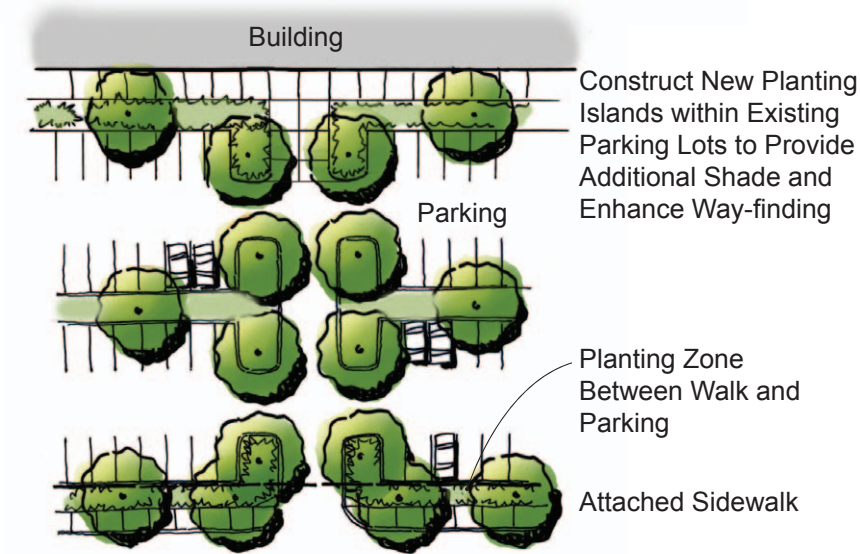
In areas where parking lots front the street, suggested improvements include defined curb cut entrances, added sidewalks, and additional plantings to define parking lot circulation and enhance way finding. Tree plantings can be added to existing parking lots by removing asphalt at the end of the parking bays, defining an entrance into the lot (Option B). Another option is to include planted islands between parking bays, which is especially effective in reducing the expanse of paved areas and urban heat islands often found in large parking lots (Option A). Attached or detached sidewalks can be added adjacent to North Avenue, depending on the specific condition and width of the ROW. Fencing, 42" tall, can also add positive streetscape character. The fencing should be steel and open, and may be an opportunity to incorporate art, static or kinetic and color.



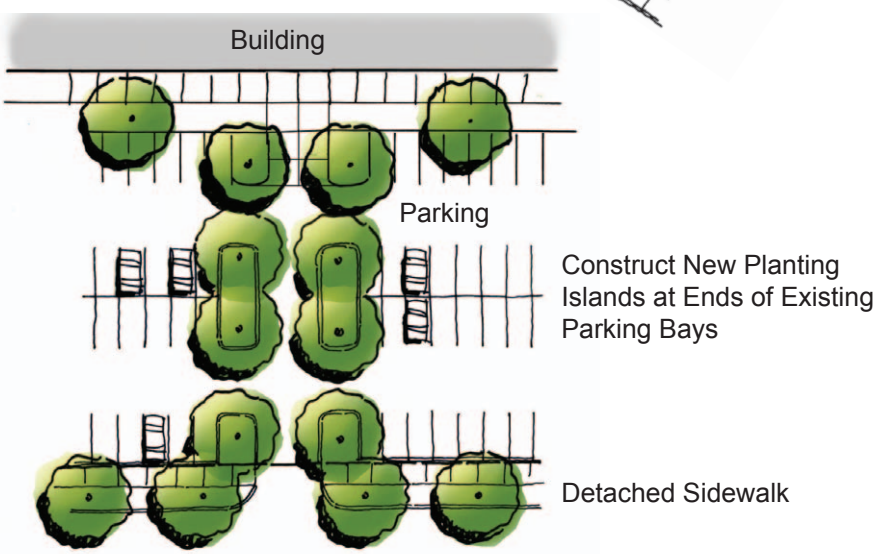
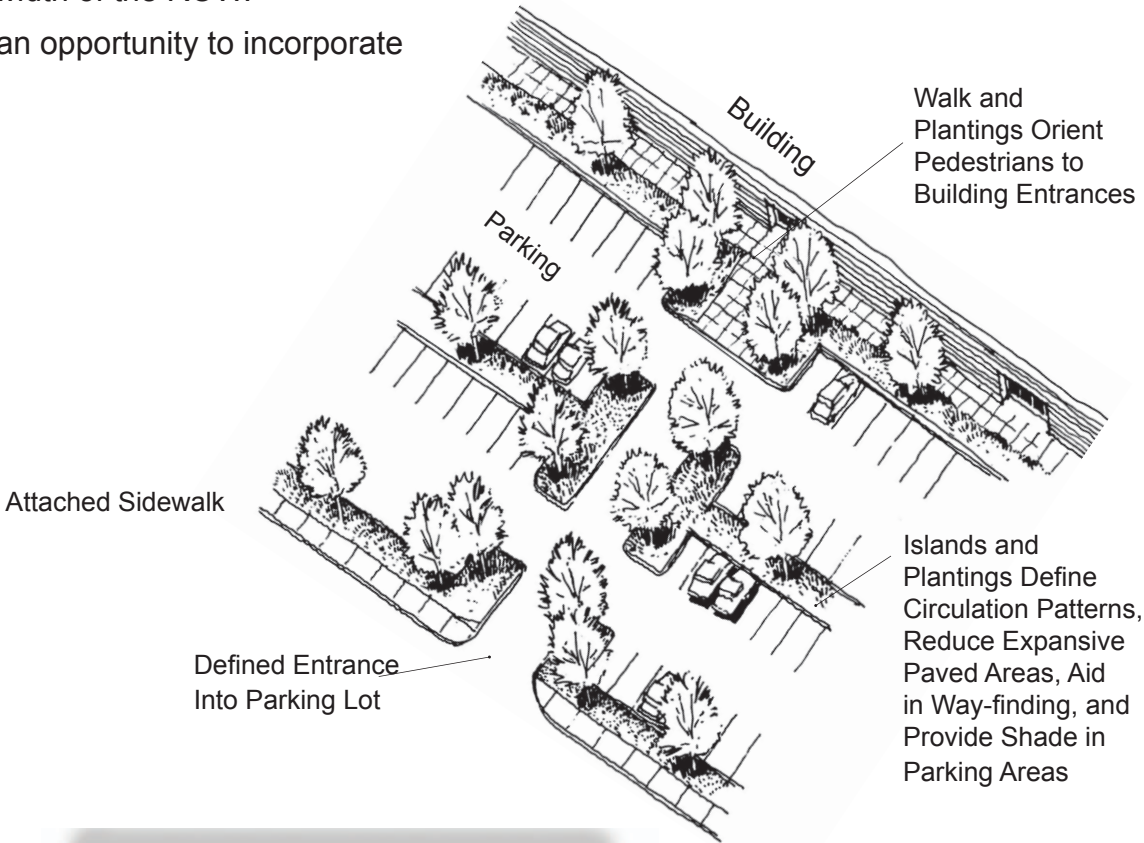
Existing Condition



Existing Condition



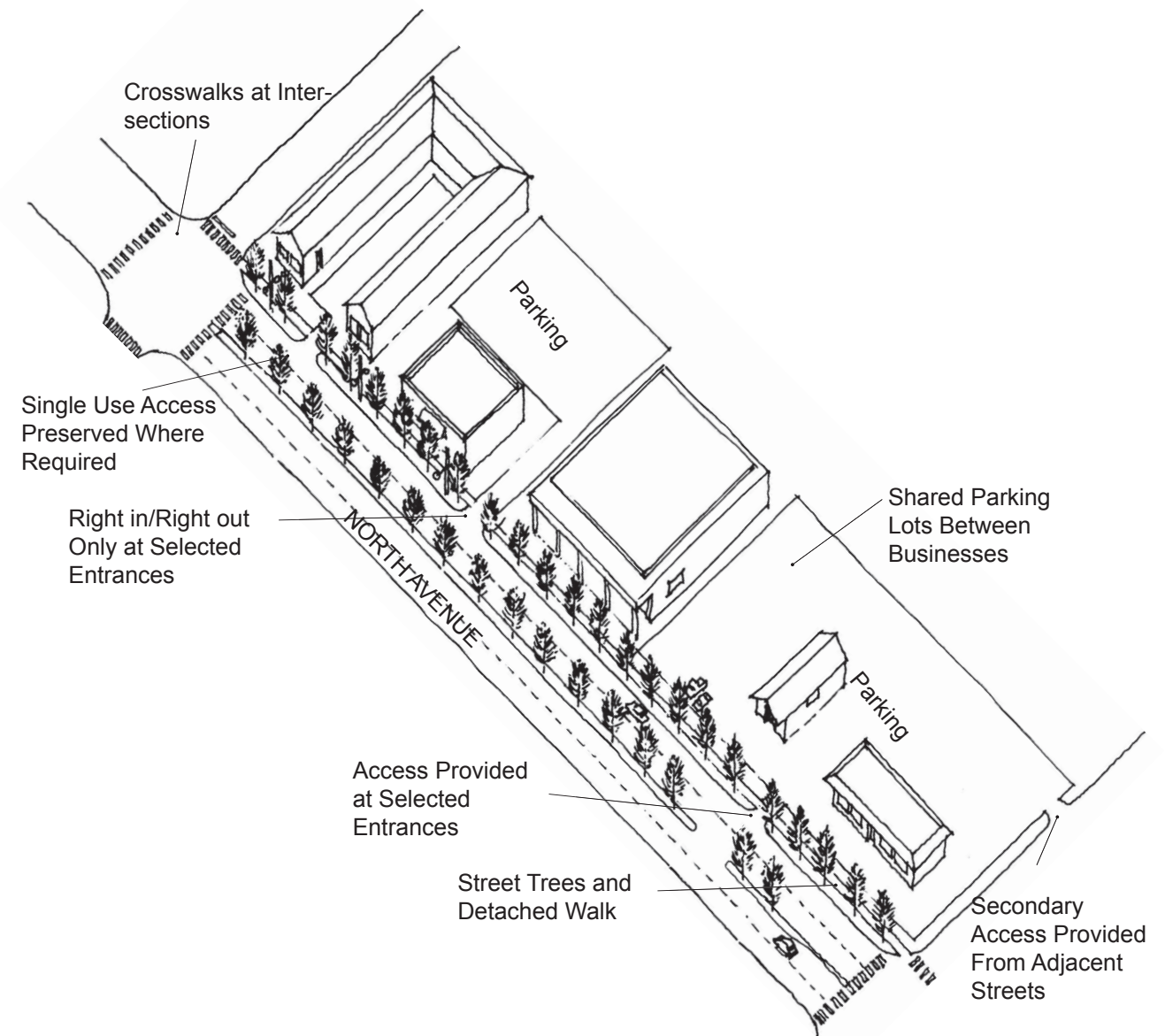
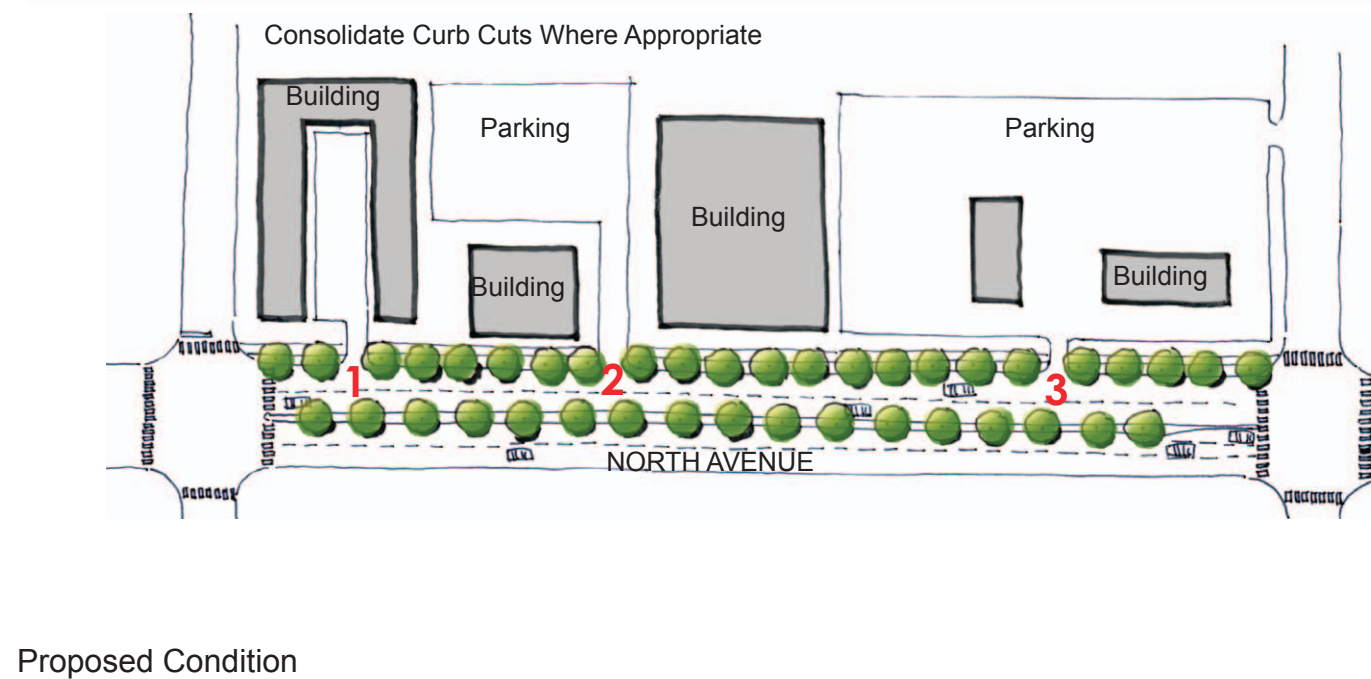
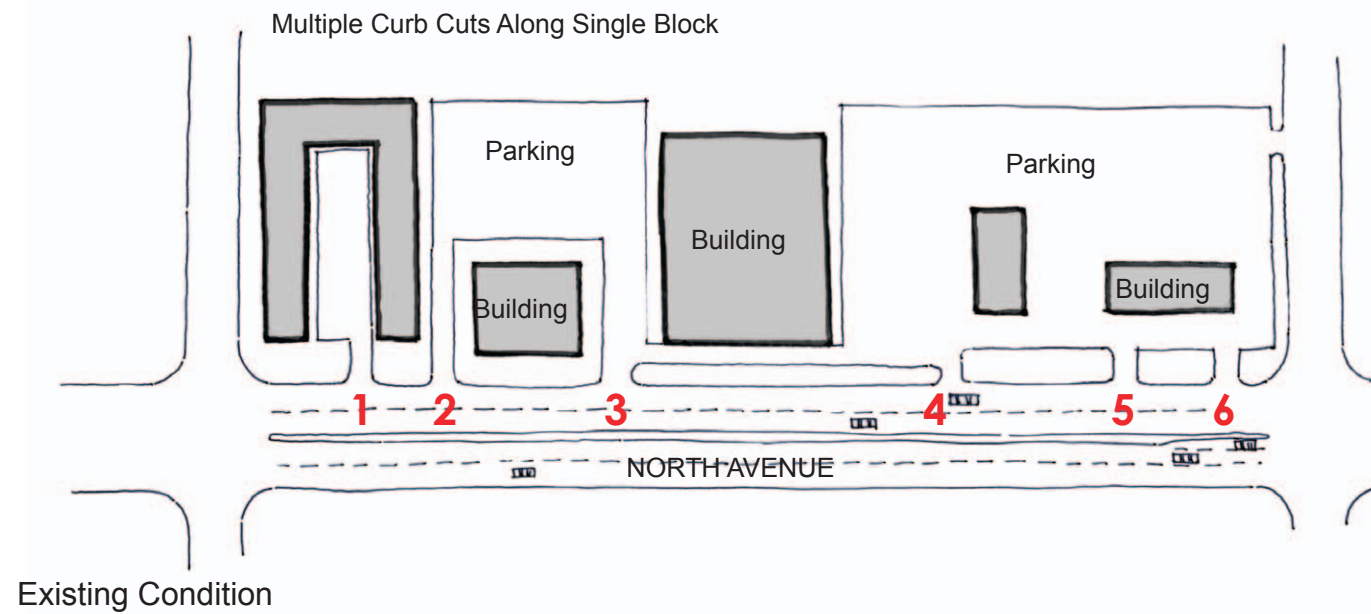
Proposed Condition - Option A



Proposed Condition - Option B

Curb Cut Consolidation

Reducing the quantity of curb cuts along North Avenue will greatly enhance the overall character of the corridor, and improve vehicular & pedestrian circulation. Curb cuts can be consolidated in areas where parking lots can be linked and shared by adjacent uses. Consolidating curb cuts also provides additional space for plantings and sidewalks adjacent to North Avenue within the ROW and less discontinuity of the sidewalk.



Areas identified for redevelopment along the corridor provide an opportunity to greatly enhance the character of the corridor, by creating pockets of redevelopment that will stimulate further improvements. As shown on the land use plans, these areas of redevelopment will consist primarily of additional residential, office, retail/commercial areas, or new regional store developments.

In all cases, each undertaken project should provide connectivity to the pedestrian network and include public open space.

Residential

Clustered townhome developments adjacent to North Avenue, will provide a distinct living option for residents along the corridor that is currently unavailable in the region. Located within walking distance of shopping, restaurants, bus service, and employment opportunities, townhomes offer a very desirable alternative to single family housing. A green corridor in a walking environment creates a pedestrian connection between the existing neighborhoods and North Avenue, providing convenient access to the street for existing residents. On-street parking provides parking for visitors.



Florence Square - Aurora, CO



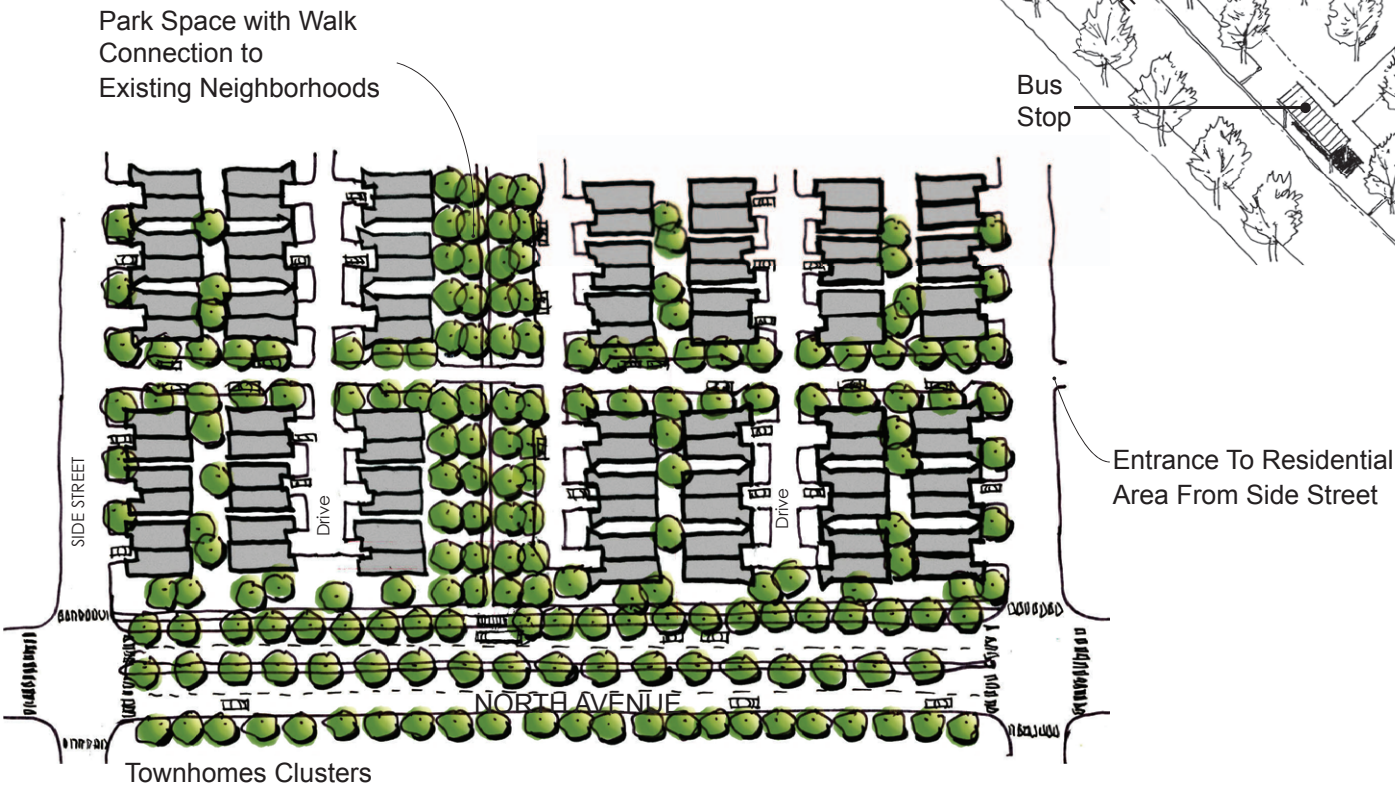
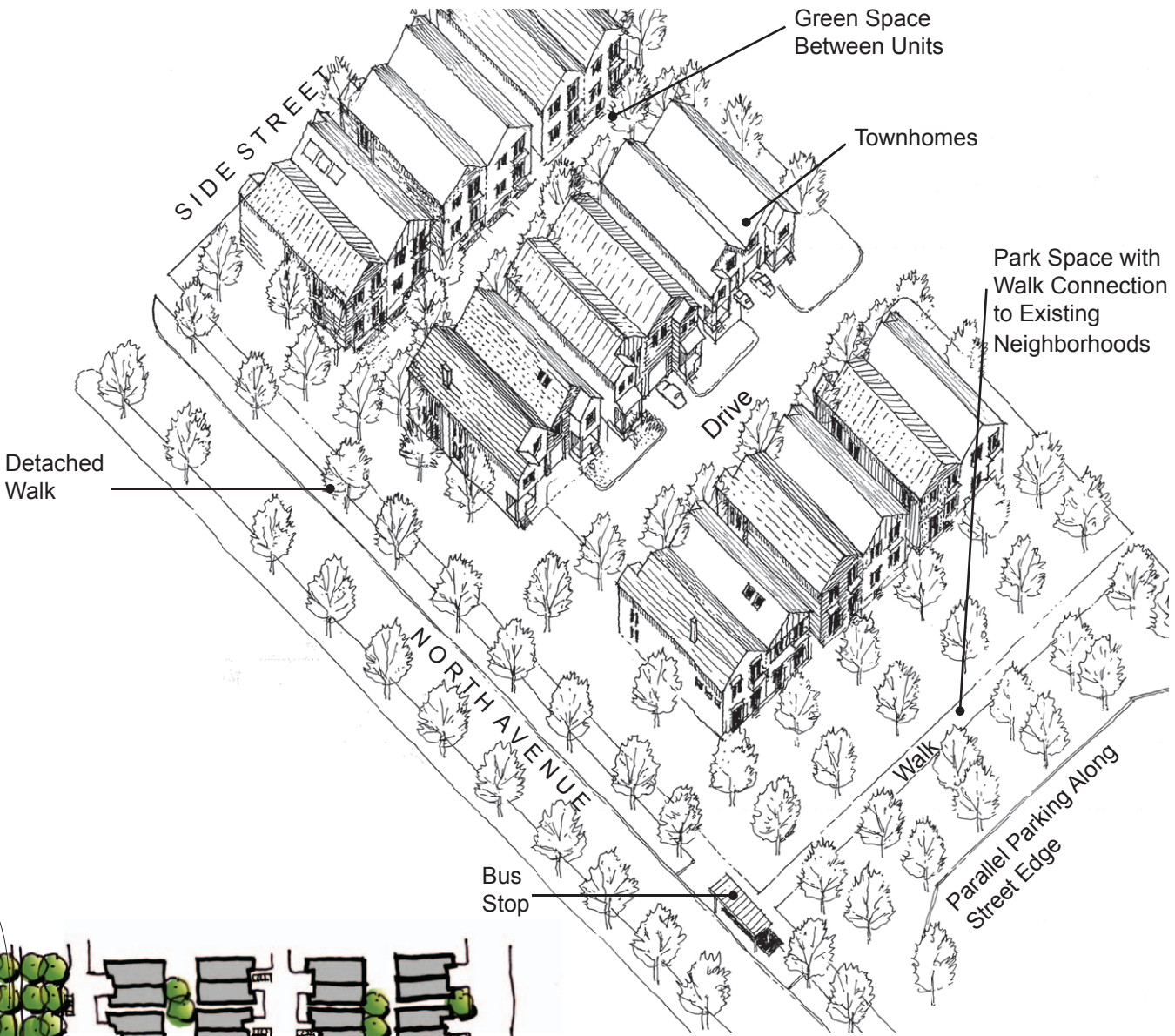
Stapleton - Denver, CO



Florence Square - Aurora, CO

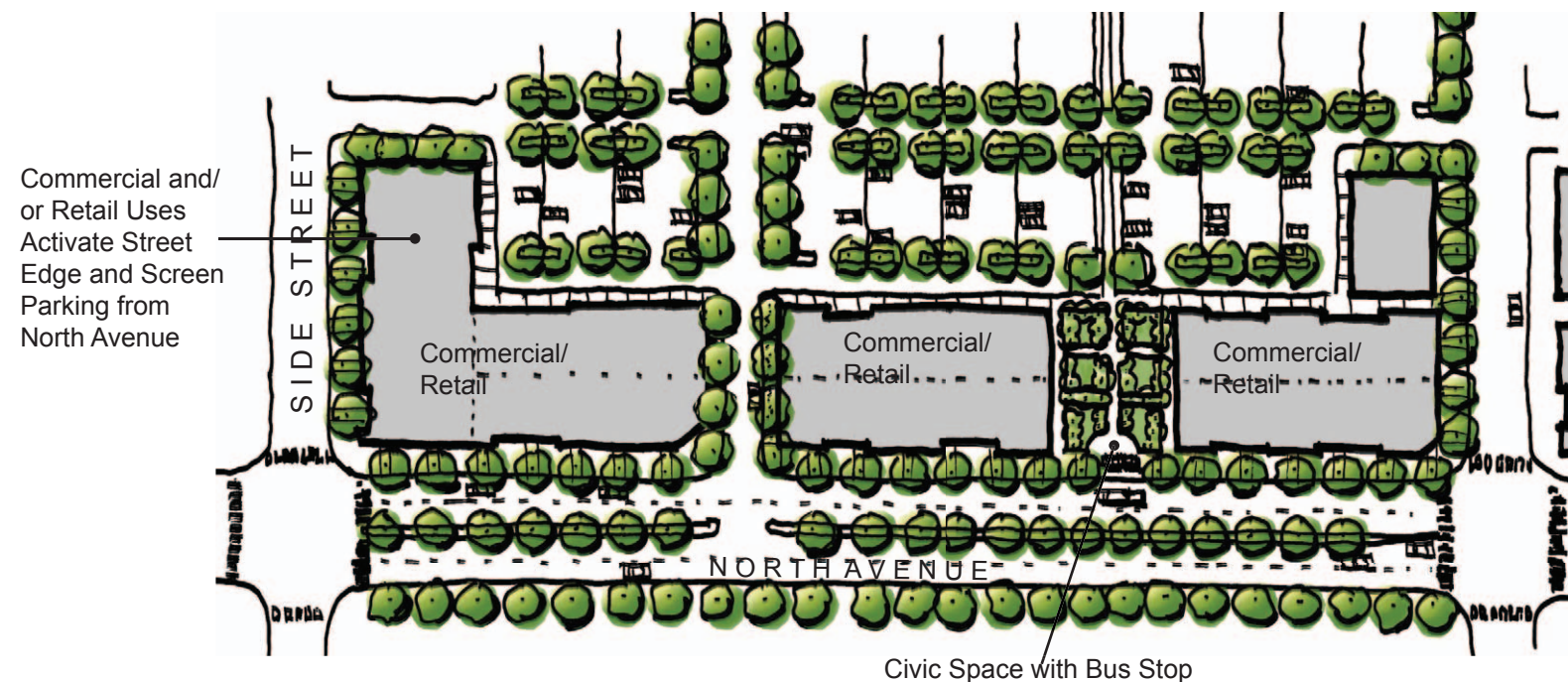


Florence Square - Aurora, CO



Commercial / Retail

New commercial/retail uses along North Avenue are located close to the street, which helps to activate the street edge. Generous sidewalks with spaces for outdoor seating, active open space, pedestrian lights, and street trees enhance the character of the corridor. Building signage can be located directly on the face of the buildings, which can be readily identified from passersby on North Avenue. Front door access to retail/commercial uses is provided on the interior side, providing easy access from nearby parking, with back door service access provide along North Avenue. Civic spaces such as plazas, corridors, or pocket parks create a permeable frontage along North Avenue that provides easy pedestrian flow between the street and commercial or retail uses.



Stapleton - Denver, CO



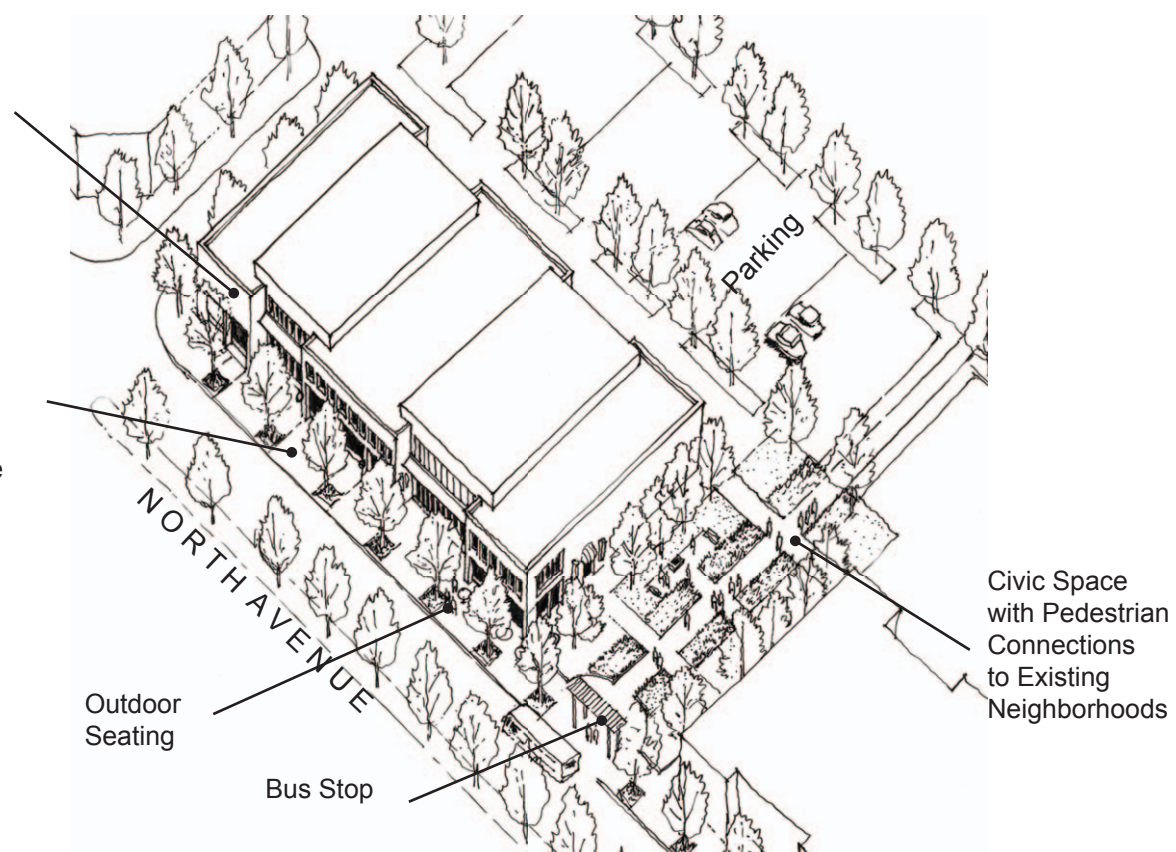
Charlottesville, VA



Longmont, CO

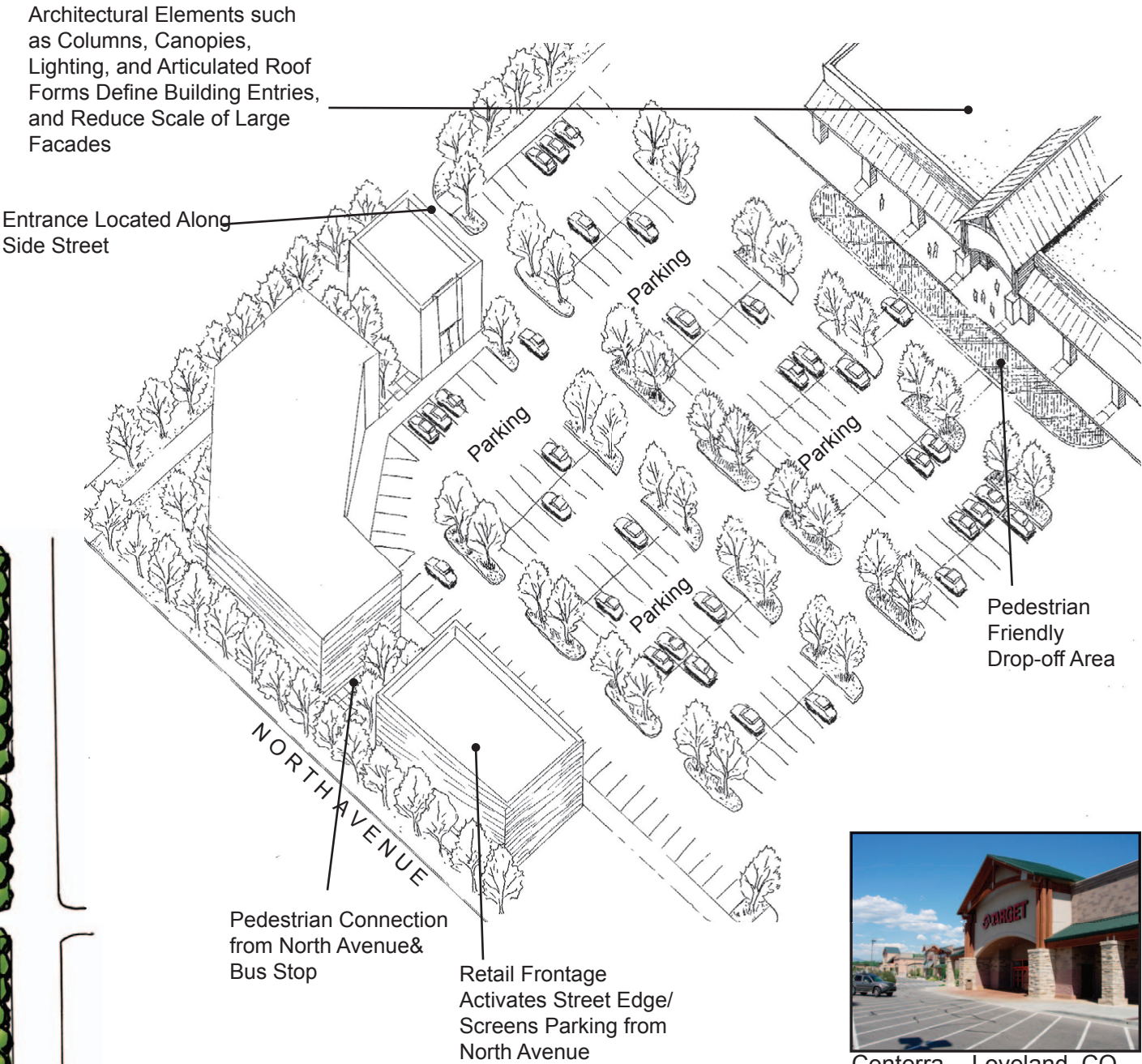
Commercial /Retail
Uses with Address
Along North
Avenue

Generous Walk
with Trees,
Benches, and
Pedestrian Light
Fixtures Articulate
Street Edge



Regional Store

New regional store developments, proposed as infill projects, along North Avenue can be designed to minimize the impacts of large expanses of asphalt parking. This can be accomplished by locating retail stores at the edge of North Avenue, which activates the street edge, and screens the expansive parking lots from view from North Avenue. Interior Parking lots can be shared between the super store and retail uses. Architectural elements such as canopies, columns, and articulated roof forms help to define the building entries of the super store, and reduce the scale of large facades. This approach will provide a more pedestrian scaled environment for merchants, and create a more inviting facade. Entrances to the super store should be located from a side street, which will prevent excessive traffic build up along North Avenue.



