



**PLANNING COMMISSION AGENDA  
CITY HALL AUDITORIUM, 250 NORTH 5TH STREET**

**TUESDAY, July 12, 2016 @ 6:00 PM**

**Call to Order – 6:00 P.M.**

**\*\*\*CONSENT CALENDAR\*\*\***

**1. Minutes of Previous Meetings**

*No Minutes Available*

**2. Amending the ODP and PD Ordinance for Highlands Apartments [Attach 1](#)** [File#PLD-2016-326]

Request approval to amend the Outline Development Plan and Ordinance No. 4652.

*Action: Recommendation to City Council*

Applicant: Grand Junction Housing Authority - Owner  
Location: 805 and 825 Bookcliff Avenue  
Staff Presentation: Lori Bowers, Sr. Planner

**\*\*\*INDIVIDUAL CONSIDERATION\*\*\***

**3. Amending Title 25 – 24 Road Corridor Design Standards [Attach 2](#)** [File#ZCA-2016-111]

Request to amend the Grand Junction Municipal Code, Title 25, 24 Road Corridor Design Standards.

*Action: Recommendation to City Council*

Applicant: City of Grand Junction  
Location: 24 Road Corridor Sub-area  
Staff Presentation: David Thornton, Principal Planner

**4. Redlands Hollow Rezone**

[File#RZN-2016-253]

[Attach 3](#)

Request approval to Rezone 2.88 acres from an R-2 (Residential – 2 du/ac) to an R-4 (Residential – 4 du/ac) zone district.

*Action: Recommendation to City Council*

Applicant: Barbara Krause – Owner  
Location: 508 22 ¼ Road  
Staff Presentation: Scott Peterson, Sr. Planner

**5. Other Business**

**6. Adjournment**



Date: June 24, 2016

Author: Lori V. Bowers

Title/ Phone Ext: Sr. Planner /  
256-4033

Proposed Schedule: PC-July 12, 2016

CC- 1<sup>st</sup> reading August 3, 2016

2nd Reading: August 17, 2016

File #: PLD-2016-326

**Attach 1**

**PLANNING COMMISSION AGENDA ITEM**

<b>Subject:</b> Amending the ODP and PD Ordinance for Highlands Apartments
<b>Action Requested/Recommendation:</b> Forward a recommendation of approval to City Council to Amend the Outline Development Plan and Ordinance No. 4652.
<b>Presenter(s) Name &amp; Title:</b> Lori V. Bowers, Senior Planner

**Executive Summary:**

The Grand Junction Housing Authority is requesting to amend Ordinance No. 4652 (see attached), an Ordinance rezoning approximately 3.76 acres from R-16 to PD (Planned Development) with a default zone of R-24 for the Highlands Apartments. The request is to add four additional dwelling units, which exceeds the maximum density range by .63 dwelling units.

**Background, Analysis and Options:**

The Grand Junction Housing Authority was granted approval of the Highlands Apartment project on January 7, 2015. The Ordinance allows for the construction of 128 senior multi-family dwelling units, in two phases, along with some ancillary uses at 805 and 825 Bookcliff Avenue. A zoning density range of 24 to 32 dwelling units per acre is provided in Ordinance 4652. The request is to amend the Outline Development Plan from 128 units to 136 units. The addition of four more residential units is internal to the structure and does not expand or change the approved foot-print, or the elevations of the building. It does however exceed the maximum density of 32 units per acre by 0.63. The Zoning and Development Code allows changes in the bulk standards up to 10 percent so long as the character of the site is maintained. The character of the site is maintained since the new additional units would be internal to the structure.

**How this item relates to the Comprehensive Plan Goals and Policies:**

**Goal 5:** To provide a broader mix of housing types in the community to meet the needs of a variety of incomes, family types and life stages.

The proposed project will provide affordable senior apartment living in an area where needed services are readily available.

**How this item relates to the Economic Development Plan:**

**Goal:** Continue to make strategic investments in public amenities that support Grand Junction becoming “the most livable community west of the Rockies by 2025.”

The proposed project is a quality development and will provide visual appeal through attractive public spaces throughout the Planned Development. It will also provide a needed housing type.

**Board or Committee Recommendation:**

The Planning Commission will make a recommendation to the City Council.

**Financial Impact/Budget:**

No financial impact can be identified at this time.

**Legal issues:**

The City Attorney has reviewed and approved the form of the proposed ordinance.

**Other issues:**

No other issues have been identified.

**Previously presented or discussed:**

This request has not been previously presented or discussed.

**Attachments:**

1. Staff Report
2. Site Location Map
3. Ordinance 4652
4. Ordinance

BACKGROUND INFORMATION			
Location:		805 and 825 Bookcliff Avenue	
Applicants:		Grand Junction Housing Authority, owner and developer. Rich Krohn, representative.	
Existing Land Use:		Vacant land	
Proposed Land Use:		Residential	
Surrounding Land Use:	North	St Mary's Hospital property and Colorado West Senior Citizens housing	
	South	Tope Elementary School	
	East	Apartment building and single-family residences	
	West	Business offices	
Existing Zoning:		PD (Planned Development)	
Proposed Zoning:		PD (Planned Development)	
Surrounding Zoning:	North	PD (Planned Development) & R-16 (Residential – 16 du/ac)	
	South	R-8 (Residential – 8 units per acre)	
	East	R-16 (Residential – 16 units per acre)	
	West	B-1 (Neighborhood Business)	
Future Land Use Designation:		Business Park Mixed Use	
Zoning within density range?		X	Yes
			No

Background

The proposed project is located on the south side of Bookcliff Avenue between 7th Street and 9th Street across from the south terminus of Little Bookcliff Avenue. The Grand Junction Housing Authority purchased the subject property in August 2013. The parcel was annexed into the City in 1964 as the McCary Tract Annexation. Air photos, dating back as far as 1937 show the property as vacant.

The property consists of 3.785 acres. Per Section 21.03.040(i)(1)(i) for the purpose of calculating density on any parcel, one-half of the land area of all adjoining rights-of-way may be included in the gross lot area. The half street right-of-way at the north boundary of the subject property is 30 feet by 550 feet (16,500 square feet) or .379 acres, making the total acreage for density calculation 4.168 acres. The applicants were specific in their proposal to develop the property into 128 units of multi-family senior residential units in two phases. In addition, areas for indoor amenities such as an office for a resident manager, office areas for service providers such as home health care, a visiting office for the Veterans Administration, together with fitness, wellness, and socializing areas will be constructed. While the Recitals of the Ordinance is specific to 128 units, a density range of 24 to 32 units per acre was provided in the Ordinance. The applicants are requesting to increase the maximum allowed density by one to provide an additional four units in Phase 2. The total number of units for the project will be 136, exceeding the maximum number of dwelling units

by just over a half a unit (0.63). The zoning density range will have a maximum of 33 dwelling units per acre, which is well under the 10 percent deviation allowed by the Code.

The additional four units are internal to the structure and does not expand or change the approved foot-print of the building or the exterior elevations. This is accomplished by deleting some of the smaller amenities that were planned for Phase 2 of the project. Onsite parking will remain unchanged at 154 spaces. This corresponds to 1.13 spaces per unit. There are also 27 on street parking spaces available on Bookcliff Avenue. Based on the Housing Authorities extensive experience with similar developments it has constructed in the past, this will provide sufficient parking for this development

Consistency with the Comprehensive Plan

The proposed ODP is consistent with the Comprehensive Plan Goal 5: To provide a broader mix of housing types in the community to meet the needs of a variety of incomes, family types and life stages.

The Future Land Use Map of the Comprehensive Plan shows this area to develop with the designation of Business Park Mixed Use. Applicable zones that support this designation include R-8, R-12, R-16, R-24, R-O, B-1, CSR, BP and I-O. R-24 is the default zone for the Planned Development.

Review criteria of Section 21.02.150(e) Amendments to Approved Plans of the Grand Junction Municipal Code

The use, density, bulk performance and default standards contained in an approved PD rezoning ordinance may be amended only as follows, unless specified otherwise in the rezoning ordinance:

- (i) No use may be established that is not permitted in the PD without amending the rezoning ordinance through the rezoning process. Uses may be transferred between development pods/areas to be developed through an amendment to the ODP provided the overall density for the entire PD is not exceeded;

This is not a request for a change in use, only to allow four more dwelling units, which exceeds the allowed density range by a little over six tenths of a percent. It is just a fraction over the allowed density range of Ordinance 4652. The default zone of R-24 has no maximum density.

- (ii) The maximum and minimum density for the entire PD shall not be exceeded without amending the rezoning ordinance through the rezoning process;

The request is to increase the maximum density currently allowed for in the PD Ordinance by one.

- (iii) The bulk, performance and default standards may not be amended for the PD or a development pod/area to be developed without amending the PD rezoning ordinance through the rezoning process.

Density is a bulk standard. The density will exceed the current density allowed within the Ordinance by one. Because Ordinance No. 4652 specified 128 dwelling units the Ordinance must be amended to allow four additional units, and expand the maximum density range already provided within Ordinance No 4652.

#### FINDINGS OF FACT/CONCLUSIONS

After reviewing the Grand Junction Housing Authority application, PLD-2016-326 for an amendment to the Planned Development, Outline Development Plan Ordinance, Staff makes the following findings of fact and conclusions:

1. The requested amendment to the Planned Development, Outline Development Plan Ordinance is consistent with the Comprehensive Plan.
2. The review criteria in Section 21.02.150(e) of the Grand Junction Municipal Code have all been met.

#### STAFF RECOMMENDATION:

Staff recommends that the Planning Commission approve the requested Amendment to the approved plan, Planned Development, Outline Development Plan, PLD-2016-326 with the findings and conclusions listed above.

#### RECOMMENDED PLANNING COMMISSION MOTION:

Madam Chairman, on item PLD-2016-326, I move that the Planning Commission forward a recommendation of approval to the City Council for the requested amendment to Ordinance No. 4652, allowing four additional dwelling units for the Highlands Apartments.

# City of Grand Junction



Printed: 6/22/2016

1 inch = 471 feet





**CITY OF GRAND JUNCTION, COLORADO**

**ORDINANCE NO. 4652**

**AN ORDINANCE REZONING APPROXIMATELY 3.763 ACRES FROM R-16 TO PD (PLANNED DEVELOPMENT) AND APPROVING THE OUTLINE DEVELOPMENT PLAN (ODP)**

**GRAND JUNCTION HOUSING AUTHORITY SENIOR LIVING PLANNED DEVELOPMENT – HIGHLANDS APARTMENTS (AKA THE EPSTEIN PROPERTY) LOCATED AT 805 BOOKCLIFF AVENUE**

Recitals:

A request for a Rezone and Outline Development Plan approval has been submitted in accordance with the Grand Junction Municipal Code. The applicant has requested that approximately 3.763 acres, located at 805 Bookcliff Avenue, be rezoned from R-16 (Residential - 16 units per acre) to PD (Planned Development) with a default zoning of R-24 (Residential – 24 units per acre). The applicant proposes to develop the property into 128 units of multi-family residential units for seniors in two phases, with each phase consisting of 64 residential units, resulting in an ultimate proposed density of 30.9 units per acre. In addition, an area for indoor amenities such as offices for resident service provider visits (such as home health care and Veterans Administration) together with common fitness, wellness, and socializing areas is anticipated to be constructed as part of the first phase.

This PD zoning ordinance will establish the default zoning, including uses and deviations from the bulk standards.

In public hearings, the Planning Commission and City Council reviewed the request for the proposed Rezone and Outline Development Plan approval and determined that it satisfied the criteria as set forth and established in Section 21.02.140 of the Grand Junction Municipal Code and the proposed Rezone and Outline Development Plan is consistent with the purpose and intent of the Comprehensive Plan.

NOW, THEREFORE, BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF GRAND JUNCTION THAT THE AREA DESCRIBED BELOW IS REZONED FROM R-16 TO PD WITH THE FOLLOWING DEFAULT ZONE AND DEVIATIONS FROM THE DEFAULT ZONING:

Property to be Rezoned:

All that part of the N1/4 of the SW1/4NE1/4 of Section 11, Township 1 South, Range 1 West of the Ute Meridian, lying East of the center line of North Seventh Street; EXCEPT the West 450 feet of said tract; AND ALSO EXCEPTING THEREFROM all roads, easements and rights of way of record in Mesa County, Colorado.

Containing 3.763 acres, more or less. See Attached Exhibit A, Outline Development Plan.

A. Deviation of Uses

The following uses shall also be allowed:

Management office with residential unit for on-site manager, including support offices for resident service providers such as home health care and Veterans Administration, together with fitness, wellness, and socializing areas. Other indoor amenities may include a coffee shop and/or sandwich shop.

In lieu of a solid fence the required fence buffer on the west side of the property can be open style fencing (to see through) or a landscaping berm.

B. Deviations from Bulk Standards

A zoning density range of 24 to 32 dwelling units per acre.

Minimum front yard setback shall be 10 feet.

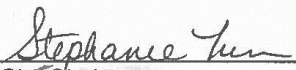
Minimum side yard setbacks shall be zero from any new lot line created by subdivision of the property.

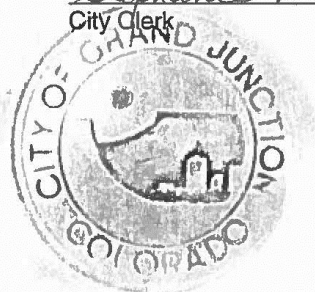
**INTRODUCED** on first reading on the 17th day of December, 2014 and ordered published in pamphlet form.

**ADOPTED** on second reading this 7<sup>th</sup> day of January, 2015 and ordered published in pamphlet form.

ATTEST:

  
\_\_\_\_\_  
President of Council

  
\_\_\_\_\_  
City Clerk



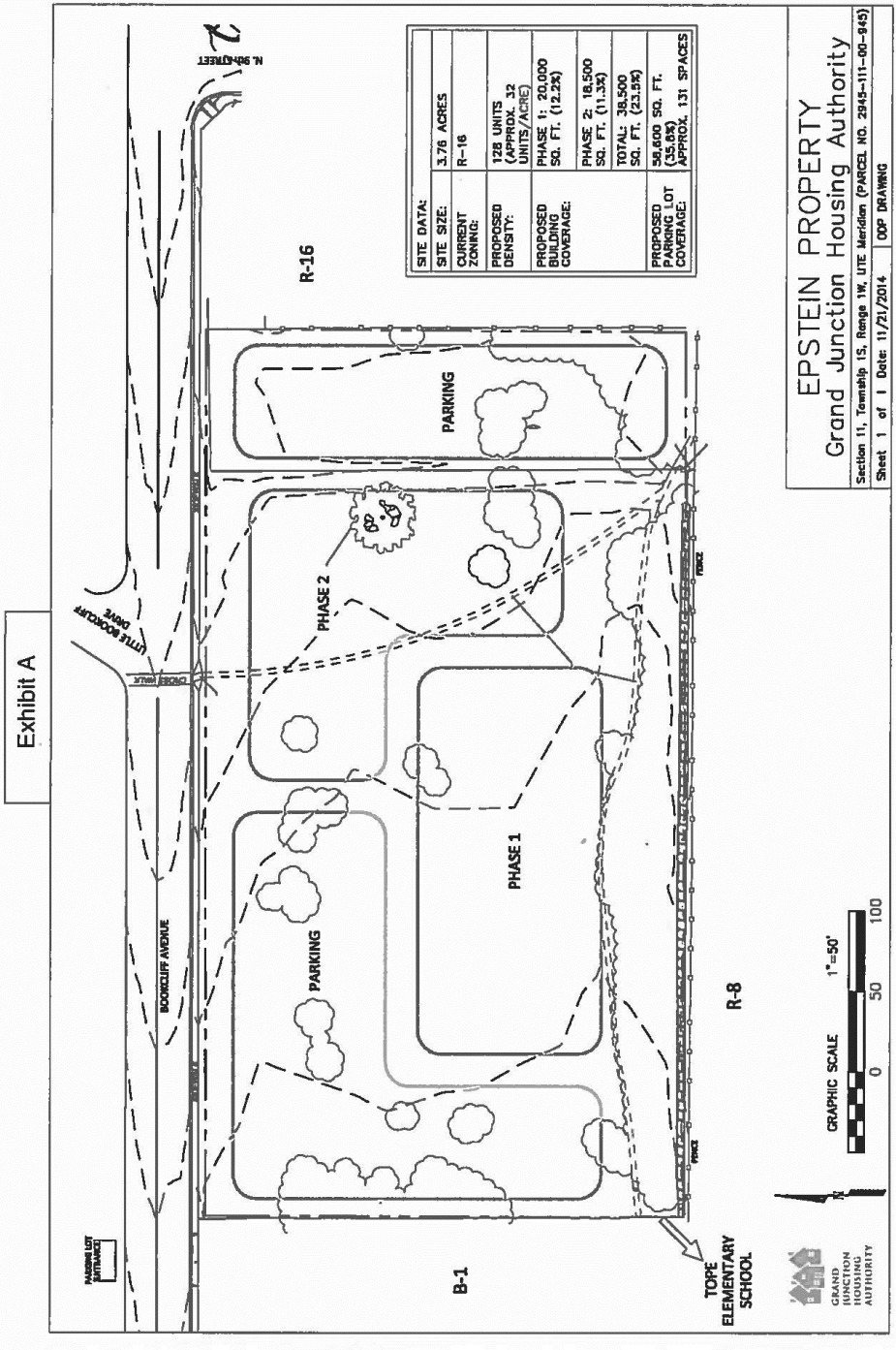


Exhibit A

SITE DATA:	
SITE SIZE:	3.76 ACRES
CURRENT ZONING:	R-16
PROPOSED DENSITY:	128 UNITS PER ACRE
PROPOSED BUILDING COVERAGE:	PHASE 1: 20,000 SQ. FT. (72.2%) PHASE 2: 18,500 SQ. FT. (11.3%)
PROPOSED PARKING LOT COVERAGE:	TOTAL: 38,500 SQ. FT. (23.5%) 58,600 SQ. FT. (35.6%) APPROX. 131 SPACES

**EPSTEIN PROPERTY**  
**Grand Junction Housing Authority**  
 Section 11, Township 15, Range 1W, UTE Meridian (PARCEL NO. 2945-111-00-843)  
 Sheet 1 of 1 Date: 11/21/2014 ODP DRAWING

TOPE ELEMENTARY SCHOOL

GRAPHIC SCALE 1"=50'  
 0 50 100

GRAND JUNCTION HOUSING AUTHORITY

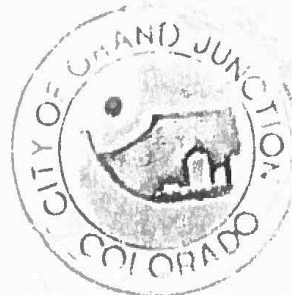
I HEREBY CERTIFY THAT the foregoing Ordinance, being Ordinance No. 4652 was introduced by the City Council of the City of Grand Junction, Colorado at a regular meeting of said body held on the 17<sup>th</sup> day of December, 2014 and that the same was published in The Daily Sentinel, a newspaper published and in general circulation in said City, in pamphlet form, at least ten days before its final passage.