Centerra - Loveland, CO



Mulberry Crossing - Fort Collins, CO

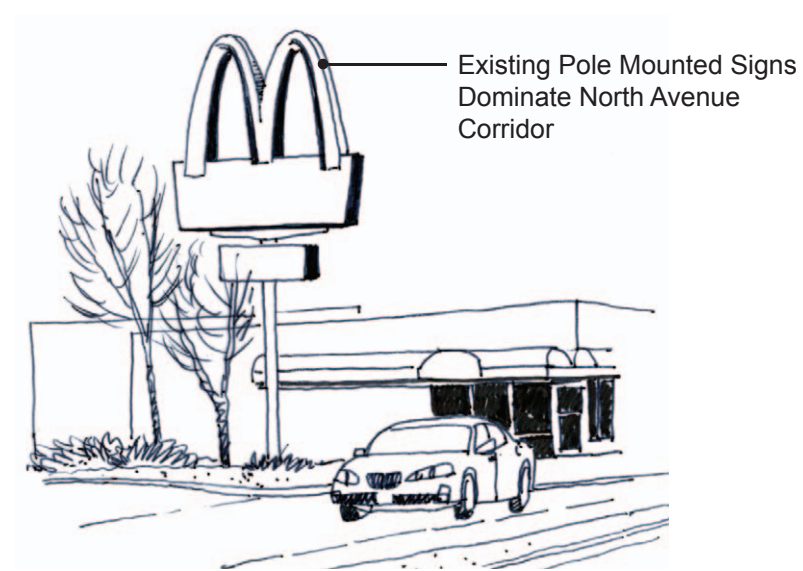


Centerra - Loveland, CO

Signage

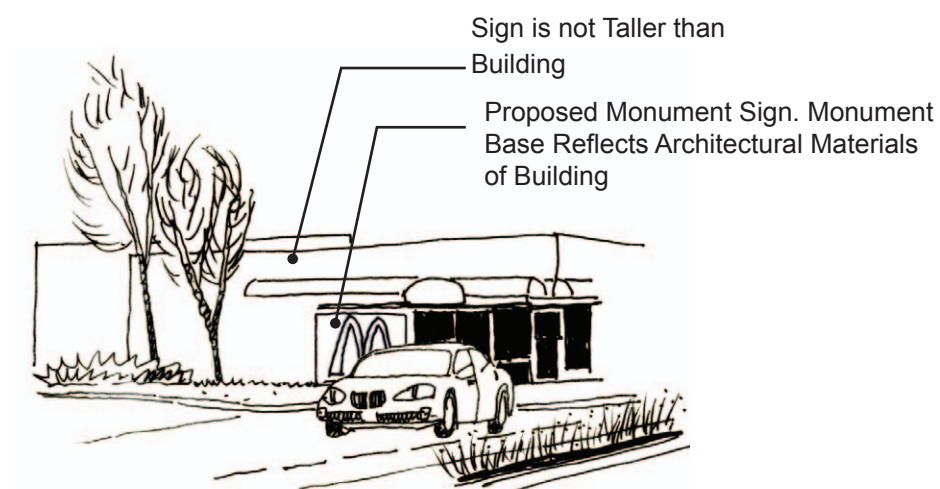
Improvements to signage along North Avenue can be accomplished by minimizing the quantity of pole mounted signs, and replacing them with monument signs. This will aid in reducing the visual clutter of the pole mounted signs, and also provide an opportunity for street trees to be planted. Because monument signs are low, they are not in conflict with the larger canopy of the street trees, and can be readily seen from passing traffic. Further detail on signage and recommended adjustments to the Grand Junction Sign Code are provided.

This philosophy supports the neighborhood or district concept, in that distinctions are to areas and not individual pad development. This encourages the walking environment and provides for a group of retailers to engage the property as a whole.

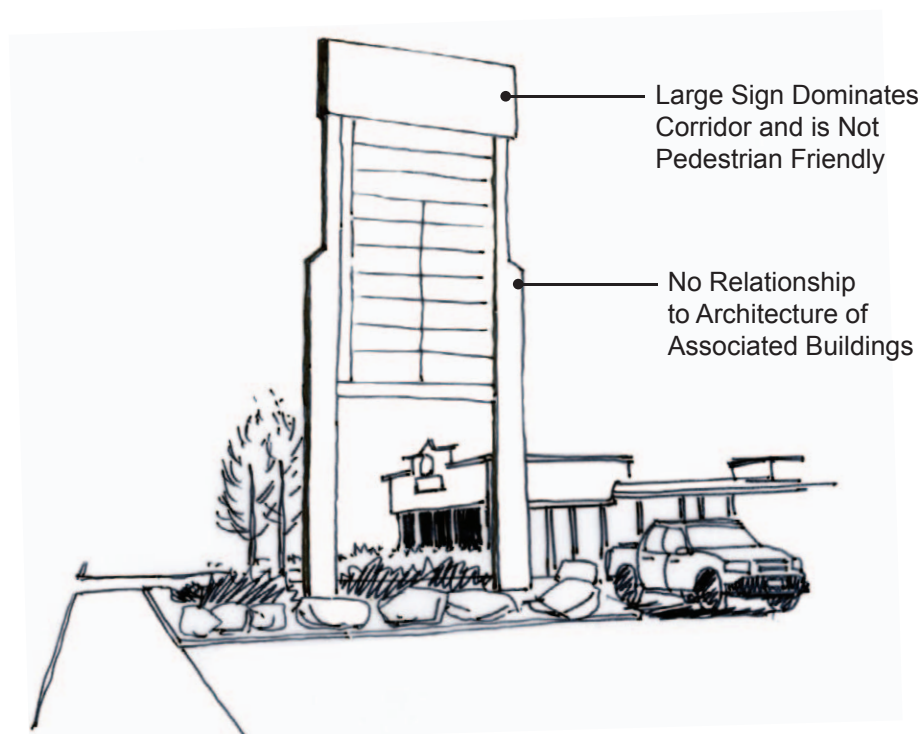


Existing Pole Sign

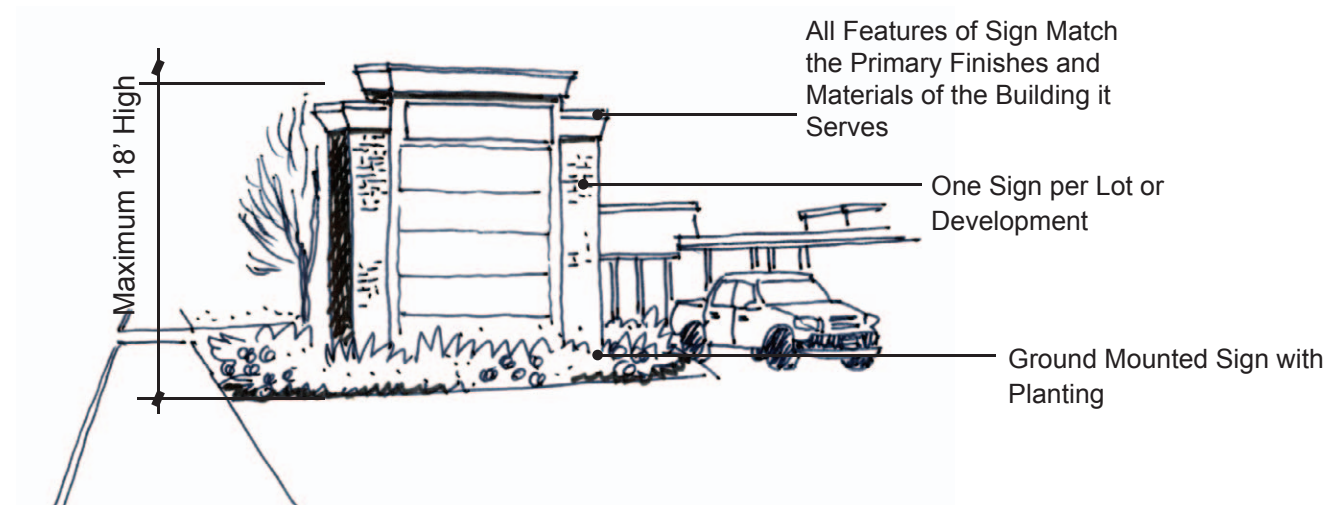
Low Monument Signs Reduce Visual Clutter Along Street Edge, and Allow Street Tree Plantings While Maintaining Sign Visibility



Proposed Monument Sign



Existing Pole Sign



Proposed Monument Sign





Summary of Grand Junction Sign Code Strategies and Recommended Improvements

Summaries of particular sections from the Grand Junction Sign Code are provided below, indicating where the current Grand Junction sign regulations could be strengthened or amended to improve the overall visual quality and pedestrian scale of North Avenue. Currently, the majority of signs along North Avenue fall within these existing regulations:

Sign Location 4.2G1e(3)

Code Summary:

Grand Junction’s current sign regulation allows signs most anywhere on the property being advertised, and the total square footage allowed for a single sign is not to exceed 300 sf.

Recommended Improvements:

- No signs permitted at the right-of-way
- Establish setbacks from the right-of-way
- Limit square footage of signs based upon to-be-established criteria

Building Mounted Signage 4.2G1e(5)

Code Summary:

There is no overall maximum square footage for wall mounted signs, although the formula is similar to other sign code regulations. Each building will have its own maximum square footage, rather than a universal not-to-exceed size.

The language also allows for wall mounted signs on any side of the building facing a street, thus a corner lot could have two signs, each with a different sign allowance.

The regulation allows for roof signs, up to 40 feet above grade in height. In addition, the regulation permits more than one building mounted sign (wall mounted, roof sign, or projecting) per structure as long as the maximum square footage is not exceeded.

Recommended Improvement:

- Limit square footage for building mounted signs based upon to-be-established criteria.
- Limit number of signs to 1 per building or tenant.
- Prohibit roof signs.

Projecting Signage 4.2G1e(6)

Code Summary:

A minimum size (12 sf.) is given for projecting signs rather than a maximum, if the projecting sign is the only sign on the building.

Recommended Improvement:

- Limit square footage of projecting signs based upon to-be-established criteria.

Freestanding Signage 4.2G1e(7)

Code Summary:

The square footage and height limitations are calculated similar to other sign code regulations, but allow for greater height (25' for 2 traffic lanes and 40' for 4 traffic lanes) and maximum square footage than other regulations. The sign square footage is also calculated based upon property frontage rather than building face frontage.

Signs are allowed to be installed at the right-of-way, with no setback requirements.

Recommended Improvement:

- Limit square footages and heights of signs based upon to-be-established criteria based upon building linear feet rather than property linear feet.
- Lower heights of signs from what is now permitted.
- No signs permitted at the right-of-way.
- Sign design reflects the associated building architecture.
- Signs shall be attached to the ground or with minimal space between the sign and the ground.

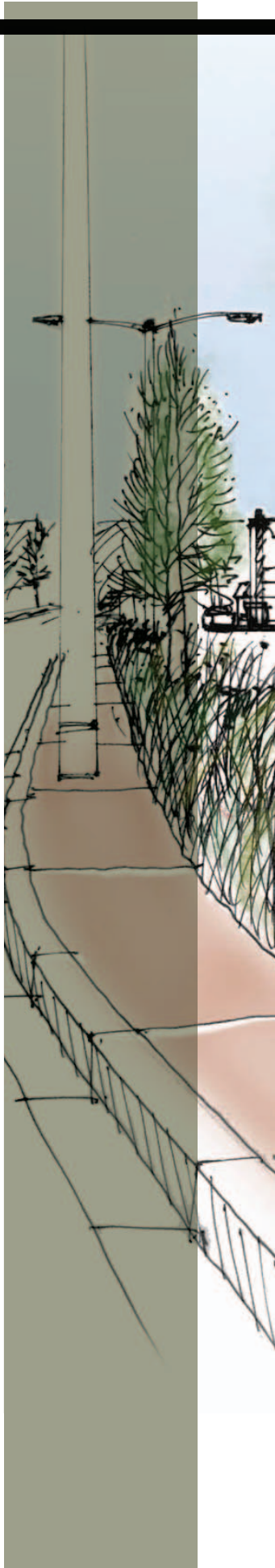
Off-Premise Signage 4.2G1e(8)

Code Summary:

Off-premise signage (building mounted, freestanding, or roof signs) is allowed in C-2 (General Commercial) and I-1 and I-2 (Industrial) zones. Maximum height is 40 feet, and square footage must be between 15 sf. and 300 sf. Some municipalities are not allowing any off-premise signs within the City limits.

Recommended Improvement:

- Prohibit off-premise signage.





Recommended Sign Implementation Measures from other Municipal Sign Codes:

The following are representative regulations found within the Sign Codes from Denver, Fort Collins, Longmont, Greeley, and Loveland. Typically, it was found that heights and square footages were regulated; often based on a formula determined by the setback distance from the face of curb or edge of pavement and/or the linear feet of building facing the street. This summary does not provide that level of detail, but rather an overview of suggested strategies.

General

- Existing on-premise signs which have become nonconforming because of subsequent Code amendments shall be brought into conformance within 15 years of the date of amendment.
- All features of the sign, including illumination, sign cabinet, base, color, lettering, and materials shall match the primary finishes and colors of the associated building(s) which it serves.
- All signs which are greater than 4 SF in area, except ground signs or signs that replicate a business logo must be comprised only of individual letters or cabinets where only the letters are illuminated.
- Signs shall be professionally designed and fabricated from quality, durable materials.
- No roof signs.
- Lighting shall be indirect.

Maximum size of letters:

Use	MAXIMUM LETTER HEIGHT	MAXIMUM LOGO HEIGHT	MAXIMUM BACINET HEIGHT
Auto-related, roadside, and business services	12"	18"	18"
Convenience shopping center	18"	24"	24"
Neighborhood services center, neighborhood commercial district	24"	30"	30"

Sign Location

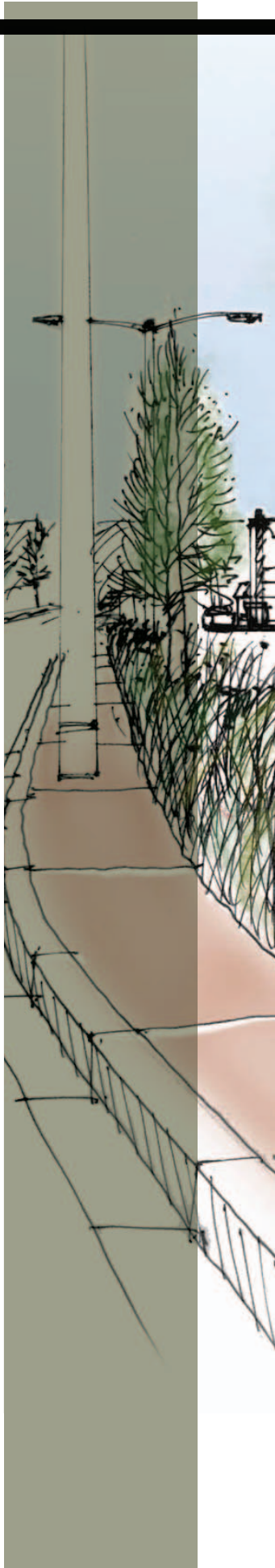
- Signs will be set back from the right-of-way based upon the sight distance triangle requirements, zoning district, or size of sign.
- No signs allowed within the right-of-way, with the exception of projecting signs which meet all requirements set forth in the regulations.
- No off premises signs.
- All freestanding signs shall be located to be compatible with required landscaping, including street trees at maturity, so that the public views of the sign will not be obstructed.

Freestanding / Ground Signage

- Freestanding or ground signs comply with the following requirements:

USE	MAXIMUM AREA PER SIGN FACE	MAXIMUM NUMBER OF SIGNS PER STREET FRONTAGE	MAXIMUM HEIGHT
Auto-related, roadside commercial, and business services	Primary – 32 SF	Primary – 1	Primary – 5 ft.
Convenience shopping center	Primary – 40 SF	Primary – 1	Primary – 8ft.
Neighborhood services center, neighborhood commercial district	Primary – 55 SF	Primary – 1	Primary – 10 ft.
	Secondary – 32 SF	Secondary – 1	Secondary – 6 ft.

- Freestanding signs only permitted with a supporting sign structure, the width of which exceeds 70% of the width of the sign face.
- Freestanding or ground signs shall have no more than 2 faces.
- Maximum height for freestanding signs shall be 18 feet above grade; and for ground signs shall be 12 feet above grade. (Another code said max. 6' height for freestanding.)
- All freestanding signs shall be of a monument design including a monument base attached to the ground with no or minimal space between the sign cabinet and the monument base. (no pole signs)
- Monument bases shall be equal or greater (up to 20% greater) in width and length than the sign cabinet. Sign cabinets for freestanding signs shall not exceed 24 inches in width.
- Multi-tenant buildings or developments are allowed one sign per lot or development parcel (no individual freestanding or ground signs).





Projecting Signage

- Not allowed to be located on the same street frontage as a freestanding sign
- Shall not exceed 30 sf. for all faces
- Shall not extend more than 5 feet from the building face nor extend beyond the curbline of any street or parking area
- Minimum of 8 feet of clearance from the ground, and a maximum of 25 feet, and shall not extend beyond the roofline or parapet wall
- Only 1 per building or tenant
- Not allowed on a wall that already has a wall mounted sign

Building Mounted Signage

- No building mounted sign shall exceed 100 sf.
- Multi-tenant buildings allowed one wall sign per street frontage of business
- Only one building mounted sign per building (single tenant). Shall not project above the roofline

Awning Signage

- Signs on canopies or awnings are limited to the name of the building, business and /or address of the premises
- Shall not exceed 50 sf. per face in area
- Only allowed on first story of a building
- Minimum clearance shall be 8 feet

Suggested Steps to Implementing New Signage Approach Along North Avenue:

The following steps are suggestions for implementing a new signage strategy along the North Avenue corridor. This strategy will require amending the existing sign code to incorporate the appropriate level of restrictions within the corridor.

1. Determine Desired Signage Aesthetic

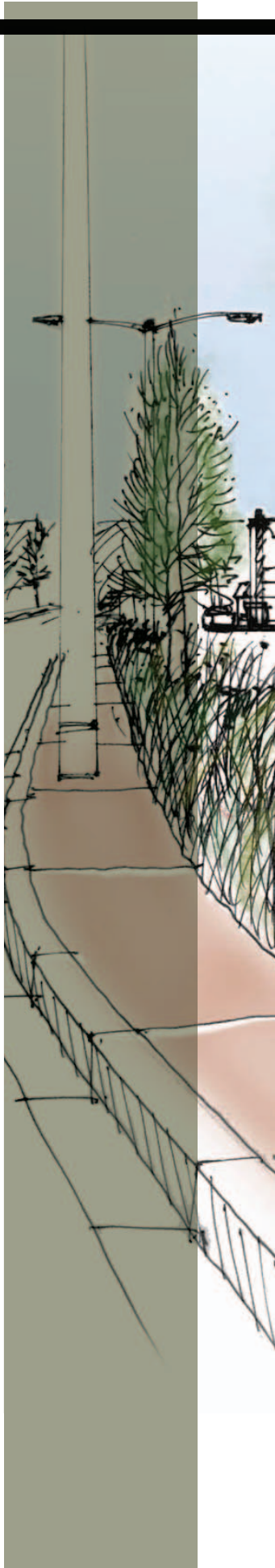
Determine the appropriate level of regulations for signage based upon other similar Colorado communities, as well as the desired aesthetic and scale of the signs within the corridor

2. Determine How to Approach Existing Non-conforming signs

Decide whether to require existing non-conforming signs to be improved within an appropriate time frame, and/or require compliance to new sign code amendments at time of business transition or sale. For example, Fort Collins requires all existing signs to be in compliance with the updated sign code within 15 years.

3. Amend the Sign Code Appropriately

Determine if this is a City-wide amendment, or limited to a special zoning district for North Avenue. Amend the City Sign Code with the desired adjustments, and begin transitioning out the old signs.





Market			
	West	Central	East
Barriers	<ul style="list-style-type: none">• Central GJ losing ground to fringe for commercial development• Retail and commercial uses too spread out -- lacking critical mass	<ul style="list-style-type: none">• Central GJ losing ground to fringe for commercial development• Veterans hospital may not generate enough demand for spin-off med. Office development• Retail and commercial uses too spread out -- lacking critical mass	<ul style="list-style-type: none">• Central GJ losing ground to fringe for commercial development• Retail and commercial uses too spread out -- lacking critical mass
Strategies	<ul style="list-style-type: none">• Capitalize on proximity to college and downtown through marketing efforts, product mix and positioning• Educate Council about this market reality and prepare them for “filling the gap” of catalyst projects resulting from inflated land prices and other factors• Consider an urban renewal district to assist with on-site and off-site costs for qualifying elements	<ul style="list-style-type: none">• Capitalize on proximity to hospital and park through marketing, product mix and positioning (including actual physical connectivity)• Encourage hospital, or unrelated clinics, to explore partnership opportunities• Encourage mixed uses including affordable residential and multi-family	<ul style="list-style-type: none">• Capitalize on substantial infrastructure investment in north-south connection (along with presumably softer land assembly costs) to pursue regional retail opportunity

Financial			
	West	Central	East
Barriers	<ul style="list-style-type: none">Fragmented Ownership/difficulty in assemblyUnsupportable Land Costs		
Strategies	<ul style="list-style-type: none">Work with private and non-profit interests to assemble and hold opportunity sites;Share market opportunities with various audiences to encourage investmentUse range of mechanisms – land swaps, low-interest loans, acquisition / write-downConsider establishing appropriate districts as holding entities		<ul style="list-style-type: none">Problem is less severe near logical development node – probably can use traditionally land assembly





Physical			
	West	Central	East
Barriers	<ul style="list-style-type: none">• Inadequate size and shape of lots for redevelopment• Building relationship to the street (inconsistent setbacks)• Balancing the role of the street (cars vs. peds)		
Strategies	<ul style="list-style-type: none">• Maintain (City) flexible position on the use and width of alleys (if relevant) in select locations• Create street standards for building setbacks, sidewalk width, signage, awnings, etc. sensitive to the feasibility of the remaining building envelope• For stores that require surface parking in front, require heightened landscaping or pad site development with interior parking – remaining sensitive to sight-lines• Establish street standards – define the street’s role by district based on the vision and desired character• Update any transportation plans relative to desired district functions, including encouraging public transportation through increased comfort and security of stops• Link parking lots, minimize curb cuts and consolidate driveways – selectively – where feasible• Redesign intersections and access points within the nodes to simplify and coordinate signal sequences		

Regulatory			
	West	Central	East
Barriers	• Over-zoned for commercial – limiting development of housing		
Strategies	<ul style="list-style-type: none">• Consider overlay district that is more supportive/encouraging of mixed-use• Revise regulations to eliminate barriers to investment in neighborhoods on the corridor; promote density and greater variety of residential products (i.e., density bonuses)• Encourage the introduction of residential development within nodes and at key locations; fill financial gaps, and support demonstration projects (look for institutional partners)• Where necessary, assist with assembling properties to accommodate a range of product types (workforce, student, senior, etc.)• Eliminate regulatory barriers which preclude or delay redevelopment buildings for residential use (building codes, limits on adjacency among uses, etc.)• Work with private and non-profit interests to provide opportunity sites for residential development		





Political			
	West	Central	East
Barriers	<ul style="list-style-type: none">• Lack of committed “cheerleader”• Limited Examples of Creative Financing Strategies (lack of catalyst / demonstration projects)• Homeless shelter is a deterrent to redevelopment		
Strategies	<ul style="list-style-type: none">• Request Council support for participation in at least two demonstration projects – enlisting the participation of existing property owners (if feasible), identifying partners, soliciting lender support, and committing a range of tools to assist with filling the gap• Once a “tool box” of incentives have been assembled, publish a request for development, non-parcel specific to encourage investment• Through this corridor plan process, identify individuals and organizations which have the desire and resources to continue this effort and ensure the implementation strategy is implemented• Develop a range of “stories” which speak to the interests of different audiences – physical, fiscal, market, financial, etc. – educating about the opportunities and commitment to overcome barriers• Encourage transitional programs (e.g. job-training, adult education) that provide positive daytime activities for homeless shelter population		

Roles and Responsibilities

City Leadership

New Initiatives

Implementing Entities (i.e., City Staff, Advocacy Entities, etc.)

Investors / Private Sector

Community

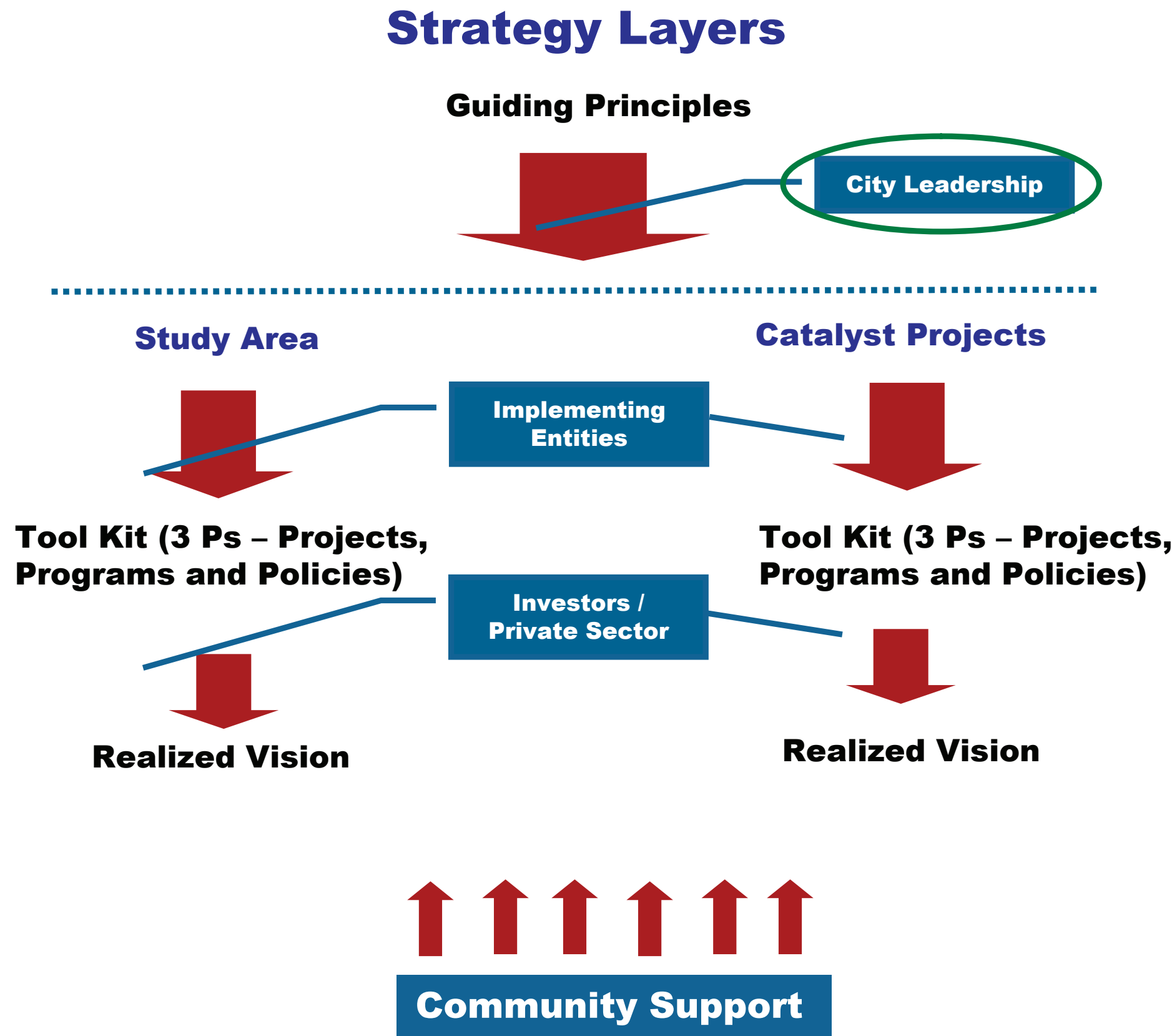
- Support at least two demonstration projects (catalyst projects) – commit support for the use of a range of “gap filling” tools
- Once potential incentives are identified – publish a request for development
- Consider the range of district mechanisms which could potentially fund improvements in the corridor

- Continue to monitor the market and development climate – establish benchmark database
- Develop targeted marketing materials – solicit regional retail opportunity
- Establish street standards
- Link parking lots, minimize curb cuts and consolidate driveways – within nodes
- Complete regulatory review – eliminate “gaps and barriers”

- Host developer / lender (“delivery system”) information meetings – progress updates
- Share development challenges – open book and open dialogue

- Continue to participate – get educated
- Support leadership decisions which advance the vision





CITY OF GRAND JUNCTION, COLORADO

ORDINANCE NO. 4486

**AN ORDINANCE ADOPTING THE GRAND JUNCTION NORTH AVENUE WEST
CORRIDOR PLAN**

**AS AN ELEMENT OF THE COMPREHENSIVE PLAN FOR THE AREA GENERALLY
LOCATED ALONG NORTH AVENUE WEST OF 12TH STREET**

Recitals.

The North Avenue area is experiencing deterioration due to aging and dilapidated structures, movement of businesses to the western areas of Grand Junction and high turnover in area businesses. Because North Avenue has been primarily zoned for commercial use, the result has been sporadic disinvestment, underutilized buildings, old strip malls and vacant property.

To remedy this and to reinvigorate the area, the City has undertaken a planning effort in two phases, one for the east end of the North Avenue Corridor, and one for the west end. The first phase occurred when the City Council adopted the North Avenue Corridor Plan (for the east end of the corridor beginning at 12th Street) in December 2007. The second phase is the North Avenue West Corridor Plan, which includes that area from 12th Street west to I-70B.

The North Avenue West Corridor Plan has been developed based on input from focus group meetings with property owners, residents and Colorado Mesa University representatives and input received through an online survey, a questionnaire, two open houses and a Technical Advisory Committee made up of representatives from CDOT, Grand Valley Transit, and City staff. The Plan was developed during a year of extensive public involvement and deliberation. The City Planning Commission has forwarded a recommendation of adoption of the Plan for the future growth of lands within the North Avenue West Corridor Plan planning area.

The Grand Junction North Avenue West Corridor Plan does the following:

1. Focuses on the Comprehensive Plan's vision for the community "To become the most livable community west of the Rockies";
2. Identifies four *Guiding Principles* that will shape the planning area's growth. Those Principles are:
 - Safety – establishing a multi-modal approach to pedestrian, bicycle, transit and vehicular safety.
 - Aesthetics – creating standards that support the vision and corridor as a destination and a crossroads.

- Placemaking – envisioning North Avenue as a corridor that is a destination itself, not simply a street to travel through.
 - Neighborhood Impacts – minimizing impacts to existing neighborhoods as growth occurs in the corridor.
3. Recommends the two block area of 3rd Street between North Avenue and Sherwood Park as the neighborhood core area for the neighborhood center established with the Comprehensive Plan.
 4. Recommends a future street cross section for the entire length of North Avenue that includes narrowing the travel lanes, adding bike lanes on each side and expanding pedestrian amenities on both sides of the street.
 5. Includes an Implementation Plan that recommends creating and establishing an Overlay Zone district to include the entire four miles of North Avenue.
 6. Respects individual property rights.

The Grand Junction North Avenue West Corridor Plan will amend the Grand Junction Comprehensive Plan and completes the corridor planning for North Avenue that was started with the 2007 North Avenue Plan encompassing that area of North Avenue east of 12th Street which is also an element of the Comprehensive Plan.

The Planning Commission is charged with the legal duty to prepare and recommend for adoption to City Council master plans for the City.

The North Avenue West Corridor Plan was heard in a public hearing by the Grand Junction Planning Commission on July 26, 2011 where the Planning Commission recommended that the City Council adopt the Plan.

NOW THEREFORE, BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF GRAND JUNCTION:

That the North Avenue West Corridor Plan, City of Grand Junction, Colorado, in the form of the document attached hereto, and as recommended for adoption by the Grand Junction Planning Commission is hereby adopted.

The full text of this Ordinance, including the text of the North Avenue West Corridor Plan, in accordance with paragraph 51 of the Charter of the City of Grand Junction, shall be published in pamphlet form with notice published in accordance with the Charter.

INTRODUCED on first reading the 17 day of October, 2011 and ordered published in pamphlet form.

PASSED and **ADOPTED** on second reading the 2nd day of November, 2011 and ordered published in pamphlet form.

/s/: Tom Kenyon
President of City Council

ATTEST:

/s/: Stephanie Tuin
City Clerk

2011

North Avenue West Corridor Plan

(12th Street west to I-70B)



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Executive Summary

The planning for the North Avenue West Corridor Plan is an offshoot of a larger effort to address planning issues throughout the North Avenue Corridor. Over the years North Avenue has lost a significant amount of business to relocations to the west side of the City, and the recent recession has resulted in many business closures. These changes present the City with an opportunity to bring together City planners, residents, and business owners to examine ways to encourage re-development along the corridor and envision what the future might look like along North Avenue.

In 2007, the City Council adopted the North Avenue Corridor Plan for the area from 12th Street east to the I-70 Business Loop. This North Avenue West Corridor Plan addresses the area from 12th Street west to I-70B. Once both plans have been adopted, implementation of these plans will include creating an overlay district for the entire corridor that establishes a street cross-section and landscape standards. Over time as redevelopment and new development occurs in the corridor, North Avenue will begin to transform into the long-range vision outlined in these plans.

The North Avenue West Corridor Plan envisions North Avenue between 12th Street and west to I-70B as a mix of retail, office, commercial and residential uses that will provide services for the student population of both high school and college students, and provide mobility for pedestrians, bicyclists, and transit riders. The vision for this area includes safety, enhanced aesthetics, and a 'sense of place.' It will be a neighborhood that attracts residents and students with entertainment, educational opportunities, and public activity areas.

This plan divides the corridor from 12th Street west into three 'districts.' The first would be Automotive Sales and Service from I-70B to First Street. The second 'district' is the Sherwood Park Mixed Use District from 1st Street to 5th Street, and the third would be the Educational/Student Commercial and Entertainment District from 5th to 12th Streets.

In order to accomplish these goals, much discussion took place about the elements of the street that would contribute to creating a 'sense of place' as well as other guiding principles of safety, aesthetics and minimizing neighborhood impacts. These elements consist of consolidating existing curb cuts and parking lots, adding sidewalks and planting, and adding pedestrian scale street lights, trees, signs, benches and other outdoor spaces to bring people back into the corridor.

The public process for this plan was as inclusive as possible, involving focus groups with residents, businesses, and Mesa State College personnel and students, who were asked to fill out a questionnaire. This was followed by an open house and questionnaire for the public. The second public open house introduced concepts and design elements and asked for comments. Six options for street cross sections were developed and presented to the public for input, and an online survey was made available that was promoted to all previous open house attendees and the public at large through the

media and the City's website and social media sites. There were 351 people who filled out the online survey.

Of the six street cross sections the first option was the most inexpensive option of just re-striping the street with a five-foot wide bike lane. The other five options all included adding 10 feet of right-of-way on either side of the street. Option 2 and 5 did not include bike lanes. Options 4, 5 and 6 included varying widths of sidewalk, buffer areas, and bike lanes.

Support was strongest for Options 3 and 4, which both included the 10 additional feet of right-of-way on each side of the street, eight -foot detached sidewalks, buffer areas and a bike lane.