I FURTHER CERTIFY THAT a Public Hearing was held on the 9<sup>th</sup> day of January, 2015, at which Ordinance No. 4652 was read, considered, adopted and ordered published in pamphlet form by the Grand Junction City Council.

IN WITNESS WHEREOF, I have hereunto set my hand and affixed the official seal of said City this 9<sup>th</sup> day of January, 2015.

Stephanie Tuin  
 Stephanie Tuin, MMC  
 City Clerk

Published: December 19, 2014  
 Published: January 9, 2015  
 Effective: February 8, 2015



**CITY OF GRAND JUNCTION, COLORADO**

**ORDINANCE NO.**

**AN ORDINANCE AMENDING ORDINANCE NO. 4652**

**GRAND JUNCTION HOUSING AUTHORITY SENIOR LIVING  
PLANNED DEVELOPMENT – HIGHLANDS APARTMENTS  
LOCATED AT 805 AND 825 BOOKCLIFF AVENUE**

Recitals:

A request to amend the Outline Development Plan from 128 units of multi-family residential to 136 units of multi-family residential has been submitted. The addition of four units is internal to the structure and does not expand or change the approved foot-print, or the elevations of the building. This exceeds the maximum zoning density range of 32 dwelling units by .63 units per acre as provided in Ordinance 4652. This Ordinance will expand the maximum density to 33 dwelling units per acre.

In public hearings, the Planning Commission and City Council reviewed the request for the proposed amendment to the Outline Development Plan and determined that it satisfied the criteria as set forth and established in Section 21.02.150(e) of the Grand Junction Municipal Code. The proposed amendment to the Outline Development Plan is consistent with the purpose and intent of the Comprehensive Plan.

NOW, THEREFORE, BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF GRAND JUNCTION, ORDINANCE NO. 4652 IS HEREBY AMENDED TO ALLOW 136 MULTI-FAMILY RESIDENTIAL UNITS.

**INTRODUCED** on first reading on the \_\_\_\_ day of \_\_\_\_\_, 2016 and ordered published in pamphlet form.

**ADOPTED** on second reading this \_\_\_\_ day of \_\_\_\_\_, 2016.

ATTEST:

\_\_\_\_\_  
President of Council

\_\_\_\_\_  
City Clerk



Date: June 17, 2016  
 Author: David Thornton  
 Title/ Phone Ext: Principal Planner /  
x.1450  
 Proposed Schedule: \_\_\_\_\_  
Planning Commission July 12, 2016  
 File # (if applicable): ZCA-2016-111

**Attach 2**

**PLANNING COMMISSION AGENDA ITEM**

<b>Subject:</b> Amending Title 25 - 24 Road Corridor Design Standards
<b>Action Requested/Recommendation:</b> Forward a recommendation to City Council to amend the Grand Junction Municipal Code, Title 25, 24 Road Corridor Design Standards.
<b>Presenter(s) Name &amp; Title:</b> David Thornton, Principal Planner

**Executive Summary:**

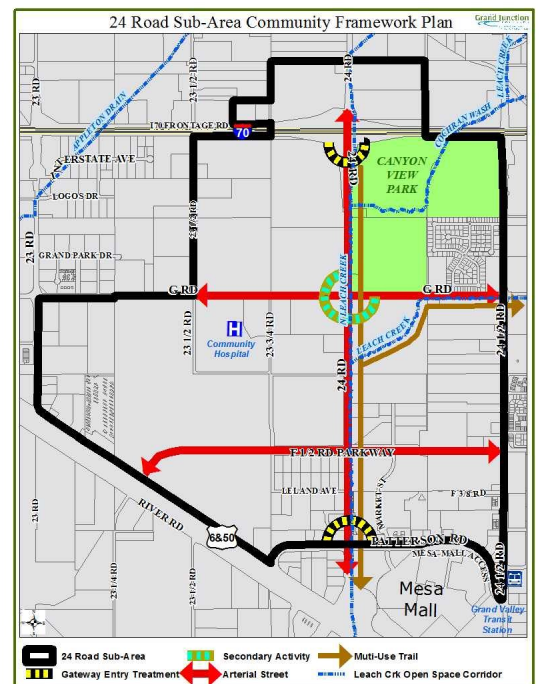
The proposed ordinance amends the 24 Road Corridor Design Standards, Title 25, of Volume II: Development Regulations of the Grand Junction Municipal Code (GJMC). The purpose of the amendments are to clarify existing requirements, eliminate redundancies and provide a more user friendly text to help development achieve the vision of the 24 Road Corridor Sub-area Plan.

**Background, Analysis and Options:**

The Grand Junction City Council has requested that Staff propose amendments to City codes and regulations as needed to be dynamic and responsive. The proposed amendment will enhance the responsiveness of the Code to the concerns of citizens and enhance its effectiveness. City Council also developed an Economic Development Plan. The proposed amendments implement this Plan by streamlining processes and support Grand Junction becoming “the most livable community west of the Rockies by 2025.

**24 Road Corridor Design Standards**

The 24 Road Corridor Design Standards were adopted by the City at the same time the 24 Road Corridor Sub-area Plan was adopted on November 1, 2000. The Design Standards implement the Sub-area Plan. The vision of the 24 Road Corridor Sub-area Plan is the following:



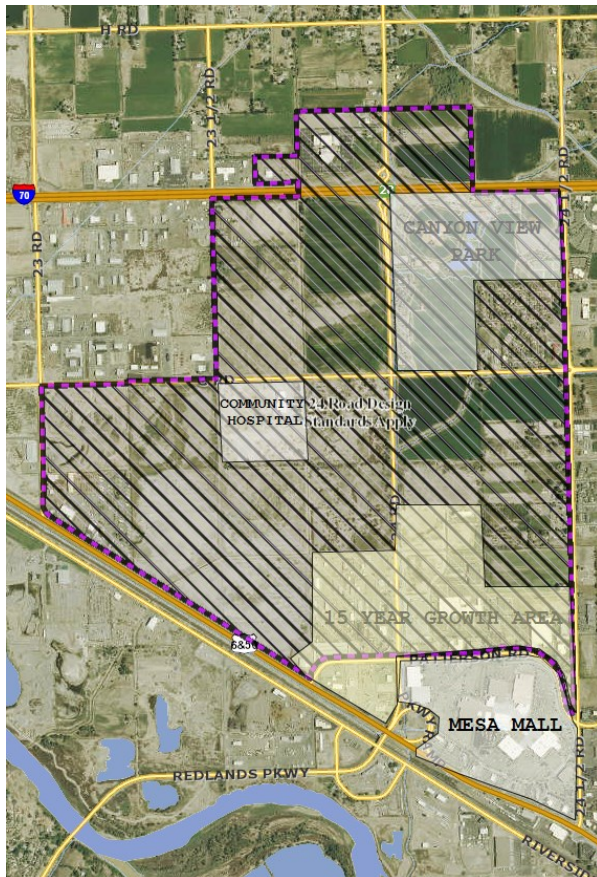
- (a) Achieve high-quality development in the corridor in terms of land use, site planning and architectural design.
- (b) Provide market uses that complement existing and desired uses and benefit the Grand Junction community.
- (c) Take advantage of and expand upon existing public facilities in the corridor to create a “civic” presence.
- (d) Achieve a distinctive “parkway” character along the roadway that can serve as a gateway to the Grand Junction community.
- (e) Encourage development that is consistent with the Grand Junction Growth Plan.
- (f) Adjust and/or amend the Grand Junction Land Use Code and Growth Plan to achieve the Road 24 vision, concept, and plan and to create a predictable environment for future development of the area.

24 Road Corridor Subarea Plan

The concept of the Subarea Plan is to provide a land use and transportation framework for future development in the 24 Road Corridor that:

- (a) Allows for flexibility in land uses (type, intensity, and density) while recognizing inherent differences between development on small parcels compared with larger parcels.
- (b) Establishes a transportation network that interconnects to create a logical urban pattern.
- (c) Establishes a high-quality image through zoning, design standards, and public improvements.

24 Road Corridor Design Standards and Sub-area Plan work together



The vision and concepts for the 24 Road area remain intact today and continue to be achieved through the provisions of the 24 Road Corridor Design Standards and Guidelines. The City is realizing consistent development meeting the vision of the corridor and standards established in the overlay in this very important corridor, a gateway to the community. However, implementing the vision through existing development regulations is sometimes confusing to the user of the Code, including staff and the development community.

It has been 15 years now and what has been learned? What can be done to make the Design Standards better understood, less redundant and more user friendly?

Development within the sub-area has grown concentrically (with the exception of the Community Hospital campus), extending out from the Mesa Mall area growing north towards Canyon View Park.

In addition, the past 15 years has seen City-wide changes to the Zoning and Development Code including new design standards for:

- site development,
- building architecture,
- site lighting, and
- landscape development.

Road and access standards have been updated through the Grand Valley Circulation Plan’s last amendment in 2010 and through the City’s Transportation Engineering and Design Standards (TEDS) manual. These newer standards are either the same as or have exceed the adopted 24 Road Design Standards from 2000. This causes confusion and redundancy and can be intimidating to the Code user developing in the 24 Road area while determining what standards apply and where.



**Planning staff proposes amending the Design Standards to:**

1. **Reduce** redundancy, include only standards and eliminate guidelines that are only advisory in nature as written and often redundant with the standards,
2. **Eliminate** sections that are better stated and regulated through other sections found in the Zoning and Development Code; and
3. **Clarify** code language overall, simplifying and clarifying what the standards are the City is requiring to achieve the vision of the corridor.

The following is a brief summary of the proposed changes to each of the sections found under Title 25, 24 Road Corridor Design Standards.

**25.04 (new 25.01) Introduction** – Proposed deleting everything except a short introduction of the background and intent of the overlay.

**25.08 Community Framework** – Propose deleting entire section since it can be found in the Grand Valley Circulation Plan and Transportation Engineering and Design manual (TED’s) except moving a couple of standards to Site Development. These include moving and updating the “Community Framework” exhibit, the “Natural Corridor Plan” exhibit and the “Natural Corridor Section” exhibit.





**25.12 (new 25.02) Site Development** – Propose deleting portions of this section’s content, but keep and clarify the Standards that have been defining 24 Road since 2000, including on-site open space; organizing features or gathering places; site grading and drainage; building and parking setback; building location and orientation; parking, access and circulation; auto-oriented uses; and pedestrian and bicycle circulation. Deletions include lot and block dimensional standards, transitions and interconnections, and the section on multi-unit residential parking areas. These proposed deleted sections are already adequately addressed in the Code.

**25.16 Landscape Development** – Propose deleting entire section. Landscaping (including street trees) and irrigation standards are already a part of the Code.

**25.20 (new 25.03) Architectural Design** – Propose to combine purpose statements; insert language from big box standards to apply to all buildings in overlay; eliminate all guidelines except one and make it a standard. Creating 360 degree architecture in the 24 Road area is a key element of the plan to make this corridor unique in Grand Junction. This update further clarifies the need for all building sizes to adhere such standards by incorporating the Big Box standards to achieve this for all building sizes.



to

**25.24 (new 25.04) Site Lighting** – Propose keeping pedestrian lighting and delete street lighting and parking area lighting which are already addressed and required in code. Also propose language to modify accent and security lighting.

**25.28 (new 25.05) Signs** – Propose to define which sign types are allowed, keep maximum size at 100 sq. ft., maximum height at 12 ft., and define what sign types are permitted. Also propose eliminating requirement for a sign program package, since an option is currently available already in the Code and continue the ban of billboards.

The following graphic depicts the key elements that have been required for development in the 24 Road Sub-area Plan and Overlay since 2000. It is important that these standards are maintained for current and future development in the corridor as envisioned by the Plan.

Key Elements of the 24 Road Design Standards



**How this item relates to the Comprehensive Plan Goals and Policies:**

**Goal 3:** The Comprehensive Plan will create ordered and balanced growth and spread future growth throughout the community.

**Policy A:** To create large and small “centers” throughout the community that provide services and commercial areas.

The Mesa Mall/24 Road area has been identified as a “Village Center” in the Comprehensive Plan. As a Village Center, mixed use development is allowed and services designed for a larger geographical area are anticipated with future development and are being realized with existing development.

**Goal 8:** Create attractive public spaces and enhance the visual appeal of the community through quality development.

**Policy A:** Design streets and walkways as attractive public spaces.

**Policy B:** Construct streets in the City Center, Village Centers, and Neighborhood Centers to include enhanced pedestrian amenities.

**Policy C:** Enhance and accentuate the City “gateways” including interstate interchanges, and other major arterial streets leading into the City.



The 24 Road interchange with I-70 has been developed with two roundabouts and public art and aesthetics enhancing and accentuating it as a major gateway to the City. Pedestrian amenities have been designed as attractive public spaces and are a large part of the vision for the 24 Road Sub-area Plan.

**Goal 9:** Develop a well-balanced transportation system that supports automobile, local transit, pedestrian, bicycle, air, and freight movement while protecting air, water and natural resources.

**Policy D:** A trails master plan will identify trail corridors linking neighborhoods with the Colorado River, Downtown, Village Centers and Neighborhood Centers and other desired public attractions.

A major component of the 24 Road Sub-area Plan is for the establishment of the Leach Creek corridor as a pedestrian/bicycle amenity providing ultimate connections to the Colorado River while preserving its natural resources as a public open space and storm water facility. 24 Road is a major arterial street serving auto and freight needs for the community. It also provides for bicycle and pedestrian needs as well.

**Goal 10:** Develop a system of regional, neighborhood and community parks protecting open space corridors for recreation, transportation and environmental purposes.

**Policy B:** Preserve areas of scenic and/or natural beauty and, where possible, include these areas in a permanent open space system.

The Leach Creek corridor is being preserved permanently for recreation, transportation and environmental purposes. Canyon View Park is a City “regional park” that is a major public amenity in the 24 Road Sub-area Plan and vision. Existing and future pedestrian and

bicycle connections are envisioned as part of the Sub-area Plan.

**Goal 12:** Being a regional provider of goods and services the City and County will sustain, develop and enhance a healthy, diverse economy.

**Policy B:** The City and County will provide appropriate commercial and industrial development opportunities.

The 24 Road corridor is a major entrance into the community. How it looks, the services it provides and the amenities it has are a part of the first impression a new visitor will make about Grand Junction and the quality of community assets residents seek. Maintaining design standards that implements a vision that has been adopted for this very important gateway is crucial.

#### **How this item relates to the Economic Development Plan:**

The purpose of the adopted Economic Development Plan by City Council is to present a clear plan of action for improving business conditions and attracting and retaining employees.

This supports the City's 2014 Economic Development Plan in the following ways:

- **Section 1.5 Supporting Existing Business:** Streamline processes...while working within the protections that have been put in place through the Comprehensive Plan. Action Step: Be proactive and business friendly and review development standards and policies to ensure that they are complimentary and support the common mission; and
- **Section 1.6 Investing in and Developing Public Amenities:** Continue to make strategic investments in public amenities that support Grand Junction becoming "the most livable community west of the Rockies by 2025", Action Step: Developing a system of regional...parks protecting open space corridors for recreation and multi-modal transportation. Action Step: Create attractive public spaces and enhance the visual appeal of the community through quality development. Action Step: Enhance and accentuate the city's "gateways" including interstate interchanges and other major arterial streets leading into the city.

#### **Board or Committee Recommendation:**

The Planning Commission will consider a recommendation to the City Council on June 28, 2016.

#### **Financial Impact/Budget:**

There will not be a financial impact.

**Legal issues:**

The City Attorney has reviewed and approved the form of the ordinance.

**Other issues:**

No other issues have been identified.

**Previously presented or discussed:**

Planning Commission discussed this at their January 21, 2016 workshop.

**STAFF RECOMMENDATION:**

I recommend that the Planning Commission approve the requested Amendments to the Grand Junction Municipal Code, Title 25, 24 Road Corridor Design Standards, ZCA-2016-111.

**RECOMMENDED PLANNING COMMISSION MOTION:**

Madam Chairman, on the request to forward a recommendation to City Council to amend the Grand Junction Municipal Code, Title 25, 24 Road Corridor Design Standards. ZCA-2016-111, I move that the Planning Commission approve it as presented in the Staff Report.

**Attachments:**

- Proposed Text Amendment Ordinance- Showing strikethroughs as deletions and underlined text as additions.
- Proposed Text – Clean Copy

## CITY OF GRAND JUNCTION, COLORADO

ORDINANCE NO. \_\_\_\_\_

**AN ORDINANCE AMENDING THE GRAND JUNCTION MUNICIPAL CODE  
RELATING TO ZONING AND DEVELOPMENT IN THE 24 ROAD CORRIDOR****Recitals:**

The City Council desires that the City's zoning and development regulations be amended as needed so that they will be dynamic and responsive to the demands of the community and development trends, without compromising health, safety and welfare, and to streamline processes and regulations that may have become, over time, unnecessarily complex or difficult to apply.

The City Council adopted the 24 Road Corridor Sub-area Plan and the 24 Road Corridor Design Standards on November 1, 2000, in order to:

- Achieve high-quality development in the corridor in terms of land use, site planning and architectural design;
- Provide market uses that complement existing and desired uses and benefit the Grand Junction community;
- Take advantage of and expand upon existing public facilities in the corridor to create a "civic" presence;
- Achieve a distinctive "parkway" character along the roadway that can serve as a gateway to the Grand Junction community;
- Allow for flexibility in land uses (type, intensity, and density) while recognizing inherent differences between development on small parcels compared with larger parcels;
- Establish a transportation network that interconnects to create a logical urban pattern;
- Establish a high-quality image through zoning, design standards, and public improvements.

In the past 15 years there have been City-wide updates to development standards for site development, building architecture, site lighting, landscaping, and road, access and circulation, which meet or in some cases exceed the existing 24 Road Corridor Design Standards; the proposed amendments will eliminate confusion as to which standards are applicable and eliminate redundancies in development regulations.

**NOW THEREFORE BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF GRAND JUNCTION THAT:**

**Title 25 of the Grand Junction Municipal Code (24 Road Corridor Design Standards) is amended as follows (text deletions ~~struck through~~, text additions underlined, graphics and pictures **added** and **deleted** as labeled):**

**Title 25**

**24 ROAD CORRIDOR DESIGN STANDARDS**

**Chapters:**

~~25.04~~ **25.01** Introduction

~~25.08~~ **Community Framework**

~~25.12~~ **25.02** Site Development

~~25.16~~ **Landscape Development**

~~25.20~~ **25.03** Architectural Design

~~25.24~~ **25.04** Site Lighting

~~25.28~~ **25.05** Signs

**Chapter ~~25.04~~ 25.01**

**INTRODUCTION**

Sections:

- ~~25.04.010~~ 25.01.010 Background and intent.
- ~~25.04.020~~ Format of the Design Standards and Guidelines.
- ~~25.04.030~~ Administration of the Design Standards and Guidelines.

**~~25.04.010~~ 25.01.010 Background and intent.**

The ~~24 Road Corridor Design Standards and Guidelines~~ (“standards and guidelines”) are intended to provide guidance and criteria ~~development standards for the planning, design and implementation of public and private improvements in the 24 Road Corridor in order to implement the goals and objectives of the.~~ If properly administered and adhered to, they should result in public and private development improvements that achieve, as a minimum, a common level of quality in terms of site design, architectural design, landscaping, and other site improvements. 24 Road Corridor Subarea Plan and to:

~~The general purposes of the standards and guidelines are:~~

- (a) ~~To establish a practical, interconnected system of streets, parks, and parkways that allows easy orientation and convenient access for all modes of transportation;~~
- (b) ~~To utilize natural open spaces, such as creeks, and developed public spaces, streets, parks and parkways, to organize and coordinate development;~~
- (c) ~~To accommodate a broad mix of development types that encourage alternative transportation, especially walking, and transit use;~~
- (d) ~~To provide common usable open space that is of mutual benefit to surrounding property owners, businesses, and residents;~~ and
- (e) ~~To construct the early phases of development in a manner that establishes~~ establish a pattern and character for the long-term evolution of the corridor.

(f) Adherence to these standards and guidelines will ensure that public and private improvements in the 24 Road Corridor will be well planned and executed in a high quality manner, which were important goals established through the 24 Road Corridor Subarea Plan.

These standards and guidelines supplement other development regulations in the Grand Junction Municipal Code. Where there is a conflict between this Title 25 and the Zoning and Development Code (Title 21), the more restrictive standard shall apply. ~~such as the City of Grand Junction Zoning and Development Code (adopted in 2000), which includes detailed criteria by zone district, planned development regulations, design and improvement standards, supplemental use regulations and sign regulations. The development code also incorporates use specific standards for “big box” development that are applicable to the type of development that is anticipated for portions of the 24 Road Corridor.~~

The standards and guidelines identify design alternatives and specific design criteria for the visual appearance and physical treatment of private development and public improvement within the corridor. They are to be adopted by the City of Grand Junction through an overlay zoning district, which will establish the means by which the standards and guidelines will be administered and enforced.

#### **~~25.04.020 Format of the Design Standards and Guidelines.~~**

The 24 Road Corridor Design Standards and Guidelines are written in a standard format to include the following:

(a) **~~Purpose.~~** The “Purpose” sets forth the goals intent for development of the 24 Road Corridor. The standards and guidelines provide direction as to how the goals may be achieved.

(b) **~~Standards.~~** Design standards are objective criteria that provide specific direction based on the related purpose statement. Standards are used to define issues considered critical to achieving the purpose. Standards use the term “shall” to indicate that compliance is required, unless it can be demonstrated that an acceptable alternative meets one or more of the following conditions:

- (1) ~~The alternative better achieves the stated purpose.~~
- (2) ~~The purpose will not be achieved by application of the standard in this circumstance.~~
- (3) ~~The effect of other standards or guidelines will be improved by not applying this standard.~~
- (4) ~~Unique site factors make the standard impractical.~~

(c) ~~Guidelines promote the goals defined by the purpose statements. Achieving guidelines may help in identifying alternative approaches to achieving standards. While the term “guidelines” is used, guidelines shall be applied unless the Director and/or Planning Commission otherwise determine.~~

#### **~~25.04.030 Administration of the Design Standards and Guidelines.~~**

Adherence to these standards and guidelines will ensure that public and private improvements in the 24 Road Corridor will be well planned and executed in a high quality manner, which were important goals established through the 24 Road Corridor Subarea Plan.

These standards and guidelines supplement City minimum regulations and may be more restrictive than other development regulations.

A process of design review is to be established by the City of Grand Junction that will provide for the administration and enforcement of these standards and guidelines. It is the responsibility of the applicant to ensure compliance with all other local codes and regulatory issues concerning development within the corridor. The submittal process and requirements are available from the City of Grand Junction.





*View corridor along 24 Road highlights natural features*

**Chapter 25.08****COMMUNITY FRAMEWORK**

## Sections:

~~[25.08.010](#) Introduction.~~

~~[25.08.020](#) Purpose.~~

~~[25.08.030](#) Open space, public parks, trail system and storm drainage~~

~~[25.08.040](#) Roadway system.~~

~~[25.08.050](#) Streetscape.~~

~~[25.08.060](#) Key gateways, intersections and entries.~~

~~[25.08.070](#) View sheds.~~

**~~25.08.010 Introduction.~~**

~~The overall planning concept for the 24 Road Corridor includes a community framework (Figure 2.1) that provides a distinctive image and organizing element for public and private development. The community framework includes public streets, parks, open spaces, natural drainages and future stormwater management facilities that serve and connect part or all of the corridor~~

~~These community framework design standards and guidelines are intended to guide the planning, design and implementation of these elements, which will occur over time through a variety of actions by private property owners and public agencies, including the City of Grand Junction, Mesa County and the State of Colorado.~~

~~These standards and guidelines include the following:~~

- ~~(a) Open space, public parks, trail system and storm drainage;~~
- ~~(b) Roadway system;~~
- ~~(c) Streetscape treatment;~~
- ~~(d) Key gateways, intersections and entries;~~
- ~~(e) View sheds.~~

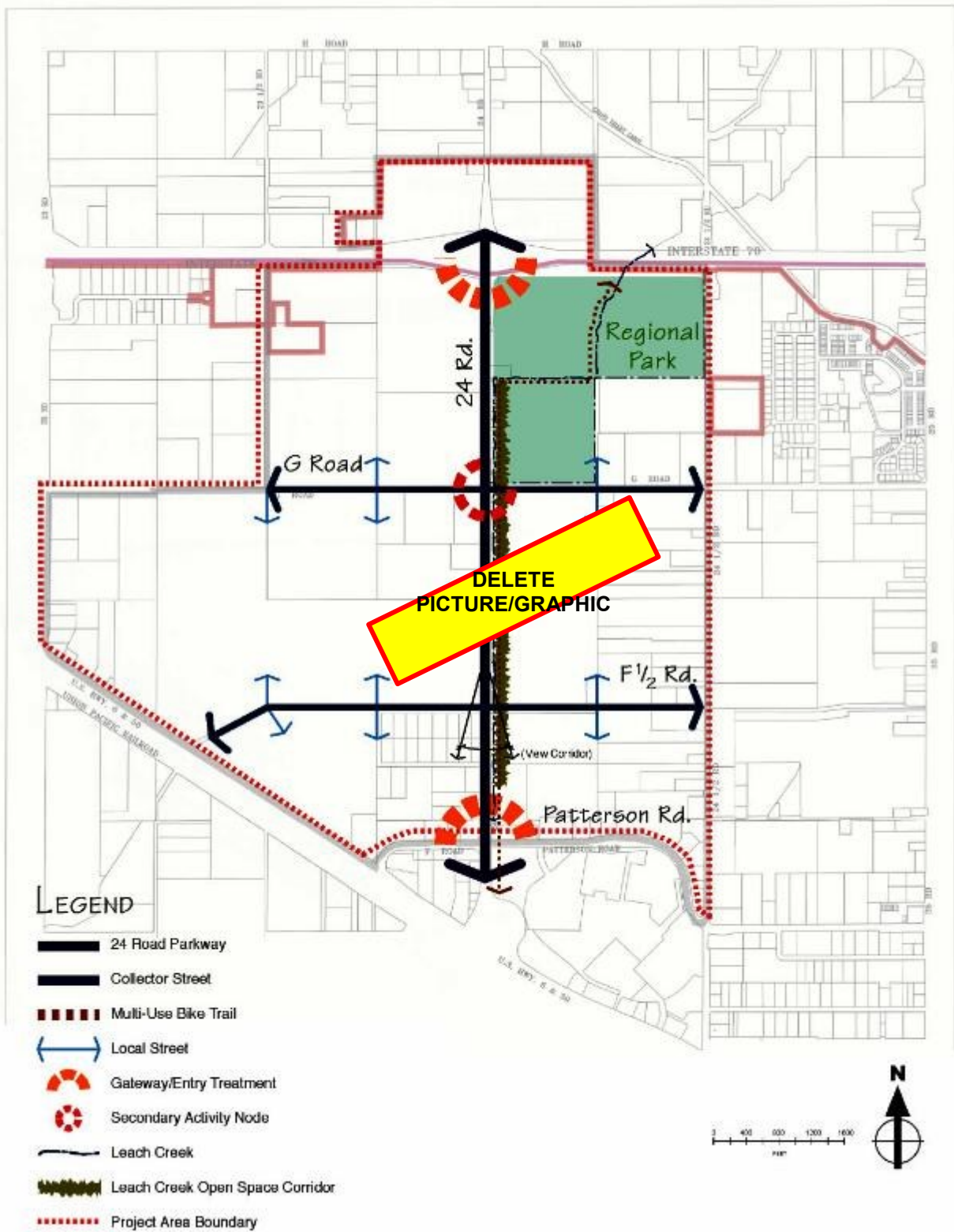


Figure 2.1: Community Framework Plan

**25.08.020 Purpose.**

The planning concept envisions a network of public streets, open spaces, drainage, and stormwater facilities and supporting infrastructure, which provide organization and identity for the corridor. Improvements to create the community framework will be constructed as City capital improvement projects or as development occurs. Primary objectives to be achieved through the framework include:

- (a) Develop a distinctive identity for the corridor that reflects a high quality of site design, improvements, buildings and outdoor spaces.
- (b) Establish an organizing framework that links the various uses and development parcels within the 24 Road Corridor, which can be implemented in an incremental and phased manner.
- (c) Create pedestrian connections between privately owned sites and the “public realm” of roadway corridors, open spaces and the natural corridor, parks and the multi-use trail system.



*Multi-purpose trail along a natural open space corridor*

**25.08.030 Open space, public parks, trail system and storm drainage.**

The open space system in the 24 Road Corridor currently is comprised of existing private open space associated with Leach Creek natural corridor and its tributaries, which run on private property along the east side of 24 Road, and Canyon View Park, a 120-acre City-owned regional park southeast of the Interstate-70

interchange. In the future, the open space system will be augmented by private open space on individual development parcels to meet the City's on-site open space requirements. (See Figure 2.2.)

The 24 Road Corridor includes a regional multi-use trail along Leach Creek that is planned between Canyon View Park and the Colorado River. Although much of this trail is planned on property that is privately owned, it is envisioned that the trail will be implemented through public and private cooperative efforts.

(a) **Standards.**

(1) Utilize existing and proposed open space to reinforce the community framework in the corridor, to create a well-planned setting and to provide for a variety of active and passive recreational and social opportunities.

(2) Develop existing riparian areas and drainages, such as Leach Creek and its tributaries, as natural open space corridors that provide for surface drainage and pedestrian trails. The preferred treatment of surface drainages is shall be an open channel with gently sloping sides and naturalized landscape, except for. It is recognized that the west bank of Leach Creek which is constrained by the 24 Road improvements and the preferred gently sloping bank cannot likely be achieved along the west bank. (See Figure 2.3.)

(3) Develop and utilize a comprehensive stormwater drainage master plan for the area that addresses future development and provides for open space and recreation.

(4) Where appropriate, utilize the setbacks on private sites as part of the common open space system to create a "seamless" open space transition between private and public property.

(5) Create a connection between Canyon View Park and the Colorado River Trail Corridor by way of an off-street multi-use trail. Establish linkages between this major trail corridor and other regional trails and parks. Provide intermediate connections to related pedestrian trails, bikeways and sidewalks.

(6) Develop Leach Creek, its tributaries and other stormwater facilities as public amenities, incorporating the planned multi-purpose trail where appropriate.

(7) Develop the trail on public and private property through cooperation between private landowners, developers and the City of Grand Junction.

(b) **Guidelines.**

(1) Build upon Canyon View Park as a major regional facility for active recreation.

~~(2) Future public parks should be developed in response to the scale and type of development in the area.~~

~~(3) Public art is encouraged in both public and private open space.~~

~~(4) The proposed multi-use trail along 24 Road should be designed to take advantage of the proposed natural corridor, which may be comprised of public and private property.~~



**DELETE  
PICTURE/GRAPHIC**



~~Examples of trail systems through drainage areas~~

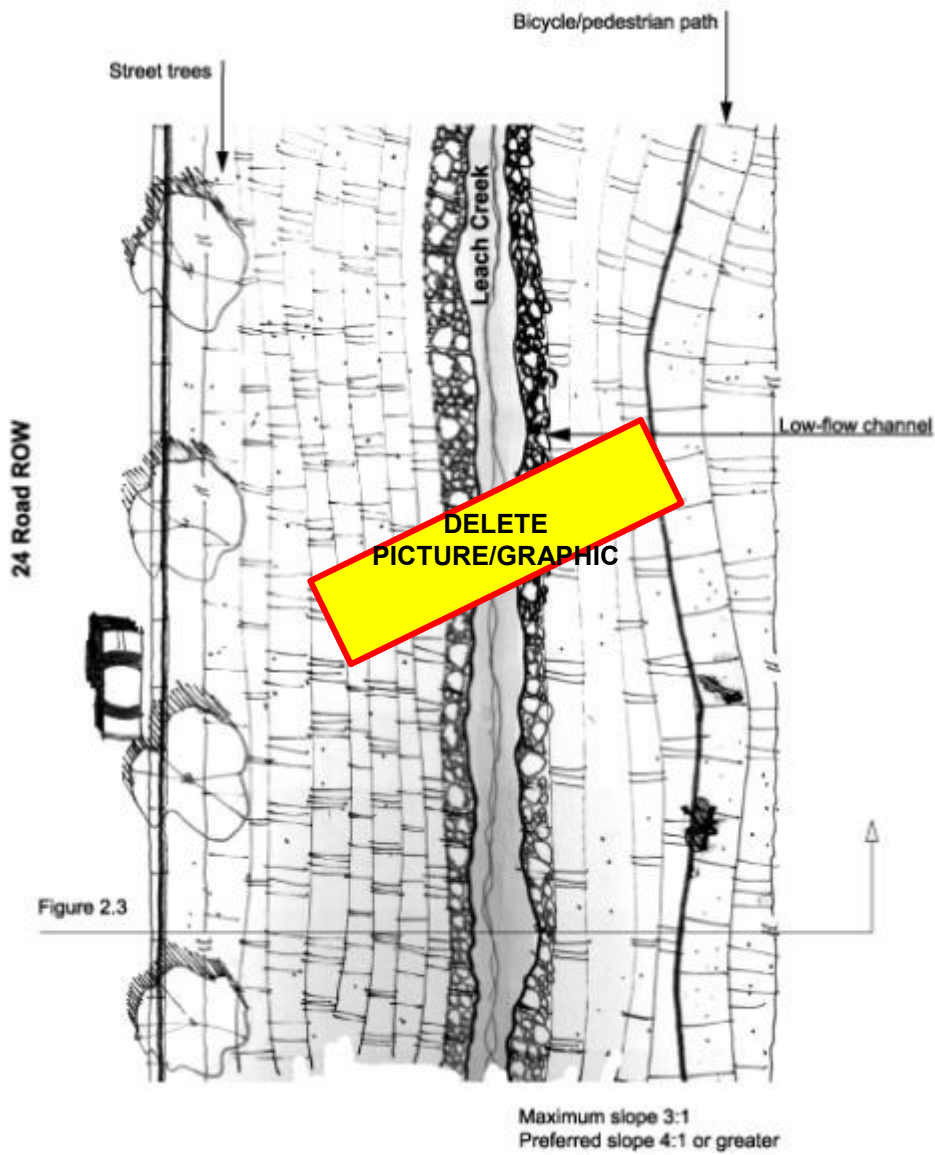


Figure 2.3

Figure 2.2: Natural Corridor Plan



Figure 2.2 Natural Corridor Section



*Existing Leach Creek drainage structure along 24 Road*

**25.08.040 Roadway system.—**

The roadway system envisioned for the 24 Road Corridor has been planned to provide for safe and efficient vehicular movement within the corridor as well as road connections to Interstate 70 and streets within Grand Junction.—

A road network has been planned for the corridor, which includes local streets, collector streets, arterial streets and the regional interstate. Each of these types of roads has functional traffic access and other characteristics. The network establishes a logical system of streets that facilitates access and connectivity for all modes of transportation throughout the site. The network also provides a balanced system of through streets, transit corridors, sidewalks, and trails in the corridor. This is illustrated in Figure 2.4.

**(a) Standards.—**

- (1) Establish a clear hierarchy of streets within the corridor, including arterial streets, collector streets and local streets, based upon City of Grand Junction standards. Coordinate plans between private property owners to ensure connectivity between streets at the arterial and collector level. Establish appropriate capacity and access characteristics and intersection controls. Refer to Figure 2.4.



- ~~(i) 24 Road—interim improvements include a three-lane section with the potential to be expanded to a five-lane arterial section in the future. Access to individual sites along 24 Road shall be controlled to maintain traffic flow with the potential for traffic signals at major intersections with arterial and collector roads. Refer to Figures 2.5 and 2.6.~~
- ~~(ii) Patterson Road/F Road (east of 24 Road)—the existing improved five-lane segment shall be completed as an arterial with access control to sites, most of which are already developed. Refer to Figure 2.7.~~
- ~~(iii) Patterson Road/F Road (west of 24 Road)—to be developed similar to the segment to the east with a five-lane section (no median) collector/arterial. Refer to Figure 2.7.~~
- ~~(iv) G Road—to be developed in the future as a five-lane section (no median) collector/arterial. Refer to Figure 2.8.~~
- ~~(v) F 1/2 Road—to be developed as a three-lane collector section. Refer to Figure 2.7.~~
- ~~(vi) Collector streets—other collector streets shall be developed according to City standards. Refer to Figure 2.9.~~
- ~~(vii) Local streets—local streets shall be developed according to City standards. Refer to Figure 2.9.~~

~~(2) Provide a rational and identifiable roadway network to serve development in the corridor and to provide connections to the surrounding region. Extend the existing street and block patterns from surrounding neighborhoods into the 24 Road Corridor. Modifications and departures from the grid will be considered for necessary and logical reasons including parks, open space, and organizing features. In all cases, the block and street system shall be easily understood, walkable, and interconnected.~~

~~— Full access intersections shall be provided on arterial streets only at locations approved by the City of Grand Junction.~~

~~(3) Pedestrian walks shall provide direct connections within neighborhoods to the nearest transit facilities, reinforced by regular block patterns that give transit patrons the widest range of connections with different transit stops serving different lines.~~

~~(b) **Guidelines.**~~

~~(1) The purpose and hierarchy of streets is critical to the determination of which type of street is applied to a specific location. Street classifications should be made with regard for both transportation needs and its ability to serve proposed land use along the street.~~

- (2) Access to development sites should balance traffic flow with land development requirements.
- (3) Local streets may be used to define the boundaries of natural or developed open space.
- (4) In residential developments, alleys should be considered as a means of concealing parking and service areas, for locating utilities, and for minimizing curb cuts on the major streets.