From the comments received on the online survey, residents and business owners alike are concerned about the future of North Avenue, and wish to see it restored as a place which attracts people and businesses, and remains a vital part of our community and contributes to our local economy. These plans and the adoption of a unified street cross-section and design standards will enable North Avenue to grow and change in the future, and remain a viable, vibrant part of our community.

Planning Boundary

Why Plan North Avenue?

Historically, North Avenue was the northern most boundary of Grand Junction. As development continued to grow to the north, the North Avenue corridor now finds itself in the City Center area of Grand Junction, in proximity to many great assets and amenities such as Stocker Stadium and Lincoln Park, Colorado Mesa University (previously known as Mesa State College) and a wide range of community services, hospitals, and easy access to downtown.

Until the 1990's North Avenue was the primary retail tax generator for the City of Grand Junction. However, over the last few years the area has experienced a dramatic loss in revenue in great part associated with the extensive development of new and large commercial and retail centers along the western edge of the City. With business pulled to these new commercial centers, North Avenue has an opportunity to reestablish itself by creating a unique community environment where people will come back to again and again.

Rather than mimic the developments occurring on the western edge, the North Avenue Corridor can reclaim its identity by promoting developments that combine retail, office, residential and civic components to establish a distinctive and thriving sense of place and character. This will provide, as supplement to downtown, a historically rich active neighborhood with a focus on educational opportunities, employment, entertainment and mixed use.

The Planning Area runs from I-70 Business Loop on the west to 12th Street on the east. It includes one or more blocks north and south of North Avenue for the northern and southern boundaries (see map below).

Questionnaire Question #3
The Grand Junction Comprehensive Plan has identified this area as a mixed use center. What land uses are lacking that you would use?

Survey Responses:
Restaurants = 61%
Professional Services = 25%
Retail = 36%
Housing = 23%
Other = 20%
None = 16%

(Note: multiple answers were received)



In 2007, the City of Grand Junction completed and adopted "The North Avenue Corridor Plan", a corridor plan for the North Avenue area east of 12th Street to I-70 Business Loop. The North Avenue West Corridor Plan for that area west of 12th Street continues the planning effort for North Avenue. This Plan incorporates many of the 2007 North Avenue Corridor Plan elements, while acknowledging and enhancing the unique features of the west end of North Avenue such as its proximity to Colorado Mesa University, the historic downtown neighborhood and the Sherwood Park neighborhood, an early suburban growth area of the city. Both Plans look far into the future, over the next 25 years, the time horizon established by the City's Comprehensive Plan.

In all long range planning, the collective ideas of many people will launch the community into the future in a way that will be meaningful and successful. The vision, guiding principles, and the street cross-section were shaped through public participation. Key elements of the process included public questionnaires, open houses, focus groups, a technical advisory committee and an online survey taken by over 350 people.



"North Avenue improvements are much needed to upgrade old conditions. It's important that North Avenue remains a vital business center for growth." - Online Survey Comment

North Avenue West Corridor Plan Vision

We are planning North Avenue for people and places, a crossroads of Grand Junction, a corridor to the City Center (see graphic on following page). A place where higher education facilities connect with medical facilities, downtown, sports facilities, historic neighborhoods, existing and future residential neighborhoods, regional retail and employment opportunities.

Questionnaire Question #1 What brings you to this part of North Avenue?

Survey Responses:

Pass through here = 52%
Obtain services here = 39%
Live within a couple of Blocks = 23%
Own a business here = 20%
Go to School, Church = 20%
Own property here = 18%
Work here = 14%

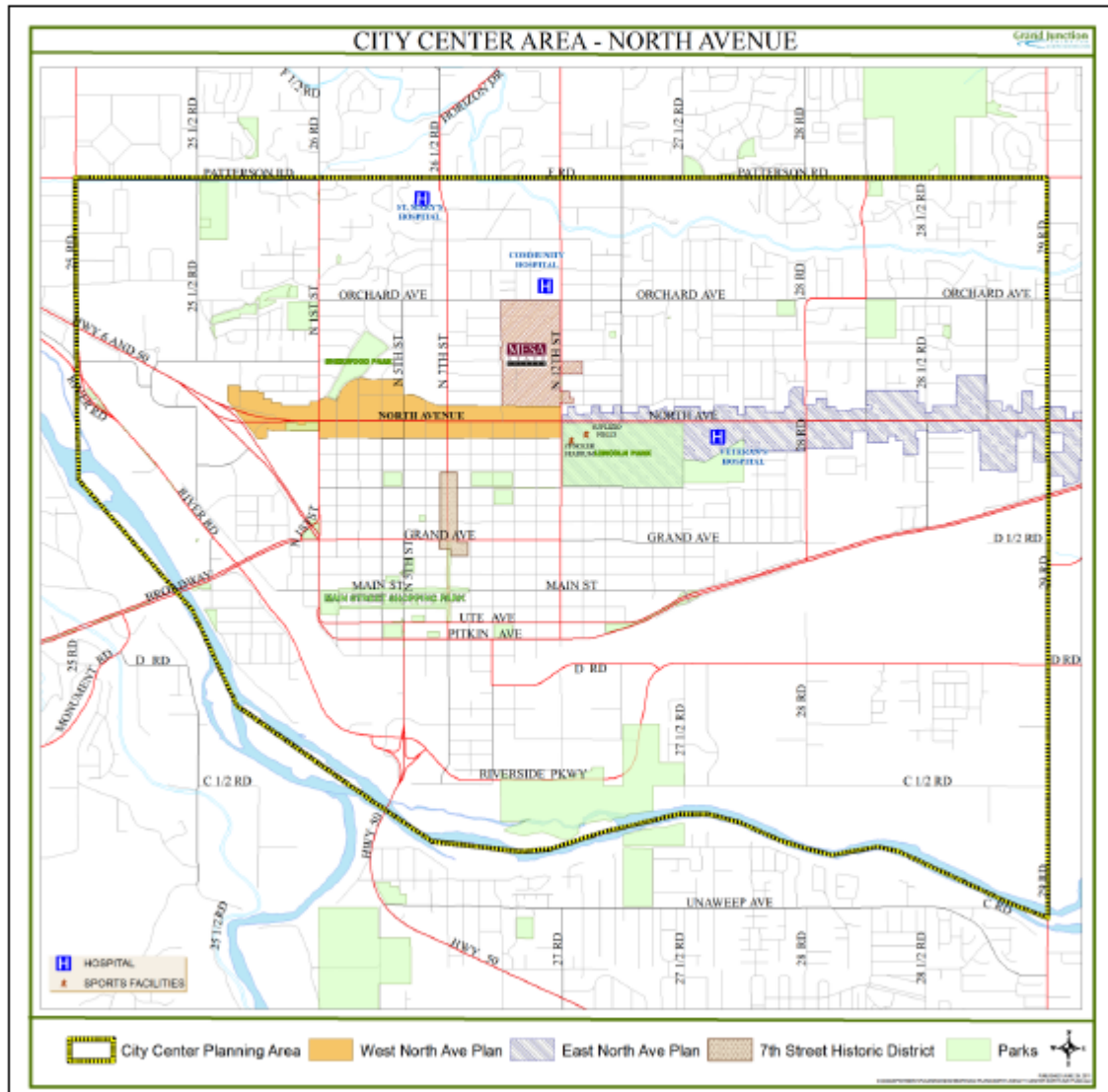
(Note: multiple answers were received)

The North Avenue corridor is suffering with a higher vacancy rate, nearly double than the rest of the City combined. Major vacancies have occurred in the Eastgate and Teller Arms shopping centers in the recent past, both of which are located east of 12th Street outside of this planning area, but no less affect the West Corridor Study area. It is certainly understandable that the entire North Avenue corridor must work together for sustainability and the future success of the corridor. Infrastructure needs crossover both planning areas and must be planned together and either be the same or at the very least complement each other.

The North Avenue West Corridor Plan includes an overall strategy to revitalize the corridor and support its continued growth in order to promote the future development of retail, commercial, office, entertainment and residential opportunities in the corridor. Specific strategies for the implementation of improvements have been identified and include the following.

1. Create services at the neighborhood level and for the student population;
2. Improve mobility for pedestrians, bicyclist and transit riders; and
3. Create a significant "neighborhood" of residential, retail, commercial, entertainment, educational and public activity areas.
4. Designing the public realm. Develop guidelines for design that answer the questions:
 - a. What is the appropriate setback related to the public right-of-way?
 - b. What should happen between the street curb and the front of the building?
 - i. What is appropriate landscaping? Should it be a combination of landscaping and hardscape?
 - ii. Pedestrian amenities, what should they include?
 - c. Where should parking be located? How should it be accessed from the building(s)?

- d. What is the function of public streets?
- What role do alleys and neighborhood streets play in traffic circulation?
 - What is the functionality of North Avenue and how does that interface with the street edges?



Guiding Principles

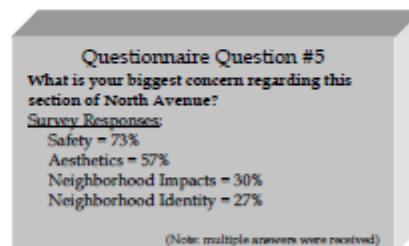
From the focus group meetings held to the open houses attended, participants identified specific elements for each of the four guiding principles; safety, aesthetics, placemaking and neighborhood impacts.

Safety - Establishing a multi-modal approach by promoting pedestrian safety and key locations for pedestrian crossings; creating safe access routes for bicycles; constructing bus pullouts and public stops for transit passengers and maintaining an efficient street for all motorized traffic.

- Safe pedestrian access on North Avenue Corridor, along and across the corridor. Key crossings include 1st, 3rd, 5th, 7th 10th & 12th
- Provide adequate lighting along the corridor.
- Provide access management by limiting the number of access points onto North Avenue and keep medians.
- Provide a safer environment for bicycle traffic.
- Provide bus pull-outs at transit stops.

Aesthetics - Creating standards that support the vision and corridor as a destination and a crossroads.

- Create standards for
 - Landscaping
 - Signage
 - Way Finding
 - Building Architecture
 - Building Location
 - Lighting
 - Entry Features
 - Banners (pedestrian scale)
 - Public Spaces (medians, pocket parks and plazas)



Placemaking - Envisioning North Avenue holistically, a corridor that is a *destination* itself, not simply a street to travel through.

- Establish an entrance, you have arrived, slow down.
- Establish three sub-areas or districts and create a vision for each.
 - Automotive Sales and Service District (I70B to 1st St.)
 - Sherwood Park Mixed Use District (1st St. to 5th St.)
 - Educational/Student Commercial and Entertainment District (5th St. to 12th St.)
- Create parking areas. Locate parking to the rear of businesses.
- Encourage outdoor spaces/uses (i.e. outdoor seating, plazas).

- Create work/live opportunities (mixed use).
- Establish entertainment venues.
- There is a need for hotel(s).

Neighborhood Impacts - Minimize impacts to existing neighborhoods as Neighborhood Centers are established on 3rd Street between North Avenue and Sherwood Park; and in the vicinity of Colorado Mesa University. As future university expansion occurs west to 7th Street and subsequent university supportive development occurs north and south of North Avenue between 5th Street and 12th Street, mitigate potentially negative impacts on existing neighborhoods.

- Establish 3rd Street as a mixed use center (increase density and intensity) and tie to Sherwood Park.
- Allow for university expansion to 7th Street.
- Minimize traffic impacts to existing and future residential areas.
- Encourage the use of secondary streets for neighborhood traffic circulation and buffering from more intensive uses.



Area residents and business owners came to the April 20th Open House to view the proposed North Avenue West Corridor Plans and give comments.



Revitalizing North Avenue

The City of Grand Junction conducted a “windshield” survey of vacant commercial building space for the North Avenue Corridor in January 2011. Results show that the area has an 11.4% vacancy rate. The same survey showed a vacancy rate of 6.4% for the entire City.

Questionnaire Question #2
How do you access the corridor?
Survey Responses:
Private Vehicle = 98% Bike = 23%
Walk = 16% Bus = 2%
(Note: multiple answers were received)

Concentrated efforts in streetscape treatment, community identity, way finding, signage and architecture can help existing businesses and spur future business development of a corridor. Improving the streetscape would set a foundation that is enticing for new development and improve North Avenue as a destination.

For North Avenue west of 1st Street, the cross section of the existing street is a highway with wide medians and frontage roads. Although pedestrian/bicycle access through this section of North Avenue is recommended, the street cross section is much different than what should be recommended east of 1st Street. The width of right-of-way west of 1st Street is much greater.



Ultimately, the right-of-way width of North Avenue east of 1st Street needs to be 100 feet wide with 50 feet of half right-of-way expected from each side. Much of the street today is 80 feet in width except where new development occurred over the past 20 years and additional right-of-way was dedicated. The same is expected of future development as well. Having 100 feet of right-of-way will allow for ample streetscape improvements; revitalizing North Avenue into a corridor that is once again a destination for the community.



The pictures above (taken on Grand Ave.) show visually the amount of space that can be set aside for streetscape on North Avenue.

Existing Pedestrian Conditions

Existing sidewalk conditions along North Avenue range from narrow 3 feet attached sidewalks to paved surfaces that are shared with parking areas, to detached 6 feet wide sidewalks with a landscaped area between the curb and the sidewalk. North Avenue is a high pedestrian use corridor within the study area particularly between 5th Street and 12th Street with the influence of the student population during the day.



Narrow 3 ft. sidewalk



Attached 4 ft. sidewalk



Attached to detached



Wider detached sidewalk more comfortable for pedestrian use

Street Cross-Sections

During the planning process existing conditions were studied, six concepts were developed and studied and public comments sought. Public input on potential street cross-sections was received at the April 2011 open house and from an online survey conducted in May and June 2011. These six concepts (Option 1 through Option 6) are included in the appendix. Generally, the street sections focus on the following elements:

1. Construct wider sidewalks - detached and/or attached; and/or
2. Provide bike lanes on North Avenue by restriping existing pavement or widening pavement section; and/or
3. Provide on-street parking by reconstructing the street within a wider right-of-way.

April 2011 Open House Results

The following table presents details of each of the six options. At the April 2011 open house, those in attendance were asked to identify their two favorite options. Options 2, 3 and 4 garnered the most support. Support for these three options placed strong emphasis on creating a corridor with wide detached sidewalks and landscaping; and gives good support for a bike lane on North Avenue, but also suggests that there are concerns with narrow vehicular travel lanes. The question needs to be asked, "Are 11 ft. wide lanes too narrow?" Clearly they are still wide enough to accommodate heavy truck traffic. They also help curb speed and slow traffic down.

What we heard at Open House?

Preferred Street Concepts

- Wide Detached Sidewalks
- Landscaping
- Bike Lanes
- Wide Vehicular Travel Lanes

Existing Conditions	Option 1 (Restripe with Bike Lanes)	Option 2 (Widen sidewalk/Add Landscaping Strip)	Option 3 (Add Bike Lane & Widen Sidewalk/Add Landscaping Strip)	Option 4 (Remove Curb and Widen Street 3 ft/ Widen Sidewalk/ Add Landscaping Strip/ Add Bike Lane)	Option 5 (Remove Curb and Widen Street 3 ft/ Widen Sidewalk/ Add Parking Lane)	Option 6 (Remove Curb and Widen Street 8 ft/Widen Sidewalk/Add bike Lane/Add Parking Lane)
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Based on citizen input from the open house, the preferred street cross sections are Option 2 and Option 3 for the following reasons.

- Both options provide wider detached sidewalks.
- Both options provide landscaping.
- Both options are simpler to implement and can be completed in increments.
- The bike lane in Option 3 can be implemented in the future by restriping the pavement to add the bike lane. What is constructed beyond the curbs is the same for both options.

May – June 2011 Online Survey Results

The City of Grand Junction conducted an online survey for 30 days between the months of May and June 2011. A total of 351 surveys were completed by the public. Using the same cross-sections introduced at the April Open House, the survey focused on seeking input from the public regarding dedicated bike lanes, on-street parking, and just how wide the travel lanes, bike lanes and pedestrian areas along the corridor should be if they are desired.

Results from this survey indicate nearly three out of four responders said that bike lanes should be

Do you think bike lanes are important to have along North Avenue?

Responses		
Yes	260	74%
No	91	26%
Total	351	

incorporated into the future design of North Avenue. However, creating parallel parking on North Avenue didn't receive much support with 92% saying that it was a bad idea.

The survey asked each person to identify their top two options for cross-sections for North Avenue. There were six options to choose from and descriptions along with the results of the survey are shown below.

	Number One Choice	Number Two Choice
Option 1. Re-stripe North Avenue with a five-foot wide bike lane.	31	16
Option 2. Add 10 feet of right-of-way width on each side with eight-foot detached sidewalks and eight feet of buffer between pedestrians and traffic.	64	51
Option 3. Add 10 feet of right-of-way on each side of the street, an eight-foot detached sidewalk, an eight-foot buffer area, and a five-foot wide bike lane.	104	143
Option 4. Add 10 feet of right-of-way on each side of the street, an eight-foot detached sidewalk, a five-foot buffer area, and a six-foot striped bike lane.	125	85
Option 5. Add 10 feet of right-of-way on each side of the street, an eight-foot parking lane, and no bike lane.	17	27
Option 6. Add 10 feet of right-of-way on each side of the street, an eight-foot parking lane, and a five-foot bike lane.	10	29
	351	351

If you combine the top two choices that people selected, Option 3 comes out as the overall top choice with a total of 247 picks and Option 4 is second with 210 people picking it either number one or number two.

The survey also asked participants to rate various elements of any future redesign of North Avenue from “Very important” to “Not at all important.” The results are shown in the following table.

	Very important	Somewhat important	Neutral	Somewhat unimportant	Not at all important
Traffic flow and convenience	70.70%	19.70%	6.80%	1.70%	1.10%
Safety	85.20%	10.80%	2.30%	0.60%	1.10%
Aesthetics (appearance)	42.50%	38.20%	13.10%	3.70%	2.60%
Bike lanes	49.90%	22.20%	6.00%	6.60%	15.40%
On-street parallel parking	2.30%	6.00%	9.40%	16.20%	66.10%
Creating a pleasant place to walk	42.50%	33.60%	13.10%	5.40%	5.40%

Traffic flow and convenience and safety ranked very important to the public. Aesthetics, bike lanes and creating a pleasant place to walk are important to those taking this survey as well with most people ranking them as either Very Important or Somewhat Important. Results for on-street parallel parking were Not Important to most survey participants.

Recommended Street Cross-Section

The recommended street cross-section is Option 3. After taking into account the survey results, public comments received at open houses, focus group meetings, the work by the Technical Advisory Committee for this corridor plan, and the financial costs for construction, the street cross-section in Option 3 was selected. Option 3 incorporates the most features the public stated as being important. These features include creating an improved, more aesthetic and safer pedestrian corridor and include bike lanes. These features are also found in Option 4, but Option 3 is financially a better choice than Option 4. Option 4 would require reconstruction of the curb and gutter and adding additional pavement to the street while Option 3 works within the existing curb and gutter or street width. Both options will require ten additional feet of right-of-way to improve the pedestrian and landscaping areas.

Plan Elements

The following elements of this Plan will aid in helping the North Avenue corridor achieve its Vision and Guiding Principles; create services at the neighborhood level; improve mobility and safety for pedestrians, bicyclists and transit riders, and create a significant neighborhood of residential, retail, commercial and public activity areas.

Creating a more unified street edge condition along North Avenue

Creating a more unified street edge will improve the overall character of the corridor. This can be accomplished by:

- Consolidating existing curb cuts and parking lots.
- Providing more definition to vehicular entry ways.
- Adding sidewalks and plantings.
- Adding pedestrian scale street lights, trees, benches and other amenities.



These suggested changes to North Avenue will create a more pedestrian friendly environment and encourage local residents to walk. Most of these improvements can be made within the existing street right-of-way. East of 1st Street, the future overall width of the right-of-way will be 100 feet with right-of-way dedication needed up to 50 feet from each development on both sides of North Avenue.

Designing Street Intersections

The design of an intersection requires a balance between the needs of vehicles, pedestrians, bicyclists, freight and transit. The following are attributes of good intersection design for pedestrians, as documented in AASHTO'S guide for the Planning, Design and Operation of Pedestrian Facilities (2004).



- Clarity - Making it clear to drivers that pedestrians use the intersections and indicating to pedestrians where the best place is to cross;
- Predictability - Drivers know where to expect pedestrians;

- Clarity - Making it clear to drivers that pedestrians use the intersections and indicating to pedestrians where the best place is to cross;
- Predictability - Drivers know where to expect pedestrians;
- Visibility - Good sight distance and lighting so that pedestrians can clearly view oncoming traffic and be seen by approaching motorists;
- Short Wait - Providing reasonable wait times to cross the street at both unsignalized and signalized intersections;
- Adequate Crossing Time - The appropriate signal timing for all types of users to cross the street;
- Limited Exposure - Reducing conflict points where possible, reducing crossing distance and providing refuge islands when necessary; and
- Clear Crossing - Eliminating barriers and ensuring accessibility for all users.

Signalized intersections typically have marked crosswalks, Walk/Don't Walk indications, and regulatory signing. Motorists expect pedestrians and pedestrians are reasonably well protected when crossing at these locations. The North Avenue West corridor currently has six signalized intersections. Most of these signals are located at approximately one quarter mile intervals.

Questionnaire Question #6
What would you like see improved or changed?
Citizen Comment:
Safety needed for bikes and pedestrians without impeding traffic.

In urban areas, pedestrians must be able to cross streets at regular intervals. They rarely will go more than 300 to 400 feet out of their way to take advantage of a controlled intersection. Consequently, the crossings at uncontrolled locations deserve serious consideration as urban arterial corridors redevelop. Treatments commonly used on higher-volume multi-lane streets throughout the United States include: high-visibility markings with double-posted pedestrian crossing signs; refuge islands; flags; and flashing beacons. Signals that are used just for pedestrian crossings are used primarily at mid-block crossings.

Pedestrian Crossings

<u>North Avenue Pedestrian Crossings</u>		
<u>Signalized & Striped</u>		<u>Unsignalized & Not Striped</u>
1 st Street	5 th Street	3 rd Street
7 th Street	10 th Street	
	12 th Street	

There are six pedestrian crossings identified within the study area that are the most ideal locations for pedestrian traffic to use. Five of these crossings are currently



opportunity to cross. Future analysis and review of this sixth location will be needed to determine signal warrants for a safer and friendlier crossing.

Over the life of this Plan (the next 25 years) as pedestrian oriented development occurs on 3rd Street north of North Avenue, an area identified as a neighborhood mixed use core for the Neighborhood Center, a signal will likely be needed.

North Avenue Streetscape

There are many tools that can be used in design of a streetscape that improves the look, functionality and vitality of a corridor along with establishing a sense of place that brings people back. Street furniture such as benches, art sculptures, plantings and trash cans dispersed within pedestrian ways and civic spaces encourages pedestrian use and provide a sense of belonging. Pedestrian lighting provides a safer environment in the evening encouraging businesses to stay open longer hours, providing a catalyst for activity and night life.

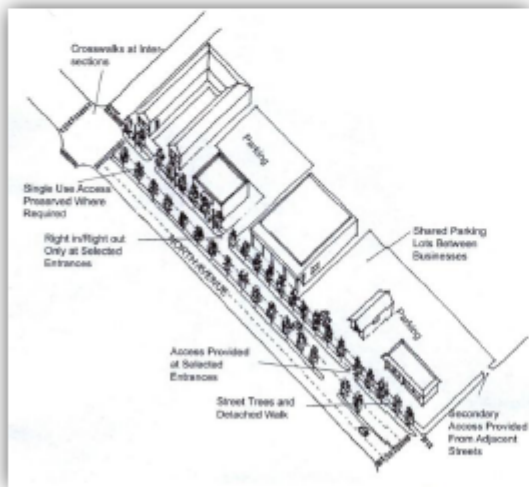
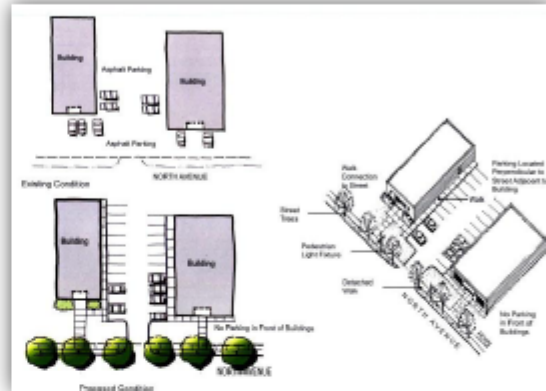
The creation of a more unified street edge condition along North Avenue will improve the overall character of the corridor. This can be accomplished by consolidating existing curb cuts



and parking lots, providing more definition to vehicular entry ways, and adding walks and plantings. The addition of pedestrian scale street lights, trees, benches and other amenities will create a more pedestrian friendly environment, and encourage local residents to walk or ride a bicycle. Most of these improvements can be made within the existing street right-of-way.

Building Adjacent To Street

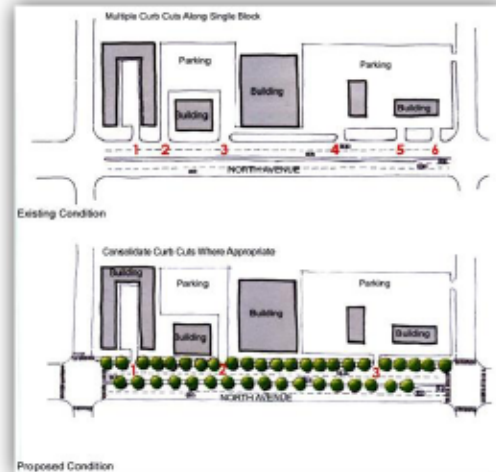
In areas where buildings front the street, suggested improvements include sidewalks that connect building entrances to sidewalks along North Avenue, defined curb cut entrances, and the relocation of parking between buildings. These improvements will enhance the street edge by reducing the number of parked cars in front of building entrances, provide space for street tree plantings, and reduce the expanse of asphalt.



Building adjacent to the street makes the buildings easier to see and identify from North Avenue. A front door on North Avenue invites foot traffic to go from one business to another. Additional sidewalks provide a clear, safe pedestrian path between parking lots to building entrances, and provide pedestrian connections to North Avenue.

Curb Cut Consolidation

Reducing the quantity of curb cuts along North Avenue will greatly enhance the overall character of the corridor, and improve vehicular and pedestrian circulation. Curb cuts can be consolidated in areas where parking lots can be linked and shared by adjacent uses. Consolidating curb cuts also provides additional space for plantings and sidewalks adjacent to North Avenue within the right-of-way and less discontinuity of the sidewalk. It will result in fewer interruptions in traffic flow. Curb cuts will be consolidated at the time of redevelopment of a site.



Areas of potential redevelopment along the corridor provide an opportunity to greatly enhance the character of the corridor, by creating pockets of redevelopment that will stimulate further improvements. Redevelopment will consist primarily of additional residential, office, retail/commercial areas, or new mixed use developments.

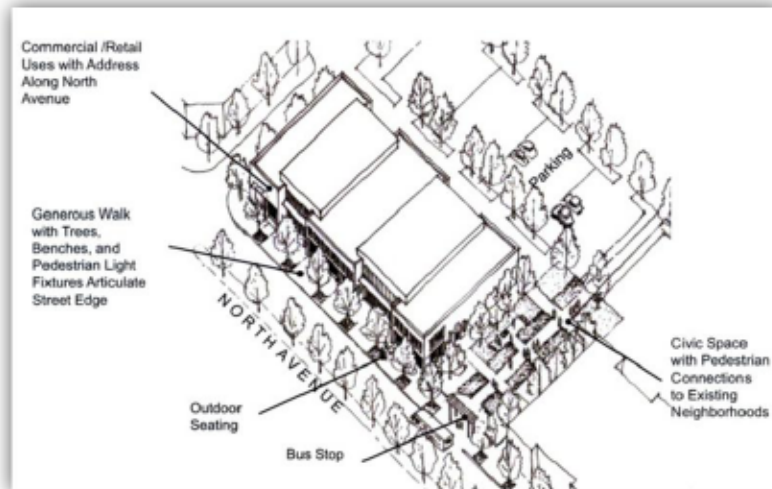
In all cases, each project should provide connectivity to the pedestrian network and include public open space.

Residential Land Uses

Multi-family developments adjacent to North Avenue will provide a distinct living option for residents along the corridor. Multi-family development is currently needed by Colorado Mesa University students. That need for housing will continue to grow as the student population grows. Located within walking distance of shopping, restaurants, bus service, and employment opportunities, multi-family development offers a very desirable alternative to single family housing.



One of the Guiding Principles includes minimizing neighborhood impacts to existing and future residential areas. Minimizing such impacts includes buffering between land uses with landscaping and berms, as well as providing good traffic circulation. Creating and enhancing a grid system of streets and corridors provides traffic a number of choices, thereby dispersing the traffic.



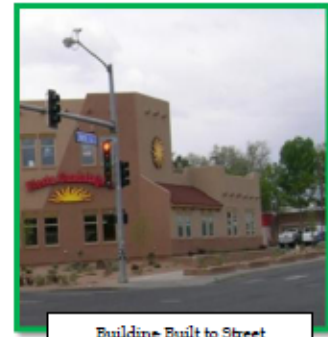
A green corridor in a walking environment will create a pedestrian connection with existing neighborhoods and North Avenue providing convenient access to the street for existing residents. On-street parking along the side

streets provides parking for visitors. On-site parking areas must provide buffering with residential areas through the use of berms and landscaping.

Commercial / Retail Land Uses

Commercial and retail buildings along North Avenue are best located close to the street, which helps to define and increase the use of the street edge. Generous sidewalks with spaces for outdoor seating, active open space, pedestrian lights, and street trees enhance the character of the corridor.

Building signage can be located directly on the face of the buildings, which can be readily identified from passersby on North Avenue. Front door access to retail/commercial uses should be provided on North Avenue with additional access from the interior side, providing easy access from nearby parking.



Building Built to Street



Outdoor Seating, Front Door Access to Street, and Side Access to Parking Lot.

Civic spaces such as plazas, corridors, or pocket parks create a permeable frontage along North Avenue that provides easy pedestrian flow between the street and commercial or retail uses. Parking lots must buffer themselves from adjacent and across the street residential areas through the use of berms, fencing and landscaping.

Transit

North Avenue is the highest transit use area for Grand Valley Transit (GVT). All transit stops on North Avenue should be off-street pull-outs. Bus Shelters should be incorporated at higher use transit stop locations.

North Avenue Bus Routes

GVT Route 7 Bus Stops

West of 12th Street (Colorado Mesa University)
West of 11th Street
East of 8th Street
West of 7th Street (REI Sports)
West of 5th Street
West of 3rd Street (former Harbert Lumber)



Annual Ridership

2010

Route 7 = 69,786

Route 9 = 189,038

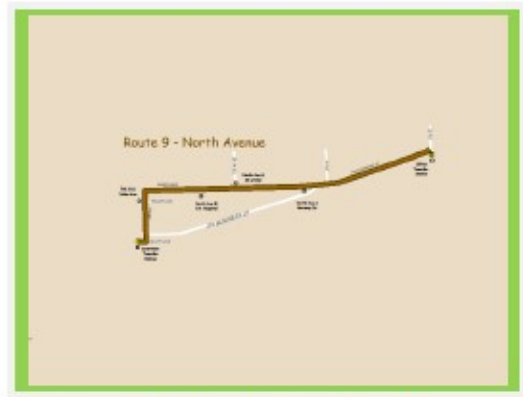
2009

Route 7 = 65,440

Route 9 = 169,105

GVT Route 9 Bus Stops

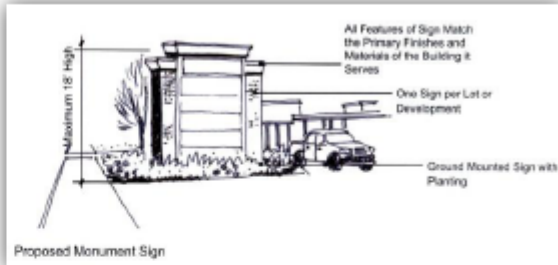
East of 8th Street
East of 9th Street
West of 11th Street



7th Street and 12th Street are bus transfer points connecting passengers to other routes in the City.

Signage

Improvements to signage along North Avenue can be accomplished by minimizing the quantity of pole mounted signs, and replacing them with monument signs. This will aid in reducing the visual clutter of the pole mounted signs, and also provide an opportunity for street trees to be planted. Because monument signs are

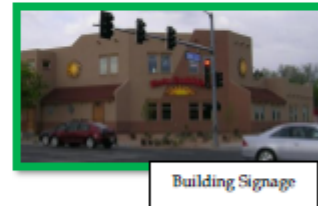


low, they are not in conflict with the larger canopy of the street trees, and can be readily seen from passing traffic. This philosophy supports a distinction of commercial areas as opposed to individual pad development. It encourages a walking environment and provides for a group of



retailers to engage the property as a whole.

Encouraging buildings to be constructed next to the street allows the building along with signs on the face of the building to advertise the business to passersby on North Avenue. In effect, the building becomes part of the sign advertising the business, helping the public identify the business.



These recommended changes to signage west of 12th Street are also part of the recommendations found in the 2007 North Avenue Corridor Plan adopted by the City of Grand Junction for North Avenue east of 12th Street.



Districts

The North Avenue West Corridor Plan is divided into three districts. Each district is unique and should transition from one to the next. The goal for each is to establish its own identity providing a sense of place. “Placemaking” is a process of creating a place that will attract people because the place is pleasurable or interesting and encourages people to come back again and again. Maintaining North Avenue as a destination is very important to its long term sustainability and for the City as a whole. Creating three districts along this section of North Avenue allows diversity and encourages a unique vision for each. (Additional maps are located in the Appendix.)



Automotive Sales and Service District

Located on the west end of North Avenue and comprising only seven properties, the Plan is recommending little change for the Automotive Sales and Service District. This area has long been established as a commercial area with car sales and services. The area currently comprises a mix of automotive sales and service businesses, a furniture store and other retail and service businesses. The continued indoor and outdoor retail and service is appropriate and expected to continue for this area of the Plan.

The Automotive Sales and Service District serves as the west entrance to the four mile long North Avenue corridor. The large existing landscaped open space within the public right-of-way along the south side of the corridor is extremely important as an entry feature. It is important to maintain this feature into the future.





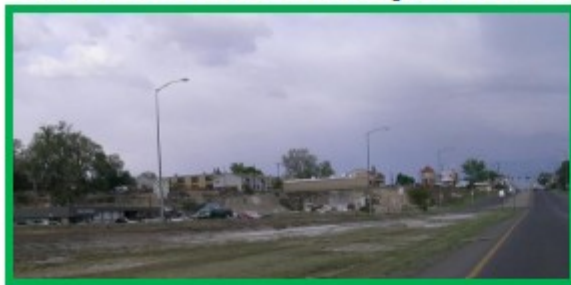
Vision

Continue to serve as an automotive and services center for the Grand Junction area.

Street Cross Section

North Avenue (through this district) is a State Highway (US Hwy 6) designed with a frontage road along the north side and an interchange using slip lanes at its intersection with I-70 Business Loop. The Plan does not recommend any changes to this street section as it does for the other two Districts, except for providing for future pedestrian and bicycle facilities.

Pedestrian and bicycles are now left to their own to navigate this section of the corridor. It is recommended that off-street pedestrian and bike paths be constructed on both sides of the corridor.



Along the south side of the corridor a future path should be located within the landscaped open space. On the north, a path should be constructed along the existing frontage road and any future frontage road that is built as part of redevelopment of that area.

Existing Traffic Issue

Vehicular traffic currently backs up along west bound North Avenue at the intersection of 1st Street. This traffic issue is the result of motorist's need to get in the right lane prior to the left turn lane for



Questionnaire Question #4
Are there currently issues you would like to see addressed?
Citizen Comment: Traffic review is needed at North & 1st Street, traffic backs up in the right lane for traffic going straight and turning right.

the Ed Bozarth car dealership. West of 1st Street the left through lane ends and requires a left turn movement at Ed Bozarth, therefore many motorist are cueing up in the right lane east of the traffic signal at 1st Street and North Avenue to avoid this conflict. This

Plan recommends that a dedicated left turn lane be constructed at Ed Bozarth allowing both westbound lanes to be used by vehicular traffic heading west (see exhibit). This will allow for the motorist not to have to move over to the right lane east of 1st Street which often creates a back-up of traffic for a block or more.