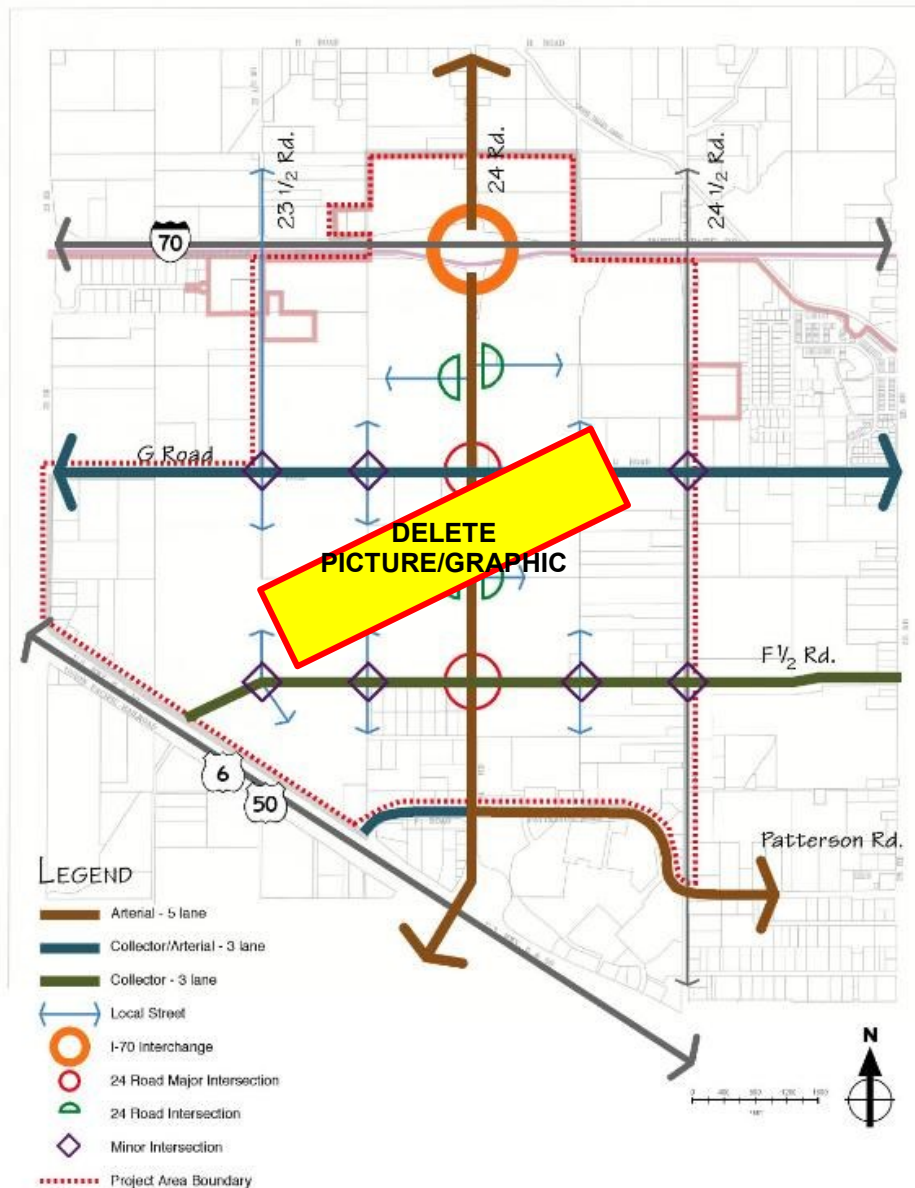


Figure 2.4 Roadway System

Figure 2.5: 24 Road Typical Section Interim

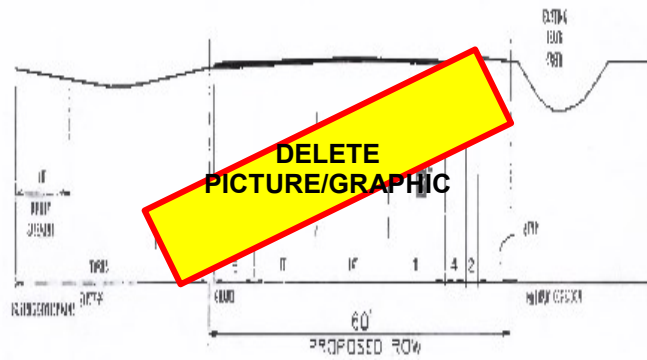


Figure 2.6 24 Road Typical Sections

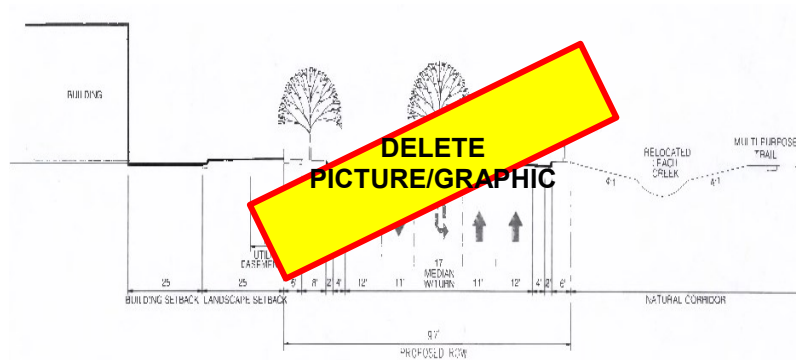
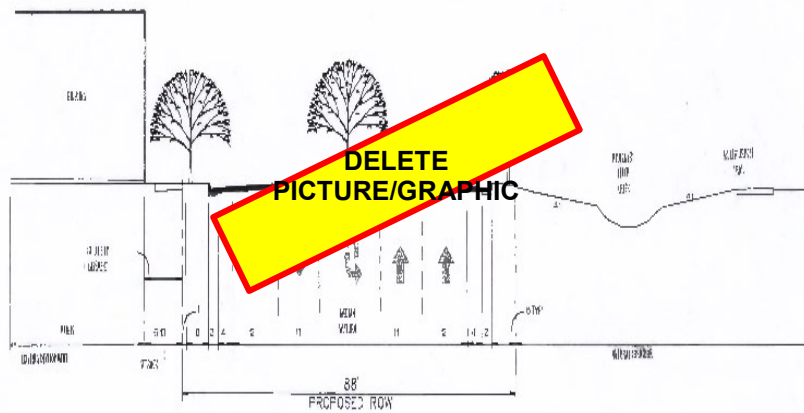


Figure 2.7: F/Patterson and F 1/2 Street Typical Sections

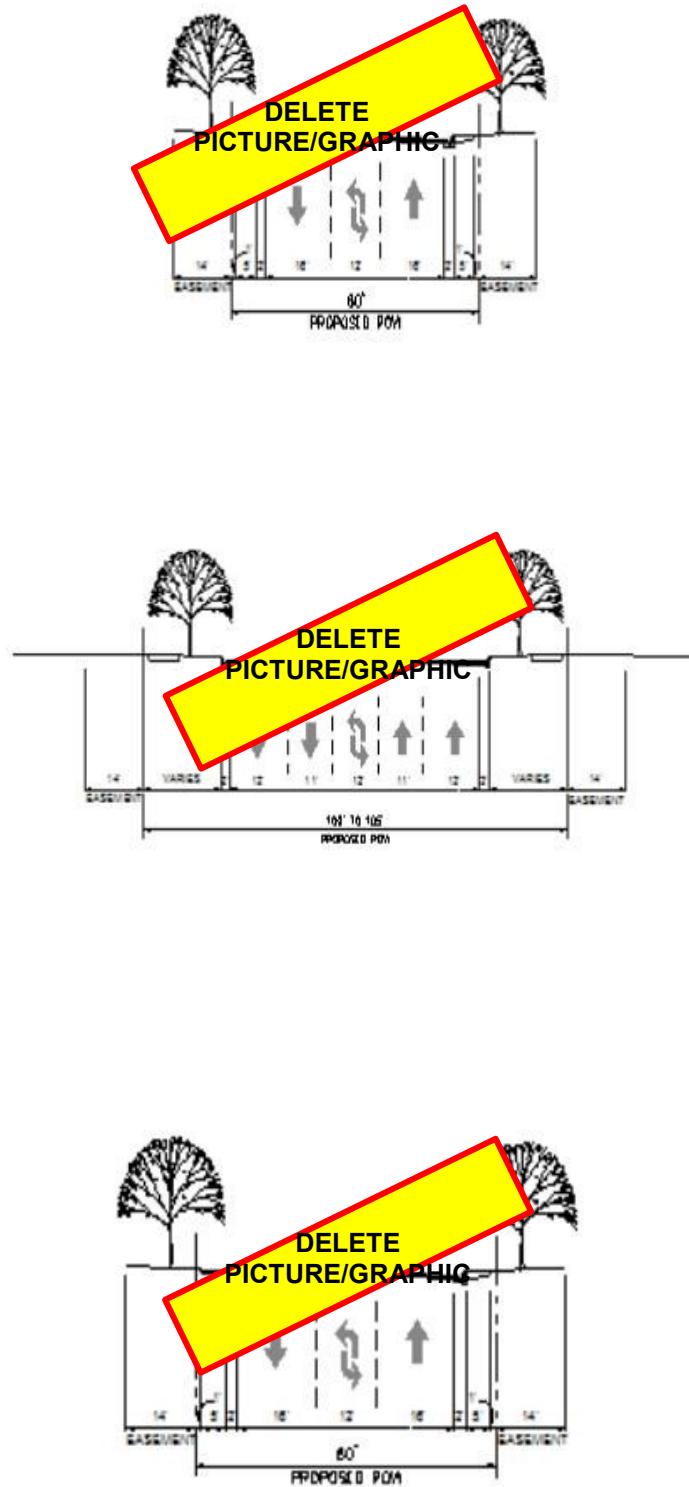
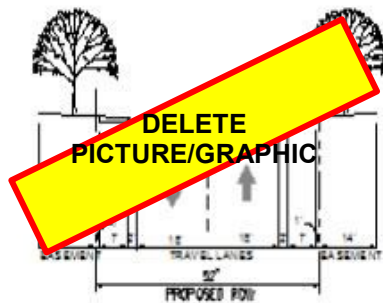
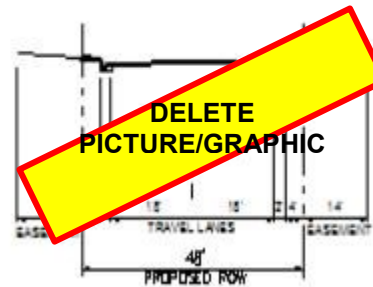
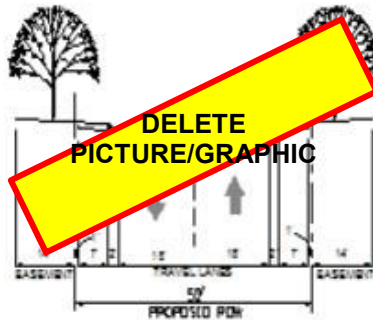


Figure 2.8: G Road Typical Section

Figure 2.9: Collector and Local Typical Sections



**25.08.050 Streetscape.**

The streetscape includes the treatment of trees, pedestrian elements, lighting, signs and other landscaping improvements that are located within or near the road right of way. The streetscape is an important feature of both the road and open space systems of the corridor because it contributes to the visual image and sense of quality of the “public realm,” provides vegetation and associated environmental benefits and helps to screen and soften the visual impact of structures and parking areas. Standard streetscape treatments are illustrated in Figures 2.5 through 2.9.

(a) **Standards.** Develop streetscape treatments for different categories of streets within the corridor to include the following:

(1) ~~24 Road. The ultimate five-lane section of this street shall be completed with a landscaped median, landscaped right-of-way on the west and east (including transitions to the Leach Creek natural corridor), street lighting and detached sidewalk on the west side. No sidewalk is planned for the east side because a multi-use trail is planned for the Leach Creek natural corridor. The interim three-lane section will have minimal landscaping and no sidewalks.~~

(2) ~~Patterson Road/F Road (East of 24 Road). The existing improved segment shall be completed with right-of-way landscaping, street lighting and detached sidewalks.~~

(3) ~~Patterson Road/F Road (West of 24 Road). Future improvements shall include right-of-way landscaping, street lighting and detached sidewalks.~~

(4) ~~G Road. Future improvements shall include right-of-way landscaping, street lighting and detached (drawing shows attached) sidewalks.~~

(5) ~~F 1/2 Road. Future improvements shall include right-of-way landscaping, street lighting and (drawing shows attached) sidewalks.~~

(6) ~~Collector Streets. Other collector streets shall include right-of-way landscaping, street lighting and attached sidewalks.~~

(7) ~~Local Streets. Local streets shall include right-of-way landscaping, street lighting and attached sidewalks.~~

(b) **Guidelines.**

(1) ~~In the development of streetscape treatments, recognize that existing development sites (such as northwest of the 24 Road and Patterson/F Road intersection) may be more constrained than greenfield development sites, and hence may require nonstandard solutions (such as attached sidewalks instead of detached sidewalks, and smaller parking and building setbacks).~~

#### **25.08.060 Key gateways, intersections and entries.**

~~Important intersections in the roadway network offer opportunities to develop a unique landscape theme and identity for the corridor. These intersections vary in scale and include the north and south "gateways" (near Mesa Mall and Interstate 70), along 24 Road intersections at Patterson/F Road, F 1/2 Road and G Road, intersections of collector and local streets and driveways to important sites (such as entrances into larger developments and Canyon View Park, for example). Each of these should be developed according to general concepts and criteria that are appropriate to their scale, function and importance. Concepts for key intersections are illustrated in Figure 2.10.~~

~~(a) Standards:~~

~~(1) Create gateways at the north and south end of the 24 Road Corridor that will visually identify the entries into Grand Junction (and specifically the 24 Road Corridor) by creating a “sense of arrival” (fountains, public art, paving patterns, landscaping, etc.), and to help promote the area to potential developers and tenants.~~

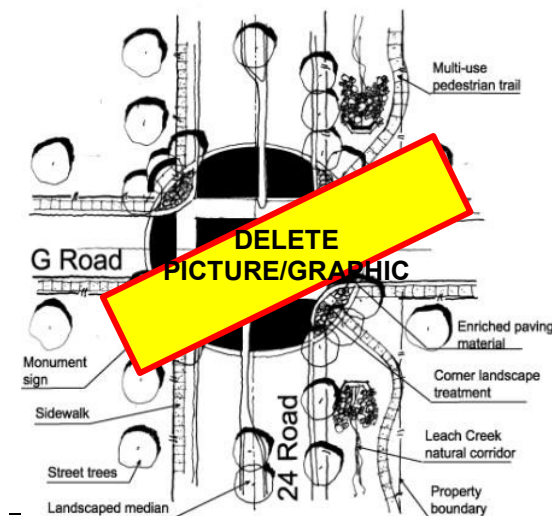
~~(2) Reinforce the “civic presence” of these gateways through special land treatment and other amenities, taking advantage of existing amenities such as Canyon View Park. Consider all four quadrants of the Interstate-70 interchange.~~

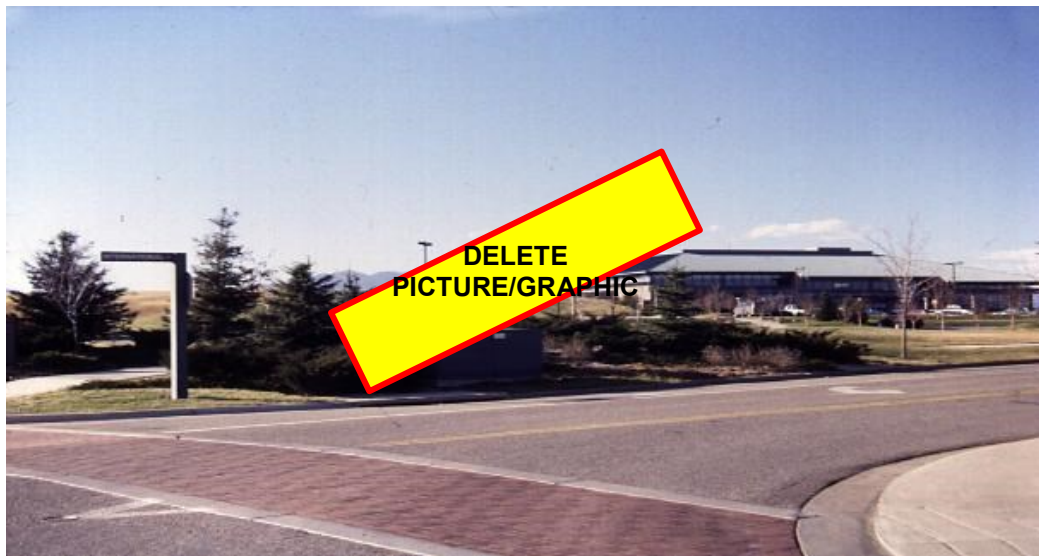
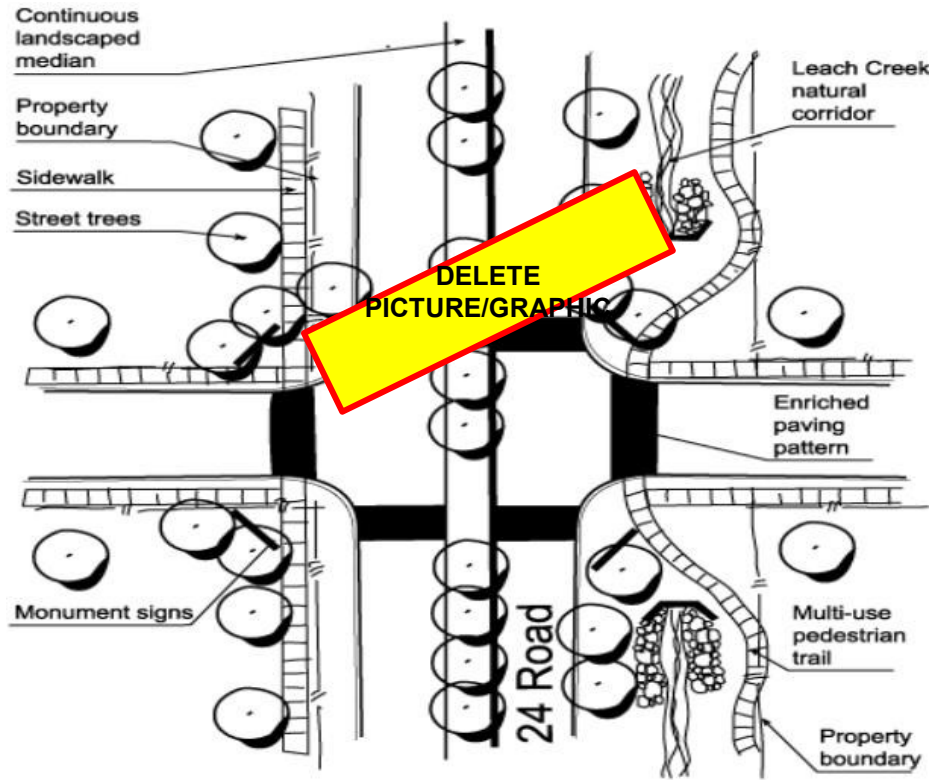
~~(3) Incorporate public art as appropriate into the design of gateways, key intersections and entries. Public art should generally constitute one percent of the cost of the project.~~



*Gateway signage at the freeway interchange identifies the local community*

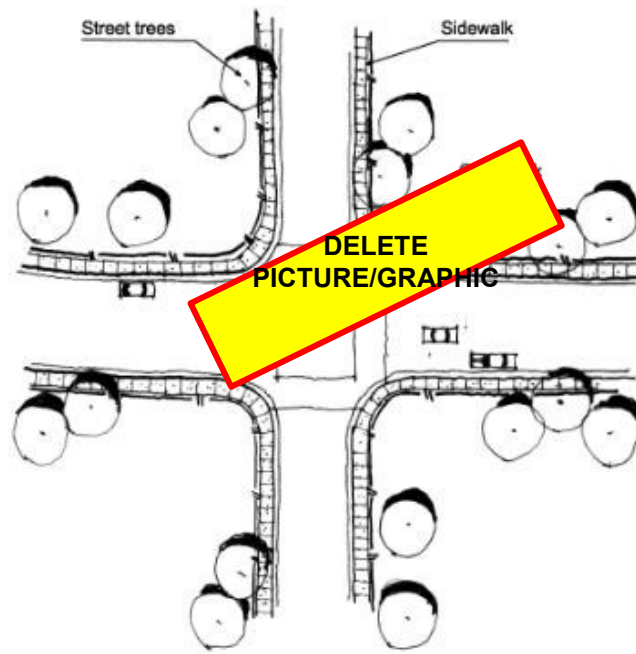
**Figure 2.10: Typical Intersection Sketches**





*Enriched paving at crosswalks identifies pedestrian crossing*





**25.08.070 View sheds.**

(a) Preserving views along the 24 Road Corridor is important to maintaining the special identity of the area and creating a “sense of place.” Currently, there are abundant and spectacular views of the Book Cliffs, Colorado Monument and Grand Mesa. The purpose of the view sheds standards is to preserve views of important natural features that are currently visible along the 24 Road Corridor, wherever feasible and desirable. Additional objectives include:

- (1) Maintain views into and from the natural open space corridor.
- (2) Preserve views to surrounding significant natural features currently visible from 24 Road (including the Book Cliffs, Colorado Monument and Grand Mesa).

(b) **Standards.**

- (1) Utilize supplemental building and parking setbacks along 24 Road and other key road corridors, to create additional open space in the front of buildings that will provide a view corridor. These setbacks are described in Chapter 25.12 GJMC, Site Development.
- (2) Investigate the potential to develop a specific view shed requirement through zoning.

(c) **Guidelines.**

- (1) Consider building height restrictions or build-to lines.
- (2) Plant trees in clusters to frame views or create “glimpses” of natural features.

Chapter ~~25.12~~ 25.02

SITE DEVELOPMENT

Sections:

<del>25.12.010</del>	<u>25.02.010</u>	<u>Purpose-Introduction.</u>
<del>25.12.020</del>		<u>Purpose.</u>
<del>25.12.030</del>		<u>Standards.</u>
<del>25.12.040</del>		<u>Guidelines.</u>
<u>25.02.020</u>		<u>Community Framework</u>
<del>25.12.050</del>	<u>25.02.030</u>	<u>On-site open space.</u>
<del>25.12.060</del>	<u>25.02.040</u>	<u>Organizing features.</u>
<del>25.12.070</del>		<u>Transitions and interconnections.</u>
<del>25.12.080</del>	<u>25.02.050</u>	<u>Site grading and drainage.</u>
<del>25.12.090</del>	<u>25.02.060</u>	<u>Building and parking setbacks.</u>
<del>25.12.100</del>	<u>25.02.070</u>	<u>Building location and orientation.</u>
<del>25.12.110</del>	<u>25.02.080</u>	<u>Parking, access, and circulation.</u>
<del>25.12.120</del>		<u>Multi-unit residential parking areas.</u>
<del>25.12.130</del>	<u>25.02.090</u>	<u>Auto-oriented uses.</u>
<del>25.12.140</del>	<u>25.02.100</u>	<u>Pedestrian and bicycle circulation.</u>
<del>25.12.150</del>	<u>25.02.110</u>	<u>Sidewalks.</u>
<del>25.12.160</del>	<u>25.02.120</u>	<u>Bicycle circulation.</u>

~~25.12.010~~ 25.02.010 Purpose-Introduction.

The location and design of buildings and site improvements are key to establishing the overall character and function of the 24 Road Corridor. Important site development considerations include large area development planning and coordination, the placement of buildings on the lot, relative amounts of area devoted to open space, buildings and parking areas, and vehicular and pedestrian circulation. ~~Prototypical site layouts for different types of development anticipated in the corridor are presented in Figures 3.1 through 3.4.~~

~~25.12.020~~ Purpose.

~~Because of the pattern of ownership of~~ With large undeveloped land parcels such as those in the 24 Road Corridor, there are several opportunities is an opportunity to take implement a “big picture” vision for the development of these large areas. The intent of this section vision for the Corridor is to increase pedestrian movement and encourage the creation of a development pattern of smaller lots and blocks that encourage a mixture of development types and increase pedestrian movement. ~~The planning for these large areas should encourage mixed uses, including a mix of~~ integrating housing products, commercial uses, neighborhood centers, shared parking opportunities, and the integration of different other appropriate land uses within neighborhoods and within buildings.

~~Lots and blocks are the fundamental “building blocks” of the 24 Road Corridor subarea pattern. During the development process, the large area planning is the time to establish in a flexible pattern of lots and blocks throughout the corridor that extends the scale of the existing development pattern in Grand Junction. The lots and blocks will to allow site planning flexibility and consistency of development patterns.~~

**~~25.12.030 Standards.~~**

~~(a) Block dimensions and proportions shall facilitate subdivision into lots that are generally regular in size and shape and avoid leftover parcels that are difficult to develop.~~

~~(b) All lots shall have frontage that is adjacent to a street. Street frontage shall typically not be less than 25 percent of the lot depth. Flag lots are strongly discouraged.~~

~~(c) Lots shall be designed to minimize conflicts between automobiles, trucks, bikes, and pedestrians, as well as to create an organized system of entrances, driveways, parking lots, and delivery areas.~~

~~(d) Neighborhood retail and service uses, and horizontally and vertically mixed uses shall be designed as part of an overall site plan that achieves an organized, walkable commercial area when completed.~~

**~~25.12.040 Guidelines.~~**

~~(a) Sites and buildings should be designed to provide edges or enclosure to streets and open space, to create linkages and gateways, as well as framing or terminating views.~~

~~(b) Consideration should be given to the opportunity for adjoining sites and buildings to share access, amenities, and relationships of form that will create a stronger overall identity.~~

~~(c) Large retail development should be organized in support of surrounding development in serving area housing, retailing and services, employment, and neighborhood public places.~~

~~(d) Blocks should range in dimension from 250 to 450 feet on any side, for a maximum of five acres.~~

**25.02.020 Community Framework**

The overall planning concept for the 24 Road Corridor includes a community framework (Figure 2.1) that provides a distinctive image and organizing element for public and private development. The community framework includes public streets, parks, open spaces, natural drainages and future stormwater management facilities that serve and connect part or all of the corridor.

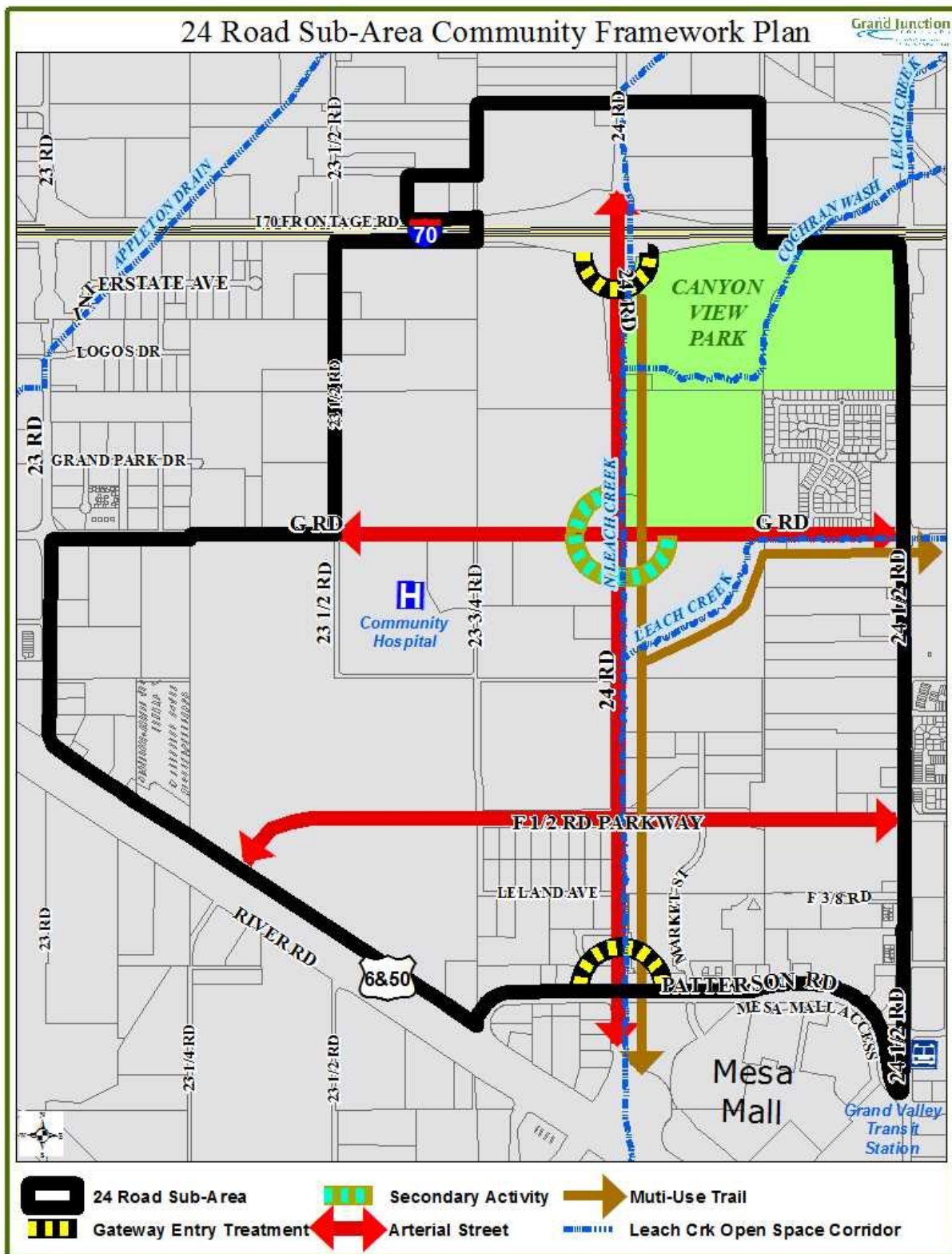


Figure 2.1: Community Framework Plan

**~~25.12.050~~ 25.02.030 On-Site Open Space.**

On-site open space ~~may be related to~~ could be provided within areas that are also set aside for stormwater drainage, landscaped setbacks and other privately owned landscape areas associated with on-site buffer landscaping and amenities. Open space shall be provided on-site so as to implement and comply with the following: ~~Residential development is also required to provide open space dedications or fees in lieu of dedications for the purchase of open space.~~

~~(a)~~ **Standards.**

(a)(1) Utilize well-defined natural and developed open spaces as features to serve as the focus of block, lot, and circulation patterns.

(b)(2) Supplement public open space such as parks and drainage corridors with privately developed open space that helps complete linkages and organize development. (Refer to open space requirements of zoning and development code.)

(c)(3) Open space, such as Leach Creek and developed parks and plazas, shall be used as a positive planning tool to organize and focus lot, block, and circulation patterns. Public access shall be provided to all public open space, natural and developed, directly from the public street/sidewalk system or through a public facility. Natural open space corridors and naturalized drainage ways (with trails) shall be publicly accessible at not less than 800-foot intervals.

~~(b)~~ **Guidelines.**

~~(1) Open space should be used to enhance the value and amenity of surrounding development. Left over, inaccessible, or nonusable open space should be avoided to the greatest degree practicable.~~

~~(2) Street, block, lot, and building patterns should respond to views, landscape, and recreational opportunities provided by proximity to natural open space.~~

~~(3) Developed, public, and common area private open spaces should be embedded into lot and block patterns and may be of a wide variety of sizes including small "pocket" parks and plazas. Lot and building frontages on public and private common areas are strongly encouraged. As long as street frontage and access is maintained, rear yards facing open spaces are strongly discouraged.~~

(d)(4) Buildings with frontage on open space ~~should~~ shall provide windows, doors, plazas, or other amenities and so forth to that encourage pedestrian activity and provide ~~informal over-sights of~~ views onto and/or are oriented toward the open space. ~~Mountain views along street corridors and from other public open space should be enhanced through careful consideration of building and landscape locations, massing, orientation, and height.~~



*Example of ~~Private~~ development oriented to open space*



*Examples of ~~privately~~ ~~Privately~~ developed and publicly developed open space amenities*

**~~25.12.060~~ 25.02.040 Organizing features.**

(a) **Purpose.** An organizing feature is a public open space around which development is focused. Organizing features ~~shall~~ can be included in large area planning to utilize natural open space, creeks, formal public spaces, streets, parks, and parkways to organize and coordinate development patterns. ~~They may qualify as required open space dedications and may be utilized to justify variation of required street alignments.~~ Organizing features should reinforce the pattern and orientation of streets and buildings through orderly arrangements of landscaping, pedestrian circulation and amenities, such as might be typical of a town square or campus quadrangle. Prominent amenities could be developed within open space to link building groups.



*Organizing features provide a focus for development*

~~(a)~~ **(b) Standards.**

- (1) At least one central feature or gathering place shall be located within a geographically distinct neighborhood, e.g., a convenient outdoor open space or plaza with amenities such as benches, monuments, kiosks, or public art. These places may be located on “civic blocks,” and may include buildings such as libraries, government offices, or public meeting places.
- (2) Buildings shall not orient rear, blank, or service dominated facades toward an organizing feature and shall include an entry that is visible, convenient to use, and connected to a public sidewalk by a direct route. Facades facing an organizing feature shall be of at least comparable architectural quality to other primary building facades.
- (3) Parking lots and parking spaces shall not be permitted within organizing features.
- (4) Multi-building developments shall use an organizing feature to create an internal campus-like arrangement of buildings and open space; provided, that the organizing feature is bounded along at least one side by a public street.
- (5) Organizing features shall utilize natural open space, creeks, formal public spaces, streets, parks, and parkways, where such are available on or adjacent to the site, to organize and coordinate development patterns.

~~(b) Guidelines.~~

- ~~(1) Organizing features may be used as a focus for related or complementary developments, particularly uses that include pedestrian activities. Organizing features may provide a transition area between diverse uses to provide both buffering and connection.~~
- ~~(2) The City should consider the variation of planned collector street alignments in order to accommodate building groups organized around developed open space features.~~
- ~~(3) An organizing feature should be publicly accessible and designed to organize the placement of buildings to create a sense of place, character, or identity within a neighborhood or district.~~



*Water features invite interaction*



*Fountain as an organizing feature*

**25.12.070 Transitions and interconnections.**

~~(a) Purpose. Transitions and interconnections can create neighborhoods among different land uses on adjacent parcels. The transitions and interconnections can be natural or manmade features, such as open spaces, drainage corridors, streets, sidewalks, and/or trails.~~

~~(b) Standards.~~

- ~~(1) Significant shifts in the scale of development, such as lot size and building size, shall occur across rear lot lines, alleys, open space or arterial streets. In order to promote consistent street character and scale, developments of significantly different scale shall not face each other across a local street. This is not intended to discourage compatible uses of differing scales, such as retail and office, from locating together, nor is it intended to prevent small scale, neighborhood serving retail uses from integrating into residential areas.~~
- ~~(2) Transitions between differing uses and scales of development shall allow for interconnections at a logical scale. Transitions shall not be defined with privacy walls or fences.~~
- ~~(3) Where incompatible scale or activities cannot be mitigated through adequate transition, buffering and screening shall be required. Buffering (horizontal) and screening (vertical) strategies shall consider building and parking placement, building orientation, walls, fences, and landscaping.~~



~~(4) Residential development within a commercial mixed-use project shall provide all necessary buffering and screening from other allowed uses within that district to ensure satisfactory maintenance and permanence.~~



*Retail establishment adjacent to residential building*

~~(c) Guidelines.~~

~~(1) Compatible scale should be considered in terms of lot size, building dimensions, building placement, and orientation. Where practicable, similar sized lots or buildings should face each other across local streets, but not to the detriment of achieving an appropriate mix of uses at edges of neighborhoods. Transitions of development scale are best accomplished laterally across side streets, side and rear lot lines, and across collector or arterial streets or natural features.~~

~~(2) New development should relate to other existing or proposed development on adjoining top properties to maximize useful interconnections and shared efficiencies.~~

~~(3) Important views and vistas, both natural and manmade, should be used as opportunities to create edges or to align public spaces and corridors to enhance the quality of the public experience. Views along public rights of way and public open spaces are of major importance. (Refer to GJMC [25-08.070](#) for additional information about view sheds.)~~

~~(4) Where development is phased, early phases should establish the long term image of the project and its relationship to the streets, open space, and adjoining development.~~

~~(5) Gated communities are discouraged.~~



*Mixed use development incorporating multifamily housing and grocery store*

**25.12.080 25.02.050 Site grading and drainage.**

~~(a) Purpose.~~ The site grading and drainage standards and guidelines ensure that development fits within existing topography, reinforces the community open space framework, and effectively diverts and retains stormwater. In addition to other site grading and drainage requirements of the Municipal Code, the following standards shall apply in the 24 Road Corridor:

- ~~(1) Encourage site grading that reflects the gentle topography of the existing landscape — rather than abrupt changes in slope or extreme.~~
- ~~(2) Establish an overall stormwater management plan for the planning subarea and/or drainage basin (this plan should encourage regional detention solutions).~~
- ~~(3) Utilize stormwater detention and conveyance facilities as part of the open space system.~~
- ~~(4) Provide for smooth grading transitions between adjacent development lots.~~

~~(b) Standards.~~

~~(a)(4)~~ Unless precluded by soil conditions, graded slopes shall not be steeper than 3:1 slope. Where space limitations demand, terracing with retaining walls is the preferred solution.