Proposed changes to intersection at Ed Bozarth's

Sherwood Park Mixed Use District

The Sherwood Park Mixed Use District comprising that area from 1st Street to 5th Street has many existing multi-family, office, retail and service businesses already. The Comprehensive Plan identifies this



area as a Neighborhood Center and encourages infill and redevelopment providing additional density and intensity. This Plan recommends that the core area of the Neighborhood Center be established along 3rd Street from North Avenue to Sherwood Park. This location is ideal with its proximity to North Avenue, a major arterial street providing access to the core area and the park a couple of blocks to the north. Sherwood Park provides open space opportunities and public facilities for the Neighborhood Center. Parking for businesses should occur to the rear or side of businesses, and shared parking facilities is encouraged.



Vision

Establish a mixed use center as identified in the Comprehensive Plan.



Educational / Student Commercial and Entertainment District

Students define a large part of what is happening and what is expected to happen in this district. This Plan encourages future business growth centered on the needs of the student population (both high school and college students), the needs of a higher learning institution, in addition to the community at large. During the planning process both Colorado Mesa University students and Grand Junction High School students voiced their desire and need for more student oriented services including food establishments and entertainment venues within this area of North Avenue from 5th Street to 12th Street.



Apartment living already exists in this district and adjacent areas, many of which house college students. Colorado Mesa University added new on-campus housing over the past several years for nearly 1,000 students. As the student population grows additional housing for students will be needed. Students commuting to the university often park on the residential streets near the campus adding to the number of

pedestrians using this corridor. With students comes the need for pedestrian access and amenities.

Parking for businesses and students creates high demand for parking on neighborhood streets. Parking along Glenwood Avenue and Belford Avenue help meet this parking demand; however shared parking facilities need to be considered and planned for as the demand for parking continues to increase.

Vision

Provide appropriate housing, businesses and services for the student population and in support of the existing and future university educational facilities.



Implementation Plan

1. Create an Overlay District for both the North Avenue West Corridor Plan (I-70 B east to 12th Street) and the 2007 North Avenue Corridor Plan (12th Street east to I-70 B).

Include the following elements in the Overlay District:

- Establish a street cross section for the entire length of North Avenue. Results of the online survey and recommendations from the Plan's Technical Advisory Committee select Option 3 as the preferred street cross-section.
- Create landscaping and setback standards for the corridor that will:
 - Incorporate design features found in the street cross section.
 - Support the placement of buildings adjacent to the street.
 - Establish desired buffering and landscaping between residential and commercial uses and other Plan elements. These standards will modify existing landscaping standards required as part of the existing zoning for properties within the corridor.

2. Establish Implementation Tools.

The following are possible tools that can be considered within or without an Overlay District. Some will require a change in current policy and will need to be formulated and approved by the Grand Junction City Council. Others will require existing property owners to join together to implement.

- a) Form a Business Association.

Businesses in a given area can come together voluntarily to create an association for the improvement and enhancement of their properties and businesses. This can include creation of covenants that run with the land and provide for assessments on the parcels of land subject to the covenants. This creates a pool of funds for improvements that benefit the group.

- b) Require new development to build the detached sidewalk and other improvements. Construction of detached sidewalks can occur along any frontage with sufficient right-of-way, but requires the sidewalk to transition back to the existing attached sidewalk on both sides of the property being developed. Local examples of this can be found on other corridors as well as North Avenue. The picture taken of 12th Street north of Orchard Avenue (to the right) is an example of this concept of transitioning the sidewalk on both sides of the development.



- c) Modify the Transportation Capacity Payment (TCP) fee for the corridor. This tool could be implemented with the previous tool where new development is required to construct detached sidewalk and other improvements along their business frontage. It can be argued that North Avenue is an area where street improvements are already built for the traffic capacity of the roadway. Widening of the road is not anticipated and appropriate infrastructure is already in place, so there is less need to collect a Transportation Capacity Payment (fee) from properties along this corridor. This argument would support collecting the fee in areas of the City where "Greenfield" development, development constructed away from the City Center, is occurring.
- d) Define and create a Business Improvement District (BID). Colorado Statute Section 31-25-101*et seq* authorizes for the formation of Business Improvement Districts (BID). BIDs are formed within a municipality and as such the City of Grand Junction would oversee the formation of the District and appoint a Board of Directors. Under the Statute, the District is granted the power to levy and collect ad valorem taxes on all taxable commercial property within the boundaries of the District. All property assessed in a BID must be commercial property. The tax or mil levy is set by the District up to a limit of 5.0 mils (.005) upon every dollar of the valuation assessment of taxable property within the District. The Mesa County Assessor would collect the mil levy for the District through property taxes. These tax dollars can be used by the District for infrastructure, aesthetic treatment and other improvements within the District which will benefit the District members. A BID can finance improvements, provide services and can issue bonds. Examples within the City where BID's currently exist are the downtown area and Horizon Drive.
- e) Special Improvement District.
The focus of a Special Improvement District (SID) is for capital improvements, infrastructure. A SID is formed by petition of property owners of more than 50% that will bear the costs assessed by the district and established by the City by ordinance. Funding comes from property assessments and the City constructs any funded improvements.

f) Create a Tax Increment Financing (TIF) District.

Colorado law allows municipalities to establish Urban Renewal Authorities (URAs) to finance public improvements such as streets, sewers, sidewalks, and other infrastructure related to residential, commercial, or industrial development; to redevelop slum or blighted areas; and to fund private economic development. The primary source of funding for urban renewal projects in Colorado is Tax Increment Financing (TIF). TIF is a method whereby a portion of the property taxes levied by all taxing authorities within an urban renewal area are reallocated to the municipality that is undertaking the urban renewal project. Tax increment financing (TIF) is a mechanism for funding redevelopment projects in Colorado exclusively targeted at improving blighted areas. State law in Colorado authorizes urban renewal authorities (URAs) and downtown development authority's (DDAs) to use TIF for projects that improve blighted areas. TIF allows an authority to issue and repay redevelopment bonds by using the "increment" of increased taxes collected within the TIF district after improvements are made (Section 31-25-101 *et seq.*, C.R.S.). Tax increment revenue may be generated from property or sales taxes. The *property-improvement fee* (PIF) is a sales-tax version of TIF: some or all sales taxes from a retail development are diverted to subsidize the development.

g) Urban Renewal Authority (URA).

An Urban Renewal Authority (URA) can be established to eliminate blighted areas for either development or redevelopment. It is done with purchasing land, rehabilitating; and/or selling land for development. Financing occurs through Tax Increment Financing (TIF) that must be approved by the county, on property and/or county approved sales tax. A URA is governed by a City Council appointed commission. The Authority has the ability to issue some types of bonds to finance projects.

h) Establish incentives for development and redevelopment along the corridor.

Establish a City infill and redevelopment policy and define what types of activities would receive consideration for development incentives. Incentives can include many different choices including paying required fees, constructing off-site improvements, undergrounding utilities, etc.

North Avenue - a destination corridor!

- *North Avenue is a great community asset with great potential.*
- *It needs to be planned as a good public place that promotes health, happiness and well-being.*

Planning/Public Process

Public Involvement

Focus Groups

The City held five focus groups during the early part of the planning process to obtain a wide cross section of issues, concerns and suggestions for the Planning area. These focus group meetings included two meetings with two different neighborhood groups, a focus group with Colorado Mesa University staff and students, and a focus group with youth group made up of mostly Grand Junction High School students and a focus group with School District 51 personnel.

Public Open Houses

Two open houses were held, one in December 2010 during the beginning of the planning process and one at the end of the planning process in April 2011. The first open house primarily introduced the planning process to attendees and asked for their involvement, comments and input. The



second open house introduced the many elements and concepts formulated for the Plan and asked for comments. Street cross sections were also introduced and comments on each option were sought. Attendees were informed on the results of the questionnaire conducted during the first half of the planning process which is discussed below.

Questionnaire

A questionnaire was created and made available to focus group attendees and participants at the first open house. It was available online on the city's website and available at the City's Planning Division's customer service counter. Results were tabulated and made available on the City's website at www.gjcity.org.

Online Survey

A survey was created and made available to the public online at the City's website. There were 351 people that finished the survey. The survey focused on seeking input from the public regarding dedicated bike lanes, on-street parking, and just how wide the travel lanes, bike lanes and pedestrian areas along the corridor should be if they are desired. Results were tabulated and made available on the City's website at www.gjcity.org.

Planning Commission Workshops

Four workshops were held to inform, discuss and obtain input from the City Planning Commission throughout the planning process.

Technical Advisory Committee Involvement

A Technical Advisory Committee was formed to provide expertise, analyze community input and provide recommendations. The committee members represented City of Grand Junction departments/divisions, the Colorado Department of Transportation, Mesa State College and Grand Valley Transit. It was with their input that the Plan's vision, guiding principles, and the various concepts, elements and options were created by analyzing the information obtained through the focus groups meetings, survey/questionnaire and open houses.

Plan Adoption

The work of many individuals including the public, property owners and business owners on North Avenue, residents, University personnel, representatives from School District 51, the Technical Advisory Committee and City staff developed this North Avenue West Corridor Plan. The proposed final draft went through a public hearing process, first with the Grand Junction Planning Commission on July 26, 2011, and then before the Grand Junction City Council on September 7, 2011.

Acknowledgements

Grand Junction City Council

Tom Kenyon, Mayor
Bill Pitts, Mayor Pro Tem
Sam Sasuras
Bennett Boeschstein

Laura Luke
Teresa Coons
Jim Doody

City Manager

Laurie Kadrach

City Public Works and Planning Director

Tim Moore

Grand Junction Planning Commission

Reggie Wall (Chair)
Lynn Pavelka (Vice Chair)
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Ebe Eslami
Mark Abbott

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Pat Carlow
Greg Williams (Alternate)
Keith Leonard (Alternate)

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Lori Bowers, Public Works and Planning
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Scott Peterson, Public Works and Planning
Kristen Ashbeck, Public Works and Planning
Trent Prall, Public Works and Planning
Lisa Cox, Public Works and Planning
Steve Smith, Geographical Information Systems
Derek Wagner, Colorado Mesa University

Project Manager

David Thomson, AICP, Principal Planner, Public Works and Planning

Special thanks to the Focus Group participants from:

Colorado Mesa University
School District 51 Administration
Students of Grand Junction High School
Business owners and residents of North Avenue and vicinity

Appendix

Street Cross Sections

North Avenue Existing Plan View

Option 1 Plan View - Restripe with Bike lanes

Option 2 Plan View - Widen sidewalk/ Add landscaping strip

Option 3 Plan View (Recommended) - Add Bike lane/ Widen sidewalk/ Add landscaping strip

Option 4 Plan View - Remove Curb and Widen Street 3 ft/ Widen sidewalk/ Add landscaping strip/ Add Bike lane

Option 5 Plan View - Remove Curb/ Widen Street 5 ft/ Widen sidewalk/ Add Parking lane

Option 6 Plan View - Remove curb/ Widen Street 8 ft/ Widen sidewalk/ Add Bike lane/ Add Parking lane

Maps

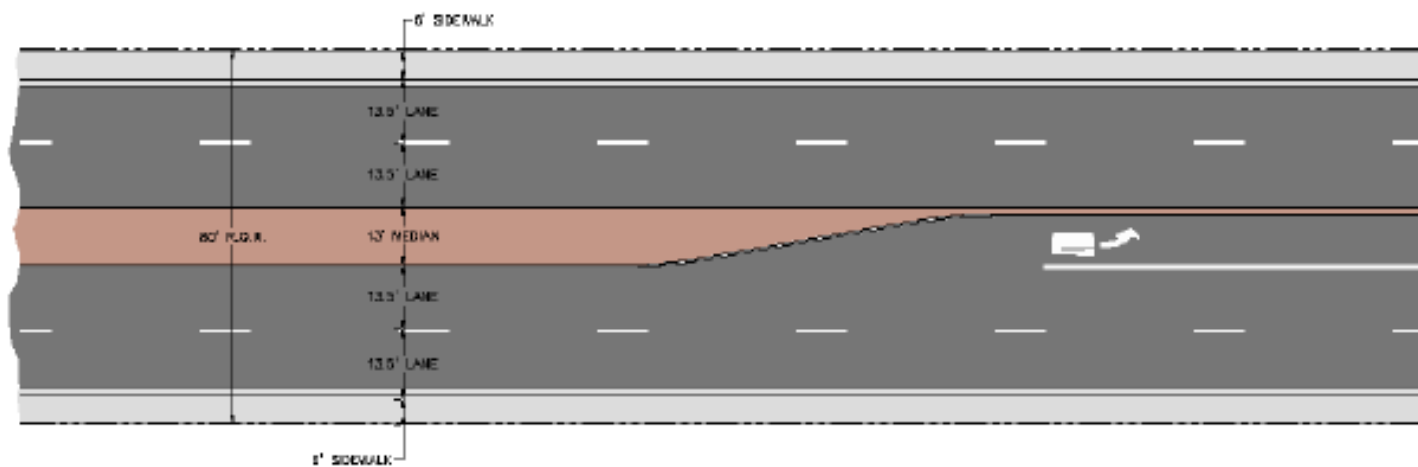
Map of Corridor Plan Area

District Maps

1. Automotive Sales and Service District Map
2. Sherwood Park Mixed Use District Map
3. Educational/Student Commercial and Entertainment District Map



NORTH AVENUE EXISTING SECTION



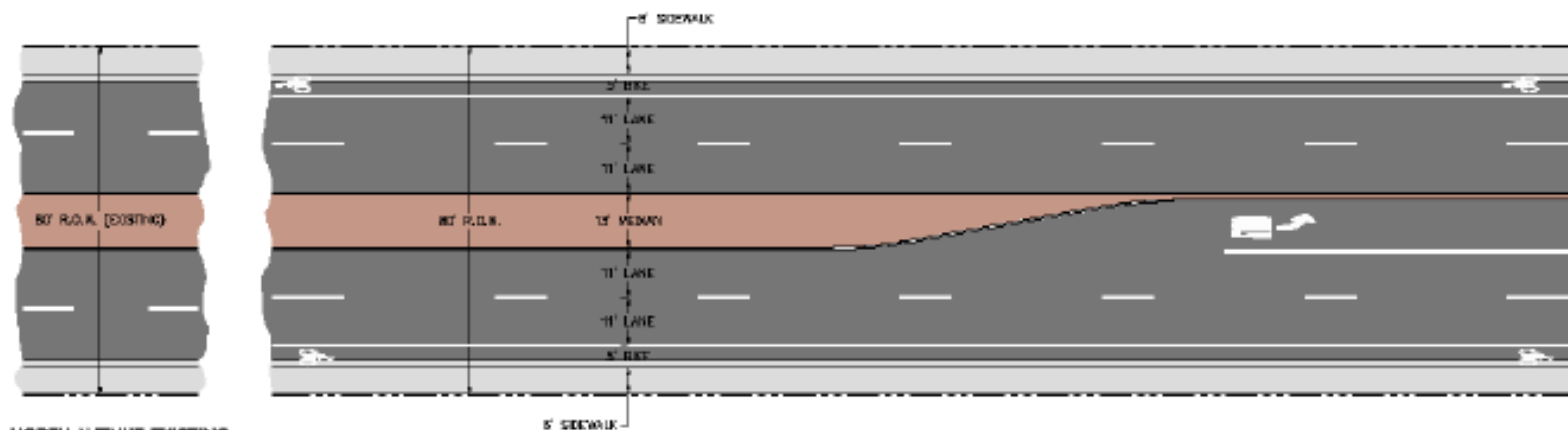
NORTH AVENUE EXISTING PLAN VIEW

Option 1

- No changes would be made to any existing structures; pavement width, curb-and-gutter location, sidewalk width, turn lanes, and medians would remain as they are. Only the striping of the lanes would be modified.
- The changes would be made within the existing 80' right-of-way. No additional right-of-way would be required.
- Provides 5' striped bike lane (3 1/2' paved width, 1 1/2' gutter width).
- Reduces the width of the existing traffic lanes from 12 1/2' to 11'. The 11' lanes provide a measure of traffic calming, but must receive special approval from CDOT.



OPTION 1 SECTION

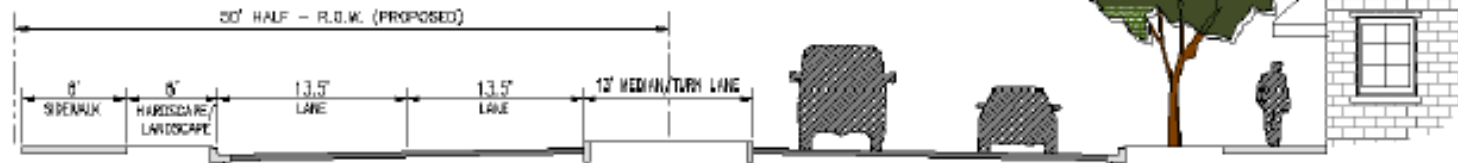


NORTH AVENUE EXISTING
PLAN VIEW
FOR COMPARISON

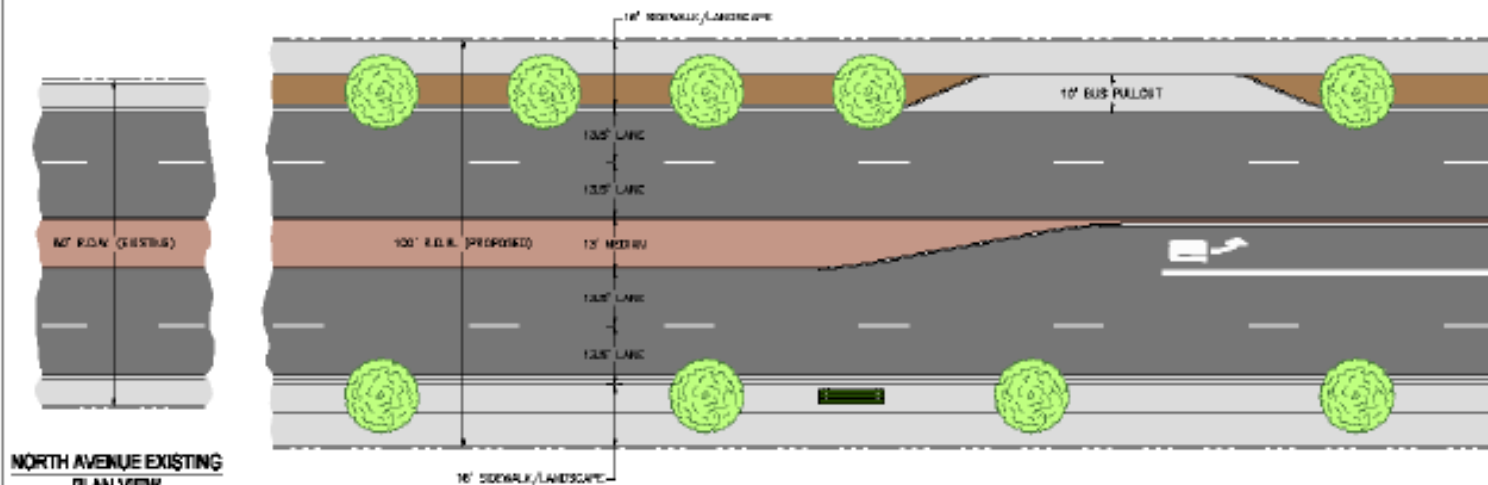
OPTION 1 PLAN VIEW

OPTION 2

- No changes would be made to the pavement width, lane striping, curb-and-gutter location, turn lanes, or medians. Only the existing sidewalk would be removed and replaced with an 8' detached walk.
- 22' of additional right-of-way would be required from each side of the street, increasing the total right-of-way width from 66' to 110'.
- Provides an 8' detached sidewalk and an 8' buffer area between pedestrians and vehicular traffic. The buffer area could accommodate a landscape strip, urban tree grates, benches, bike racks, public art, and/or bus pull-outs.
- Does not provide any bicycle facilities.



OPTION 2 SECTION

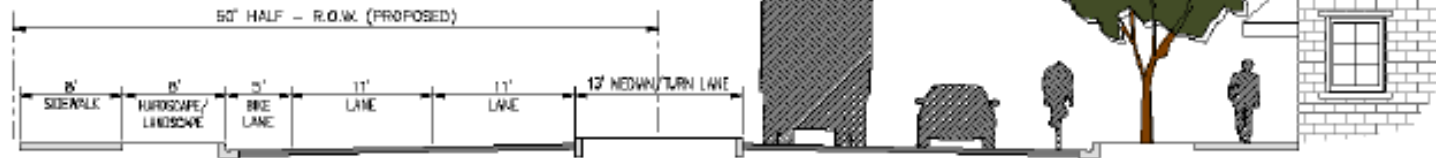


OPTION 2 PLAN VIEW

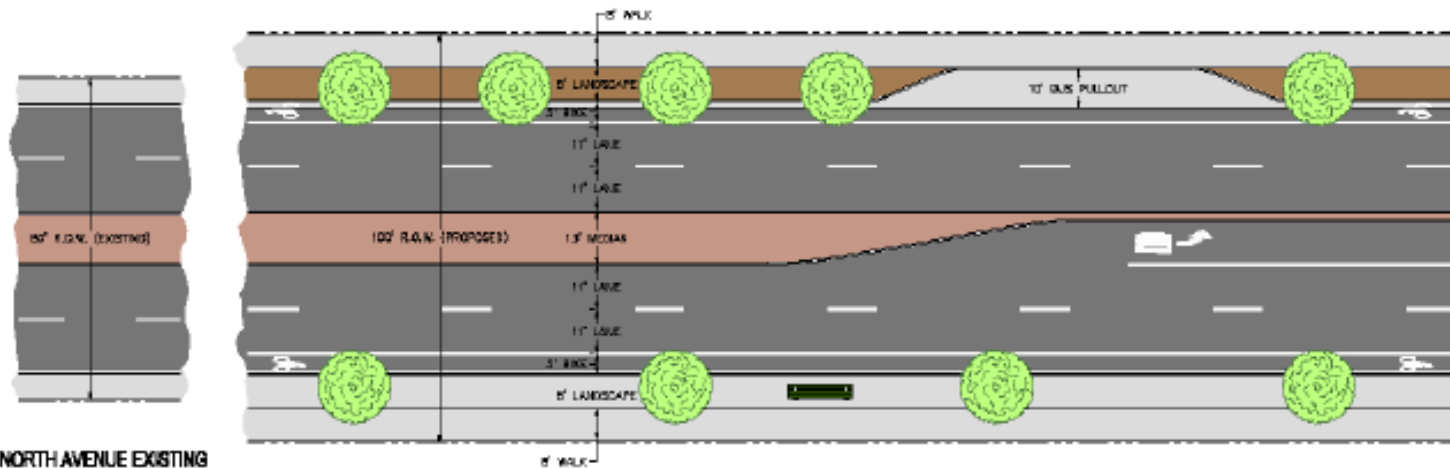
RECOMMENDED STREET CROSS SECTION

Option 3

- No changes would be made to the pavement width, curb-and-gutter location, turn lanes, or medians. The existing sidewalk would be removed and replaced with an 8' detached walk, and the pavement would be re-striped to accommodate a bike lane.
- 33' of additional right-of-way would be required from each side of the street, increasing the total right-of-way width from 60' to 126'.
- Provides an 8' detached sidewalk and an 8' buffer area between pedestrians and vehicular traffic. The buffer area could accommodate a landscape strip, urban tree grates, benches, bike racks, public art, and/or bus pull-outs.
- Provides 5' striped bike lane (3 1/2' paved width, 1 1/2' gutter width).
- Reduces the width of the existing traffic lanes from 13 1/2' to 11'. The 11' lanes provide a measure of traffic calming, but must receive special approval from CDOT.



OPTION 3 SECTION

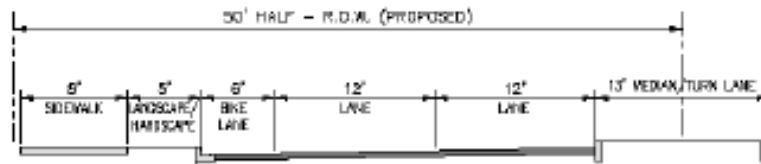


NORTH AVENUE EXISTING
PLAN VIEW
FOR COMPARISON

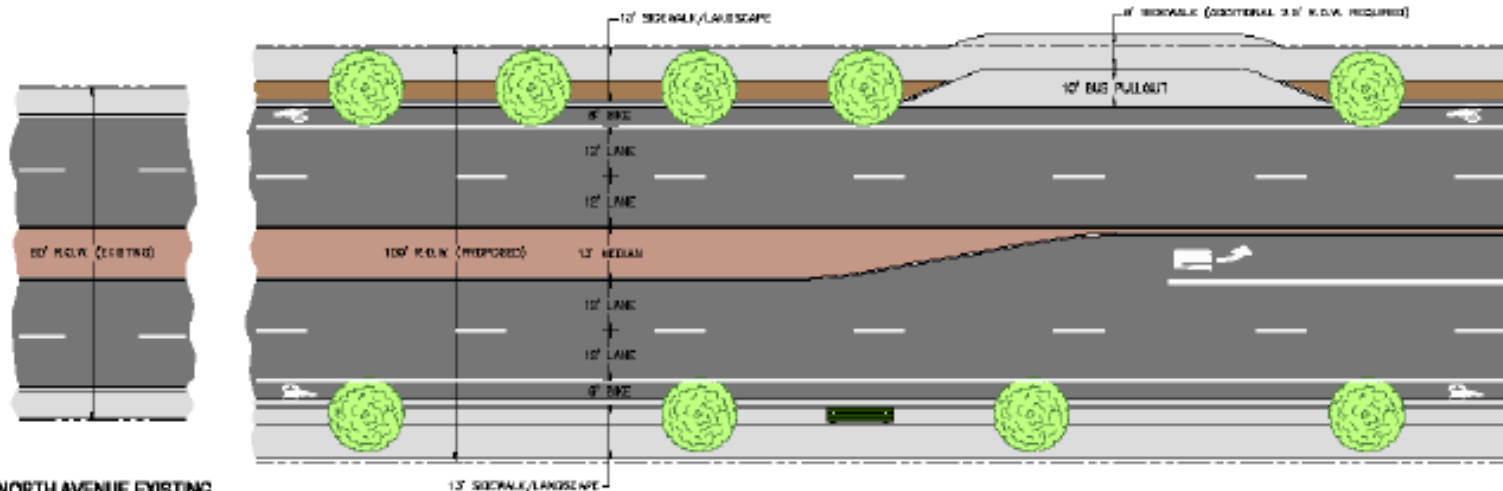
OPTION 3 PLAN VIEW

Option 4

- No changes would be made to the turn lanes or medians. The existing pavement would be widened approximately 3' on each side, the existing curb, gutter, and sidewalk would be removed and replaced with an 8' detached walk.
- 10' of additional right-of-way would be required from each side of the street, increasing the total right-of-way width from 90' to 100'.
- Provides an 8' detached sidewalk and a 5' buffer area between pedestrians and vehicular traffic. The buffer area could accommodate a landscape strip, urban tree grates, benches, bike racks, public art, and/or bus pull-outs.
- Provides 6' striped bike lane (4 1/2' paved width, 1 1/2' gutter width).
- Maintains the standard CDOT width of the existing traffic lanes at 12'. No special approval from CDOT would be required.



OPTION 4 SECTION

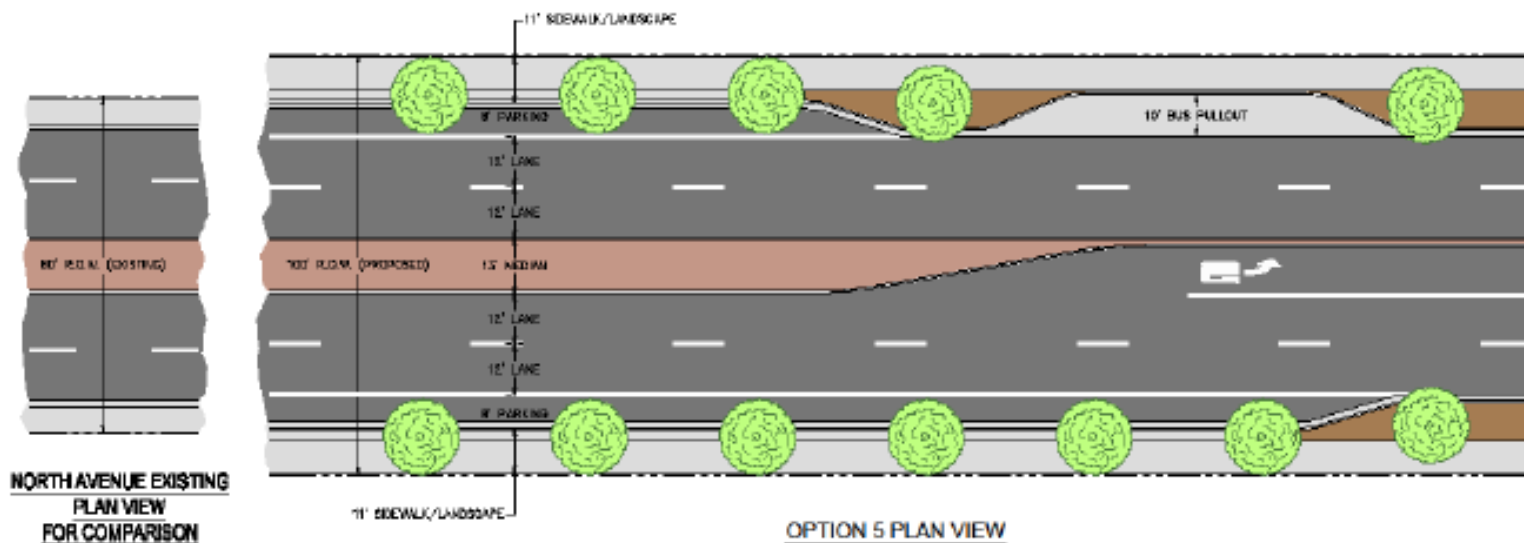
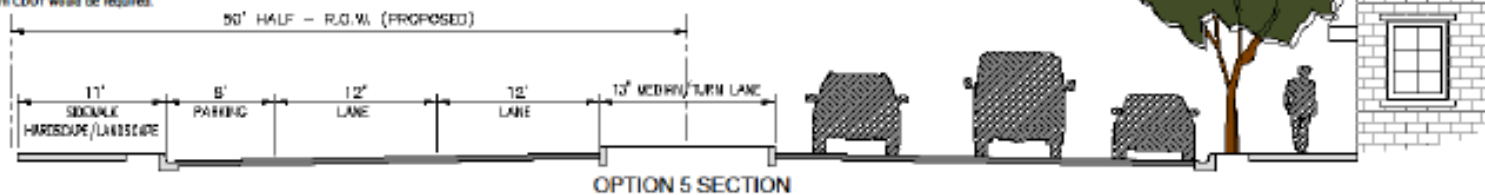


**NORTH AVENUE EXISTING
PLAN VIEW
FOR COMPARISON**

OPTION 4 PLAN VIEW

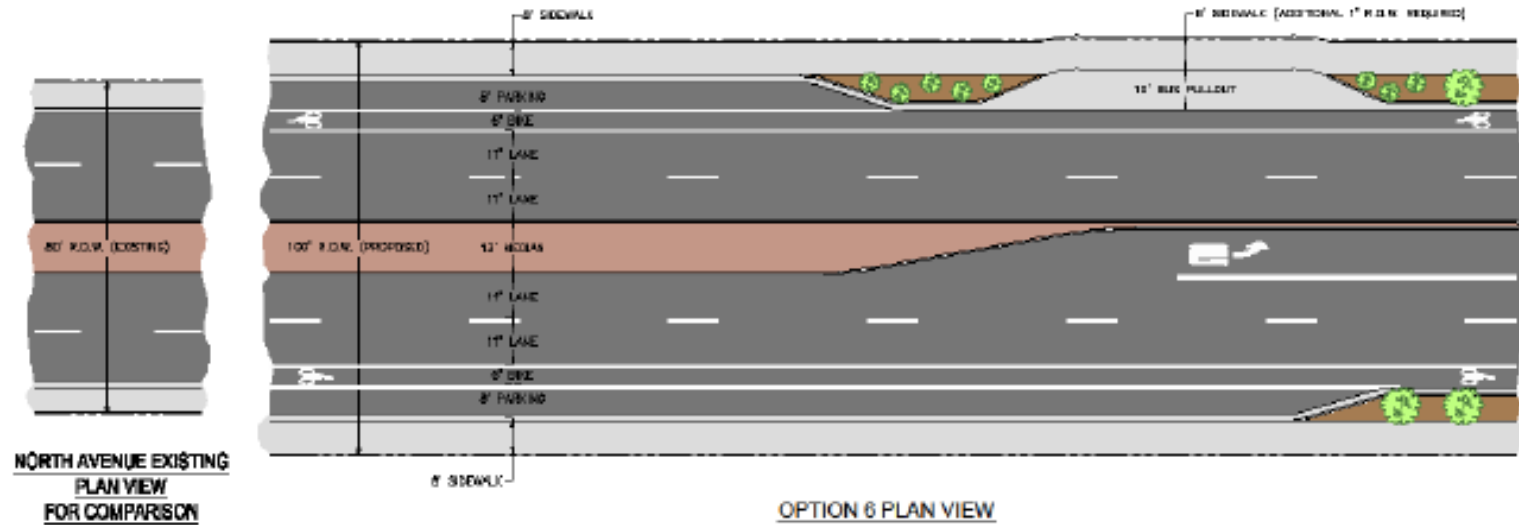
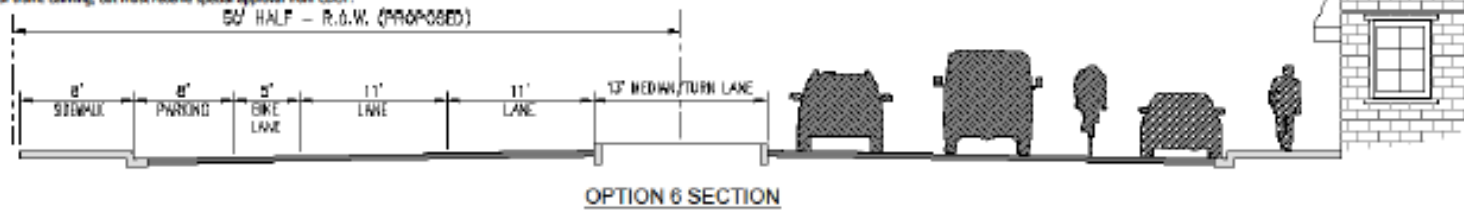
Option 5

- No changes would be made to the turn lanes or medians. The existing pavement would be widened approximately 5' on each side, the existing curb, gutter, and sidewalk would be removed and replaced with an 8' parking lane and an 11' sidewalk with urban tree grates, benches, bike racks, public art, etc.
- 10' of additional right-of-way would be required from each side of the street, increasing the total right-of-way width from 80' to 100'.
- Provides 8' striped parking lane (6 1/2' paved width, 1 1/2' gutter width). Parking would be eliminated where bus pull-outs were needed. The on-street parking must receive special approval from CDOT. On-street parking will cause a slight decrease in volume capacity of the outside traffic lanes.
- Does not provide any bicycle facilities.
- Maintains the standard CDOT width of the existing traffic lanes at 12'. No special approval from CDOT would be required.