~~(2) Water quality “best management practices (BMPs)” shall be employed for all stormwater drainage facilities.~~

~~(3) Within development sites, concentrated drainage across walkways and other pedestrian areas is not permitted. Drainage across driveway entries is prohibited.~~

~~(b) (4) All open space drainage facilities shall be landscaped with a natural “naturalized” treatment (for example, similar to the treatment recommended for the Leach Creek corridor). Existing riparian areas and drainages, such as Leach Creek and its tributaries, shall be developed and maintained as natural open space corridors that provide surface drainage and developed pedestrian trails. The treatment of surface drainages shall be an open channel with gently sloping sides and naturalized landscape, except for the west bank of Leach Creek which is constrained by the 24 Road improvements. (See Figures 2.2 & 2.3.)~~

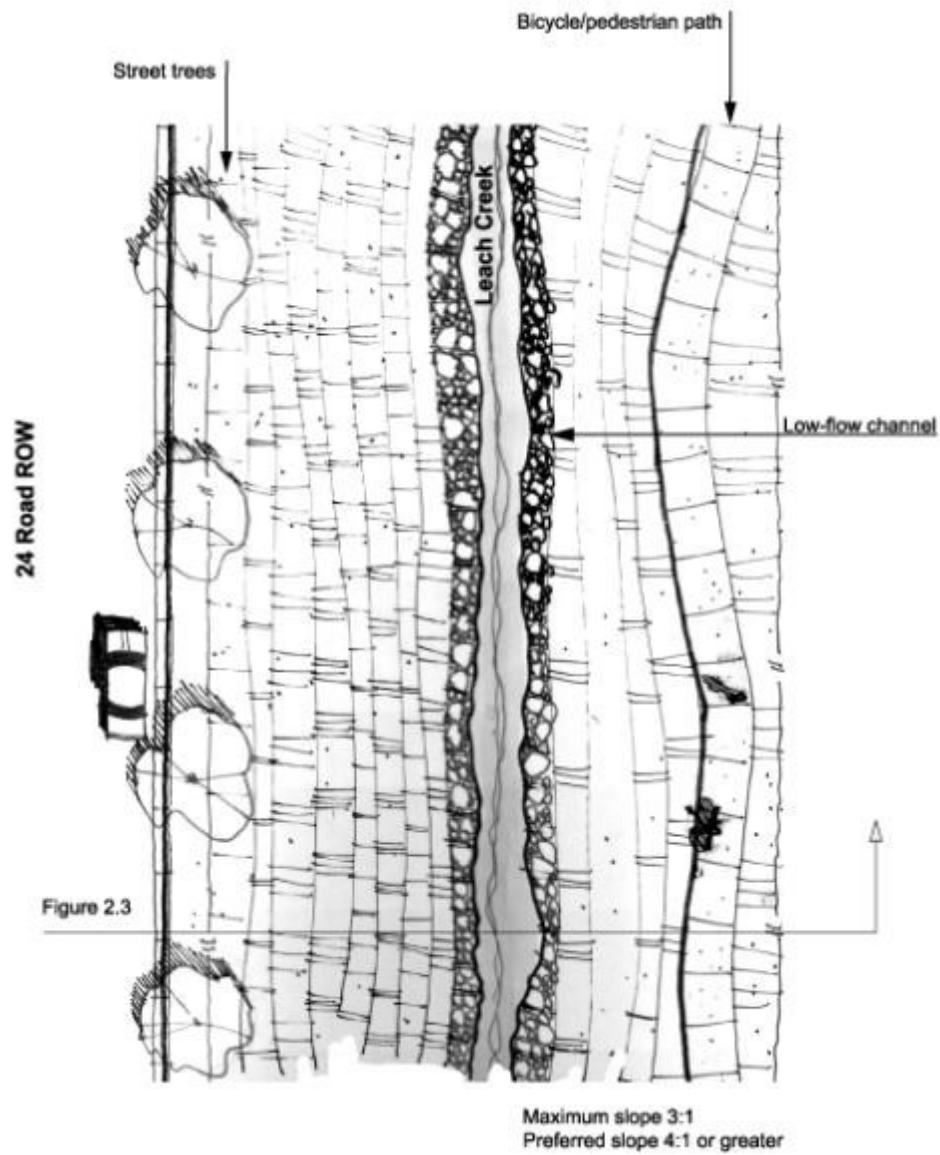


Figure 2.2: Natural Corridor Plan



Figure 2.3: Natural Corridor Section

(5) Provide open space for residential sites in accordance with City of Grand Junction requirements. Locate this open space in a manner that it is usable for the project and connects with the off-site open space system.

~~(6) Open space for nonresidential sites is primarily for visual purposes, to provide screening and to reduce the impact of large parking areas.~~

~~(7) Drainage to adjacent sites should be limited to historic flows.~~

~~(c) **Guideline.**~~

~~(1) Wherever possible, combine drainage facilities with common open space to achieve aesthetic and functional facilities that maximize use of resources, land and which provide benefits to residents, employees and other users.~~



*Open space incorporates drainage facility and amenities*



*Buildings at the edges of project site with pedestrian connections through parking areas*

**25.12.090 25.02.060 Building and parking setbacks.**

(a) ~~Purpose.~~ **Purpose.** The building and parking setback standards and guidelines are intended to establish a coordinated streetscape image, and provide sufficient space between buildings, roads and parking areas for adequate light, and privacy, views, and to provide sufficient space between roads, buildings and parking areas for privacy, sound control and landscaping.

~~The City should establish setbacks to achieve the following objectives:~~

- ~~(1) Provide an additional landscaped area on the lot perimeter that supplements minimal City requirements.~~
- ~~(2) Preserve views from 24 Road and other road corridors.~~
- ~~(3) Encourage the “village concept” in new development, such as minimizing dependence on the private automobile, and encouraging pedestrian activity and use of alternative modes.~~

(b) **Standards.**

- ~~(1) Minimum building and landscape setbacks according to zoning districts are provided in the zoning and development code.~~

(2) In order to establish additional open space and maintain views, ~~establish supplemental building and parking setbacks and parking setbacks along key road corridors. These are~~ shall be provided as specified in Table 3.1.

**Table 3.1: Supplemental Building and Parking Setbacks**

<b>Frontage</b>	<b>Building Setback</b>	<b>Parking Setback</b>
24 Road – west ROW	50'	25'
24 Road – east ROW	35' from edge of Leach Creek corridor	10' from edge of Leach Creek corridor
Patterson Road (F Road)	Per zoning	10'
F 1/2 Road	Per zoning	10'
G Road	Per zoning	10'

Notes:

(a) Width of Leach Creek drainage corridor to be determined and will include multi-use trail. (See Figure 2.3.)

(b) All measurements are from the right-of-way.

~~(3) Driveway setbacks from adjacent property lines, other than along street frontages, shall be a minimum of 10 feet, except where access driveways are shared by adjacent owners and must comply with City standards.~~

~~(4) To establish an open landscape area at key intersection corners, no building or parking lot shall be located less than 35 feet from the intersection of the right-of-way of two public streets (arterial, collector and local).~~

~~(5) All areas within the parking setbacks, and the building setback if there is not an intervening parking lot, shall be landscaped and irrigated.~~

~~(6) No side or rear parking setback is required between adjoining industrial uses; provided, that the subject area is not visible from any public street, nonindustrial land use or common open space. Other parking setbacks are in accordance with zoning and development code regulations.~~



*Coordinated landscaping in the building setback complements ROW landscaping*

**~~25.12.400~~ 25.02.070 Building location and orientation.**

~~(a) Purpose.~~ The purpose of the building location and orientation standards and guidelines is to To encourage the development orientation of buildings that provide orientation and access toward the street, ~~reinforcing~~ reinforce the character and quality of public streets and sidewalks, maximize useful interconnections, and enhance the appearance of the properties from the street, the following standards shall apply:   

~~(b) Standards.~~

~~(4a)~~ No development shall be permitted to place or orient buildings, parking, circulation, or service facilities on a lot in such a way as to treat primary street frontage(s) as a rear lot line. “Rear” shall be defined to mean a portion of the property lacking public access and containing a predominance of service functions and/or service facilities that significantly diminish the architectural or landscape quality of the development.

~~(2b)~~ All sides of a building frontages that are visible from a street, residential area, public park or organizing feature shall have the equivalent architectural treatment of the primary building façade, and completely screen all service and loading facilities.

(c) All service and loading facilities that are visible from a street, residential area, public park or organizing feature shall be completely screened from view.

~~(3d)~~ Nonresidential uses located in the same block with residential uses shall be located on higher traffic streets and at the periphery or the end of each block having both uses and be effectively screened of light, noise, and pollution from service area or other incompatible activities.



(4e) Large retail buildings (over 20,000 gross square feet) shall be located to minimize the impact of windowless walls and service areas on public streets. On sites that include large retail buildings, smaller buildings in-line or on pads shall be located to form edges that frame and reinforce the space and appearance of public streets. Pad buildings shall locate at least one facade including windows and similar architectural features within 35 feet of the public right-of-way. Pad buildings shall be located at site corners and entries.

(5f) All sites ~~abutting adjoining~~ 24 Road shall treat 24 Road as a primary frontage orientation with regard to the quality and orientation of buildings, site design, architecture~~architectural features~~, and parking area design. This shall be in addition to any other required access and orientation. Primary frontage is intended to include landscape and building design that conveys the project identity and character and is of equal or superior quality to any other frontage of the project. No truck docks or service areas shall be permitted to face 24 Road.

~~(c) Guidelines.~~

~~(1) Site planning should relate as much as possible to the existing or proposed development on adjoining properties, maximize useful interconnections, and enhance the appearance of the properties from the street.~~

~~(2) Buildings should be located as close to the street as possible, after setback and/or build to zone requirements have been fulfilled.~~

~~(3) Smaller in-line tenant spaces may be “saddle-bagged” onto the outside of large retail buildings to relieve large blank facades and provide activity fronting streets or parking areas.~~

~~(4) The front setback of commercial uses on collector or local streets may be paved as an extension of the pedestrian zone with decorative paving and street trees in grates. Primary building entry should be located directly accessible and adjacent to the public sidewalk.~~

~~(5) I-70 frontage and U.S. 6 and 50 frontage is of secondary importance to 24 Road frontage.~~

**25.12.110 25.02.080 Parking, access, and circulation.**

~~(a) Purpose.~~ The purpose of the vehicular circulation and parking standards and guidelines is to In order to provide for safe and convenient movement of pedestrians and motor vehicles, limit vehicular/pedestrian conflicts, reduce paved areas, provide screening for paved areas and soften the visual impact of parking lots by providing interior planting, ~~breaking up large lots into smaller increments.~~ the following standards shall apply:

~~—Comprehensive, multi-site parking strategies should minimize redundant access and maximize open space and landscaping as well as convenient auto and pedestrian circulation within and between sites. The standards and guidelines will contribute to the creation of a clearly organized system of entrances, driveways, parking areas, and pedestrian circulation.~~



*Sidewalks and landscaping break up and soften the visual impact of large parking lots into smaller increments*

~~(b) **Standards.**~~

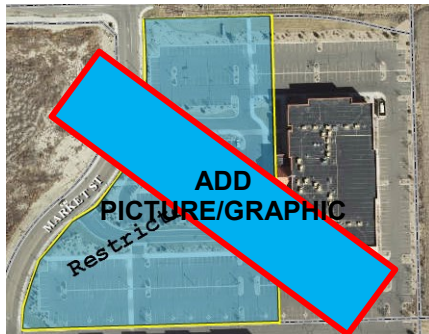
~~(1) Contiguous developments shall coordinate circulation to minimize curb cuts. Access for each lot will be reviewed with the project's overall traffic circulation and capacity needs, and located according to City of Grand Junction standards. Direct driveway access to arterials, streets, and parkways shall minimize the number of driveways per block frontage.~~

~~(2) Shared parking and circulation is encouraged wherever practicable.~~

~~(a) (3) No more than two double-loaded bays of parking, with a maximum length of 125 feet, shall be allowed in front of buildings smaller than in neighborhood retail (i.e., consumer retail and service, small-scale uses less than 20,000 square feet in size). The maximum allowable length of the parking area in front of retail buildings is 125 feet.~~

~~(b) (4) For retail buildings larger than 20,000 to 30,000 square feet in size, no more than 50 percent of the total surface parking area shall be located in the *restricted area* between buildings and arterial streets the street and a building in the restricted area. For retail buildings larger than 30,000 square feet, no more than 60 percent of the total surface parking area shall be located in the *restricted area*. the allowed parking in the restricted area may be enlarged to 60 percent.~~

The restricted area is defined by that area located between the front facade of the principal structure and the primary abutting street. The restricted area shall be determined by drawing a line from the front corners of the building to the nearest property line as shown in the example. ~~lines extending toward the street at 45 degrees outward from the center of the building.~~



Example of Restricted Area

- ~~(5) Vehicle access and circulation into retail sites shall be provided at each adjoining cross street, unless traffic safety precludes access. No perimeter of a retail site shall exceed 600 feet in length without vehicle access, except along arterials.~~
- ~~(6) All parking lots shall have an identifiable internal circulation pattern. Vehicle circulation on site shall be clearly organized to facilitate movement into and throughout parking areas. Parking drive lanes and intersections shall align wherever practicable. On-site intersections shall be located to preclude stacking of vehicles across intersections and onto public streets.~~
- ~~(c) (7) No truck parking or docks are permitted. Service entrances, service yards and loading areas shall not be located on a side of any building that faces facing 24 Road, any arterial street, I-70, or residential uses.~~
- ~~(8) Access drives for nonresidential and large scale multifamily uses shall be coordinated with other access drives. Access drives across from other existing or planned drives shall meet City standards.~~
- ~~(9) Minimum parking quantities and stall dimensions shall be in accordance with applicable City of Grand Junction standards.~~
- ~~(d) (10) Developments bounded by undeveloped parcels shall consider potential e Opportunities for future auto, pedestrian, and bicycle connections to adjoining sites shall be considered in all site design. Such connections shall be provided where reasonably practicable.~~
- ~~(11) Sites requiring large areas of surface parking shall attempt to distribute parking into smaller areas broken up by intervening areas of landscaping, open space and buildings wherever possible, rather than aggregating parking into continuous street facing strips.~~



*Examples of pedestrian circulation systems within parking lots*

**(c) Guidelines.**

- (1) ~~Driveway and curb cut widths should be minimized at the sidewalk to reduce their impact on the location of street trees and maximize the continuity of the tree lawn.~~
- (2) ~~Parking directly adjacent to buildings should be avoided wherever possible. A minimum setback of 15 feet shall be reserved for pedestrian circulation and landscaping between building and parking areas except for drop off and loading zones. This distance may be reduced to 10 feet in the industrial areas and may not require landscaping, depending on its proximity to streets and common open space.~~
- (3) ~~Driving lanes should not be provided between the building and adjacent public streets, sidewalks, or amenity zones. Off street parking for small retail uses is strongly encouraged to be located behind the structure.~~
- (4) ~~Parking and vehicular circulation between the street and building should be limited where possible. Shared parking and circulation is encouraged wherever practicable. Auto-oriented and drive-through uses, where permitted, should locate drive-through lanes away from street frontage. Automobile gasoline service stations should orient parking, car wash, and service bays away from view of arterial streets.~~
- (5) ~~Wherever possible, office parking for employees should be located behind the building, with visitor parking between the building and the street.~~

**25.12.120 Multi-unit residential parking areas.**

(a) **Purpose.** ~~The purpose of these standards and guidelines is to establish residential streets and their associated open space as positive, useable features around which to organize the location and orientation of buildings in a manner that promotes a sense of security and community.~~

**(b) Standards.**

- (1) ~~No more than one double-loaded bay of parking shall be permitted between the street and the building's street-oriented frontage.~~

~~(2) Parking lots, garages, carports, and building service areas shall be located so that their presence and access requirements minimize disruptions to adjoining public streets, sidewalks, and open space.~~

~~(3) Carports serving more than two vehicles shall not be permitted to be accessed directly from the street and shall use buildings or landscaping to screen parked cars from the view of public streets and open space. Carports are not allowed within the front setback area.~~

~~(c) Guideline.~~

~~(1) Back-out parking spaces into the major circulation system of development larger than 100 dwelling units are discouraged.~~



*New multifamily housing with garages in alley*



*Sidewalk and landscaping buffer pedestrians from traffic on arterial streets*

**25.12.130 25.02.090 Auto-oriented uses.**

~~(a) Purpose. The purpose of the auto-oriented uses standards and guidelines is to To minimize impacts of auto circulation, queuing, drive-up facilities (including speaker systems and similar activities) and to promote on street-oriented building design, and pedestrian amenities, and orientation. the following standards shall apply:~~

~~(b) Standards.~~

~~(a)(1) Auto-oriented and drive-through uses, where permitted, shall locate drive-through lanes away from street frontage. Drive-up and drive-through facilities (order stations, pick-up windows, bank teller windows, money machines, car drop-off areas for auto service or rental, etc.) shall be located on the side or rear of a building and away from residential uses.~~

~~(b)(2) For buildings greater than 100 feet from the street and with no intervening buildings, drive-through windows may be allowed to face a perimeter street, and drive-through lanes may be allowed with adequate landscaping buffer from the right-of-way line.~~

~~(c) Guidelines.~~

~~(1) Automobile gasoline service stations should orient parking car wash and service bays away from street views.~~

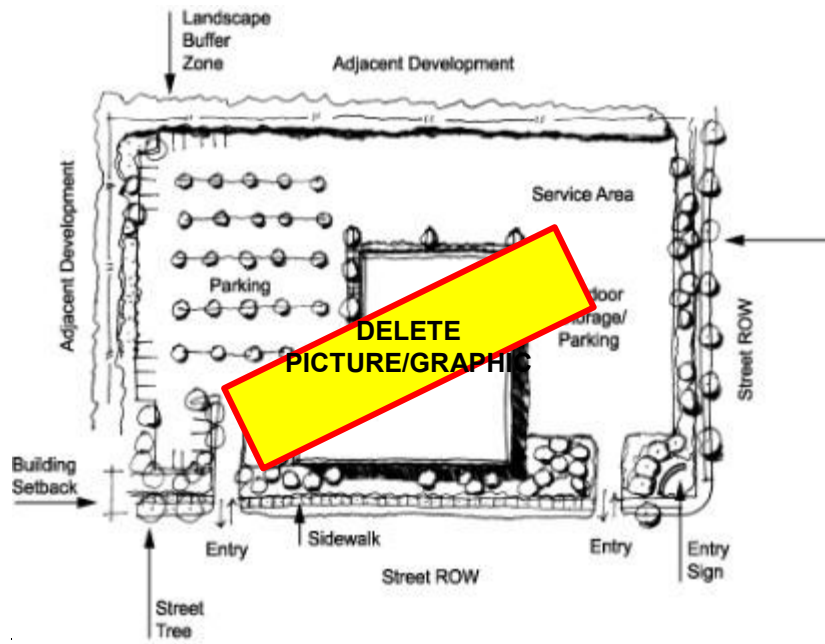


Figure 3.1: Industrial Site Plan

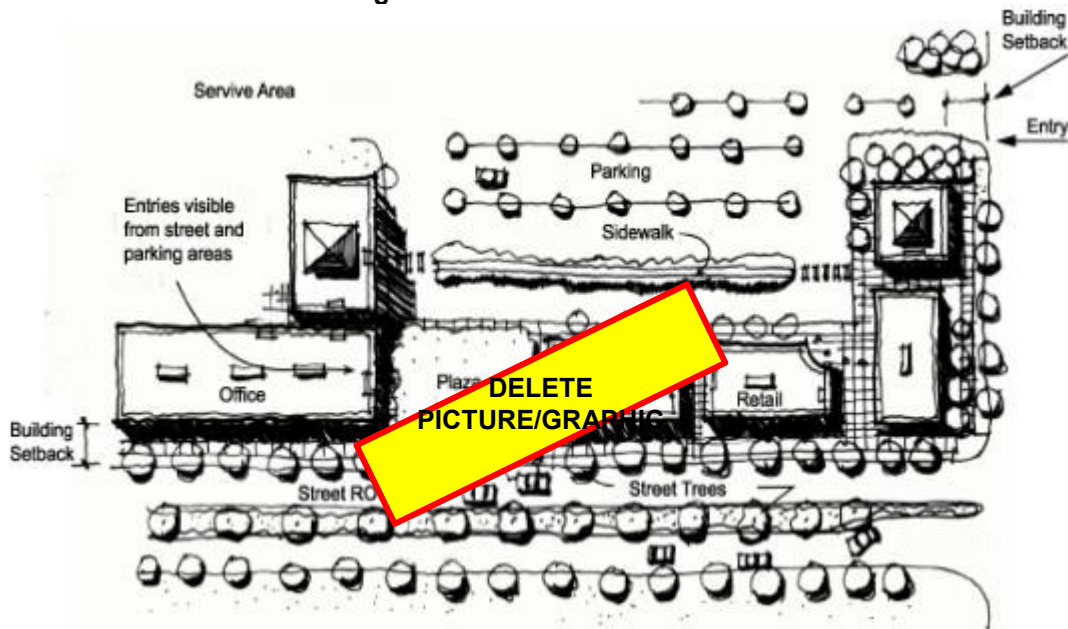
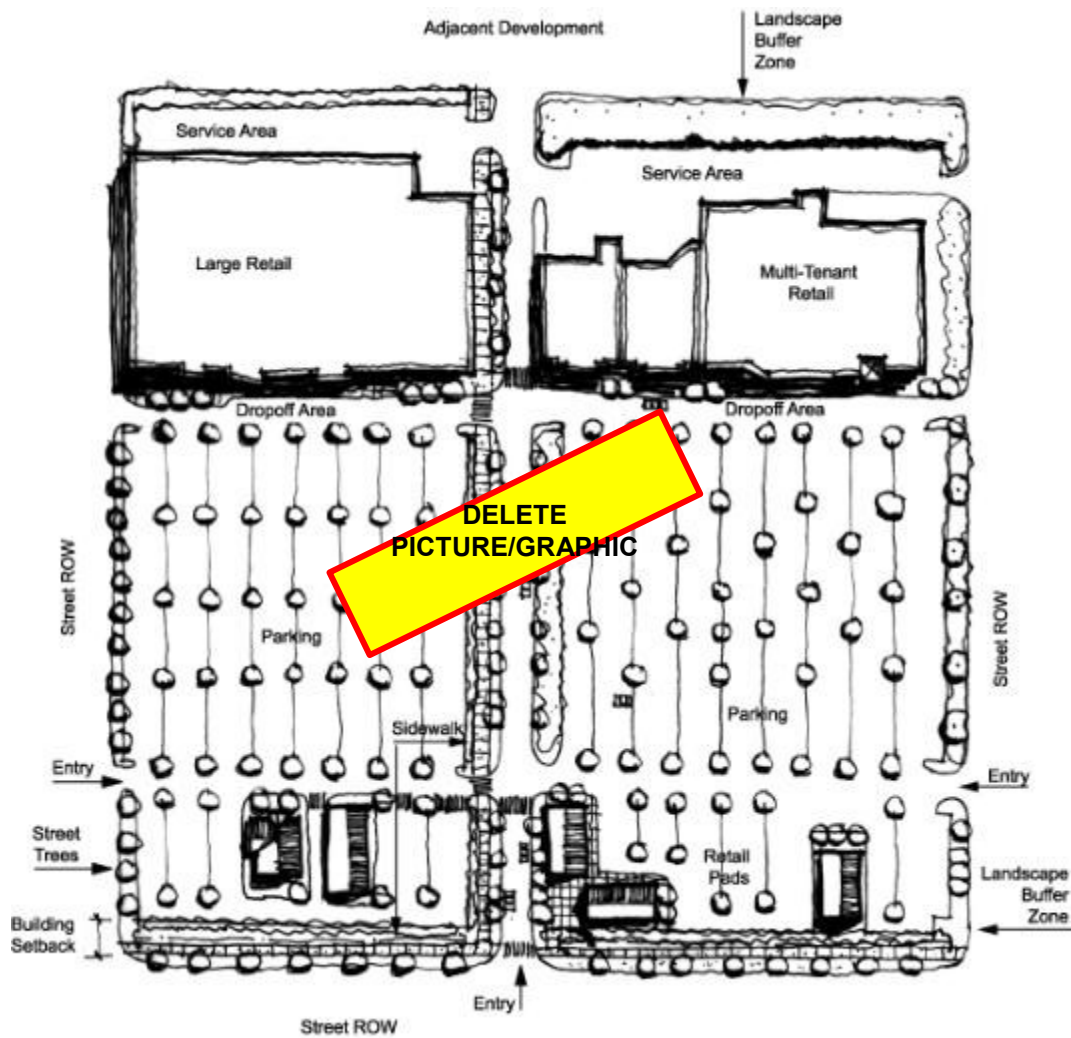


Figure 3.2: Office/Mixed Use Site Plan



**Notes:**

- Use similar materials, roof forms and architectural styles.
- Buffer loading docks and trash storage areas with landscaping and fencing.
- Arrange pad buildings to create: a project gateway; interest along the street; shared entries; outdoor eating areas; and parking.
- Place project "monument" signs along roadway.
- Group street trees at entries, and along major pedestrian and vehicular routes.
- Provide continuous pedestrian pathways, lighting and building entries close to transit facilities.
- Incorporate art, plazas, and other amenities where possible.

Figure 3.3: Retail Site Plan

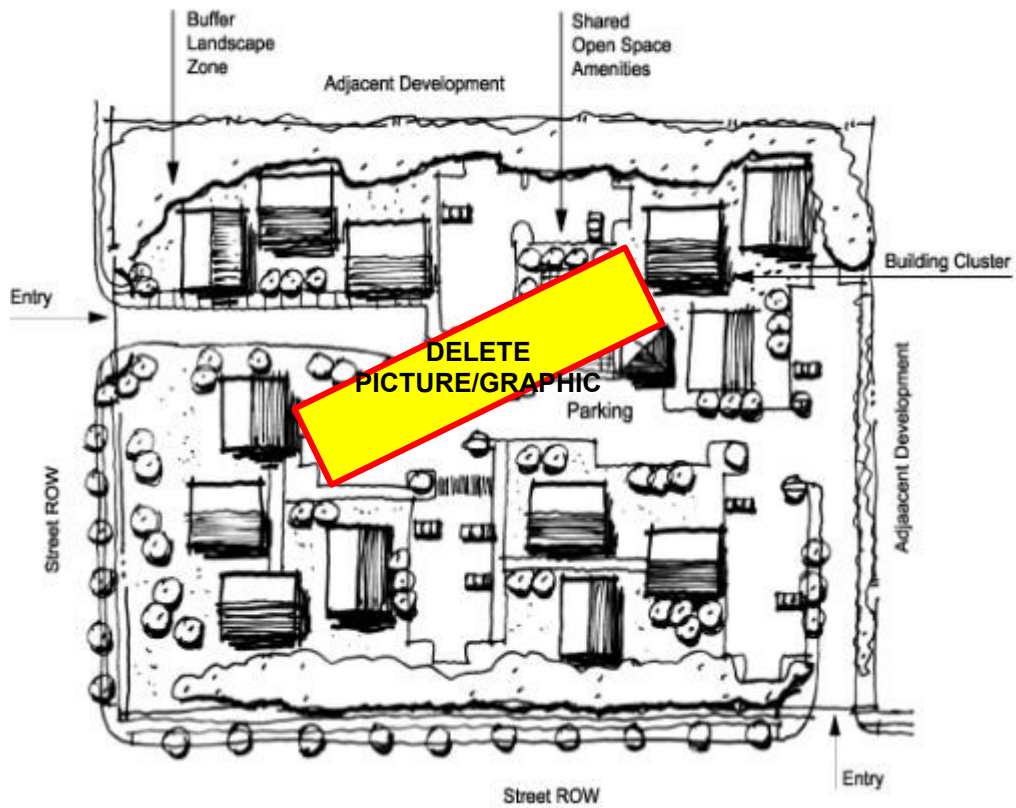
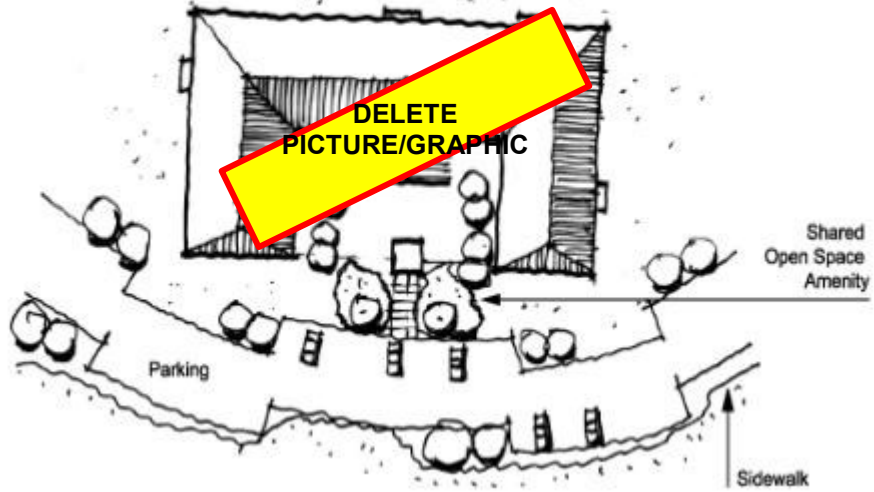


Figure 3.4: Multifamily Residential Site Plan



**25.12.140 25.02.100 Pedestrian and bicycle circulation.**

(a) ~~Purpose.~~ The purpose of the pedestrian and bicycle circulation standards and guidelines is to To promote free efficient and safe movement of pedestrians and bicyclists through the 24 Road Corridor. ~~This will provide sidewalk and/or multi-use trail links shall be provided between development sites and open space, including but not limited to an off-street multi-use trail connecting Canyon View Park and the Colorado River Trail. by way of sidewalks and multi-use trails.~~



*Direct pedestrian access from sidewalk*



*Pedestrian circulation in retail area*

**25.12.150 25.02.110 Sidewalks.**

(a) ~~Purpose.~~ The purpose of the specific standards and guidelines for sidewalks is to provide continuous opportunities for pedestrian movement through the corridor.

(b) ~~Standards.~~

To provide opportunity for continuous pedestrian movement through the 24 Road Corridor, the following standards shall apply:

(a)(1) A direct pedestrian connection to the building entry shall be provided from the public sidewalk.

(b)(2) Development shall provide pedestrian circulation from public walks to parking areas, building entries, plazas, and open spaces. Walkways shall be provided to separate pedestrians and vehicles, and shall link ground level uses. Primary walks shall be a minimum of five feet wide.

(c)(3) Clear and safe pedestrian routes shall be defined through parking areas to provide pedestrian access between buildings with minimum conflicts with vehicles. ~~Where walks~~ Walkways crossing drive aisles, they should ~~shall~~ be clearly marked with signage, special paving, landscaping or other similar methods means of demarcation.

(d)(4) Sidewalks, ~~no less than at least eight feet wide in width,~~ shall be provided along any all building facade abutting facades that about public parking areas and featuring feature a public entrance. ~~Such sidewalks shall be located at least six feet from the facade of the building to provide planting beds for foundation landscaping, except where features such as arcades, or entryways, or display windows are part of the facade, or street trees in grates or planted areas no less than 40 feet on center) are provided along the length of the facade.~~

(e)(5) Pedestrian walks incorporating bicycle lanes shall be not less than 10 feet in width.

~~(f)(6)~~ All parking areas located between a street and building shall include a landscaped pedestrian walk linking the building with a public sidewalk.

~~(g)(7)~~ ~~Contiguous developments shall avoid erection of physical~~ Physical barriers between projects contiguous development shall be prohibited unless necessary for safety or the mitigation of adverse impacts.

(h) Development shall provide pedestrian linkages between the Leach Creek trail corridor and other public ways and open space areas.



*Shared plaza at building entries*



*Landscaped public sidewalk*

~~(c) Guidelines.~~

~~(1) All building entries, parking areas and public open space should be interconnected through convenient systems of pedestrian walks.~~

~~(2) Adjoining developments should create opportunities for interconnected pedestrian walk systems to facilitate pedestrian access between different developments, buildings, activities and uses; however, in no circumstance should on-site pedestrian walks substitute for required public sidewalks.~~

**~~25.12.160~~ 25.02.120 Bicycle circulation.**

~~(a) Purpose.~~ The purpose of the bicycle circulation standards and guidelines is to To provide a safe, direct, and attractive system of interconnected public ~~and private~~ bikeways and bike routes throughout the corridor and ~~to accommodate bicycle access by providing~~ defined routes to primary building entries, the following standards shall apply:

~~(b) Standards.~~

~~(1) Bicycle access shall be provided between bicycle lanes or multi-use trails and on-site bicycle parking areas.~~

~~(2) Two-way bikeways that are not combined with drives or parking lot surfaces shall be of concrete.~~

~~(3) Bicycle parking shall be provided in accordance with the City of Grand Junction requirements.~~

~~(c)~~ **Guidelines.**

~~(a)(4)~~ Bicycle parking ~~should~~ shall be located in visible, active, and well-lit areas; near building entries, convenient to primary bicycling access, and not encroaching on pedestrian walkways. If possible, ~~locate racks where parked bicycles are~~ bike parking shall be located so it is visible from the inside of adjacent buildings.

~~(b)(2)~~ Bicycle circulation ~~should~~ shall connect and align with pre-existing and planned off-site bicycle routes. Crossings at intervening streets ~~should~~ shall be located where safe means for crossing can be provided. .



*Public bikeway provides an interconnected system*



*On-street bicycle route designation separate from the roadway*

**Chapter 25.16**

**LANDSCAPE DEVELOPMENT**

Sections:-

- ~~25.16.010~~ Introduction.
- ~~25.16.020~~ Standards.
- ~~25.16.030~~ Guidelines.
- ~~25.16.040~~ Parking lots.
- ~~25.16.050~~ Streetscape within the public right-of-way.
- ~~25.16.060~~ Irrigation.

**25.16.010 Introduction.**

~~Landscape improvements are of primary importance to the establishment of the design character of the 24-Road Corridor. They are intended to enhance the landscape appearance through the use of common materials, to promote a well-maintained appearance in areas not covered by buildings or parking, to minimize the adverse visual and environmental impacts of large paved areas and to promote the conservation of water. Xeriscape and “naturalized” concepts are encouraged, particularly in large natural open spaces and passive use areas.~~



*A wide range of plant materials are appropriate for local landscaping requirements*

**25.16.020 Standards.**

~~(a) The City of Grand Junction Development and Zoning Code requirements for landscaping shall be in full force and effect unless specifically superseded by more stringent criteria herein.~~

~~(b) Landscape design for individual lots shall be developed according to a landscape plan. Each building or cluster of buildings within each development shall provide a plan that indicates all planned landscape materials, and their location, minimum size, quantity, and irrigation. All of the landscaped site area shall be included in one of the following categories:~~

- ~~(1) Landscaped and irrigated;~~
- ~~(2) Low water landscapes;~~
- ~~(3) Native landscaping within drainage areas.~~

~~(c) All land areas not covered by buildings, streets, paved areas, or other planned and approved surfaces shall be planted with living plant material and mulches.~~

~~(d) Along arterial street frontages, landscape treatment shall be maintained to the greatest degree possible with allowance for required access drives.~~

~~(e) Landscape areas shall be continuous from one lot to another and shall incorporate landscape materials that are compatible with landscaping an adjacent lots, public streets, drainage corridors, and landscape easements.~~

~~(f) All plant material used shall meet the minimum standards established by the American Association of Nurserymen, as published in the American Standards for Nursery Stock (comply with ANSI Z60.1).~~

#### **~~25.16.030 Guidelines.~~**

~~(a) All development areas should recognize the unique climate, open character, and gentle open topography of the western slope environment and should employ development, construction and landscape forms, materials, and methods that are appropriate to that environment.~~

~~(b) Landscaping should visually frame buildings and buffer parking, garage, and service areas. It should define and enhance the sense of arrival at appropriate site locations.~~

~~(c) Landscape pattern of adjacent lots should be consistent.~~

~~(d) Landscaped areas bordering natural open space should create a transition from developed and irrigated landscape to natural unirrigated landscape and vegetation.~~

~~(e) Landscaping should be used to mitigate areas of large undifferentiated building mass and screen walls.~~

~~(f) Water conserving planting design and irrigation practices should be employed.~~

~~(g) Existing healthy trees or other significant landscape features should be preserved to the greatest extent practicable. Existing damaged, decayed, or diseased trees or scrub vegetation should be removed.~~

~~(h) Construction near existing trees should follow established practices to ensure their survival.~~

~~(i) Substitute irrigation should be supplied to trees or other vegetation that have natural or drainage water diverted or eliminated due to site development or construction.~~

#### **~~25.16.040 Parking lots.~~**

~~(a) **Purpose.** The purpose of the parking lots standards and guidelines is to reduce the visual impact of surface parking lots.~~

~~(b) **Standards.**~~

~~(1) Provide landscaped islands in parking lot interiors per City of Grand Junction requirements.~~

~~(2) Landscaped areas in and around surface parking lots shall be laid out with the intent of minimizing the perception of large, continuous expanses of pavement.~~

~~(c) Guidelines.~~

~~(1) Landscaping around parking lots should be designed so as to buffer the view of parked cars from the street and reduce the impact of headlights on nearby development.~~