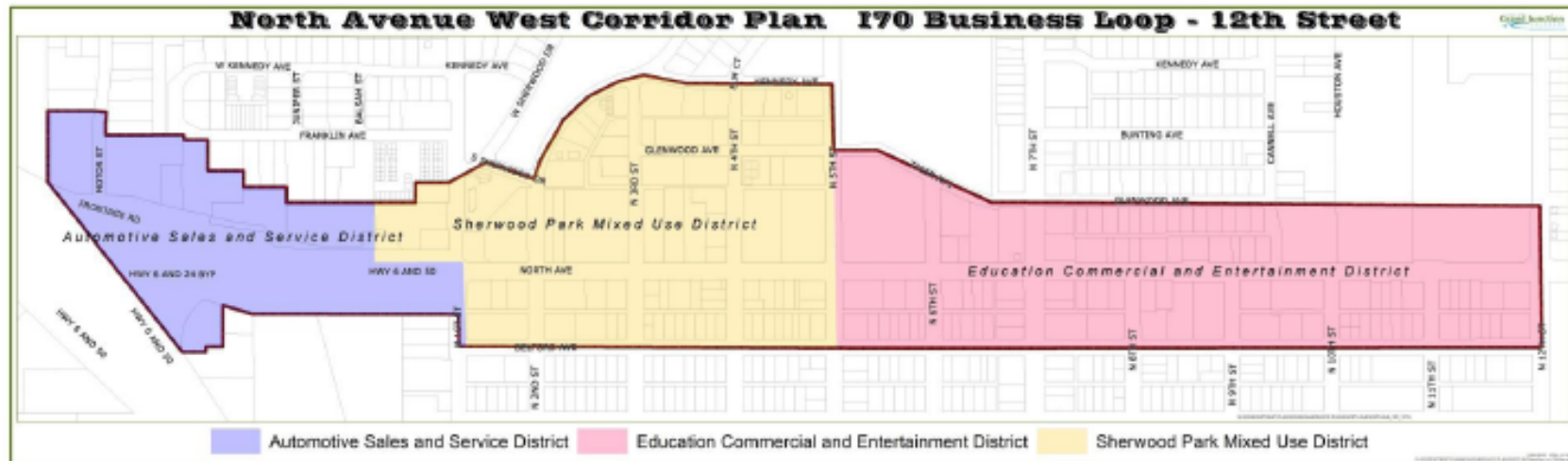


Option 6:

- No changes would be made to the turn lanes or medians. The existing pavement would be widened approximately 8' on each side, the existing curb, gutter, and sidewalk would be removed and replaced with an 8' parking lane and an 8' sidewalk.
- 10' of additional right-of-way would be required from each side of the street, increasing the total right-of-way width from 80' to 100'.
- Provides 8' striped parking lane (14' paved width, 14' gutter width). Parking would be eliminated where bus pull-outs were needed. The on-street parking must receive special approval from CDOT. On-street parking will cause a slight decrease in volume capacity of the outside traffic lane.
- Provides 5' striped bike lane.
- Reduces the width of the existing traffic lanes from 13 1/2' to 11'. The 11' lanes provide a measure of traffic calming, but must receive special approval from CDOT.



MAP OF CORRIDOR PLAN AREA

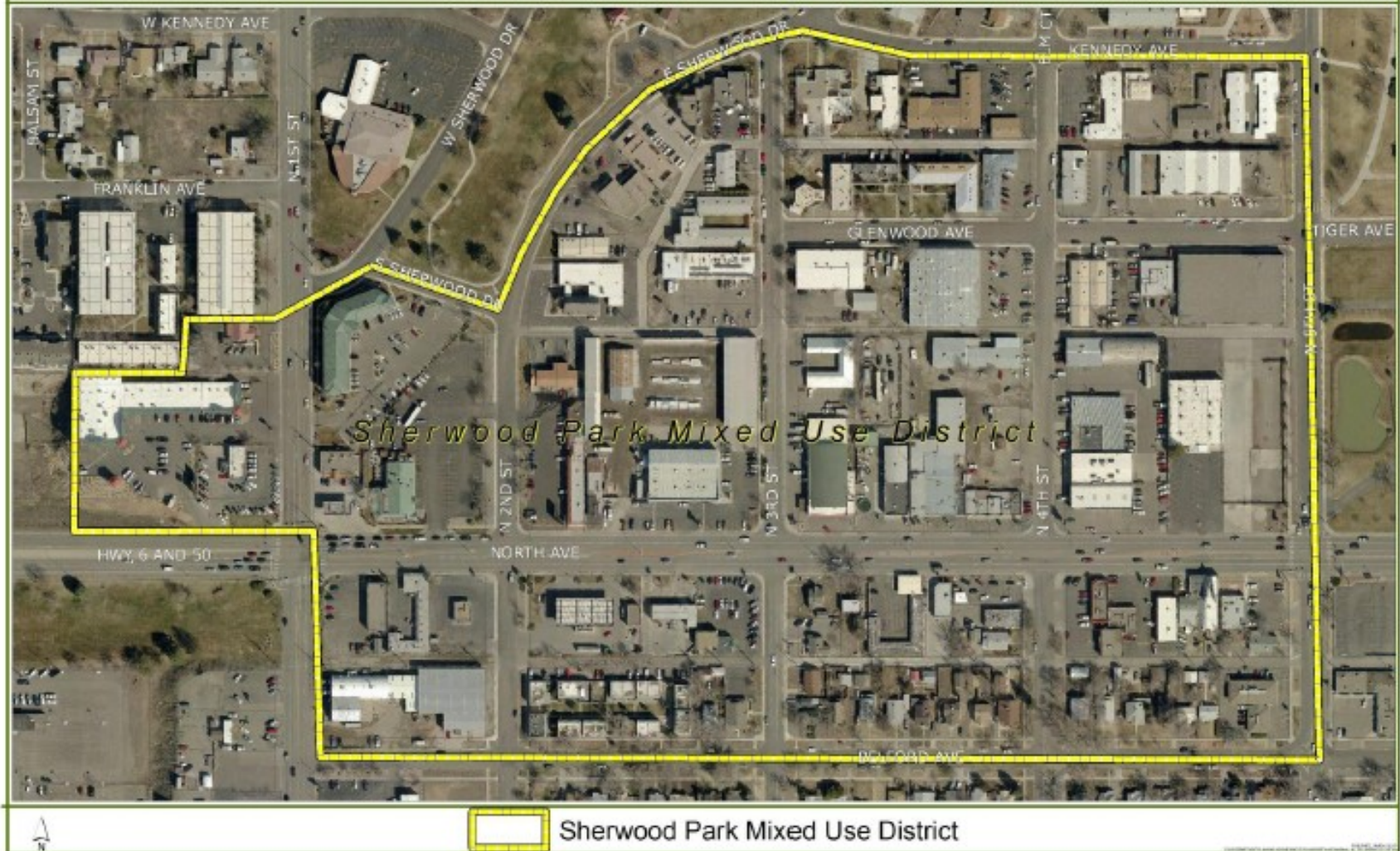


North Avenue West Corridor Plan 170 Business Loop - 12th Street
Automotive Sales and Service District

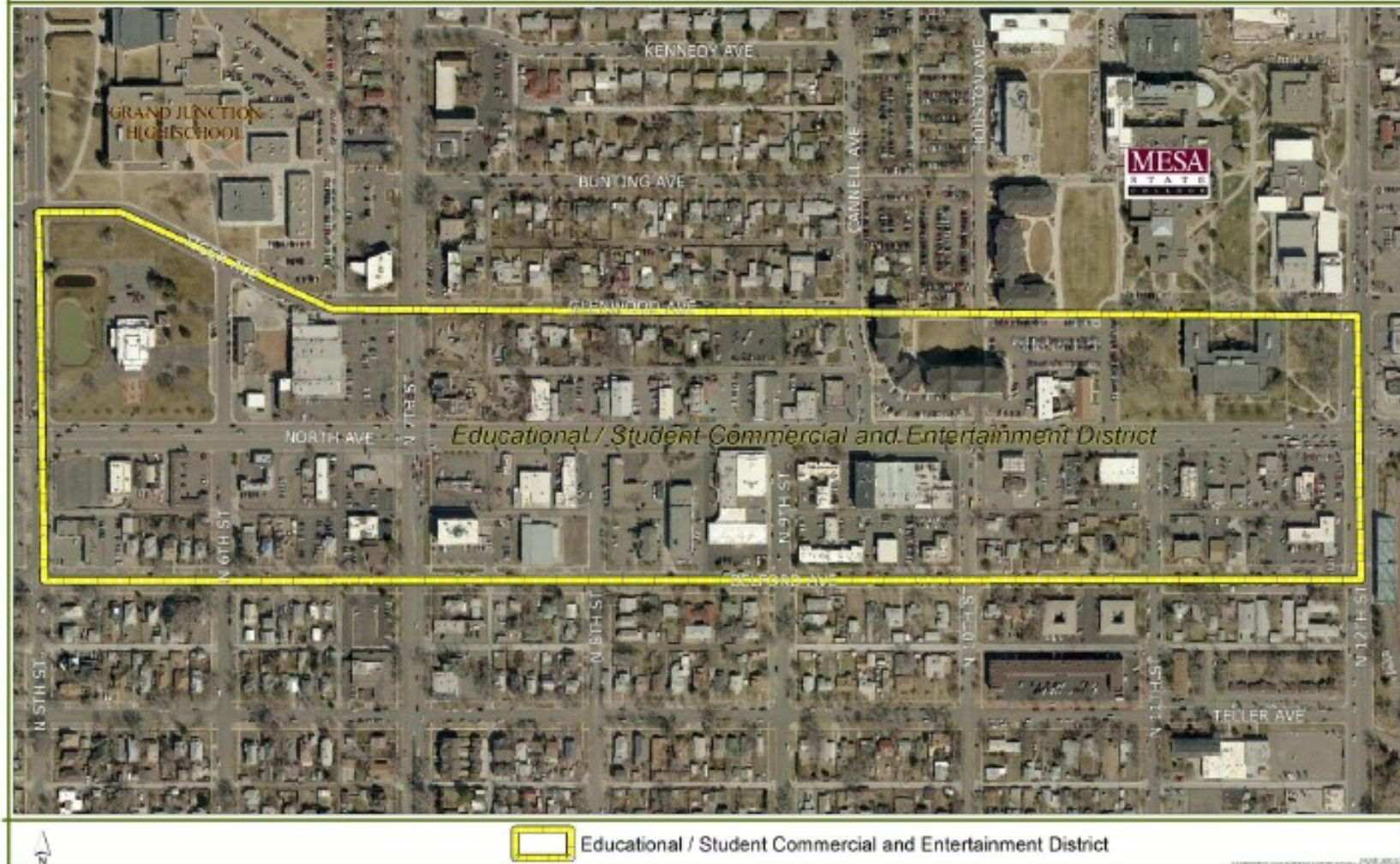


North Avenue West Corridor Plan 170 Business Loop - 12th Street
 Sherwood Park Mixed Use District

Grand Junction
 COLORADO



North Avenue West Corridor Plan 170 Business Loop - 12th Street
Educational / Student Commercial and Entertainment District



North Avenue Survey Results

Thursday, June 16, 2011

356 started the survey, 351 completed it

1. After reviewing the six options for cross-sections for North Avenue, drag and drop the option that you would consider to be your number one choice into the appropriate box and do the same for your number two choice.

	Number One Choice	Number Two Choice
Option 1. Re-stripe North Avenue with a five-foot wide bike lane.	31	16
Option 2. Add 10 feet of right-of-way width on each side with eight-foot detached sidewalks and eight feet of buffer between pedestrians and traffic.	64	51
Option 3. Add 10 feet of right-of-way on each side of the street, an eight-foot detached sidewalk, a eight-foot buffer area, and a five-foot wide bike lane.	104	143
Option 4. Add 10 feet of right-of-way on each side of the street, an eight-foot detached sidewalk, a five-foot buffer area, and a six-foot striped bike lane.	125	85
Option 5. Add 10 feet of right-of-way on each side of the street, an eight-foot parking lane, and no bike lane.	17	27
Option 6. Add 10 feet of right-of-way on each side of the street, an eight-foot parking lane, and a five-foot bike lane.	10	29
	351	351

2. Do you think bike lanes are important to have along North Avenue?

Responses		
Yes	260	74%
No	91	26%
Total	351	

3. Why or why not?

There is so much traffic and congestion along North Ave, it is in the best interest of the community to have safety measures for all. Improvement in those areas for pedestrians, vehicles, and bicyclists are very important especially considering transportation to and fro the schools (Mesa State, Dist. 51, baseball games, etc...).
I think the bike lanes should be in the street because that is where bikes belong...not on the sidewalk.
If installed they would be used.
There are a lot of pedestrians and college students who ride bikes and get in the way of the walkers on the sidewalk or the drivers on the street.
There are a lot of pedestrians and college students who ride bikes and get in the way of the walkers on the sidewalk or the drivers on the street.
There is a great deal of traffic and I fear that there would be more accidents involving bikers as well as an increase in accidents due to vehicles trying to avoid them. I have no problem at all with bikers but I fear for their safety. I would strongly discourage bike riding on North Avenue and as a resident of this County I would be disappointed to see encouragement for bike riding on that street.
Bike lanes would aid in keeping both Automobile and Bike traffic safer.
they still will ride to close to the traffic like they always do every where else.
in this country we should be, in general, encouraging use of bikes instead of cars
Bikes have a place, but North Ave, is not one of them.
I lived on White Ave, when the 12 th st , bike lane was put in.
What a waste of traffic flow, for the little use it gets, even to this day !
There is so many college students.
Being bicycle friendly is one of the important factors in the livability of a city. In an area like ours, where outdoor activities are so important to the lifestyle and even economy, we should stay as current as possible.
Safety
They cause nothing but frustration for drivers in Mesa County. While I think they are worthy, encouraging bikes on the roads in Mesa County will only add to traffic problems, crashes and bike riders being injured/death because drivers in Mesa County don't know how to drive with bikes on the roads. Just like they don't know how to use round-a-bouts either.
There are more people biking and a bike lane - safety - would be a good idea. Could ease accidents on other streets that do not have bike lanes and add revenue to local stores.

Traffic is too heavy and Bikers are safer on sidewalks than the Street. Less chance of a cyclist getting hit or hurt.
You don't see very many people on bikes today. The weather in GJ is such that the bike lanes are not useable for many months out of the year. Bike lanes near the college may be prudent but not along the whole of North Ave.
In the future, Mesa College will be a large university with a number of bike commuting students. In addition, as gas prices rise and people become more aware of the consequences of driving a car such as obesity and global warming. More people will want to bike commute or travel by bike for pleasure and exercise. As they say, every time you drive a car you cause some harm and every time you ride a bike you do some good.
College and town bikers
Bikes will always be a part of multimodal public transportation. It would be better to separate bikes from walkers.
Changes the character of the street by making it less of a highway
North Ave is a major road through town and should therefore be available to bikers.
Bike lanes are crucial for Grand Junction's future - when recreation is more important to the local economy, gas prices are high, and people want to use alternate types of transportation. Widening North will just generate more traffic and fill the added lanes, and will encourage faster speeds.
Bike traffic is absolutely minimal on that corridor. Considering the excessive cost it would take to construct them, I believe the benefit/cost ratio is very small. Considering the limited resources available to the community right now, I think we have better things to spend money on than bike lanes that will benefit an extremely small percentage of the local population.
Bike lanes do nothing to aid in rejuvenating North ave.
Just not enough space for bikes--alternative routes are available.
Lots of people use bikes and more may look at bikes as an option to cars or the useless GVT buses.
I think that all cities should encourage citizens who wish to ride bikes to be able to do so safely and conveniently. This will become more important as gas prices rise.
This is a biking community and not creating a place that is bike friendly will be extremely short sighted.
I think clearing traffic and making turning lanes safe with pedestrians is priority for a future. Bikers can travel safely along sidewalks safely. Key point along North Av. is flow of traffic.
Clear that, clear parking.
creates option for alternative means of transport. this is a major, essential artery and bikers need safe access to it.
The large european cities have similar designs with trees, grass strips and cycle paths, especially in the hot Mediterranean cities.
It is very important to encourage alternate means of transportation. There are limited options for safe bike travel and without bike lanes, some will ride bikes on the sidewalk which endangers pedestrians. Without the bike lanes ,bikes in the street are unsafe. With the growth of Mesa State and other business we need to encourage safety and calming measures with things like chicanes to slow traffic and make it safer for bikes, cars and pedestrians. We need to think forward with our design and encourage non auto dependent transportation to help us become a more modern city that encourages people to get out of their cars.
Wider sidewalks would help on the south side.
North Avenue is too congested for safe bicycle riding. Bicycles should be on the less used routes.

If the lanes are there, people will use them, especially knowing that they can go a long way on them. Otherwise, it would be a scary ride!
this is a beautiful place to live that attracts families and retirees who want a healthy lifestyle. Many people would bike if it weren't so impossible here.
I had a hard time reading the drawings...option 6 worries me if bike lane and parking are on same side. Could bike lane be on opposite side than parking? people in gj are not used to bike traffic and have a tendency to run into bikers.
North Ave is a very busy street - bike lanes would bottleneck traffic more (motorists slow down around bikes - many decrease way too much and can cause near accidents).
Grand Junction is naturally a bike city. It has great weather and tremendous biking opportunities outside of town, as well as a very bikable downtown. The one sore-spot, however, and a great impediment to those looking to bike more, is the state of north ave. I personally have biked down north ave, however only for brief periods between traffic or at night. during another other time, such an action would be extremely ill-advised. Adding a bike lane to North Ave. would be a great benefit to businesses along the corridor, as well as to outside perceptions of the city itself.
we have plenty of east to west bike lanesdon't see the need for any on this busy street.
With fuel costs the way they are (and more than likely stay). Give us the option to ride and feel safe. With the lower income population tending to migrate to that part of the valley, help us out.
many nearby alternative streets
More people are using bikes for transportation
As an avid bike commuter, bike lanes along North Avenue would provide greater access to the college and businesses.
Currently, there are no bike lanes on North Ave and bike travel on North Ave is VERY dangerous. I ride my bike on this route 5-7 times per week. Many vehicle drivers are hostile toward bicycles because the vehicle drivers have to change lanes to give the bicyclists any berth. Some type of bike lane on this route is imperative. Most bicyclist now take Elm St East and West because North Ave is so dangerous.
Safety and ecofriendly reducing our carbon print especially as the college is expanding and growing.
Near the collegen they may be, but along the rest of the corridor there is very little ped traffic and the bikers and pedestrians can share the sidewalk, even main street sidewalks could be shared with the higher ped traffic
As a biker myself, there is too much traffic going to swiftly. I always try and get to a lesser arterial such as elm or Gunnison.
A lot of people are riding bikes. They either are riding on the sidewalk, which is dangerous to pedestrians, or riding on the street, which backs up traffic and is a danger to everyone involved.
I believe an overall bike friendly community is important to Grand Junction and surrounding communities. When we talk about the area, biking always plays an important part in that conversation and therefore is important that our infrastructure reflects that. Additionally, I think the college students deserve a safe and bike friendly "home."
It's a main corridor across the center of town. Bike lanes would add safe access on the street to the East and West areas of the North Center of town and provide safe travel for the Colo Mesa Univ. student population
Bicycle and pedestrian safety are critical to encourage non-motorized use, improve the livability of an area and reduce traffic congestion.

It's an important part of the transportation mix
I would actually answer yes and no. I think that pedestrians and vehicles should take precedence. Not to discount bikes, but driving this stretch daily, I observe a greater need for pedestrians and vehicles
So that people can safely ride a bike instead of having to drive a car!
They are a vehicle.....let them share the roadway.
Since the city has already put bicycle lanes on some of the major busy streets it's only logical to continue with putting a bike lane on one of the most traveled roads in the city.
In a college town, right near a college? Hello...lots of bike traffic potential!
Because it is a major street for the college and a lot more students are biking
We have got to start making more options for non-car travel everywhere.
Bike lanes promote alternate transportation usage and improve safety.
The community encourages biking and wants tourists to come for biking events so give them a bike lane. Also a number of people have no other means of transportation, so provide them a safe avenue.
Many college students use bikes. More residents who aren't students would use them if they don't have to put their lives on the line around North Ave.
Too much traffic. Utilize secondary streets for bike traffic.
Biking is an important and optimal form of transportation for health and environmental purposes. Individuals who bike for transportation and/or exercise should feel safe and drivers of vehicles should consider their obligation to share the road with cyclists. A lane designated for bikes would achieve this.
To much traffic and won't be used.
North Ave is a major connector to important areas of commerce and employment, and needs to be safe for bicycles to be used for travel to these areas--without resorting to the sidewalks.
So many students ride their bikes to the college and it is unsafe/problematic for traffic, pedestrians, and bikes the way North avenue is now. The college seems to be growing and so there may be even more traffic there in the future and especially in an area with students, it is important to have room for bikes and pedestrians to travel safely and to slow down cars and/or make efficient routes for cars that are separate from where bikes are.
FOR SAFETY REASONS
Provides safer means of transportation for bike riders in lieu of having to travel the back streets. As our valley population increases the need for safer bike lanes will also increase. This is especially true in the vicinity of Colorado Mesa University and surrounding complexes and parks.
Bicycles are here to stay!! I believe with the continued increases in fuel more bicycle traffic is inevitable. North is an important east/west corridor and would facilitate the increased bicycle traffic if it were made safer for that riders.
We need to get people to use other forms of transportation to improve the quality of life for everyone in this community and we have to give them choices to do that.

<p>Making GJ bike friendly needs to be at the center of future development plans. The bike community in GJ is growing. Many tourists are already traveling here for access to biking and adequate development of bike lanes may stimulate this further. Local bike usage will also increase as safe corridors are created for the more timid cyclists, reducing traffic and heavy vehicle use and air quality issues. Many of us already ride these streets at some considerable risk! Why are the cops running speed traps on the residential corridors when we can't ride safely downtown? Local bike-oriented businesses (MRP, White Bros, DT Swiss, the numerous local bike shops) may benefit increased sales and therefore TAX revenues and JOBS. We may even be able to recruit a b-cycle franchise (similar to Denver, Boulder, etc.). Bike lanes are for these reasons an essential part of planning for the entire downtown area, not just North Ave.</p>
<p>cost of gas!!! People are riding there bikes!</p>
<p>There are very few E/W bike routes across Grand Junction.</p>
<p>Many riders aren't too smart about traffic's ability to kill or injure them and will ride on a busy street no matter what. I think a bike lane is a safety feature for drivers as much as for riders. Personally, I'd ban bike riders from all principal arterial streets (Patterson, North, Grand, Main, 1st, 4th, 5th, 7th and 12th) and make it a first offense \$100 fine and a second offense \$100 fine and confiscation of the bike and a third offense \$500 fine, confiscation of the bike and ten days in the county slammer.</p>
<p>dangerous for bikes on North Ave bike should take alternative route</p>
<p>I think it would be to dangerous...Especially the way people drive on North Avenue and the amount of traffic...</p>
<p>I feel its important to draw the bikers to that area and to keep them safe.</p>
<p>Too busy. Look at Fort Collins, Colorado as a guide. On College Avenue, which is also Highway 287 that runs through the heart of town, there are no bike lanes. The bike lanes/route along College Avenue are on secondary streets.</p>
<p>Glenwood Avenue, or Belford Avenue would take care of this rather nicley.</p>
<p>Need to encourage alternative transportation methods. Also good for tourism and "sense of place".</p>
<p>There is a decent amount of bike traffic in the downtown area and it would be good to give each mode of transportation their own piece of the thoroughway.</p>
<p>Bike lanes are important but too dangerous if you diminish the traffic lanes to 11 feet. Since no offered option increases the total width of the street bike lanes simply must go to other streets. P.S. -- I am an avid and frequent bike rider.</p>
<p>This would help reduce traffic, as well as improve environment.</p>
<p>We have spent a vast amount of money on bike lanes and they are seldom used. TOO much money for too little use.</p>
<p>the SIDE STREETS DO NOT WORK WELL FOR BIKE LANES. WITH THE COLLEGE AND THE HIGH SCHOOL BOTH ALONG NORTH WE NEED TO PROVIDE FOR BIKES.</p>
<p>As a main corridor North Ave. needs safe options for pedestrians and cyclists. There are schools, parks and Mesa State College all along the corridor. Students need a safe route to commute to school.</p>
<p>attract people that do not drive into the area</p>
<p>Bike lanes should be a part of all street projects in the city. The only way the benefits of cycling are going to be realized is if there is a convenient, safe and connected system from everyone's home to all destinations. The benefits of cycling are: energy conservation, health, international security, reduced parking demand and reduced traffic congestion but it isn't going to happen if it's not safe and convenient.</p>

Implementing a SAFE East West Corridor in Grand Junction is MUCH needed. This would allow cyclists to travel to commercial shops via bicycle. Great work GJ City Council. North Ave needs a face lift and adding bike lanes is the sustainable direction the City should work towards. A healthier, safer, active city will only draw more people to settle here and relish in the beautiful community in which we live.
A lot of people commute by bikes around town. Unfortunately North Avenue is the least bike friendly route we have in town, yet it could provide great bicycle access to LOTS of stores.
It would be much safer for both drivers and bikers if there was significant room for each. It is also a major route that is unfriendly to both bikers and walking pedestrians. The extra space would make it feel more comfortable.
Because north avenue is currently a suicide ride. I avoid it like the plague
If you want North Avenue to have a sense of place and to be a longterm \$ generator for GJ, you've got to make it accessible for bikers and walkers. Bikers and walkers cannot share the sidewalk. Bikers and cars cannot safely share the road. As gas prices continue to rise, bike friendly infrastructure (including bike-specific lanes) will just become more important. At this point, North Avenue is one of the most dangerous places to ride in GJ. Cars are moving very fast, there is no shoulder, sidewalks are inappropriate for bikes but represent the only option. A few other issues: crossing North is dangerous because of speed that lights change and because pedestrian crossing buttons are often tens of feet away from the corner and make it very difficult to push the crossing button on a bike.
People bike around grand junction and being that North is a major thoroughfare it only makes sense to have bike lanes. What is currently there not only for pedestrians (in some spots) but cyclists especially is unacceptable and dangerous.
This community has so many great resources for bikers, but North is scary and dangerous. As a major E-W corridor, a primary commerce route and a primary boundary for the University, it has to become more bike friendly, safe and efficient.
Bike lanes should be available on every busy street. It doesn't make sense to encourage bicyclists (especially children) to break traffic rules to protect themselves from aggressive traffic. Bicycling should be a viable option for the entire area.
Bicycles are an absolutely vital mode of transportation in any modern city. A city that is inhospitable to bicycles will never attract young professionals and will lead to more traffic congestion and more accidents. Gas prices set to rise and numerous health benefits make bicycling an increasingly viable, popular and responsible mode of transportation. A city without bicycle lanes is living in the past.
It isn't safe to bike it now, and bike lanes promote healthier lifestyles & provide inexpensive transportation methods.
There is currently not a safe way to navigate North Ave. The bike lanes would provide this and would also provide a reminder to drivers to share the road. The sidewalks are currently far too narrow and are right up against the road and quite tight to some of the existing buildings. The traffic moves at such a swift pace that a separate bike lane is necessary to allow for bikers to safely use this main arterial to get around town.
Bike lanes would open North Ave. to safe bike traffic. As a cyclist, I avoid North Ave. because I feel it is a dangerous place to ride. If I do have to travel a portion of North Ave., I end up riding on the sidewalk to stay away from motor vehicle traffic.
Bike lanes should be considered as often as possible. Bike infrastructure only adds to the appeal of Grand Junction.
biking should be promoted for health, transportation, and family/social benefit of city
It is a mainstream of the city and it would be nice to not have to weave in and out of neighbor hood to to travel across the city.

We have a lot of bicycle commuters in this town and could have even more, if we get more bike lanes and if the price of gas continues to go up. GJ should become a bike-friendly town since it is such an outdoorsy, healthy place to live anyway.
Yes, with rising gas prices non-motorized transportation is increasing.
to make it and safer for bicyclists. there are many students who use bicycles as well as many non-students. in order to access businesses and the campus safely by bicycle, it is now necessary to ride on the sidewalk
The city should encourage commuting by bike for ecological and health reasons. North Ave is a major thoroughfare through Grand Junction and is currently extremely unsafe and dangerous for bikers and motorists alike.
It is important that bikes have a right-of-way on all major streets.
The advantages of living in a city include choosing to ride a bicycle to do errands. Bicycles are inexpensive, good for the environment and the health of riders. Easy access to North Avenue businesses by bicycle is important.
Bike riding on North Ave is pretty much impossible. Way too dangerous. Having bike lanes would allow college students/citizens/etc a safe way to access the school, shops and restaurants on north ave.
North Avenue is a main thoroughfare in Grand Junction and is incredibly dangerous for bicyclists at present. A bike lane would create a safer corridor for bicyclists along North Avenue and make biking in the city (commuting etc.) much more feasible.
North Ave is the best option for cross-town bicycle commuting or walking but I only do it in the middle of the night (after my hospital shift) when there are few cars. North Ave bike lanes would provide safety for existing riders, would encourage potential bike commuters, could significantly reduce auto traffic, and would improve the quality of life and downtown liveability.
Bikes belong on all roads.
Bikes make inexpensive transportation and get people where they are going in a good mood!
I think it's important to encourage safe bicycling.
This would be a great thoroughfare for cyclists. It currently is difficult at best to ride on North avenue. With the Expansion of the University & the increase in students, North Avenue will see an increase in both motor & bike traffic. Providing a safe space for cyclists will be to the advantage of all.
Only if they can be done safely removed from traffic - perhaps separated by a median for the buffer zone. I think bikers would be more likely to avoid a North Ave that retains much of its present character if there were just a striped bike lane adjacent to traffic lanes
A major east-west corridor is needed for bike traffic. This is a start. / / Paterson Road really needs this also!
I love to bike (to save on fuel cost and increase daily exercise for health reasons). I have biked North Avenue to work and for pleasure, and it is currently dangerous because bikers are not fast enough to ride safely with rushing, starting/stopping vehicles (which is the legal way bikers should be riding). I have resorted to using the sidewalks, but this is also problematic because motor vehicles aren't watching for me, so I have to use caution at every intersection and at business entrances. A bike lane would significantly impact my enjoyment and safety along North Avenue. A dedicated bike lane would increase safety, promote health, and save fuel costs for other bikers and the college community.

North Avenue is still one of the main East West corridors and is heavily traveled by cars and bikes. Bike lanes create a safer area for people to ride a bike. Riding bikes is good for ones health and good for the environment and good for our local economy.
Because we are leagues behind. Build it and they will come. Any of you experts familiar with complete streets?
I would like to see the Grand Valley become more and more bike friendly. A lot of people not only would like to have an ability to safely ride a bike across town but for some people that is their only transportation.
Bicycle traffic on North Ave as it is now is very dangerous. Cyclists must share the traffic lane with drivers and many drivers swerve into the inside lane to avoid them (despite the presence of other drivers in that lane).
I think an east-west bike path is important, and it should be such that it encourages bike commuting--i.e., not necessarily for pleasure riding. However, I don't think that pathway has to be along a street as busy as North Ave.
Safety of bikers.
To promote alternative transportation. To keep bicyclists safe and traffic flowing. To keep our town bike and sidewalk friendly. It is very difficult to walk or bike without sidewalks and bike lanes. Let's promote health and make North Ave a nice place to want to visit. Right now it's old and gross and dirty. It needs to be updated to keep up!!
Yes! There are an increasing number of people in our city who are choosing cycling as a form of transportation, and the city needs to ensure that there are safe means to do this. As a recreational cyclist and commuter cyclist, North Avenue is incredibly dangerous to bike on the way it is right now. In fact, I never bike on North Avenue because of the risk involved.
Promotes bikes and safety issues. More family safety
People try to bike it anyway - might as well be safe and offer more room on sidewalks.
Safety first; and to promote non motorized transportation.
One of the busiest streets in town with businesses that would most likely employ those that need to ride bikes(fast food,retail businesses). The sidewalks are not suitable or safe for bikes and pedestrians at same time.
We are a bicycling community and ease of bike access to the college, stores and housing is an efficient and clean transportation alternative not available on North Ave now.
If there is a right of way/expanded sidewalk, a bike lane is not necessary. If the road is not widened, I don't think there is enough room to comfortably have two lanes of traffic and bicycles, with or without a stripe for a bike lane.
Orchard Avenue is the only other dedicated place for a bike lane and there are many parts on Orchard where it does not exist.
In thinking about rising gas prices and reducing emissions, it would be a GREAT idea to include bike lanes on more of our existing roads. Cyclists need that in this town, especially since the public transportation system runs so infrequently.
Cyclists don't follow the law as they should anyway. They act like they can do whatever they want.
Because of the college, high school & middle schools. They don't all drive so they ride their bikes & it's really dangerous to try & ride a bike on North Ave now.
Bike lanes provide access to the area for a greater number of vehicles. I'm hoping the redevelopment of the area will include a far greater percentage of multi-family housing than there is now and bike lanes and landscaping will make the area much more attractive. Narrower lanes will encourage more cars and trucks to use I-70B and the Riverside Parkway.

The street is very busy and there are a lot of students. Bikes are encouraged in our community, so we need to put safety accommodations in place.
If you make the pedestrian area wide enough, you will not have a problem and don't expect that many pedestrians/bikers. Major problem with 12th/North Ave to 18th/North Ave, South Side: no room to ride a bike and a deep fear of getting hit by a car, and North Side: broken up/missing sidewalks and empty dirt lots/missing sidewalks.
No one rides a bike to work. Kids won't even ride a bike to school. Waste of money. We need more traffic lanes, more turn lanes and definitely pull-out of traffic lanes for buses. Serve the majority for once, instead of the vociferous minority. Most of us just want to get from Point A to Point B as calmly and safely as possible.
Without Bike Lanes it is very difficult to get across town safely. Especially, since Patterson is not safe to ride a bike on at ALL
Preferred mode of transportation for many Mesa State students and VA employees.
Grand Junction has an ideal climate to encourage more and more bike commuting. Providing safe routes is important!
Currently, North Ave. is dangerous for bikes.
Bikes will be an increasingly important mode of transportation. With the growing newly designated University, bike lanes will be a boon for students and faculty commuting by bike.
It is one of the main corridors for east west transit through town. It provides access to many shopping areas and restaurants
Grand Junction needs more bike-friendly routs and the lanes would slow traffic
More travel capability
It is an important arterial. I ride a bike quite a bit and I appreciate a buffer between other traffic and myself. Considering how heavy traffic is on North Avenue, I believe a buffer between the traffic lanes and a bike lane is prudent. Bike lanes may encourage more people to bike commute rather than drive motor vehicles, especially around Mesa College.
lets cause more trafic jams by adding bike lanes... DUH!
We need thoroughfares to move traffic and bike lanes are best when provided on side streets to keep the cyclists completely out of harm's way.
Bicycle lanes should be added wherever possible to help reduce traffic, lower pollution and help people be more active.
people do not use bike lanes, much to the chagrin of urban planners. I drive in areas of GJ where there are bike lanes every day and I almost never see bicyclists utilizing bike lanes. They seem like a great idea, but if you really measured usage, I'm sure you would see bicycle usage drop after bike lanes are installed. I cannot account for or explain why this is the case, but it simply seems to be true. I think sidewalks, vehicle-ped buffer spaces, medians or landscaping improvements would be much better options to explore for improving the North Ave corridor. Street parking might also be a good option, too. The Riverside Parkway is beautiful and well thought out, and the folks involved should be commended. I love driving on it, even if it means it will take me a few more minutes to reach my destination. (The bike lanes there are real pretty, but again, seldom utilized)
Safety
People need to be able to ride bikes safely on streets. Many are riding to work.
Added pleasure but if costs are too high a sidewalk and buffer zone are more important for daily use
Given the increasing cost of motorized transportation, all viable alternative modes of travel must be encouraged.

I live right off North Ave. I know how to avoid North west of the ditch on 28 Rd. Riding east of 28 Rd is a true menace either on the sidewalk or in the street. The problem w/ a lane next to a sidewalk is that is where all the garbage goes that is dangerous to a bike rider. If cars are parked on the curb, a rider must always be alert to an opening car door. There is no perfect solution. An eight foot buffer zone will fall into the same disrepair as the medians. The ultimate solution is for all businesses to adopt the Fiesta Guadalajara strategy. (Business districts?) That leaves the improvement of North Ave to businesses. There must be another reason why businesses are abandoning North Ave as fast as they can. The bike lanes added by the city in the last ten years add greatly to the aesthetic of the city. Two gas stations from 1st St to 29 Rd? Gotta look deeper. Run a shopping bus up and down North Ave.
If they want bike lanes then they should have to license their bike and pay road taxes to use the road just like I have to do with my car, besides half the idiots still ride in the middle of the road even when there are bike lanes. I chose one with bike lanes because I know it would not matter what I thought you would still install them so I chose the one with the wider traffic lanes.
The number of driveways into businesses make bike traffic too dangerous. I do not want to watch for bikes while turning into businesses. Remember, you are trying to encourage more businesses along North and the customers who visit. Biking through a business district is no fun for biker or driver. There are PLENTY of other streets in GJ with good bike lanes and less danger for biker and driver. Being politically correct regarding bike lanes makes little sense when North is such a congested area. Sidewalks are necessary for foot traffic between parking and businesses.
Definite need for alternative travel mode
take too much right of way and bike riders tend to not use them.
Many people use the sidewalks for biking, and so many drivers don't see them when they decide to make a turn . So many close calls happen.
Nothing could be more disruptive to smooth traffic flow. They disregard traffic rules, ride side-by-side, swerve into traffic, etc. I observe these problems daily on my drives into the city from the Ridges, and going west on Broadway toward Fruita. They are rude, and yell obscenities when passed. When I have complained to law officers, they shrug & say that bikes aren't able to be ticketed.
More people would likely ride bikes along North Ave. because it would be much safer.
much safer for bikers
no, in other parts of the city that have bike lanes they are not used the way they are supposed to be. Why waste money to put in something thats not going to be used correctly anyway
I believe that bike lanes are a critical element missing from North Avenue as well as on the other major thoroughfares in this town. These busy streets are usually the most efficient and direct routes of travel for cyclists and motorists. Not having a bike lane forces cyclists into pedestrian areas where there is potential for verbal and physical altercations, not to mention injury resulting from a collision. Grand Junction is a magnet for cyclists all over the country. Why not provide a safe urban cycling environment to compliment the world-class road biking and mtn biking?
See many bike riders on the sidewalks because it is not safe on the road.
people are trying to save money by biking and making a safe way along a main thoroughfare is important for the city to do for it's citizens
It provides a less expensive, alternative form of transportation for citizens. It also promotes a health-conscious community that enjoys the outdoors.
Cyclists are in danger on North Ave because of the heavy traffic flow and little room for riding.
with increased development of the areas around mesa state and rising fuel costs, more people are walking and biking in this area
safety

It is INSANE to have bike lanes on heavy-traffic roads! Gunnison Ave. should be the main east-west route for bikelanes, or even Grand Ave. If any of these plans require downsizing North Ave. to only one lane in each direction, then you've all lost your minds. You ruined 12th St. "for the sake of safety" as it & N. 1st St. should be the same, 4 lanes of traffic. Why can't bike lanes be restricted to N. 10th St. & another st. close to N. 1st? It's also nuts the way so many roads are mismatched, going from 4 lanes into 2, such as N. 1st & N. 12th.
Bikes are already on North Avenue, and will continue to increase in volume. With designated bike lanes, automobile and bike traffic can co-exist safely, and efficiently. Bike lanes will safely maintain a convenient and safe traffic flow.
it is the most direct way to travel east/west through the city and bikes already are ridden along North Ave., although it makes me shudder to see them!
Bike lanes are better suited on streets that do not have such heavy traffic from such a variety of uses, e.g., cars, vans, delivery trucks, transit buses, etc. There are plenty of "side streets" for bike lanes that can provide a safer route to and from the same points.
Bicycling keeps needed money in our local economy. For every car that is ditched in favor of a bicycle, we keep thousands of dollars here that otherwise would go to big oil or the middle east. It also makes for a much more attractive place to live without so much car congestion.
Build it and they will come.
Area is too congested with auto traffic. Bike lanes would impede the flow of traffic. I only like the first option I selected. The Number 2 choice was only added because my survey would not be accepted otherwise. If it had been allowed only option 2 with no bike lane and no parking would have been selected.
North Avenue is so congested already. There are cars stacked up for several blocks now. With people talking on cell phones and texting the street isn't safe now. A bike lane would add safety issues. People ride around with music playing in their ears. They often don't look up, even when you drive next to them.
the main thorough way through eastern part of town. although another option would be to use a parallel street, although the first 3 south are stopped by Lincoln park... how about a bike lane off the street, like a wide sidewalk due to traffic...
Too much traffic, better to utilize side streets with less traffic.
We need to re-brand Grand Junction as an outdoor sports mecca. Bike lanes on the main road through the city would be a positive draw for cyclists.
Not safe at present for Bikes. Need alternative to cars. Narrower traffic lanes might help slow traffic.
There are many bike riders in Grand Junction, North Avenue is totally unsafe as it is for bike riders - they have to ride on the sidewalks (where there are sidewalks) which affects pedestrian safety. There needs to be more bike paths within Grand Junction, not just on North Avenue
Biking is a healthy and energy-efficient form of transportation. At present it is unsafe to bike along North Avenue.
Would make bike commuting more feasible to have a long through-street, like North Ave, more bike friendly.
Another opportunity for people to use their bikes and get current bike riders off of the sidewalks since they have nowhere else to feel safe when they ride.
We need to ensure safety for those who already ride and promote more people riding bicycles to encourage healthy physical activity, reduce America's rising obesity rates and alternative means of transportation.
It is important to incorporate this mode of traffic into all future planning to encourage other forms of transportation and healthy living.

There is no where to ride at this point, especially east of 29 road. I live in Fruitvale and over the years have seen many bikes and wheelchairs in the street. There MUST be accomodations made for these people.
both bikes and people using mobility devices can safely travel with cars.
Bicycles are quiet, they promote good health and a healthier environment, and they may help slow down traffic.
We are not bike friendly and we need to be, and we need to take into consideration how many people either have to bike, walk or ride the bus to commute. I think it's important to encourage safe bicycling.
No. There are pleny of side roads one can take instead of a busy road that taxpayers have to fork out money to provide their safety.
We need safe bike lanes everywhere. We should do all we can to encourage riding bikes. It is good for individual health, saves gas, and increases sense of community.
they are a hazard to traffic and businesses
Provides an east-west corridor for bicycle transport and separates bicycles from pedestrians and faster moving cars.
It's a main artery for traffic of all kinds. It would provide a better east-west route (and with lanes, be safer) than using side streets for bicycle traffic.
Bicycles are a major mode of transportation and this area is a major access road. Also it allows for wheel chair access when ditches etc are in the sidewalks and wheelchairs cannot tranverse.
Provides a straight through access from 6-50 east to 6-50 west. Great for people to bike to work, or to get to recreational areas
Too major of an automotive route to consider bicycling on
bikers are becoming more and more around the grand valley and we need to meet their needs by proving bike lanes.
Bike lane is important for safety.
It's just flat insane to ride a bike on North Avenue - the traffic is too heavy. But, if you're going to have one at least make it as wide as possible.
While some secondary streets are available as alternate routes, the interruption of the street grid by the high school, college and Lincoln Park makes that difficult for bicyclists. The population in the area includes many bike riders; they have to be safely accommodated.
College area which is expanding would benefit from bike friendly roadway.
Without a designated bike area in this heavily traveled corridor (bike, pedestrians, cars, all of them), there will be too many dangerous conflicts between bikes, pedestrians and cars. There are bicycle commuters here more than recreational bicycling.
Biking along this avenue is impossible as it is now. North Avenue, as a result, is not a place that you casually stroll into. Instead, it is an eyesore, and a place that I dread going to.
Safety ! More and more people are riding and North avenue is used a lot by students. I think all of Grand Junction should have bike lanes and even alternate bike routes for all with stop signs and ped. crossings
Public roads should be safe for all users, and riding on sidewalks is not a safe option for cyclists or pedestrians (even if the sidewalks are 8 feet wide). On a road like North Avenue with very heavy automobile traffic, its difficult and intimidating for a bicyclist to ride in traffic. Wide bike lanes would change that perception and provide the best east-west bike route in that part of the City. From the options above, I would actually prefer a hybrid that uses 8 foot sidewalks with 8 foot landscape buffer, but also pinches the travel lanes down to 11 feet and adds the extra 2 feet on each side into 7 foot wide bike lanes.

This is a central business location and also the college zone, many people ride bicycles to school and/or work not only for exercise but also to reduce carbon emissions and save on gasoline. Bicycling is a green activity that we should not discourage.
I use the side streets to access the shopping centers on North Avenue. Riding along with lots of traffic isn't that great for me.
North Avenue is an intimidating road for riders. There are no bike lanes, and traffic is heavy and often aggressive. The road is a major artery through the city, and one of the quickest routes for getting from Fruitvale to Mesa Mall. Bicyclists are forced to take other, longer routes because North is simply unsafe for bicyclists. / / It should also be noted that it is a relatively flat road (unlike Patterson and the Riverside parkway). Flat roads are very accommodating to commuting bicyclists.
Bicycling is one of our community's biggest draws. Our city needs to continue its commitment to creating a bicycle-friendly community.
much safer for all concerned
Improved safety
people ride on the street now and are constantly in danger from the cars passing by
Encourage bike riding safely. Less gas, more exercise, but without a bike lane, it can be unsafe. /
Too much traffic
There is a lot of bike traffic on North Avenue and it's a mode of transportation that I think is important in a town like ours. Not only that, but I would like to be able to take my children on their bikes and now that we have a place to ride safely on North Avenue. / Thank you for giving us a voice!
Bikes are becoming more popular and this needs to be encouraged.
The college is on North Avenue and lots of students bike to school. / Biking is a good form of exercise.
There is very little room for bikers now and there are lots of bikers who use North Avenue
There are a lot of people in Colorado that bike. Taking into consideration the environment and gas prices, it is good to have safe alternatives.
People use bikes all the time, especially more so with the price of gasoline being so expensive.
There does not seem to be enough room on the road right now for cars AND bikes. Riding on the sidewalk conflicts with all the people walking there (especially around the college). There are SO MANY people on bikes in this area (especially around the college) that there needs to be some sort of exclusive bicycle lane. It seems that people here are bicycle commuters (they aren't on North Avenue because it is a fun and pleasant place to ride!) which makes the bike lanes here more important than they would be in other areas of the City (I think bike lanes are better for commuting than sidewalks or trails, for the most part).
Adding bike lanes encourages residents to live healthier, more environmentally-conscious lifestyles.
We have a lot of bikers in the area and it will be safer for bikers, pedestrians and traffic
If you get rid of driveway cuts--otherwise it's too dangerous.
There is already so much traffic. Adding bicycles to the mix is just another thing to watch out for if you are driving. It is also dangerous for the bike riders. So many intersections and bus stops, I can just imagine drivers not paying attention turning into a cyclist. I think this street is way too busy for a cyclist path. It would make more sense to make the bike lanes on the streets north and south of North.
Safer for bike riders.
It would be a lot safer than trying to have them on the sidewalk, or weaving in and out of traffic.
because this is a city that attracts the kind of people who bike and this would encourage less car traffic.
Keep them out of traffic, and off the sidewalk
It's an important alternative form of transportation and this is a significant connection across town. It makes sense to connect the two.

Traffic hazard. Other methods of transportation available as well as transportation corridors that would be safer for both biker and vehicles.
People need to have a safe place to ride
As a bike rider it is not safe to ride on North ave at any time, adding a bike lane will greatly help.
Hopefully if businesses return to North Ave. it will be way to busy for bike traffic.
Corridor to downtown area and other areas of the community, including access to the college.
Too much traffic. Bikers rarely stay in their bike lane. Bikes do not belong on such busy streets. Cars slowing down or swerving around bikers to give them more room, and bikes coming in and out of their bike lane, will cause accidents between cars and injuries to bikers.
Central transportation residential to commercial and employment epicenters. Growing number either on bikes by choice or necessity.
I think it is a good idea to do something about the bikes. But North ave dose not have the area for bikes with or without a lane it is not safe. Way to many people on bikes do not fallow laws and right single file. Makeing it unsafe for both bikers and cars
So that they are not slowing up traffic, and are not on the sidewalks.
North Avenue just doesn't seem like a place that people are going to be ridding their bikes around. it's more of a business street with heavy traffic not the ideal place for a bike ride.
Easy and safe travel for college students as well as eco modes of transportation.
If I were riding a bike I would not take North Ave. but rather a side street.
NO we dont have funds
Bike Lanes will greatly improve safety and provide better recreation opportunity as well a promote alternative transportation. In addition, a large amount of student bike to school and need an appropriate place to ride safely.
To be energy efficient encourage healthy exercise, many people already attempt to ride their bikes along North and it's not safe. We need to make accommodations for them.
The traffic moves too fast and the lanes are too narrow-most bikers ride on the sidewalks when they ride on North or they avoid it all together.
North Ave is the only way to access many of the businesses there. It is far too dangerous to ride on North Ave now. I do ride my bike to run errands, etc. But I also drive sometimes, and it is really scary when bikes are on the street.
Lots of people travel by bike in this town. North Ave. is a main road and should accommodate all travelers.
Bicyclists need a safe way to travel on this road.
It is legal to use the sidewalk and the number of bikers on N. Ave is small. No widening is needed because the traffic is / way down because of the installation of the Parkway and the improvements that have been done on Patterson. / The street is not the problem. The problem is that the stores have closed.
Grand Junction should be encouraging their citizens to bike more for too many reasons to list.
Not a lot of people bike on north
Bikes should stay on the side walk.
Too dangerous. Traffic moves too fast. Becomes a battle ground for all vehicles. Besides that, grouchy old people enjoy toying with bicyclists in this town.
I don't like any of these because they will not work to solve your problems. They are designed to fail. /
Provisions for bike traffic are necessary for current use, but more importantly, future use.

First, road bikers in the Grand Valley seem to rarely use the bike lanes and instead choose to ride in the road. Secondly, North Ave. is a business/shopping district that doesn't seem to have heavy bike traffic currently. There seems to be more pedestrian traffic, which is why I believe sidewalks are more important than bike lanes.
Exercise! Good for students. Cuts down on carbon emissions.
we are all trying to save on gas and help the environment. we need SAFE streets to ride.
As this street should be used as a business and shopping district I think that traffic flow and safety should be the chief concern.
I have rarely seen people riding bikes on North Avenue, it might just be the times that I am on North Avenue though. I see many more people walking.
Because more and more people are getting around on bikes, creating safety issues.
Even if you not have bike lanes, people ride there bikes on the street. It is dangerous for the bikers if there is no bike lane and it slows traffic down because motorist have to navigate around the bikers.
I have not noticed much in the way of bike traffic along that area and don't see it increasing in the next decade.
Lots of traffic making it unsafe for bikes. Encourages people to ride bikes instead of drive.
Lots of bikes in the middle of the road.
To encourage safe, carbon-friendly commuting
As an avid cyclist and a daily bike commuter and pedestrian around town, the idea of more bike lanes and pedestrian friendly areas is appealing. But in thinking about North Ave it's hard to say whether these would work well. It's a narrow road with a lot of fast vehicle traffic. Would the developed areas be narrowed to one lane of car traffic (fine by me, but many people would probably object) for Options like 3 or 4? Even if there is a nice bike lane there is still a lot of traffic turning into businesses and side streets that could be a hazard to bike travelling in the bike lane. I may avoid riding in a bike lane on North Ave for that reason and choose a less busy parallel road. / / I probably would spend more time walking on North Ave if it was a more pleasant place to walk. / / In an ideal situation North Ave would be one lane each direction for cars, roundabouts at intersections and dramatically slowed car traffic. Like Main Street. Plenty of pedestrian and bicycle access, reduced car access. Main St is quite nice and I will sometimes go a bit out of my way to deliberately ride my bike down Main St because it is a pleasant little stretch.
cycling as a form of transportation should increase in the future
Due to the amount of traffic on North Ave., not sure this is the best location to encourage bicycles.
Because of the pedestrian traffic associated with the college and the downtown area, it seems to make the most sense to add a safe lane of travel for people who choose transportation by bike. If there is not a proper bike lane, bicyclist will ride on the sidewalks.
More and more people are using bicycles as their preferred form of transportation. Transportation planners need to include safe ways for people to ride their bikes from point A to point B.
This type of transportation will become increasingly important as gas prices go up!
For the safety of cyclists and to help the flow of traffic.
Bicyclists will use the road regardless of whether the bike lanes are there. Bike lanes make it more safe for the cyclists and motorists.
I think as we look toward the future we should plan for alternative forms of transportation as energy costs will undoubtedly continue to rise.
To make it a little safer for bike riders. We have seen a lot of different times when bicyclists have been almost hit by cars and a few times where they have been hit. It gives bike riders a little more of a safety zone for riding and keeps the sidewalks clear for pedestrians.

There are no other continuous east west routes on the north side of North Ave for bikes. Those routes on the south side of North Avenue are somewhat continuous but lack signals at major intersections and with the exception of Gunnison all dead end into 12th St. Speeds on North Ave between 1st and 28th are 30-35 mph which is more bike friendly than the 40-45 mph on I-70B and Patterson Rd. / / Option 1 should be implemented as soon as practicable maybe with next chip seal followed long term by Option 3 or Option 6. Option 1 will help towards speed limit compliance as the proposed 11 foot travel lanes "squeeze" the feel of the road for the motorist reducing average speeds. / / Thanks!		
Many people ride their bikes and as a driver, it is a hazard to get around them especially when traffic is busy. This is not safe for the bike rider nor for the drivers.		
link with other bike lanes		
This makes it safer for everyone involved. The bikes are there anyway, so let's make it safe.		
Road too busy - move bike lane north to Elm or Glenwood		
students		
Traffic is a problem already - adding the need for cars to wait for bikes to turn on or off of the street will make traffic back-up even more.		
North Avenue is a major traffic route and you should maximize the motor vehicle traffic. You have taken too many routes from four lanes to two already so we could have "traffic calming" and bike lanes. If you get traffic much more calm we'll all have to ride bikes!		
Alternate forms of transportation are an important part of Comprehensive growth plan.		
safety, more appealing to riders		
Traffic is bad enough on the street the way it is. Adding another lane for bikes will just increase the risk of accidents.		
Too busy of a street and there are many other routes that bikes can take.		
It would create a safe route for college students who rely on bicycles for transportation.		
Not enough bike traffic on North Ave.		
There are too many turning movements with numerous driveways along North Ave. and we already have alternate east west bike routes that run parallel to North Ave. (Gunnison Ave. and Elm Ave.)		
It's a commuting road.		
Not really		
Why should gas tax and vehicle registration fees pay for bike lanes, since they do not pay an sort of user fee or tax. It is a waste of a significant amount of money for the small number of users that would utilize the lanes.		
Traffic is too heavy with cars on North Ave. The "element" that use North Ave are not good drivers. They do not pay attention to the road, let alone anyone on a bike.		
Absolutely NO bike lanes! If traffic picks up again, it will be a hazard. Cyclists have a tendency to ignore laws and think the roads belong to them.		
Too dangerous for the traffic that travels at lunch time and rush hours. Possible lawsuits for the three foot right of way will hamper business and cause		
Somewhat, as more bikes are in use for safety there needs to be designated areas that Bikes would ONLY be allowed in.		
Separates cars from bikes, from pedestrians.....		
There are adequate alternate routes to ride a bike between 12th St. and 1st St.		
Total Responses		
310		

4. Do you think on-street parallel parking is important to have along North Avenue?

Yes	28	8%
No	323	92%
Total	351	100%

5. Why or why not?

The parking can be congested in that area, especially when you have a special event, you get walkers, bikers, vehicle traffic, etc... Any area to allow for extra parking is an asset to North Avenue.
College kids will park on the busy street and accumulate expensive parking tickets which their parents will end up paying. The parking should be reserved for the businesses located on North Avenue.
The road is far too busy to allow for parking and doors opening and most businesses are set back far enough with their own parking that this would be a waste.
It is such a busy road it would be dangerous to have people getting in and out of the cars along the road.
It is such a busy road it would be dangerous to have people getting in and out of the cars along the road.
Once again, I believe that we would see an increase in accidents if parking were allowed on North Avenue. Something needs to be done about the congestion on North Avenue before something like this is even considered. Honestly, I avoid North Avenue as often as possible.
Too busy a street
it will help with the "NO PARKING" along North Ave.
Most businesses, already have their own.
Again. Traffic flow, is extremely important.
Traffic will only get worse, in the future, and
ans much as some folks hate to admit it.
Bikes, will, never, ever, replace the car.
This aint China !
There is enough off street parking.
It would slow down traffic too much, and there already seems to be plenty of parking.
Safety
The types of businesses along North Ave primarily have their own parking lots. Parking along that very busy street would be unnecessary and confusing.
Heavy traffic on North and going to fast for street parking. It appears that most businesses have ample parking. On street parking would only add to congestion on North.
Yes so that it would be easier access to businesses also events happening at Stocker Stadium. My only concern with this is that people would have to yield to oncoming traffic.

Grand Junction have very few roads that people can use to get across town. If parking is necessary it should be either at the business, behind the business or on a block either side of North Ave. The danger of accidents with people parking on the street in any manner needs careful consideration. Consider this... cars, people on sidewalks, bikes and then you want to add parking... I think that's a bad idea.
Hopefully we will have clusters of businesses and residential structures off the street and not need to park right on the street. Less parking may also promote more use of public transit in the future. We should look at a society that is less car centric.
business corridor not neighborhood
The street scape is unlike downtown and has many existing parking lots.
Makes the street more user friendly and slows down traffic
It would create too much congestion.
Especially with bike lanes, parking creates danger for bicyclists. Parking should be on side streets.
I believe parking should be done in parking lots. I feel uncomfortable parking next to relatively high speed traffic and feel it creates a dangerous situation for both parked vehicles and vehicles traveling on the highway.
Businesses already have enough parking on site.
Not enough places you want to stop to shop or see anyway!
Parking along the street could create more accidents. Off street parking is available with most businesses along North Ave.
Many businesses have parking. And the bike lanes are more important.
Parking on north avenue is difficult to find. Creating parallel parking along with narrower traffic lanes will create a calming affect and essentially make north avenue between 1st and 12th a more pleasant place to be.
Clear traffic. Set up side street parking. East end of N. avenue seems to have clear parking areas.
there is already sufficient parking. If anything, I suggest removing some of the parking lots.
SOME BUSINESSES DONT HAVE A PARKING LOT IN FRONT AND SO THEY HAVE LIMITED PARKING
Most if not all business have parking already. I would rather see space used for bike lanes or an increased median with trees.
Lots of off street parking except for JUCO and graduations.
On-street parallel parking will make the flow of traffic too iratic with people having to stop or slow down while allowing another vehicle to park. Parallel parking adds a danger to bicyclists. Parking lots are the best answer.
Seems like businesses will have their own parking. Riding a bike next to parked cars is also scary.
seems like most of the shopping has parking lots. If planning to build on parking lots then we will need parking alternatives.
Parallel Parking would be just as bad of a nightmare. Are you trying to take the thousands of cars off of this thoroughfare?
I think some parking along North Ave. would be a great idea, especially for storefronts which have little parking available, and would do wonders to boost business along the corridor
most businesses don't have enough parking places.
Need vs want.
slows traffic
Most of the places have parking lots
I feel that on-street parallel parking would hinder traffic flow and could create a safety issue with the added bike lanes.
North Ave is essentially now a highway. Unless traffic is slowed on this route considerably it seems to make littel sense to have parallel parking on it.

I think it is important if parking becomes a major concern. Otherwise have each business provide parking.
Its too busy of a street and parallel parking will cause too many delays
Parking is not the problem with business retention on north avenue. It has not been a pedestrian friendly atmosphere, walking has not been encouraged, crossing the street is flat dangerous, and the ambiance and beautification you see in other areas has not occurred. In short, there are not enough "captive" customers in the area, nor sufficient draws to bring and keep customers there.
With all the traffic, it would be a jumbled up mess! Besides, what business is there on North that needs the on street parking?
Most businesses have parking lots and there is significant traffic flow on North Avenue. I believe adding parallel parking will cause traffic to slow down and I don't believe that is the safest option.
Most stores and business have parking now. Let's keep that and not impede traffic flow with parking activity.
The businesses along North have plenty of on-site parking. In many areas it's already a sea of asphalt.
Is dangerous for bike lane
I would like to see more street frontage of buildings and parking away from the street.
I think parking should be behind the shops/restaurants, keeping the storefronts in front for better pedestrian access and to just make it look nicer.
A business should be required to provide parking for its customers.
You are just asking for trouble if you put parking on North Avenue even with a parking lane. While I think people are used to seeing bicycles on North Avenue (usually on the side-walks by the way), we haven't had parking along North Avenue in as long as I can remember (I grew up here).
no room, too much traffic
There is more than sufficient parking for businesses along North Avenue
There's plenty of parking in the parking lots.
most business have off Street parking.
would impede the flow of traffic.
Some, but not the whole corridor. Most businesses already have their own lots.
I believe it would clog the traffic flow.
The businesses and shopping centers along North Avenue have adequate parking.
There really isn't a need.
I may be wrong, but I have not experienced any problems with parking on North Ave with the available parking areas. Perhaps with the changes in the right of ways, buffers, and bike lanes, the parking areas would be smaller. In that case, it might be OK. However, on-street parking tends to conflict with bike lanes and pedestrian traffic (at times).
There is too much traffic on North Ave to accommodate Parallel Parking
It would be better to create more parking elsewhere if possible as the college is doing with parking garages, etc. - it is likely to just be filled with student cars if there is parking on North Ave. Also, it would be better for everyone if students who are able to walk or bike to campus.
MOST BUSINESSES ALREADY HAVE SUFFICIENT PARKING AND THE TRAFFIC MOVES TOO QUICKLY TO ALLOW FOR SAFE PARALELL PARKING
Most businesses along North Avenue already have adequate off-street parking.
Businesses along North have their own parking, so don't see this option as important - even in the next 25 years. Like the idea of bus pull-outs though.

If businesses weren't set back so far my attitude might be different, but at the moment, parallel parking seems pointless and potentially dangerous.
Parking is important, but many of these businesses already have some parking. If we are loosing a significant number of spaces by 'emminent domain' to create bike lanes sidewalks bus stops, etc. It will be important to find new spaces, garages etc. Parallel parking doesn't seem to be the only way to do that. New construction setbacks and off street parking seem a better choice for the North Ave corridor.
Not needed
Another issue for bicycles to deal with. Impedes traffic flow.
Businesses should bear the responsibility for providing parking for their patrons. Smart drivers/owners don't park on busy streets for a reason. Learn from them.
No parallel or angle parking on busy arterial streets, especially North Avenue.
impedes traffic
That also would be to dangerous...
Too much traffic to parallel park
It is too busy a street.
Too much money to provide on street parking. Let the business owners pay for that as the properties are redeveloped.
Too much traffic in corridor to support parallel parking.
Parking in the lots by the businesses is just fine. I've never thought "Oh, I wish there was more parking on North Avenue." Parallel parking would be too dangerous on this road.
The speed and volume of traffic rule this out in my opinion. The idea of someone backing into a spot with everyone else in the lane being held up seems quite dangerous.
This would hinder traffic flow.
Because the feel of the area would reflect back to the way it was in times past. Many towns had an area for parking along the street. The old time feel of the main street where the street was the area you were going to.
DANGEROUS AND BAD FOR TRAFFIC FLOW
We want to promote a healthy lifestyle and encourage fewer cars. Every car trip costs money, every bike trip saves money and pollution. Parking will create more hazards for drivers and cyclists.
there are a lot of parking areas already
Not enough space.
The road is too narrow to allow room for door swings and bike lanes. Also, cars add to the visual clutter and will detract from the landscaping.
The commercial businesses along North Ave seem to have sufficient parking. I have never had a problem parking in the vicinity.
right now there seems to be no problem with stores having plenty of parking. parallel parking could actually cause more traffic congestion with people trying to get into spots on the street.
I think that if new attractive businesses come into the area additional parking will be needed. It would also slow down traffic on the road and make it feel less like a hwy.
Car doors are a menace to bike riders

Yes, if you want to create a sense of place where pedestrians actually shop. If you don't want that, then no.

I'd urge you to move away from the strip mall pattern (huge parking lots, fast traffic, car-centric, not pedestrian friendly) that currently exists on North. I avoid North Ave. like the plague. It is one of the reasons that people throughout western colorado often refer to this town as the cultural armpit of the state. Once those folks see downtown, they're often pleasantly surprised. In fact, many of those folks have seen main street and said: "maybe there is hope here."

Having a bike lane on the outside of || parking is dangerous for not only the auto drivers but the cyclists with doors opening and cars pulling in/out. All the businesses along north, currently, seem to have plenty of storefront parking and if not there are plenty of places within a minute walk to the store.

All the retailers already have designated parking along the cooridor.

It slows down traffic too much on this busy east-west road. There are massive amounts of vacant parking lots currently being under-utilized.

Dangerous and unnecessary

I can't think of any place I've ever gone on North Ave. that needed additional parking

Again, the traffic moves much too quickly and at much too high a density for parallel parking to be feasible. Parking should be provided at businesses along North Ave. such as the new Fiesta Guadalajara at 7th & North or such as the REI shopping Center or at Copy Copy.

As a cyclist, I think on street parking would bbe a hazard to bikers in the bike lane.

too busy

There are enough parking lots attached to each business, parallel parking would be crazy and dangerous. DONT DO IT please!

I think there is plenty of room in the redevelopment process to create sufficient off-street parking.

limited space already and most businesses have their own parking anyways. Plus traffic would probably cause accidents with people opening their doors due to high traffic.

if the right of way is added on either side of the street, then some parking which exists now in front of businesses may be eliminated.

Most businesses have parking areas.

On-street parking blocks the view of traffic turning onto the street.

Most businesses already have off street parking.

Most stores have parking lots already

Extra parking along north Avenue is unnecessary with businesses providing ample parking already.

Seems like there is already plenty of off-street parking and having a more bike and pedestrian friendly street could encourage spending more time on North Ave. As is, North Ave is too busy with speeding traffic to want to spend much time there - it ends up being a highway. As a walker and biker I would do more on North Ave if there was safer travel.

Fast-moving traffic, plenty of asphalt space behind and around building for parking. Safer, too.

Dangerous to bicyclists and other drivers; would slow the traffic on North Ave.

There is more than adequate parking off street on North Avenue. Cars pulling into & out of on-street parking creates a hazard & will greatly effect traffic-flow.

Maybe if a certain stretch of North were designated more of a walking, storefront, Main St type locale

My thought is no. Does it exist anywhere now? From my recollection of driving North Avenue, all businesses currently located on North Ave have offstreet parking. Keep it that way. Use the extra space for bike lane, sidewalk and tree lined buffer area.
North Avenue is one of the main corridors through town. In my opinion, on-street parallel parking is an inappropriate use of space along North Avenue. It poses a danger to vehicles who are not parking and a danger to those attempting to pull out into traffic. North Avenue should be kept open to the flow of traffic as it is already cluttered enough, particularly through the college area.
I believe that parallel parking would create more traffic congestion and accidents along North Avenue and thus would be horrible for North Avenue.
We should go with that back in again, then we can at least guarantee some job tearing it out in two years.
There is plenty of parking in all the empty retail lots. Businesses should be providing parking not the city.
I think as busy as North Ave is, it would be dangerous to have parking along the street. It seems like it would really cause a lot of congestion. The businesses along North Ave seem to have adequate parking on their lots.
Most businesses along North Ave have adequate off-street parking lots. Adding parallel parking to a road as heavily traveled as North Ave will cause hazards as drivers attempt to either park or enter traffic.
Parking along North would only be to support businesses. I believe the businesses should have parking lots that they pay for, rather than parking spaces that the taxpayer pays for. Also, I think North has too much traffic for safe and efficient on-street parking.
Many parking lots
The business have parking lots and that is adequate.
It doesn't seem like there is enough demand for parking right now. I suppose that could change if there are more businesses along North and/or fewer parking lots for those businesses.
I've never felt the need to park on North Ave. and wouldn't feel like I was gaining something from having the option - there are plenty of parking lots for North Ave. businesses.
Plenty of side street parking
It appears most businesses have plenty of off street parking.
Dangerous with bike lanes on a busy road. /
NO - a total waste of space and money and will make cycling even scarier waiting for a door to open on you.
Many business have parking lots.
Most businesses on North Avenue already have ample parking lots to accommodate its patrons.
The condensed areas of North Ave. have inadequate parking so curb-side will help greatly.
Just stupid. Some people won't realize that it IS parking space & try to drive in it. Will cause more accidents.
Most businesses have ample off-street parking now. Parallel parking slows traffic and is dangerous for adjacent bike riders.
Individual businesses have their own parking, all spaces would be taken up by college students and would not benefit businesses much.
Knowing the driving habits of locals, there are to many dangers areas that can't support stopped or slow moving traffic. To much police presents or lack of presents, along with not wide enough streets and side-walks, and beautification with colors/plants, loss of/bad businesses, impeding golf-course on road-way, have resulted in the slow down of visitor to businesses.
Park to go where?? Most businesses are set back from the street with their own parking lot.
Business has parking
Increases risk of accidents

Many North Ave. businesses have parking already. Parallel is not an ideal option for a busy thoroughfare. /
I'm not sure of the benefit of on street parking, but think it would be a hazard to bikes.
Off street parking is available.
There is parking for most shops already
It's dangerous to have people moving in and out of traffic like that on a main route
Too dangerous
I am conflicted over this one. I can see how business owners may prefer it. However I believe that vehicles slowing and coming to a stop on North Avenue so as to park parallel would lead to additional delays and congestion. I realize that not all business establishments have the luxury of having a lot of off-street parking available.
there are no bussiness cause you ran them to the west end of town by the mall.
Parking is provided by the businesses in this corridor--keep it that way. Cars getting into and out of the spaces will only impede traffic--again, can we have a few streets that are designed to move traffic rather than calm it, causing road rage, because you can't get from one end of town to the other without intentional stop and go traffic??? A person shouldn't have to swing out to the north or south to avoid getting across town.
Unlike Main Street almost all the businesses on North Ave. have parking lots already.
I do not think it would hurt, but I do not believe that should be a high priority.
Most established buildings on North already have parking accommodations---How can parking spaces be created in an already defined space along with the proposed sidewalk, buffer, lanes, bus pullouts, bike lanes. This would affect traffic flow on a very busy street.
Parking should be off-street in parking lots.
too busy of a street to have cars parking in such a fashion when most people struggle to do so without any pressure
If a business believes parking is important to its business, it can pay for the parking. The city, however, must plan for the future. We should not be subsidizing a form of transportation that is in an inevitable decline.
Let 'em park off street. Again, follow Fiesta Guadalajara. It's the trend nationwide. / It also keeps the inside of the businesses cleaner. There's gotta be another city w/ the same challenges, but I'm stumped as to where.
Businesses should have to provide their own parking not the taxpayer
This is even a worse idea than bike lanes! There is plenty of parking for the businesses now. Opening doors into traffic is dangerous and parallel parking is time-consuming for many of us who rarely do it anymore and would stall traffic flow. Watching for children jumping out of cars is dangerous. Really bad idea! / Since you forced me to choose two options in question #1, I chose the one without a bike lane, but I would rather have voted for option #2 twice and not be forced to choose parallel parking!
Parking is now in up front lots, but as it develops, can be provided in rear.
traffic hazard - stopping and backing in. adds only nominal number of spaces.
This will make for many accidents I think, and since it's once of the main roads this will be very bad.
,Hardly anyone is capable of parallel parking any more. Not used enough to be proficient. Holds up traffic behind them.
I thing off street parking is safer
plenty of parking right off N. Ave
Too expensive
I see the heavy flow of traffic on North Avenue being problematic with people trying to enter or leave parking spaces along the street.

Most businesses have parking available
no it will impede traffic on an already busy street
It does not seem like there is currently inadequate parking for the businesses along North Ave. It would seem the space could be used more effectively with one of the other options presented.
There is enough off street parking.
most businesses already have parking and there is parking available on most of the side streets. for the safety of walkers and bikers, there should not be street safety
This is an equally insane idea, as it takes times to parallel park & that causes traffic flow to really slow down, unless you provide extra space in which to do it, out of the traffic flow. Parking spaces are adequate at the businesses as it is, IMO.
Because the businesses along North Avenue already provide parking for their customers. Parking has never been an issue for me when patronizing any businesses along North Ave. Additionally, On-street parking will inhibit the safe and convenient flow of traffic.
don't think it is necessary and will make it more dangerous for driving and biking.
This is more of a "shopping thoroughfare," rather than a small, compact shopping area. The parking needs to be off North Avenue for reasons of safety and traffic flow.
I have never had a problem finding parking at various businesses along North Ave.
Too dangerous for passing traffic and bikes.
No parking of any kind on the street. It would impede the traffic flow.
Many businesses structures are already a sidewalk away so how can you shrink the road to add parking. Most businesses have their own parking. / Maybe you should clean up the medians you have in place and put money into getting businesses into all the empty building. Changing the street and getting rid of Lincoln Park Golf Course isn't going improve the street.
it will interrupt traffic flow, which will detour people from the area. help traffic and parking by developing the next street north and south of North with some parking...
Takes up too much land, land better used for traffic flow. Parking will slow up traffic.
There is an ample amount of parking off of the street.
Most all of the business have off street parking.
I think it would be more of a hazard then a help - few if any businesses need street parking, they already have parking at their place of business.
Businesses seem to have adequate parking off of North Avenue. Moreover, the tradeoff for on-street parking is a loss of traffic lanes, bike lanes, or sidewalks, none of which is a good idea.
Most businesses on North have off-street parking. Also, the combination of busy traffic and biking don't go well with parallel parking.
No reason to park on North Avenue when the businesses along North Avenue have their own parking. It isn't like downtown where there is no parking for each individual business. I think this would be a waste of money.
North Avenue is already too busy to add more chaos (parallel parking) to the mix of heavy traffic and heavy pedestrian traffic that is already associated with Grand Junction High School and Colorado Mesa University and events at Suplizio, Stocker Stadium and Lincoln Park.
People can park at places of business that already have off-street parking.
I feel it would congest things too much on a busy road.

the street is too much like a hwy. Backing in and pulling out would be a problem. Off street parking lots and bus shelters is the best.
Off-street parking should be promoted along North Avenue.
So many of the store fronts have parking, it's not like Main Street.
Dangerous to bicyclists and other drivers; would slow the traffic on North Ave.
Absolutely not. There are enough parking lots and no businesses on the street that need street parking.
On street parking reduces safety for pedestrians and cyclists as a result of reduced visibility.
too busy a street.
The majority of businesses have sufficient private parking. Side streets can also be utilized for parking.
There is plenty of parking available as it is now. Most businesses are set back from the street enough.
Parking lots are available with most businesses.
Most businesses have sufficient parking, in my view. Of course, I would want to ask the business people this too--their response is most important. I have never had any problems finding parking to access the many businesses I use on North Ave because they all provide parking.
Works just fine as a major route
north avenue doesn't seem to have a lot of "draw" shops that require parallel parking
Parking is important for businesses.
It will interfere with flow of traffic and most businesses have off-street parking.
Traffic volumes on North Avenue are too high to have the constant traffic flow interruptions of people trying to park or leave spaces. Accidents are inevitable. Businesses will be better served by improving their own on-site parking and working with neighboring businesses to have shared parking. Also, there needs to be consolidation of curb cuts to limit traffic movement onto and off of the street.
On-site parking and sidestreet parking should be adequate.
There seems to be plenty of parking spaces in the area for most businesses/residences (I've never had trouble parking). It seems way too congested for on-street parking to be retro-fitted into this area. It seems that it would create a lot of blind spots for people who are trying to pull into traffic or cross the road (and this hold true for pedestrians, bicyclists AND motorists). I would rather the tight space be used for more pedestrian and bicycle amenities. The street is in need of street trees!
There is already too much parking along North Avenue. This is part of the reason why the avenue is so ugly.
most stores on North avenue have parking lots and the likely hood of an accedent happening in a parking lot is less then if you pull out from the side of the street with the speed that people travel on that road.
Between a "yes or no" I have to choose no, but I qualify that by saying that there may be some locations where on-street parking could be appropriate (ie, between 1st Street and about 28 Road where the development pattern is denser and more urban). But for most of the length of North Avenue, its a suburban development pattern where traffic moves faster and the road is used primarily for mobility to different destinations around the City. For those segments, its more important to provide good buffering for pedestrians and wide bike lanes so bicyclists feel safe on the road. Plus, if conditions on North Avenue change dramatically in the future, its easy enough to add in the on-street parking later when its more feasible (by removing sections of the landscape buffer and switching to a more "downtown urban" section).

I think this would create traffic back-ups and accidents, North Avenue is too busy to have parking like Downtown - businesses along North Avenue should be forced to provide parking in the rear or on the sides of their businesses. This should be at the businesses expense and not tax payers as they are making a profit and done so for many years along this corridor. Business must be held accountable to pay for their own improvements including access and medians.
I think it's a good idea and could work, it's just hard to imagine with the present configuration of North Avenue. I think Americans can't get past the park-out-front mentality but maybe some parking in the front, with more in the back will get people used to the idea.
I am a strong advocate of bike paths, and I find that on street parallel parking represents a considerable danger to bicyclists. This danger presents itself in two ways: First, and most obviously, people opening their car doors, which any bicyclist on the adjacent bike path will then slam into and sustain serious injuries. / / Second, with on street parking encouraged, it is likely that occasionally a driver will simply park in the bike lane.
There is already adequate parking, and the street is too busy for parallel parking to be safe.
all or almost all business have private parking
Unsafe
businesses have ample parking spots now and do not need anymore
There is plenty of parking in parking lots and off-street parking.
Most current businesses have parking lots
I feel it would congest traffic as well as make it dangerous for bikes.
to busy
Most businesses have parking lots.
maybe? Not sure since i do not frequent areas that I woud use a parking spot...let others weigh in on this one...
Parallel parking is not important for north avenue due to how businesses are set up along North, they typically have their own parking, and there is such high traffic that parallel parking would not be the safest option.
Historically, there have not been many requests for parking along North Avenue. We should keep it as a quick fareway to get from one side of the City to the other.
Isn't there enough parking here already? I do not have trouble parking at businesses on North Avenue (and I patronize a lot of the businesses here!). Wouldn't providing even more accommodations for cars, seemingly at the expense of bikes and pedestrians, make it very difficult to create a 'sense of place' and a place that people want to come back to which is the point of the study/plan? I only shop here because this is where some businesses are located that provide the goods/services that I cannot find elsewhere. Believe me, if I could go somewhere else to get what I need, I would have no reason to be on North Avenue. I'm sure the CMU students, faculty and staff would agree!
Retailers all have huge parking lots. Plus, car lined streets aren't as attractive.
It will slow traffic flow and could cause accidents
It is a major arterial and there are enough large parking lots to accommodate the businesses there.
Plenty of parking spaces already. Waiting for someone to parallel park (back up into the space) would just stall traffic further. Not to mention cars pulling out into oncoming traffic.
all the businesses have their own parking lots - its way too congested after 7th street going towards 6th and 5th to have parking and it would all be taken up by Mesa State Students
Too much traffic to park on North Avenue. To dangerous.

There are a number of parking lots at the businesses, and the side streets. The parking just adds to the congestion.
It would be nice, but not necessary.
traffic is too heavy to have any parking, parallel or angle
I think it's a great idea, but at the bottom of the priority list. It falls into the category of if I could have my cake and eat it too, we'd have detached walks, at least an 5 ft buffer, parking (personally I'd do angle parking and make the street 2 lanes - one each direction) and a bike lane. Just don't think that's going to happen.
Parking is already available in area businesses and additional buffer will take away the existing parking that some of those businesses already have.
too dangerous.
North Ave. is not Main Street and people will not be strolling up and down the street. North Ave. is a going in to a specific busy and get out.
Too much traffic if continue as primary access.
Again, too much traffic. Cars slowing down to find parking, backing up traffic to pull in and out, or stopping to wait for a space that is about to become available, are all problems that will occur on a constant basis (especially near the college) if there was parking on North Avenue.
Too much potential for accidents with high volume of traffic during busy periods. More than adequate off street parking currently available.
North Ave. is a thoroughfare designed to move large volumes of traffic quickly and efficiently. Someone trying to parallel park would be a significant hinderance.
Same with the bike prob to many people not paying attention more parking lots or maybe a parking grage someday
There is plenty of parking off of North Ave.
most businesses have their own parking so i don't think that it is necessary to have on-street parallel parking. also it would be troublesome to parallel park on such a busy street.
Business should provide parking along North Ave.
I really like this idea and I think that it would add character to North Ave. as it would create more of a shopper friendly environment and would be somewhat similar to a downtown Fort Collins. In Fort Collins (Old Town) the parking spaces are in the median at a slight angle.
no we dont have funds
It will only congest traffic more blocking views and become unsafe in attempting to find a parking space. Traffic is only going to increase and on street parking on a main thorough-fare does not make sense. This is not like Main St or other small downtown side streets.
Too many people drive fast through that area whether or not they are supposed to we are increasing the risk of cars getting hit and people getting injured.
Parking on North would be a disaster! People already don't watch where they are going and to add traffic moving in and out of parking spaces would be asking for trouble.
Businesses have their own parking. Parking on a busy street is hazardous - to the person entering or leaving the car, to the drivers, and to bike riders.
It is hard enough to get in and out of he parking lots on North Ave. Trying to park, or exit a parking space on such a busy road is a whole lot of accidents waiting to happen. /
This road is simply too busy for that type of parking. I believe it would cause all manner of accidents and traffic delays. The traffic load is prohibitive for safe parking, and exiting/entering one's vehicle.

There is no parking needed because the stores are closed. The stores that required parking have parking lots. / North Ave is Not main street. There is no way a "Park and walk to stores" idea can fly. The stores are too far apart. / The buildings have parking lots and do NOT need parking on the street.
All the shops on North Avenue already have their own parking lots. Parking is NOT an issue on North Avenue like it is in older parts of the city.
I don't see a lot of people parking parallel because north has a lot of people driving on it.
more parking equals more shoppers, and meters could be used for extra money.
Motorists won't slow down enough to allow someone to parallel park. Then trying to inch your way out of a space would be a disaster.
No one will come to any poor quality establishments.
Parking will promote unsafe conditions for motorists and pedestrians. It will also lead to further congestion.
The slightly outdated buildings along North Ave. have poor parking. Adding parking would be a benefit to potential customers of North Ave. businesses.
would cause too many accidents with cars trying to pull out.
Any room used for parking should be used for pull outs to improve traffic flow.
North Avenue is a very busy street and I think that having the on-street parallel parking could create more accidents.
Too much traffic on North Ave. Businesses should provide parking off-street.
It depends if you want a main street feel for North Ave. parallel parking might work. If you want a more commercial appeal I feel the parallel parking would make North Ave too congested and would slow down the traffic flow.
Most if not all of the businesses located on north avenue have their own parking lots. Also, parallel parking on a business street such as north avenue would be very unsafe.
North ave is crowded enough as it is. NO parallel parking. Bike lane is more important. Businesses have their own parking lots. Parking on the street is unnecessary!
Parking will produce problems for traffic flow.
there are many parking lots that could be spruced up for usage
There are plenty of businesses with plenty of parking areas along North Ave already. I assume that parking would be located to the right of the bike lane, often putting the bike lane right into the "door zone" where cyclists are at risk from car doors opened suddenly. In those type of situations I avoid a bike lane and ride in the car travel lane for my safety.
If the planning of North Avenue is to increase the commercial viability of the properties, then parking will be required.
North Ave. is a state highway.
North Ave is already congested enough without the added confusion and sudden starts and stops added by people trying to parallel park on such a busy road. Not a good idea.
North Avenue is already a difficult street to maneuver without having to worry about people trying to parallel park (which most people aren't very good at) or when they pull out into traffic without looking.
I personally would be uncomfortable parallel parking on North Avenue. I would avoid those parking spots because it is a very busy street and would be nervous having to back into a parking space.
I think it would be very dangerous. North Avenue is a very busy thoroughfare at a higher speed limit (and more people speeding at any given time) than other streets which have parallel parking (downtown).

Most businesses provide their own parking and there is ample room for that on North Ave. Parallel parking can obstruct traffic as cars pull in and out.
I think the additional parking would give more access to businesses for customers and if the parking includes parking meters it will also give more revenue to the city.
No as important as bike lanes as most of the area as developed with off street parking over the last 50 years. As with the bike lanes it's slowing effect on the corridor would encourage compliance with speed limits.
I think it will be difficult for people to park and not avoid tying up traffic that wants to continue down North Avenue. They will end up waiting for the person to park and this may back up traffic unless there is enough space that the parker can get out of traffic to park.
esthetics and safety
Businesses have their own parking.
Again, many use this street as a thoroughfare. This will add even more stoppage of traffic that will cause back-ups.
Most businesses along North Ave already have off street parking.
Businesses should have enough on site parking. It would very expensive to add another lane for parking.
More accidents
Not enough room, too busy of a street.
It would disrupt the flow of traffic too much as people take time to parallel park.
The businesses on North ave have plenty of parking spaces.
On street parallel parking would be very dangerous since this is a major arterial and backing and exiting movements would cause a traffic hazard
Too much traffic. Use of off road parking. Parallel brings traffic to a halt while someone parks, lane changing to avoid a car in the process of parking is hazardous.
It isn't that kind of street.
Too dangerous.
Traffic is too heavy for parallel parking. There will be many accidents. If you added those in addition to a bike lane, you're asking for trouble.
Once again, it would be a safety hazard once the traffic picks up again. If businesses don't or can't offer parking, I would rather see parking areas interspersed.
Parking on a heavily congested traffic zone would be like putting parking on Patterson Road. Bad idea
Traffic would be impeded and businesses have parking lots, this would just create additional blind spots for more traffic accidents.
Any real business would need more space other than frontage..... off street in lots
Parking would slow down traffic flow, especially in the right lane, as people slow down to pull over and park. Further, everyone starts moving to the right lane as they approach 1st Street in preparation for merging into I-70 B west of 1st Street. Most businesses between 1st St. and 12th St. already have adequate off-street parking, so there is no need to add parking spaces along North Ave.
Total Responses

308

6. How important would you rate each of the following to the redesign of North Avenue?

	Very important	Somewhat important	Neutral	Somewhat unimportant	Not at all important	Responses
Traffic flow and convenience	70.70%	19.70%	6.80%	1.70%	1.10%	351
Safety	85.20%	10.80%	2.30%	0.60%	1.10%	351
Aesthetics (appearance)	42.50%	38.20%	13.10%	3.70%	2.60%	351
Bike lanes	49.90%	22.20%	6.00%	6.60%	15.40%	351
On-street parallel parking	2.30%	6.00%	9.40%	16.20%	66.10%	351
Creating a pleasant place to walk	42.50%	33.60%	13.10%	5.40%	5.40%	351

7. Is there anything else you would like to add?

Keeping the area prestigious will contribute to the overall economy.
More lanes.
In order to really make north avenue aesthetically pleasing some of the old buildings should probably be torn down or remodeled.
In order to really make north avenue aesthetically pleasing some of the old buildings should probably be torn down or remodeled.
North Avenue east of 12th avenue is deteriorating rapidly. Businesses are leaving the area and the quality of the neighborhoods are suffering. I have noticed however, that people are buying houses and making an effort to fix them up. Lets fix up north Avenue as well!!
bike lanes on the entire stretch of "G" road so people can drive in our lanes without swerving into on coming traffic in order to miss the bikes.
Just read aloud, twice to yourself, what I wrote above .
Thank you
Joe Higginbotham
Palisade, Co.
Be sure and over communicate why this change is being done, why it is needed and how it is funded. Our community is struggling with the recession and it is difficult to see local government spend money on projects while laying off staff.
North is a main thoroughfare from east to west. It doesn't appear to be the destination that Main Street is. Traffic just keeps moving. Now that the west end of town has been built up it is important for the City to not let too many years go by without marketing that area or it may die and be a blight for Grand Junction. Market those empty stores. The empty restaurant (Sizzler?) building location may be a great place for a park. We have beautiful parks all around the City but none for use on North Avenue. Visitors could stop for a picnic and get a sense of the area, etc. A small visitor center?

<p>The scope of this is rather large for a single plan. For instance, the area near the golf course (12th and North) is certainly different than the area near 32nd road. The dynamics of the college brings a whole different need compared to the area of, say... WalMart or the car dealers on the west end of North Ave. I do like the idea of compatibility and a BASIC plan of traffic lanes, bike lanes and sidewalk. Possibly a minimum number of trees along the road per business or per 100 feet. What about turn lanes and a minimum number of feet for a turn lane... most are too short for today's traffic. How about no left turns without a turn lane? What about lighting? OK, I guess you have to get the basics before the specifics. Thumbs UP!</p>
<p>It would be great to see an area that emphasizes the businesses and residences much like Main Street and not the "straight shot" approach to making a road with little or no landscaping and no character. A new North Avenue that encourages people to slow down and enjoy the scenery and study the businesses or appreciate the residences along the thoroughfare may not only increase the business opportunities of the area but make it a desirable place to live and work as well.</p> <p>Thanks for all that you are doing to improve our quality of life!</p> <p>Chris</p>
<p>no</p>
<p>It will be a real challenge to make it safe for bikes, pedestrians, and motor traffic... but any improvements will make it safer than it is now. Thank you!</p>
<p>Tree plantings would be desirable.</p>
<p>What about a bus lane?</p>
<p>thank you for asking our input. I would like to see the city of gran junction expand in thoughtful ways, making the north ave corridor a mix zoned area with nice housing, and accessible services. This would incorporate creative multi-use structures that would include multi-family dwellings, commercial centers, places of business, retail, cafes, bars all together as small clusters with North Ave connecting them.</p>
<p>I do not ride on streets but my son was run over in January by a car coming out of a gas station while he was on the sidewalk. A bike lane would help improve safety.</p>
<p>It really sad to see all the empty building. Could any of them be used by the city for a recreation center?</p>
<p>Please void my number 2 choice on the first question. I really don't like that answer or any other choice, but the system would not accept my survey with that question being incomplete.</p>
<p>I was completely impressed with the communication about traffic, bike lanes and canals etc last year, but have not heard anything lately. I love the way you are keeping people in the loop and educating us along the way. Thank you!</p>
<p>I use this street as a main thoroughfare daily - as do thousands of other people. Why would you try to deter this? Adding bike lanes or parallel parking would create a traffic jam nightmare - it already is bad most of the day. This street is so busy because it is one of only three thoroughfares for east-west traffic (North, Patterson, and 70-B). Adding bike lanes and/or parking would just cause more headaches on Orchard, Grand, and other streets that are currently not able to handle the traffic. You should try DEALING with the traffic problems rather than trying to force people off of the street.</p>

I highly, above anything else, support an creating a pleasant aesthetic along north ave. the street is a huant for the poor, trashy and homeless. its not good for business, and its not good for our community. the medians REALLY need to be fixed up! along with those terrible sidewalks. Wider sidewalks would be a great idea. more cross walks wouldn't be a bad idea either. i would also support adding camera's to the intersection of 12th and north to catch people who run red lights.
The more that the east side is "forgotten" the less willingness to shop or commute, shop, or live there.
There needs to be more plants and trees on North Avenue. it's turning into nothing but ugly. The signs are garish and there are vast areas of blacktop on some blocks with no landscape. The businesses need to get some kind of coordinating look , or something else might be created to bringing a sense of community instead of urban blight.
Thank you!
All progress depends uon the unreasonable man - George Bernard Shaw
Make it more attractive to locate there by slowing down traffic, making it more attractive through landscaping, perhaps expand art on the corner, create a walking atmosphere. Currently it is a barren race track
You can "beautify" the road all you want, but you still see a bunch of big empty buildings when you drive down North Avenue. That's sad.
This area has been neglected for a long time it's it's business importance atrophied with the development of Downtown and the Malls. It's beginning to become more active, especially around 12th & North. Improvements to safety and appearance will improve that whole area and make it a part of the overall attractiveness of our City. I love that we take pride in our appearance, and care to spend the money to do so. It elivates our impression to visitors, and will make us more attractive for continued outside investment for new businesses who want to be part of a vibrant place to live. BP Mahoney
Thanks for the opportunity to comment.
As I am sure has been discussed, the number of access points to North Av should be reduced. While I think aesthetics are important, I don't think it should be heavily emphasized in this area. I think that the downtown serves this purpose. North Av can be cleaned up gradually without pooring dollars into expensive landscape options. It can be made to be pleasing without over doing the landscape and "aesthetics".
With right and left turn lanes, many of the traffic lights could be eliminated. This would help speed up traffic flow. Of course, one might have to wait 15-20 seconds longer to get onto North Avenue, but patience is a virtue. They're probably talking on their cell phone, anyway, and wouldn't notice the wait.
As long as you put in turn outs for the buses and turn lanes at least for the major intersections I think either one of the choices I put above would be good.
While redesigning North Avenue itself is an important step, it is more imperative to replace the anchors (such as grocery stores, etc) that make it a family-friendly area to live in.
North Avenue is an eyesore... anything you do would be better than what's there now.
I like the pullout areas for the bus. Aesthetics are important.
Don't break the bank.
Bikes could use the sidewalk. There isn't that much pedestrian traffic or bike traffic. They could co-exist.
I am most concerned with the safety on North Ave to pedestrians, motorists, and vehicular traffic. I think Option 3 or 4 addresses those concerns best for me.

As the city and college grow and especially as gas gets more expensive, it is important to make the area around the college, the area schools and the hospitals where so many people work safer for bikes and pedestrians to get to. This could also ease some of the congestion in terms of traffic and parking. Also, where there are bike lanes - the other lanes can be left for cars who will not have to drive around, wait for bikes in order to pass as often. It is more efficient. North Ave could really use some work - thank you for doing this.
GOOD LUCK
North Avenue has long been neglected, and it's good to see forward planning in the process. Our downtown area is a large tourist draw and improvements to other amenities will only add to the desirability. Also, Los Colonias Park would be a wonderful addition to the downtown and trail attractions.
Thanks for allowing us to have input ...
North Avenue is dying clearly. All development seems to be moving out to the mall area. I live in the lincoln park area. I moved in when it seemed like things were on an up tick and the neighborhoods in the area were improving. Now the exact opposite is happening and it almost appears terminal. If things don't turn around by the next real estate recovery, I will cut my loses and move to a more prosperous part of the city (or out of Grand Junction entirely) rather than seem the slow Cliftonization of my area.
An exclusive bus loop for main and north might be worth considering.
Good Luck
Re traffic flow: the only way to improve this is to add a through lane in each direction, right and left turn lanes and no bicycles. I doubt that there is sufficient room anywhere along North Avenue to do this.
Nope...
Why has it taken so long for the city to realize there is a problem with the east end of town?? Anyone thought about what it looks like to a visitor to drive in from Highway 50 and end up by a bum park and Ute and Pitkin Ave with all the deserted boarded up houses?? For a town this size, there are some serious problems going on.
Try to make it safer. Many older people avoid driving North Ave. because of the traffic and speed of that traffic.
Excited for North Avenue to get an uplift! No matter what it looks like. Thanks!
North Ave needs much help!!
People cruised North Ave in the old days. For some reason the government decided that people cruising in cars was a bad thing for that street and that area. (not only here but in most towns and cities like Colfax in Denver) I am not sure why but I believe that now it would not just be the younger drivers that would be attracted to an area where cars, shoppers, and folks just walking could mix and mingle but a mixed bag of all of these. An area like the main streets of times past. It's exciting to see the possibilities of a safe route on North for cyclists and pedestrians.
Thanks for the opportunity to comment.
More trees!
North Avenue is a major roadway in GJ. A major beautification project would reflect well on the city and also help people take pride in the place they live.
North Ave. has become a major eyesore, at no fault of the city of course since development has long ago moved out more west. North has great potential at least for providing bicycles and automobiles an easy way to move across the city. Bike lanes are very important on our major streets. Thank you for taking the publics interests into consideration.

Bike enhanced communities are healthier, happier and more desirable. Let's make Grand Junction a leader by placing a significant emphasis on cycling.
Make it neat or it will be blighted again in the future.
Making North Ave pedestrian and bicycle friendly will attract professionals and business variety. It could be an attractive area for young professionals, college students and retirees alike with the addition of pedestrian and bicycle considerations. Cars are a convenience and important but they are not the end-all in today's world. Give people places they can get to without having to drive and they will gladly do it!
Hurry up, it is an eye sore.
The more infrastructure in place for biking, the more people will feel safe to bike, and the more motorists will take notice of bikers thus decreasing traffic and wear and tear on the roadways. Mesa State College's very prominent location along North Avenue would also benefit greatly for the students if biking navigation of North Ave. is improved.
Thanks for taking input.
Keep up the good work!
North Avenue is in dire need of updating. I am often embarrassed with out of town guests and avoid taking them that route.
Beef up the center median so that pedestrians cannot jaywalk. Right-hand turn-lanes for traffic need to be available at all intersections.
Great idea!
North ave is pretty ugly, it could use a serious facelift
I highly encourage the city to provide safe and accessible bike routes throughout Grand Junction.
The Main Street facelift is great: as a newcomer to Grand Junction I am so impressed by the attention to the heart of downtown. North Ave could blossom as a commercial area by being more beautiful and easier to navigate as a non-driver.
Thanks for the opportunity to comment.
Good job providing options and asking for feedback! Thanks!
Create a Boulevard with landscaped median through parts of North or median separating traffic from bike lane and sidewalks
Good luck with this! It's a hard one. North Avenue is like the ugly step sister to GJ's downtown! If the City can come anywhere near as nice in the redevelopment of North Avenue as it has for the downtown area, you will have worked a miracle!
Having a bike lane could potentially increase public health (exercise contributes to weight loss, and positive mental health and well-being). A dedicated bike lane would contribute to a sense of community in the valley, and provide a viable alternative to expensive fuel consumption which pollutes our environment. Bike lanes have been needed in the Valley for a long time. I hope the bike lane gets added soon.
Thanks for taking the time and putting the effort into long range planning that is better for all concerned in Grand Jct.
Let's make sure we hire plenty of outside consultants, gather tons of useless information and stats, and have lots of fancy meetings and attend symposium interacts in far away places. Maybe do some more 45,000 dollar logo design. Get those folks involved again. They're good at spending our money.
Businesses should pay for things that benefit business, while the city should pay for things that benefit everyone, such as traffic flow, safety, and encouraging alternate transportation.
Make North Ave more like Main Street. A pleasing and welcoming place to visit.
no
no

North avenue is a commercial zone. I can't imagine anyone walking for pleasure along North Ave when a block north or south is more residential/less traffic. No one walks to go window shopping in strip malls.
It is good to see that the City is considering adding more bike lanes in our community. Perhaps we would have less car traffic on the roads if we had more access for people choosing to use their bicycles in a safe designated lane. As it stands, there are only a few routes one can safely use without feeling like their life is in the hands of a careless driver.
Glad to see there are bus pullouts planned, instead of holding up traffic, like they do now.
In the 3rd paragraph of text headed "Why plan North Avenue?" you used it's instead of its. Its is correct because the context requires a possessive pronoun rather than the contraction of "it is". The use of incorrect grammar takes away from the credibility of this project and is distracting to the reader. Please correct your error.
It would be great if there were store front grant opportunities on North, like there is for downtown businesses. Some of the buildings on North ave. are becoming very run down.
There are several other things I have thought of over the years, but don't have the time right now to put on your survey. I grew up in Grand Junction during the 70's and I can be contacted by dansrco@hotmail.com.
<p>Yes. There has been a great waste of money all the way around. We are not and never will be Japan. No one rides a bike to work. Our kids wouldn't even ride a bike to school. They preferred to WALK! If you really wanted to rejuvenate North Avenue you should have put the Police Department at Eastgate where City Market and StarTek moved out instead of building an expensive monument to yourselves downtown that the City can't afford and didn't need. Police presence would have made that area safer so business would want to be there. Let me say the reason we moved to GJ 30 years ago was because we fell in love with Downtown. Unfortunately, that changed. First there was the 7th Street fiasco, narrowing a busy thoroughfare and creating a huge bottleneck; How many times has that brick flower garden been repaired because a truck couldn't get around it? Then, back-in parking- which no one will use; Now, Downtown renovation which eliminates on-street parking, HELLO! can you say retirees? How about "HANDICAPPED"? We can't walk six blocks from the dark, scary parking garage and then shop for two hours AND eat lunch. It is too far, and if the meter runs out, we get a ticket. Have you seen the graffiti down there? Can you say "MUGGING"? And it the symphony goes to the Avalon, I am done with that, too. I personally have abandoned downtown. It is no longer user friendly, and pouring tons of concrete into wider sidewalks merely makes my back ache more. I liked Hobby Lobby on North. It was easy access and easy to park. Now, in its new location there is an 8-lane gridlock to get there and idiot drivers who have no idea what lane they need, so they just cut at will. Making a left turn out of Lowes or Walmart to get to the light is next to impossible. One way in, one way out, super long waits at the intersection, talk about pollution; and have you ever counted the number of accidents between Golden Corral and Pier One? I am not against progress or improvement, but I am for reasonable spending of taxpayer funds for genuine needs, and in NOT creating a solution for problems that don't exist. I shop at the Mall at the three stores that are not geared to teenagers, and online where I don't have to park at all. I am not the only retiree in Grand Junction. Our ranks are growing. Do we count?</p> <p>I think the east end of North Ave. is the bigger eyesore, and with so many vacancies in the old malls, restaurants, etc., now would be the easiest and least disruptive time to improve the area.</p> <p>North Ave. will always be a main east/west route through the city. It is important to slow traffic to a reasonable pace, but also allow for smooth traffic flow with good timing of traffic lights.</p> <p>Drainage during rainstorms is poor.</p> <p>Thanks for asking for input!</p>

You would have more shopping if you provided safe access for bikes and pedestrians along north ave
More landscaping!!!!
I would like to see bike lanes installed as soon as possible. It appears that this option can be implemented quickly and at minimal cost. Long term, I think it is important to try and make North Avenue a place that people want to visit. The city has been successful in transforming the downtown area. There is a reason that people like to go there and not so much to North Avenue.
don't screw it up like yu always tend to do
We have a lovely main street for ambling pedestrians. It is important that people may walk along North Avenue, however, it isn't a promenade--getting from point A to point B is the "point".
I'm glad to see this is finally being addressed.
thank you for taking the time to seek input from the community. I agree that North Ave needs "some work". I think that it primarily needs aesthetics (i.e. landscaping, etc) because it just looks too ugly-urban-dirty in some areas. Pedestrian access is also important along this key E-W corridor. But people will not want to walk in areas where they do not feel invited (i.e. safe, comfortable or welcomed), so you have your work cut out for you if this is one of your goals. Partnering with the local business will be a key to success in this endeavor, I would believe.
--Jason Bittle
I gather from the designs that the center median areas that contain planters and light poles would no longer be there?
It's time to do something about the look of North Avenue, even if it just makes it a nicer place to "cruise".
is there any plans in the future regarding zoning of building and aesthetics of those? Many of the buildings over the years have been allowed to be run down and create a very poor image of our town along a busy part of the city. Renovations by Mesa State and the businesses near by have done a great job for that stretch but further west is a bit of an eye sore.
Surely the city can duplicate it's success with downtown and all the commercial development west of 1st St. The old design of US Hwy 6 is obsolete. Time to get with the times.
Do not put trees in the center or on the corners. Aesthetic beauty should not compromise public safety. If you need an example just go down the riverside parkway. I can't count the number of times the trees have hindered my ability to see oncoming traffic while turning.
Encouraging business participation in the beautification of the area by asking small groups to form an alliance along their section of North could improve store front appearance, keep the landscaping under control, and encourage visits. Reconfiguring North Ave by the City would encourage the busines owner to do his part in upkeep of the area, esp. if the business is part of a small group along their section of North. Pride in the appearance of a busines goes a long way to keep customers coming. / On a separate issue, have you considered overhead ped. walks, particulary at 7th where GJ High students completely dominate the area at certain times of the day, without regard for traffic flow or their own safety?
no
I walk down north ave a lot and it is always trashy. Pot holes in sidewalks, uneven side walks, weeds make this whole area look like a dump. It needs a complete make over. Covered bus stops would be nice. A buffer on the south side between side walk and road by the VA would be nice. I would hope these improvements would bring more business to this area, but since many stores have closed or moved it looks like terrible and it used to be such a nice place.

Take a look at the medians and side right-of-way strips that are currently on North Ave. They are overgrown with weeds, lots of dead foliage, not at all attractive. Who is taking care of them? This can go a long way to improving the esthetics of the street. How about adding this care to the volunteer opportunities available to food stamp recipients. / The parking lots at K-Mart, Eastgate and Big Lots could stand a lot of improvement, too. I realize they are private property, but / you can bet their patronage would pick up a lot if the surroundings were nicer looking. Use the new college buildings fronting North Ave. as a model. Some guidelines as to paint colors on the existing buildings could be developed. Ex. no purple.
Drop the speed limit to 30mph along the busy sections of the street
I am curious about why the bike lane on G Road from 27 to 24 Road is intermittent. There is a mile where there is no bike lane delineated. I don't think it would be difficult to connect the "dots" with a stripe to remind motorists that cyclists have a place on the road.
For years North Ave has been neglected. It is time to offer incentives to businesses to locate their operations there and make improvements to the area. either re-route buses or make dedicated pullout bus stops as to not impede traffic when a bus stops to pick up and drop off passengers.
I wasn't aware of the reason that the city ceased mailing the regular info sheets on these topics - I used to read them completely & save for future referral. Glad this article was in the 5-30 paper so I found this survey. It seems to me that your top priority is to slow traffic in any way you can, whether using safety or whatever as the reason; guess you think most drivers/passengers want to dawdle along or the business owners think slow traffic will increase their business. Or maybe you mainly want to cater to visitors & ignore those of us who live here, who are mainly affected by traffic flow. I will make an effort from now on to attend the input mtgs. Then again, I'm not sure how much you intend to take our opinions into consideration, if your minds are made up & you're required to have these mtgs. just for the formality. What percentage of the population actually walk that much along North Ave.? All bus stops need to have their own pull-off area for stops, as this is also a no-brainer. What could be the rationale for making it worse for the 98% who drive North Ave. vs the 2% who walk it? It strikes me as if your motto is "change for the sake of change" or let's spend more money than we need to. I do understand that improvements are necessary, as stagnation is not a good thing. Let's strike a happy balance.
This part of the State is gaining a Nation-wide reputation as a mountain bike mecca. Our City's attitude should encourage and embrace this reputation through demonstrating our commitment with bike and foot friendly public thoroughfares.
Maintaining the current access points for all of the businesses on North Avenue needs to be a primary focus, concern and goal. This is especially true for existing left-turn lanes. These small businesses are an important part of this City, providing many jobs, sales tax dollars, and a vibrancy and variety that contributes to the City's character. Doing anything that makes it harder for these small business people to thrive, or survive, would be irresponsible and tragic. The first and crucial focus must be on the businesses, and stay on the businesses throughout the process, not on walking, biking, traffic engineers, aesthetics, or any other commendable concern.
Thank you for taking my opinion into consideration. I would love to ride my bicycle more often and I hope we can create a good environment in which to do so.
Glad to see this happening. Landscape strips with detached walks create a friendly and inviting area. Bike lanes are the most important plus keeping the roads as wide as possible.
Be careful with landscaping. It can be a detriment to sight lines and can create problems in the winter with snow and ice removal on the street.

it is not a walking area and will not be without a major overhaul, which would be costly. GJ could use more nightlife and lower north ave has potential, say from 1st st to 28th st. give some good deals to bring businesses into that area, make each property more attractive through the owner agreement and let the owners pay for it.... / / from what i see of the city engineering for street planning, well it is moderately poor. downtown GJ main st. is okay for the atmosphere it creates but ever tried to ride a bike there ?? the newly redone Colorado ave. is horrible for driving, it is too narrow as 2 F150's can hardly pass each other. trucks are a big part of the west. also the landscaping must be expensive to maintain for the city so they created a poor road for driving, and gave up parking for landscaping that costs money.... the city needs better planning and engineering for these side streets.....

A store like Costco is needed to anchor the redesign project. A store like that would draw new businesses to the corridor.

Landscaping along North Ave. would greatly improve the look, would give some scale and a sense of safety to people walking along the street.

There are times that I have walked on North Avenue to retrieve my car at Big O, as it is now it is very unsafe and not a pleasure. There is no shade, not always a sidewalk and very few places to cross North Avenue safely. I have to plan ahead on where I should cross before I get to where I'm going!

Thanks for soliciting community feedback on this issue.

Mixed use!!!!

Must have a sidewalk that people with all abilities can utilize.

We need pull in areas for busses with shelters and ramping. They would also be rest stops for walkers.

Yes, I couldn't get my Option choices to type in...my first choice would be #3, with both pedestrian way and bike path...and a row of trees either on the curb side on the business side. I would suggest that the sidewalk and bike path could be side-by-side, color-coded to differentiate...as I've seen in Germany. But then the city would have to require bells on bicycles...and require a warning bell when pedestrians are present. It works very well in Europe, the walkers are warned, and it does slow down the bicyclers. Bicycles are a means of getting from one place to another, and North Avenue should not be a racing course for them. My second choice would be #4, but a 5 ft. bike path is adequate. The thought of a friendlier North Avenue is very exciting...I walk it often. Myra J. Hoecker

North Avenue needs help BADLY! /

Good job providing options and asking for feedback! Thanks!

If we are taking the time and spending the money to make changes, we need to be forward thinking enough to be very encouraging for bicycle traffic. make it look nice and new. then work to bring businesses back to the area!

The proposed improvements could be done in stages, beginning with the restriping for a bike lane and increasing the width of the road at certain locations adjacent to new development or redevelopment. This will allow the public (cars, bikes, and pedestrians) to get accustomed to the changes incrementally.

Wheel Chair Accessibility is a MUST

Require those businesses that have huge parking lots to set aside some land near the street and tear up their asphalt and plant trees. Also, reward them for this and for adding more trees to their parking lots. It would be really nice to be able to find a shade tree to park under while shopping or using businesses on North Ave. Reduce their property taxes commensurate with their improving the appearance and comfort of their property.

<p>Something need to be done with North Avenue ! Create a reduced tax zone to encourage businesses to relocate on North. If the city doesnt consider this soon North will be "ghetto "</p>
<p>i really hope the city is serious about fixing up north avenue, espeically considering all the stores that have closed down recently.</p>
<p>nothing</p>
<p>Work with the various economic development interests to redevelop the Teller Arms shopping center, K-Mart and the old City Market. These are blighted and a waste of real estate, and will continue to drag down the entire length of North Avenue until improved. / / Continue installing bus pullouts. If you can do nothing else (bike lanes, sidewalks) at least do this.</p>
<p>The 1st question about traffic flow may be misleading. My first reaction was that it pertains to motorized traffic, then I thought that bikes and pedestrians are traffic and although traffic flow and convenience are important for all, too much emphasis in the past on motorized traffic to the detriment of other traffic makes me want to answer the question as 'not at all important' even though I know darn well that it IS important. So, yes it is important, but I'm OK with less flow and convenience for motorized traffic IF it gives bicycle and pedestrian traffic more fair consideration regarding their flow and convenience.</p>
<p>I moved to Grand Junction a little over 2 years ago, and I love it here. I live in downtown and I bike to work. Main street is amazing. Most of downtown is a wonderful place to ride, walk, and live. But that said, there are certain parts of town that are an embarrassment. North Avenue is one of those places. This is a very major and very imporant street- you can't get very many places in town without travelling on it- and its current condition for bicyclists and pedestrians is shameful. There is no excuse for the dirt paths worn on the side of this major street where people walk because there are no sidewalks. I'm so glad to see that efforts are being made to correct this situation, because the fact that it was ever allowed to get this way represents an absolute failure of government (and a failure of the People to hold thier representatives accountable). We all suffer when investments in bicycle and pedestrian infrastructure are neglected, but none suffer worse than those who can least bear it- the poor, disabled, and underprivleged Shame on all of us, and lets hope we can correct the situation out there sooner rather than later.</p>
<p>I think their should be an arched pedestrian crossing going over the roadway at 12th St. and North Ave. for the safety of all attending events at the college or at Stoker Stadium. This would be a nice idea at 7th or 5th Street for the benefit of GJHS students and the businesses in the area. Improved and aesthetically appealing bus stops with water fountains and trash cans can also provide a shaded place to sit and rest for pedestrians and the elderly who are walking in the area.</p>
<p>Looking forward to the next 25 years!</p>
<p>Roads that are hostile to bicycles ultimately discourage people from commuting via bicycle. The Riverside Parkway, though accommodating to cyclists, is not an ideal choice for commuting because it is on the outskirts of town and often takes up to 20 minutes longer to reach your destination via this route (and many people don't like going over the bridges, though I don't mind at all). Patterson is a slightly better choice, but has extremely heavy traffic and infrequent/inadequate bike lanes. / / North Avenue with complete bike lanes running the entire length just makes sense. It is conveniently located. And if you plan to build sidewalks, you should build bike lanes as well (to keep cyclists off of the sidewalks where they are a danger to pedestrians).</p>
<p>more than anything else, there must be sidewalks all along North Avenue. The current situation is terrible and and dangerous especially in the winter. Finally, the existing situatioon is especially dangerous to those who must use wheelchairs, for example, to ge to Walmart.</p>
<p>Thank you for making this survey. North Avenue is a very different from Main Street, I think it is important to understand the community's priorities with this specific project.</p>

Landscaping - trees, shrubs, flowers
North Avenue is mostly business related and pedestrian traffic isn't as much of a concern. F Rd is a better place to concentrate "pleasant" areas.
If we keep a median in the middle, please dress it up with desert plants.
I want to point out that in the above question regarding traffic flow, convenience and safety, these are very important, but understand that my answer comes from a walking and biking perspective. Terrible traffic flow, almost a total lack of convenience and dismal safety are what greet bicyclist and pedestrians in this area, which is why I always feel compelled to drive here instead of walk or bike. As bad as traffic flow, convenience and safety may be for motorized traffic, it is nothing compared to what bikes and pedestrians deal with along this corridor. So yes, these issues are important, but recognize that it is important for all modes of travel, not just cars. How many people will answer this question from a motorists perspective? Can you at least pretend that everyone who answered this question like I did was thinking what I'm thinking? Ha! Thank you for the opportunity to provide comments.
It would be awesome to have some hip urban living options built on North. It could become a hot and happening area with multi-use real estate.
By adding bike lanes, sidewalks, trees, flowers, etc. it will make our city more safe and will also make it more inviting and comfortable for everyone including tourists and people that come here for special events such as JUCO Country Jam etc. and it could help attract others to bring there events to our area and help to boost our economy.
North Avenue is Grand Junction's Colfax - North Ave. definitely takes away from what Grand Junction is all about. Make it more inviting and attractive so businesses and people will want to go.
currently North Ave is very Sixties look, and needs an upgrade.
No
creating pedestrian crossovers or ticketing pedestrians that J walk would alleviate some of the rear end accidents. the pedestrian crossings on 12th are dangerous because of low visibility and instant activated not allowing traffic enough time to slow down or stop safely. watch peds push the button and not even look but just start walking because they immediatly have right of way, really a dangerous practice but if i push the button and immediatly start walking and get hit, the driver is ticketed. /
It is very sad that the City allowed North Ave. to collapse while spending to much money and time on the Mall and Main Street. It has been allowed to become a slum (except for the area by the University) in parts with no incentive to large stores to remain, while forcing major traffic problems by the newer Walmart and the Mall. The City has forgotten that they represent all of the people not just the ones who live in the special areas. I am a third generation Grand Junction resident and am very sad to see how poorly the North Ave. businesses have been treated. Thank you
No bike lanes. No parking. Definitely no parking.
I really believe that looks are everything for a place like this. North ave is a large street in GJ, and tall weeds and untrimmed lawns and poorly painted building and street lanes are unattractive for business and guests to the city, such as JUCO.
North Ave looks like a low class area, you need to work with the business to give not only the street a face lift, but also the buildings. North Avenue should be the showcase strip of Grand Junction, when possible students come to view the campus they go down North and it looks like a dump. It should be something that the Community can take pride in.
Cleaning up the medians by adding flowers and trees, planting more trees along the entire length of North Ave. Adding safe and clean areas for bus stops. Around the college area, adding stop lights that count down on the light itself for students and drivers, or adding a 3 second delay before any light turns green to prevent accidents around the college.

we dont have funds and we dont need to barrow anymore money ,get grant or find any other way to get money that we dont have. When the public says no it means no you didnt listen on the police department you still found away to spend money that we said not to spend. Also making us chose 2 to complete this survey is wrong I dont want any of it and should have to pick any.
It is disappointing to see that just this small section will require such a long term planning approach. It seems the focus really should be on the eastern end as that is the area that will take the most impact. The College will keep the current area for planning heavily attended and the eastern section is in more dire need of attention and attraction for new business.
It would be wonderful if we could turn a sort of dumpy looking area in Grand Junction into something nice. It improve morale and make you feel good about the place you live. Thank you
Please, please, please do NOT put roundabouts on North Avenue!!!! It seems that most people approach the roundabout in town like a stop sign and stop and wait for traffic to clear before they move into it. It's okay to move into the roundabout when there are other cars in it! While I agree that the sidewalks are too narrow in many places, there are no businesses to walk to so widen the sidewalks on Orchard Ave between 15th street and 28 road first-kids walk that street to and from school and they can't walk 2 abreast because of the 3 foot sidewalks. On street parking on North is a bad idea-look how the back in only parking has worked on 7th--it didn't and now we have to pay to have it fixed. We need to encourage businesses to move back to North Ave-we have no craft stores now, no grocery, no restaraunts and no gas stations in the middle of North Ave now. I live very close and it's not fun to drive so far simply for gas or groceries.
Keep it clean. Add landscaping to increase the aesthetic appeal. The shopping center where Big Lots is located would be an ideal area to have an outdoor shopping space, with cafe's, seating, trees for shade, some sort of water feature, such as a fountain, perhaps a small ampitheater for music. That parking lot is horribly underutlized, and could be something very special.
The project is worse than a just a waste. North avenue can only become alive again if it is allowed freedom to prosper. / Rather than sink money into this project, make North avenue a tax free zone. The construction is misguided and can only / interfere with the few businesses that remain. The street is already wide enough and traffic has deceased a great deal since that the projects inception. The entire idea is flawed and whatever can be done to minimize the impact of this needless and / destructive activity should be done. WE DO NOT HAVE CARS ON NORTH AVENUE! THEY ARE ALL OUT AT THE MALL!!
All of this is moot if we lose all the shops on North Avenue- that issue is FAR more important than any upgrades to be done to the road. And is it really going to take 25 years to get all this done?
Definitely clean up old and obnoxious business signs along ALL of North Avenue. Looks junky. Thanks!
Do a YouTube search of Junk town and you will see this is Grand Junctions nickname. It has nothing to do with appearance of the city. / I can't believe the naiveté of the people in this city when they hear something they never knew about the first time. / Even if the city milks all the homeowners and makes this the most pristine place on earth, it will still be called "JUNKTOWN". / Actually, this town needs some really good east coast food restaurants all we have are chain restaurants and the mom and pop ones that are mediocre at best. Something like Katz's Deli, Or Geno's Steaks. The product will bring the people to your establishment if it is outstanding. Look at these restaurants on the web and you will see they are nothing special to look at, but the quality and quantity of their product is well above the mediocre. / These restaurants are run by people who believe the "customer is king" and show it in their product. / I haven't seen one business in this town that does that. /
North Ave improvements are much need to upgrade old conditions. It's important that North Ave remains a vital business center for growth.

North Ave. is certainly an important district in Grand Junction and does need attention. It is becoming outdated and faces potential trouble if it's not cared for and made into a more accessible, beautiful part of this city.
drivers need to be more careful of pedestrians and cyclists. they need to SLOW down. these changes might make them more aware of alternate forms of transportation.
As someone that uses this street on a daily basis the biggest frustration is the lack of pullouts for right hand turns and pullouts for the mass transit service. If this street had the pullouts needed to improve traffic flow it would be a much nicer for morning and evening commuters. I have in the past actually decided not to shop on North Ave., on my way home in the past, because traffic was too heavy and I didn't want to slow traffic even more by trying to make a right turn with no pullout.
N/A
North Avenue in many areas has a very run down look. (old signage, buildings that need exterior attention, etc) / / I am not sure if you can have a program with some kind of incentive (like tax breaks or grants or other assistance) where all the merchants would be required to give their business a face lift, by a certain time period. It would probably involve have a special committee to approve the new designs and improvement before they would be implemented. I would like to see codes regarding signage and business aesthetics that would increase the overall appearance of north avenue. The whole area is beginning to look like Colefax in Denver!
Can't wait to see the revision!!
Please note that when you plan for a higher density commercial center, detached sidewalks may not be appropriate, as they disrupt curb to sidewalk flow. (people tend to cut across the grass/landscape). Additionally, it can also hamper the visibility of the building or façade. Should you wish to create a "greener" atmosphere utilize a required % green space, garden, ect. after the sidewalk and before the building.
The city needs to be concentrating on making North avenue a clean, inviting street that businessesss will want to invest in. This includes re-vamping the area of North from 15th to 29th. That area is becoming a ghost town. Clean up the old hotels, make a nice park, give incentives to businesses that move to North ave, or that work to clean up the open/vacant properties on that stretch.
Thank you for requesting public comments.
While one of the more expensive options might create more overall convenience and aesthetic appeal, one of the less expensive options should be considered above all. In this time when jobs and budgets are being cut, a sense of place is important, but so is working within the means available.
I hope there is a plan to attract some businesses to North Ave because the alarming trend is businesses shutting down and moving toward the mall area. North Ave is becoming a ghost town.
The city needs to do whatever it takes to make this side of town more pleasant. I have noticed that a lot of the businesses on North ave. after 12th have been leaving giving the look of a ghetto. Some areas along North ave. remind me of areas from downtown Los Angeles. Please make our side of town more attractive to new businesses and tourists as well. Thats how I want to see my tax dollars at work.
Hope you can find some funding! Thanks to Dave Thornton and the rest of the City and RTPO staff for all of their efforts. /
North Avenue is the hub and any improvements will be greatly appreciated!

The priority should be the appearance of North Avenue. Right now it's an embarrassment to the city. When you drive down past the college it feels like you are driving into the ghetto in downtown denver. Grand Junction should be a proud city. Look at Montrose, the aesthetics of their city if beautiful and they don't receive near the amount of visitors as Grand Junction. They also have more desirable companies setting up shop their. The look and feel of the town could have a lot to do with that.	
project a corridor with good lighting and greenbelts	
Please leave some routes for motor vehicles!!!	
I suggest making a multiple use right of way on one side of North Ave - similar to the Riverfront trail. I doubt if the bike-perdestrian traffic will excede that on the Riverfront trail. I know that it's not "ideal" but with the limited space on North Ave and funding problem why design a plan that is not practical.	
Sign controls are needed to phase out the current hodge podg of signs and require monument unified signs. Way finding signs are also needed.	
This web page doesn't allow me to put my way I want to answer. It seems like it is programmed to only accept a / certainm response. The survey is unfair because the page doesn't work right.	
Most people that I know, only use North Ave as a traffic corridor. It will never be what it used to be as far as a shopping mecca, so I think the planners need to focus on other areas. Leave the memories behind. I personally do not drive past 12th street. I stay on the west side of town to avoid the druggies and addicts and the homeless that abound on North. If I have to drive past 12th, I use Patterson. I also do not feel safe on that side of town and I have the safety of my 4 year old to think of. The "element" that live on the east side, are not people that I associate with, so until you get them off the street, I will stay near the mall and out in Fruita.	
Some architectural and signage control would be good. A recent drive the length of North Avenue left me with a distaste for the area, not only because of the loss of businesses but more because of the mix of structures & signs. It's as if there is/was no sense of pride. Give the area some aesthetic appeal that all valley residents can be proud of and visitors won't refer to it as Grand Junkyard. Interesting that given the fact that of the choices above only (1) does not include bike lanes or parallel parking. It's a no brainer you've already put plans in place & public opinion doesn't really mean much so why this survey?	
The parking on 7th street should teach that if people cannot find it convenient it will not be used. Too dangerous to park on North, too dangerous to have North Avenue as a bike way to get to work unless banning of cars and trucks are banned from driving on North between 29 Road and 1st street	
North avenue is a major artery for traffic in Grand Junction and proper expansion and planning in regards to bike/scooter lanes, bus stops off of traffic, appropriate policing.	
It's a dying corridor of businesses that need all the help they can get and encourage new and existing businesses.	
People who want to walk would probably prefer to do so in less congested areas where they don't have to breathe in exhaust fumes. North Avenue is still a major east-west route through town, and people look to get through town quickly. If people want a shopping park, etc., let's put our efforts into further developing Main St. downtown.	

Total responses

195

North Avenue West Corridor Plan Questionnaire Results

Updated on: 3/8/2011

	Totals
1) What brings you to this part of North Avenue?	
a. Live within a couple of blocks of the corridor	10
b. Work here	6
c. Own a business here	9
d. Own property here	8
e. Obtain services here	17
f. Pass through here to get to other places in town	23
g. Other: 1)Go to school. 2)Go to church. 3)grew up three blocks from area. 4)Try to ride my bike across North Avenue. 5)High school.	9

Comments 1)Church I attend is at 7th and Kennedy.

2) How do you access the corridor area?	
a. Walk	7
b. Bike	10
c. Bus	1
d. Private vehicle	43

Comments 1)Never walk or bike on North Avenue due to safety concerns. 2)Very difficult to walk on 12th. I ride my bicycle to run errands in the summer. 3)I wish I could ride my bike on North Avenue. 4)Light rail would be cool. 5)Cyclists & pedestrians are in harms way.

3) The Grand Junction Comprehensive Plan has identified this area as a mixed use center. What land used are lacking that you would use?	
a. Retail	16
b. Housing	10
c. Professional Services	11
d. Restaurants	27
e. Other: 1)Entertainment. 2)Hardware store. 3)Hotel 4)Services 5)Bicycle & pedestrian access 6)Movie	9
f. None	7

Comments 1)Combined retail and housing. 2)Please no bars; too close to residential areas. 3)We're not lacking in quantity of restaurants, but access is difficult and parking is almost non-existent (see Arby's and Taco Bell).

4) Are there currently any safety issues you would like to see addressed?

Comments 1)Pedestrian and vehicle traffic, especially to/from GJHS and Mesa State. 2)Bikers must use sidewalks with walkers. 3)Sidewalks right on road are uncomfortable. 4)College traffic at 1st and North, right turn on 1st. 5)Bike access on wider sidewalks. 6)Bike routes on side streets that parallel North Avenue. 7)Traffic 8)Wider sidewalks, bike paths, ease for pedestrians. 9)Foot traffic crossing North near Mesa State. 10)Line of sight when entering traffic. 11)12th and North pedestrian traffic - tubes? 12)Property vandalism 13)Sidewalks and cleanliness. 14)People drive too fast on North Avenue even though the speed limit is 30 mph. 15)North Avenue turning to one lane at west end past 1st Street. 16)Lighting and sidewalks. 17)Pedestrian and bicycle safety, especially in high school and college area. 18)Kids walking to and from school, especially at lunchtime. They need nice wide crosswalks and drivers need clear pedestrian notifications. 19)Traffic review at North & 1st. Traffic backs up in the right lane for traffic going straight and turning right. Motorists speed up in the left lane to pass motorists in the right lane. Sometimes it works if several cars are turning right. It's a bad area for cyclists and pedestrians. 20)Bike paths. 21)Pedestrian safety for GJHS and MSC. 22)Sidewalks too close to fast moving traffic. 23)Need better pedestrian crossings at 7th and North and 12th and North. 24)Good turn pullouts (left turns). 25)Pedestrian traffic looks like a problem. 26)College crossings Under 12th and North? 27)Pedestrian and bike traffic for GJHS students during lunch-better sidewalks. 28)Pedestrian walkways for MSC students. 29)Pedestrian and bike crossing at 12th and North. 30)Flashing/ blinking billboards and signs are too bright at night and distracting during the day. 31)12th Street/ Mesa State pedestrians. 30)Bicycle & pedestrian - we need "grade-separated thoroughfares" for bikes. 31)Forward parking. 32)Wider sidewalks. 33)Walking and biking for high school students. 34)Skinny sidewalks right up against the street. 35)The small sidewalks. 36)The sidewalks are too narrow. 37)Pedestrian risks. 38)Sidewalks just end or are not present in some locations. 39)Size and location of sidewalks. Also in regards to right turn lanes. 40)Crossing North Avenue.

5) What is your biggest concern regarding this section of North Avenue?

- | | | |
|----|------------------------|----|
| a. | Aesthetics | 25 |
| b. | Safety | 32 |
| c. | Impact to Neighborhood | 13 |
| d. | Identity | 12 |

Comments 1)Pedestrian and bike crosswalk at 12th and North, event pedestrian access. 2)All very important. 3)Plant trees by bus stops. 4)Some parking in front of buildings conflict with sidewalks. 5)Long term economic viability. 6)Pedestrian and motorized traffic movement during major public events (JUCO, 4th of July, sports, etc.) 7)Closure of businesses. 8)It's hard to shop on North Avenue, too many bloody cars! It's dying as a shopping area because it's an unpleasant place to be. 9)The better looking, the more people. 10)Looking good brings in people.

6) What would you like to see improved or changed?

Comments 1)Concerned about nuisance bars and vehicle and pedestrian traffic generated by GJHS and Mesa State. 2)Sidewalks need to be off road a bit. 3)Some sites dated on road, such as tattoo parlors bring down neighborhoods. 4)Lighting, area needs to be more inviting. 5)Median need work. 6)Smaller signs 7)Make it a destination. 8)More pedestrian friendly. 9)Mixed use property all along North Avenue, such as what has been done at Mesa State. 10)Improved bus stops, more green space (parks) entrances. 11)Aesthetics, safety and maintain neighborhoods/housing. 12)Bus pull outs and redo sidewalks. 13)Quality of bike/walk sidewalks/lanes. 14)Encourage tree canopy for shade and aesthetics. 15)Incentives/tax incentives to owners and developers to remodel, upgrade and redevelop. 16)Incentivize green building and energy-efficiency. 17)More flexibility and support for development from the City. 18)We need better access and parking to help make North Avenue businesses successful. 19)Avoid u-turns, wider lanes and install wider crosswalks near the schools and stadium. 20) Revitalize area; look and feels is old like the 1960's. 21) Store front parking is hazardous 2nd to 5th Streets. Harbert Lumber's is the best. 22) City to help owners to renew store fronts. 23)Pedestrian and bike safety. 24)Make us of side streets for parking access. 25)Improve signage so intersections are more prominent to facilitate #24. 26)Uniform signs. 27)More landscaping. 28)College crossings Under 12th and North? 28)Roadway/center island. 29)Planning for this segment of North Avenue seems moot without a firm agreement with Mesa State College as to where and how they will grow in the future. No agreement? No City tax dollar support. 30)Safety for bikes and pedestrians without impeding traffic. No roundabouts. 31)Remove billboards and electronic signs. 32)Redevelopment to stave off the westward business movement to 6 & 50 Corridor. 33)Walking bridge for students across 12th Street. Too many pedestrians just hit the button and start walking without even looking. 34)Traffic congestion. 35)I know this would be very expensive, but bury or elevate the road in some areas, increase the width at the right-of-way, add bike & pedestrian sidewalks/bike paths-not right next to the road. People will not use sidewalks or bike paths that are too close to North Avenue, due to the speed and volume and type of vehicles. 36)More bike friendly.

Other Comments: 1)Would like to see an area near the college similar to Olde Towne in Fort Collins. 2)Bicycle friendly. 3)Stop light or round-a-bout @ 3rd. 4)Thanks for asking for our input! 5)North Avenue in this section is a hodgepodge of buildings, signs and uses. It's not appealing and I avoid it. 6)Future development, south side of alley between 3rd and 5th Streets, rezoned for commercial redevelopment. 7)Thanks. 8)Areas around MSC, GJHS and Lincoln Park need to have pedestrian and bike access that does not impede traffic and provides safety to them (under and over passes). 9)Make Glenwood/Tiger from 7th to 5th one way west. 10)During GJHS lunch hour, program lights for four-way walk (all vehicles stops) similar to 16th Street in Denver. 11)Cut new right turn only lanes at 7th and North to speed up traffic. 12)Build diagonally an overhead crosswalk at 12th and North for student and Lincoln Park foot traffic. 13)Get the fast food restaurants (McDonalds, KFC, etc.) to help pay for it; let them have a 4' x 8' sign below the top of the bridge. 14)I like what you're doing. The concerns already listed cover my feelings. Grand Junction can make North Avenue a much more appealing area and has my support. I don't have any reservations about doing what is needed. 15)I would like to see the junction of North and the I70 Business Loop landscaped. 16)Traffic and pedestrians will not go away, they will get worse. "People and places" will beget cars and traffic. The problems exist now and require mitigation now.

By far the largest stakeholder affecting the success of any improvement strategy is Mesa State, which can tell the City to go to hell if it wants to in terms of planning and zoning. This is unacceptable. Without some type of consensus and a binding agreement between the City and the college as to where and how they can grow, this entire process seems an exercise in theoretical community planning. Might as well stay at home and play Sim City.

17)I would like to see some redevelopment of properties that could benefit from working together to improve parking, landscaping and pedestrian access. 18)I would love to see pedestrian overpasses or underpasses and 7th and North and 12th and North. 19)Bus pullouts. 20)Trees and xeriscaping. 21)No roundabouts. 22)New hotel needed. 23)I think that all of the street stuff is unnecessary. We need to focus on our schools. We are laying off teachers. We are crowding our classrooms and we need to give this money that is allocated for streets to our schools and help our children. 24)Need public transportation for the North Avenue corridor as well as other corridors, that are tourist friendly, perhaps seasonal or year round. The City needs an evening bus to take people to the various activities at Mesa State College, downtown, Lincoln Park, etc. We need more public accessibility. 25)Need to be careful about gentrification of the area. 26)North Avenue is not a pedestrian friendly environment. It is not a bike friendly environment. 27)Panhandling should not be allowed. 28)Leash laws for animals are not enforced and need to be. Animals are allowed to roam free at area parks, etc. 29)Need to make Mesa State College a University. 30)As a business owner attempting to reopen a two year closed business, could you waive our fees; give us a waiver of some kind to raise incentives? 30)Businesses supporting student population needed. 31)Improve safety for students, pedestrians and vehicles. 32)Reduce North Avenue from four lanes to two lanes, at least from 5th Street to 12th Street. 33)User friendly services. 34)The aesthetics of the land. 35)It needs to look a lot nicer. 36)I think there needs to be more eating establishments. 37)More modern and good looking. I feel it needs to be more pleasing to residents as well as tourists. 38)User-friendly look, facilities and transportation routes. 39)6 & 50/North Avenue is an eyesore.

Subareas

This section presents Subarea Plans for two specific areas of the City. The Subarea Plans guide site-specific opportunities for each of these two areas and are intended to support the broader vision of the Comprehensive Plan. The Subarea Plans address land use, development character, access, mobility, functional improvements, as well as a catalyst site for the type of development desirable along the corridor. The Subarea Plans should inform development decisions within these key areas of Grand Junction and guide the vision for these areas.

24 Road Corridor Subarea

The 24 Road Corridor Subarea is a corridor plan for an important gateway road in the Northwest area of the City. Stretching approximately 1.5 miles from Interstate 70 to State Highway 6 and 50.

Lower Downtown Subarea

The Lower Downtown Subarea builds on existing assets, to increase the appeal of this important part of the City center while maintaining and improving connections between Main Street and the riverfront. Loosely bounded by Pitkin Avenue to the north, 5th Street to the west, and Riverside Parkway to the south and east, the area is comprised of distinct character areas defined by their form, uses, conditions, and characteristics.

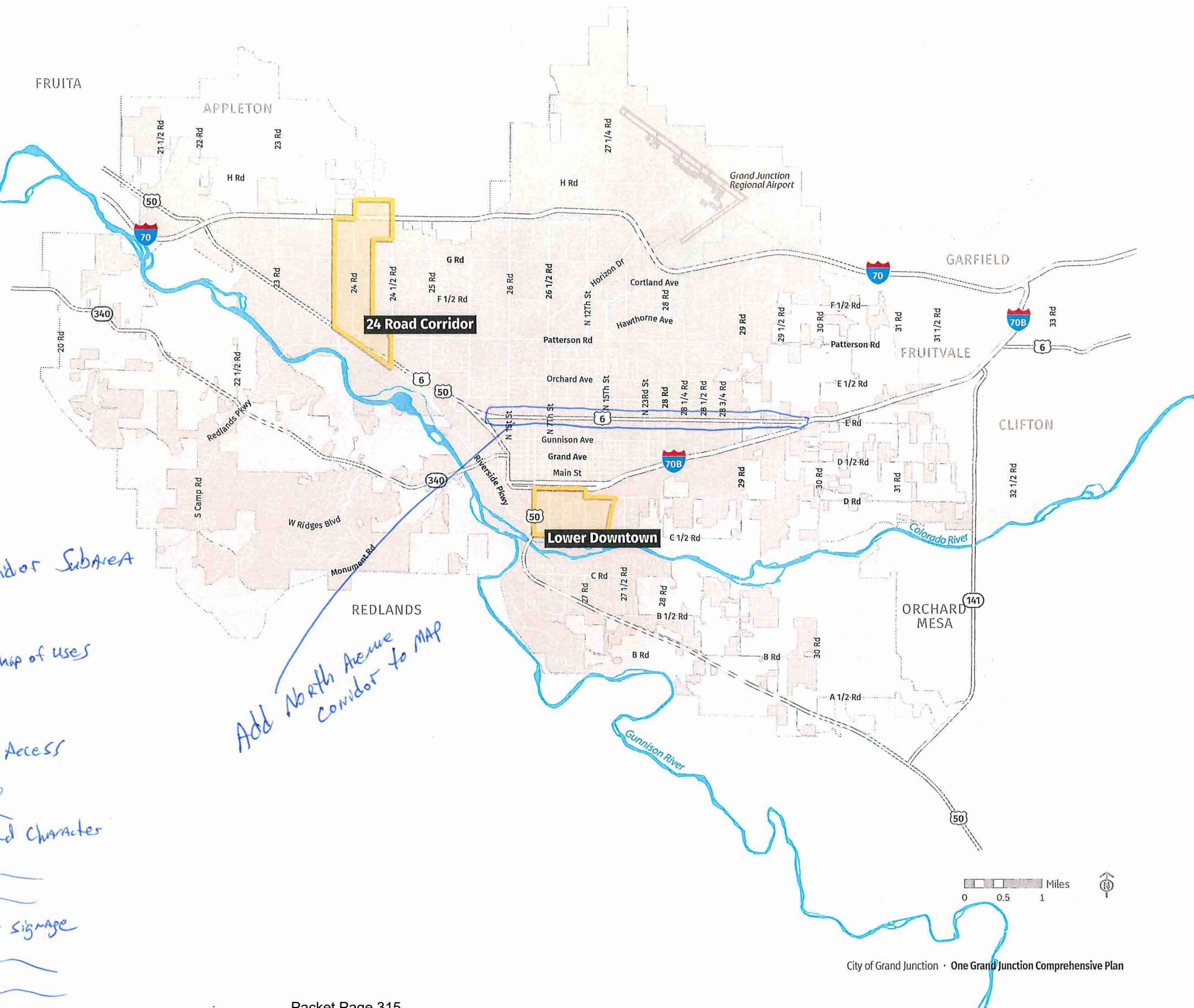
NORTH AVENUE Corridor Subarea

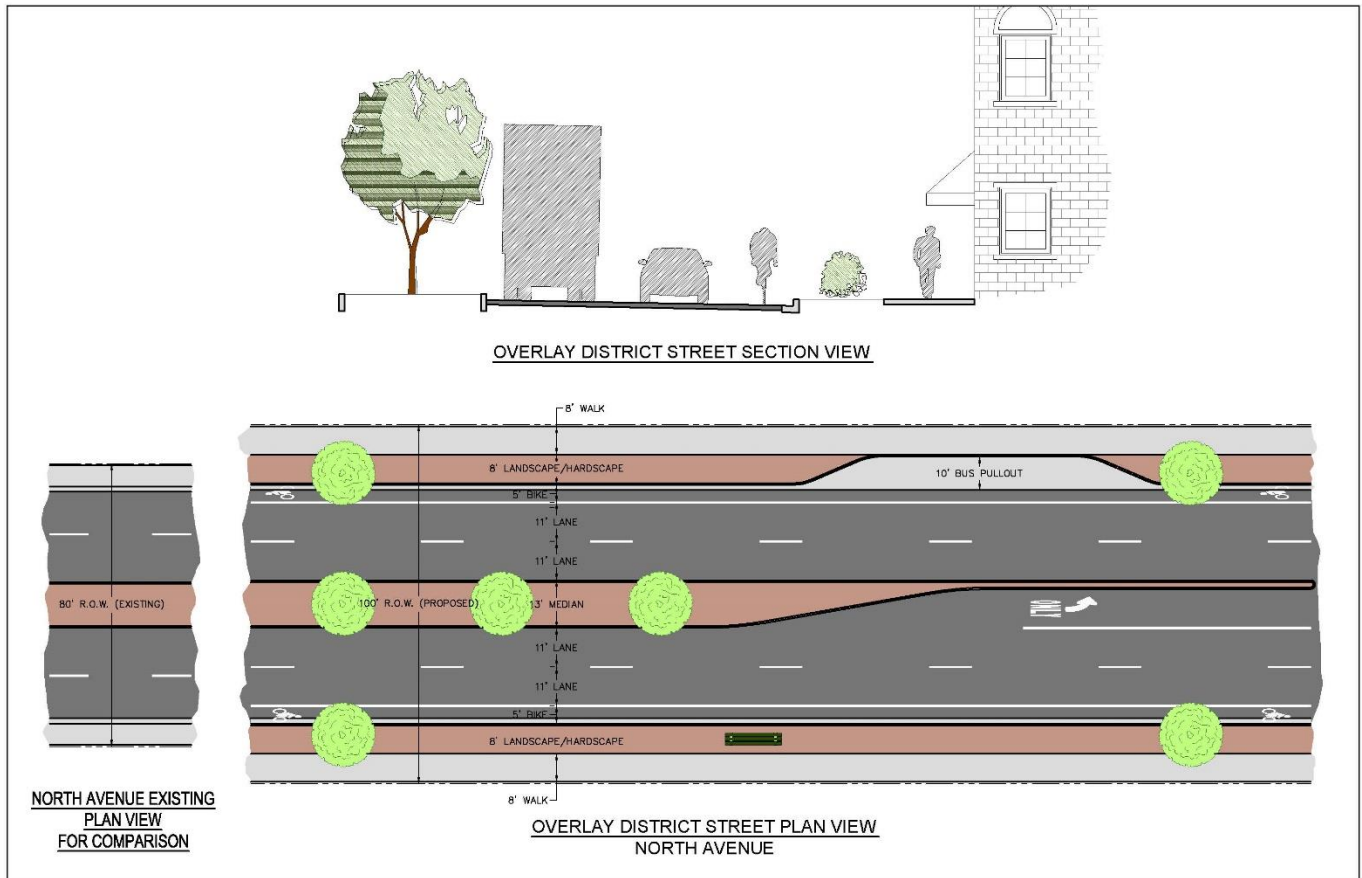
→ MvX and Relationship of uses

→ Circulation and Access

→ Building Form and Character

→ Streetscape and signage





North Avenue ROW 100'											
Type	# of Travel Lanes	Travel Lanes	Median/ Turn Lane	Bike Lane (on Street)	Bike Lane (Off Street)	Bike Buffer	Curb and Gutter	Sidewalk Buffer	Sidewalk/ Trail	Multi-Purpose Easement	Frontage
Minor Arterial with Trail	4	11	13	5				8	8	Not Required for north Avenue	