~~(2) Parking areas between buildings and the street should consider the use of special paving materials to create parking courts with a higher level of pedestrian amenity.~~



*Trees and landscaping soften the visual impact of parked cars*

**25.16.050 Streetscape within the public right-of-way.**

~~(a) Purpose. The purpose of these streetscape standards and guidelines is to create tree-lined streets in the tradition of older neighborhoods and to create consistencies in tree plantings without creating monoculture problems.~~

~~(b) Standards.~~

~~(1) Street trees shall be provided along all public streets except in the industrial area. Along Patterson and 24 Road where there is a tree lawn, the trees shall be aligned in straight rows parallel to the curb, centered in the tree lawn. Irrigated turf and street trees shall be provided in the tree lawn. Berms and inorganic groundcover shall not be permitted in the tree lawn area. The tree spacing shall be approximately 40 feet on center.~~

~~(2) Street trees in paved walks shall be covered with minimum five foot by five foot tree grates or planters.~~

~~(3) The minimum width of a tree lawn is four feet.~~

~~(c) Guidelines.~~

(1) ~~Street tree species should be selected to maximize the cohesiveness of each block without creating monocultures that may be susceptible to disease.~~

(2) ~~Planting within the tree lawn area should be limited to grass and trees.~~



*Well-designed pedestrian paths incorporating healthy landscaping*

**~~25.16.060 Irrigation.~~**

(a) ~~**Purpose.** The purpose of the irrigation design standards and guidelines is to ensure that the landscape is provided with appropriate irrigation to ensure proper growth and maintenance. This applies to xeriscape and non-xeriscape planting and includes improved water conservation and water-efficient and low maintenance irrigation systems.~~

(b) ~~**Standard.**~~

(1) ~~All developed sites are to be irrigated with a permanent automatic system. All irrigation systems are to be below ground, fully automated systems in compliance with all applicable building codes. Use of water conserving systems such as trickle (drip) irrigation for shrub and tree plantings is encouraged. All backflow control devices are to be located or screened so that they are not visible from public streets or parking lots. All parking areas, drives and walks are to be “trimmed” to minimize spray into pavements.~~

(c) ~~**Guidelines.**~~

(1) ~~Where appropriate, low water use landscape and xeriscape is to be irrigated (i.e., trees and shrubs).~~

(2) ~~Trees that are appropriate in the Grand Junction environment tend to be indigenous and introduced species established and thriving in the lower elevations of Colorado’s Western Slope. A recommended plant materials list is available from the City of Grand Junction.~~

Chapter ~~25.20~~ 25.03

ARCHITECTURAL DESIGN

Sections:

- ~~25.20.010~~ 25.03.010 Purpose Introduction.
- ~~25.20.020~~ 25.03.020 Building form and scale.
- ~~25.20.030~~ ————— Architectural details.
- ~~25.20.040~~ 25.03.030 Building materials.
- ~~25.20.050~~ 25.03.040 Multi-unit residential development.
- ~~25.20.060~~ 25.03.050 Fencing and walls.
- ~~25.20.070~~ 25.03.060 Service and storage areas.

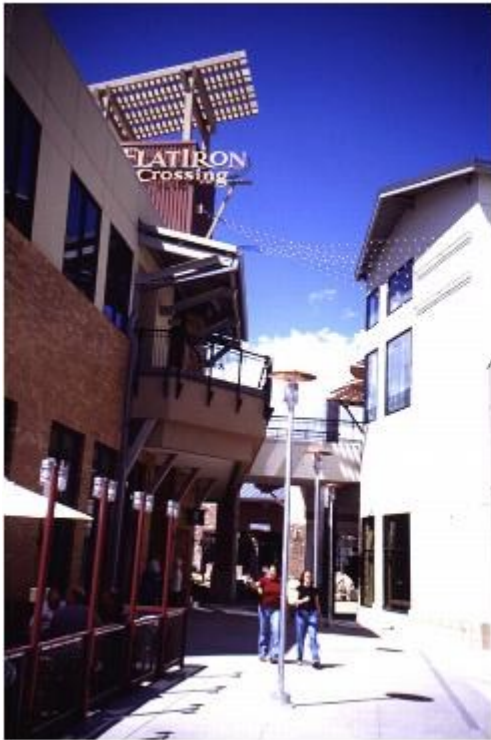
**~~25.20.010~~ 25.03.010 Purpose Introduction.**

These architectural design standards ~~and guidelines~~ are intended to:

- (a) ~~encourage~~ Encourage a consistent level of architectural quality throughout the 24 Road Corridor;
- (b) ~~and Create~~ a unified architectural character at the project level. ~~They addressing~~ building form, ~~and scale and for nonresidential and multifamily residential structures and architectural details and building materials for nonresidential structures.~~ Criteria are also included for including for walls, fencing and service and storage areas. ;
- (c) Reinforce the urban character of streets and open spaces;
- (d) Provide human scale and orientation;
- (e) Define the base, body and top of building elevations through the use of color, materials and details;
- (f) Ensure that the highest level of architectural detail occurs adjacent to areas of pedestrian activity;
- (g) Ensure that service and storage areas are functional yet do not visually impact views from adjacent uses and public areas;
- (h) ~~The standards and guidelines are intended to be flexible~~ Provide flexibility to allow for a number of design alternatives for different building types and to encourage design creativity.

Note: Additional ~~criteria~~ standards for architectural design are included in ~~the Title 21~~ Grand Junction Zoning and Development Code.





*Buildings in a variety of forms reinforce the pedestrian scale*

**25.20.020 25.03.020 Building form and scale. (Nonresidential and Multifamily Structures)**

**(a) Standards.**

The following standards shall apply to all building facades and exterior walls that are visible from adjacent public streets and other public spaces. These standards are intended to reduce the massive scale of large buildings, which, without application of these standards, may be incompatible with the 24 Road Corridor Overlay desired character.

(a) Facades greater than 150 feet in length, measured horizontally, shall incorporate wall plane projections or recesses having a depth of at least three percent of the length of the facade and extending at least 20 percent of the length of the facade. No uninterrupted length of any facade shall exceed 150 horizontal feet.

(b) Ground floor facades that face public streets shall have display windows, entry areas, awnings, or other such features along no less than 60 percent of their horizontal length. If the facade of the building facing the street is not the front, it shall provide the same features and/or landscaping in scale with the facade.

(c) Variation in roof lines/roof materials, in order to add interest to and reduce the massive scale of large buildings, is required. Roofs shall have no less than two of the following features:

(1) Parapets concealing flat roofs and rooftop equipment, such as HVAC units, from public view. Parapets shall not exceed one-third of the height of the supporting wall and shall not be of a constant height for a distance of greater than 150 feet;

(2) Overhanging eaves, extending no less than three feet past the supporting walls, for no less than 30 percent of the building perimeter;

(3) Sloping roofs that do not exceed an average slope greater than or equal to one foot of vertical rise for every three feet of horizontal run and less than or equal to one foot of vertical rise for every one foot of horizontal run; and

(4) Three or more roof slope planes.

~~(1) Buildings shall be designed to relate directly to and reinforce the pedestrian scale and quality of street, civic, and open spaces. The following techniques shall be used to meet this objective:~~

~~(i) Shifts in building massing, variations in height, profile, and roof form that provide human scale while maintaining a consistent relationship of overall building form to the street edge.~~

~~(ii) Minimizing long expanses of wall at a single height or in a single plane.~~

~~(iii) Varying floor heights to follow natural grade contours if significant variation is present.~~

~~(2) Buildings shall be designed to provide human scale, interest, and variety. The following techniques may be used to meet this objective:~~

~~(i) Variation in the building form such as recessed or projecting bays.~~

~~(ii) Expression of architectural or structural modules and detail.~~

~~(iii) Diversity of window size, shape, or patterns that relate to interior functions.~~

~~(iv) Emphasis of building entries through projecting or recessed forms, detail, color, or materials.~~

~~(v) Variations of material, material modules, expressed joints and details, surface relief, color, and texture to break up large building forms and wall surfaces. Such detailing could include sills, headers, belt courses, reveals, pilasters, window bays, and similar features.~~

~~(d)(3) Building facades facing arterial streets shall either be the primary entry facade or shall be of comparable quality in terms of architecture, materials and detailing. Primary building entries shall be connected to the public street sidewalk by the most direct route practical. Corner buildings need only provide public entry on one street-oriented facade.~~

(e)(4) Ground floor retail shall have direct pedestrian entries onto public streets, parks, or plazas. Primary building entries must be easily and directly accessible from a street and shall be either oriented to or easily visible from the street.

~~(5) New construction shall reflect the building form associated with its function. Building design should emphasize horizontal elements and facade treatments. Vertical elements are to be used sparingly for special architectural statements such as entries or to delineate uses within a large structure.~~

~~(6) Building form shall incorporate projected and recessed elements to provide architectural variety, such as entryways, special functional areas, rooflines, and other features.~~

~~(7) Large, monolithic expanses of uninterrupted facades are not allowed.~~

(f) Entryway design elements and variations should give orientation and aesthetically pleasing character to the building. The following standards identify desirable entryway design features. Each principal building on a site shall have clearly defined, highly visible main entrances featuring no less than three of the following:

(1) Canopies or porticos;

(2) Overhangs;

(3) Recesses/projections;

(4) Arcades;

(5) Raised corniced parapets over the door;

(6) Peaked roof forms;

(7) Arches;

(8) Outdoor patios;

(9) Display windows;

(10) Architectural details such as tile work and moldings which are integrated into the building structure and design; and

(11) Integral planters or wing walls that incorporate landscaped areas and/or places for sitting.

(g) To the greatest extent possible, mechanical appurtenances shall be located within the structure. External mechanical appurtenances such as heating and air conditioning equipment shall be screened and finished to match the colors and materials of the building.



*Primary building entrances present inviting facades along arterial streets*

**(b) Guidelines.**

- (1) Buildings should be designed to meet site and context design objectives, such as providing edges or enclosure to streets and open space, creating linkages and gateways, as well as framing or terminating views.
- (2) Large scale variations of massing, such as simple shifts in building form and roof shape, may be important to providing light, air, and transitions to nearby properties.
- (3) Providing human scaled architectural features is particularly important in areas where pedestrian activity is occurring or encouraged. The highest level of detail should occur close to pedestrian areas, near streets and entries, and around the ground floor.
- (4) The design of the roof form and other related elements such as roof material color, trim, and lighting should be an integral part of the architecture.
- (5) Nonresidential building facades adjoining or oriented toward streets and pedestrian areas should incorporate a substantial proportion of transparent glazing at all occupied levels.
- (6) Ground floor retail areas should have windows along sidewalks to create visual interest for pedestrians. All individual retail uses should have direct access from the public sidewalk.
- (7) For larger buildings, simple flat roof at parapet profiles is preferred as the predominant non-residential roof form (i.e., buildings larger than 20,000 SF in footprint).
- (8) Roofs should not be designed as attention getting devices related to the reinforcement of signage or as an identifiable corporate image.

(9) Building entry areas should express greater architectural detail and articulation than other portions of the building. Building entries should be designed at a pedestrian scale.

(10) Building facades should incorporate three-dimensional elements that provide detail and articulation of large surfaces, such as fenestration, offsets, undulations, and variety in surface pattern. Arcades, trellises and single-story lobbies and service appendages should also be used to help reduce monolithic building facades.

(11) The size of the building height and length should be in proportion and related to the site and its proposed function.



*Retail development includes pedestrian-scale open space*

**25.20.030 Architectural details.**

**(a) Guidelines.**

(1) Fenestration on the building shall respect the fundamental design of the building and create a rhythm and organization, although not necessarily symmetrical.

(2) Windows and openings should be used to break up the horizontal facade. Windows should consist of discrete openings in the wall surface, rather than large, continuous walls of glass.

(3) Building designs should define the base, body and top of the building elevations through the use of color, materials and details.

(4) Awnings are encouraged; however, they should respect the architectural integrity of the facades on which they are located. For example, awnings should be placed below the ground floor cornice line and should repeat the vertical, structural divisions of the building facade.

~~(5) First floors of buildings facing the public right-of-way should be 60 percent transparent.~~



*Quality materials and attention to detail in newly constructed multifamily housing projects*



**25.20.040 25.03.030 Building materials. (Nonresidential Structures)**

~~(a) Standards.~~

The following standards shall apply to nonresidential structures:

~~(a)(1) All primary buildings shall use materials that are durable, economically maintained, and of a quality that will retain their appearance over time including but not limited to stone, brick, stucco, pre-cast concrete, and architectural metals.~~

~~(b) Facade colors shall be nonspecular, neutral or earth tone colors. Use of high intensity, metallic, black or fluorescent color is prohibited.~~

~~(c) Building trim and accent areas may feature brighter colors, including primary colors.~~

~~(d) Predominant exterior building materials shall not include smooth-faced concrete block, smooth-faced tilt-up concrete panels, or prefabricated steel panels.~~

~~(2) The following cladding materials shall be prohibited:~~

~~(i) Pre-cast concrete and tilt-up wall systems that are primarily structural in appearance (such as Twin-Ts) shall not be permitted.~~

~~(ii) Natural wood or wood paneling shall not be used as a principle exterior wall cladding system except for single family development. Durable synthetic materials with the appearance of wood may be acceptable.~~

~~(iii) Natural cinder block is not permitted as exterior finish for any development.~~

~~(iv) Materials intended for indoor finishes are not permitted for any exterior cladding.~~

~~(3) Reflective glass whose percentage of outdoor, visible light reflectivity is greater than 19 percent or having a transmittance factor of less than 60 percent shall not be used. Reflective glazing shall be permissible for limited detail and aesthetic effects. Glazing within a facade which adjoins a public street pedestrian walk or bikeway should be generally transparent as viewed from the exterior during daylight hours. No first surface reflective coatings shall be permitted.~~

~~(e)(4) Ancillary buildings, enclosures and projected building elements shall be designed as an integral part of their primary facility. Where detached buildings are necessary, they shall be compatible to the main building in design, form, use of materials, and color.~~

~~(f)(5) Building materials should be used in a manner that achieves a coordinated design on all building facades ("360-degree design").~~

~~(g)(6) High quality, durable materials (brick and concrete masonry, pre-cast concrete, stone) should be the dominant treatment on all building facades facing 24 Road, Patterson Road/F Road, F 1/2 Road, and G Road.~~

~~(b) **Guidelines.**~~

~~(1) High quality, durable materials that provide scale and detail, such as architectural masonry, should be included in street-facing facades. Where a variety of wall materials are used, changes in material should generally occur at inside corners or where the transition is accommodated through an architectural detail such as a cap or belt course.~~

~~(2) Durable, long-lasting materials that also provide scale and detail should always be incorporated close to pedestrian areas, near streets and entries, and around the ground floor.~~

**25.20.050 25.03.040 Multi-unit residential development. (Multifamily Structures)**

~~(a) **Purpose.** The purpose of multi-unit residential development standards and guidelines is to ensure that the form and scale of multifamily residential architecture reinforces the urban character of streets and open spaces, as well as to provide human scale and orientation.~~

~~(b) **Standards.**~~

To ensure that multifamily residential architecture provides human scale and orientation and reinforces the urban character of streets and open spaces, the following standards shall apply to multifamily residential development in the 24 Road Corridor:

~~(a)(4) Buildings shall be designed to provide human scale, interest, and variety. The following techniques may be used to meet this objective:~~

~~(1)(f)~~ Variation in the building form related to the scale of individual dwelling units or rooms such as recessed or projecting bays, shifts in massing, or distinct roof shapes.

~~(2)(ii)~~ Diversity of window size, shape, or patterns that relates to interior functions.

~~(3)(iii)~~ Emphasis of building entries through projecting or recessed forms, detail, color, or materials.

~~(4)(iv)~~ Variations of material, material modules, expressed joints and details, surface relief, color, and texture to break up large building forms and wall surfaces. Such detailing could include sills, headers, belt courses, reveals, pilasters, window bays, or similar features. Changes in materials should generally occur at inside corners or where the transition is accommodated through an architectural detail such as a cap or belt course.

~~(b)(2)~~ Portions of buildings that are functionally limited from including significant window areas shall either be oriented away from public streets or shall make extensive use of the scaling methods defined above.

~~(c)(3)~~ Garages and carports shall either be integrated into the primary building form or shall be constructed of the same materials as the primary buildings.



*Examples of ~~Multifamily~~ multifamily housing that provides variety and human scale*



~~(c) **Guidelines.**~~

~~(1) Building forms and facades should provide an awareness of the activity within the buildings through frequent doors and windows oriented toward public streets and open space. Visibility of public spaces from within residences should contribute to the sense of community safety. Ground floor residences that adjoin a public street or open space should provide direct resident access to the public street or open space by entrances or gates of similar design quality and prominence as the primary entries.~~

~~(2) Variations of massing, fenestration, materials, color, and detail should be combined and interrelated to create effective expressions of human scale. The highest level of detail should occur adjacent to areas of pedestrian activity. .~~

~~(3) Garages, carports, and service areas should be screened from on-site residential and recreation areas to the greatest degree practicable. If separate from the primary residential buildings, they should be broken up into small structures that relate to the scale and location of individual residential units.~~

~~(4) Buildings within a development should share service areas to the extent possible.~~

**25.20.060 25.03.050 Fencing and walls. (Nonresidential and Multifamily Structures)**

~~(a) **Purpose.** The purpose of the fencing and walls standards and guidelines is to provide for security, to screen unsightly areas, and to provide visual relief and buffers. Additional objectives include:~~

~~(1) Screen all loading and storage areas from public streets and nonindustrial land uses utilizing walls, fences and landscaping.~~

~~(2) Provide screening that is aesthetically pleasing and complementary to the building and its surroundings.~~

~~(3) Provide for the coordination of design and location of walls and fences to maximize the positive interrelationship of buildings, public streets, and open space.~~

~~(4) Avoid the predominance of long, unarticulated street-facing walls or fences and prevent “fence canyons.”~~

~~(b) **Standards.**~~

~~(1) No fence or wall of any kind shall be constructed unless specifically approved by the City of Grand Junction.~~

~~(2) Walls and fences exceeding four feet in height that are located within the setback area adjoining a public street shall provide variety and articulation at intervals not exceeding 100 feet through not less than two of the following methods:~~

~~(i) Changes in plane of not less than two feet;~~

- ~~(ii) Expression of structure, such as post column, or pilaster not less than one foot in width;~~
- ~~(iii) Variation of material;~~
- ~~(iv) Variation of form, such as from solid to open pickets.~~

To provide for security, to screen unsightly areas, and to provide visual relief and buffers, the following standards shall apply to multifamily and non-residential structures in the 24 Road Corridor:

(a) (3) The design and materials for walls and fences shall be coordinated with the design and materials of the principal buildings in terms of color, quality, scale and detail. This is not intended to require identical materials and design.

(b) The combined height of walls and fences on top of an earth berm shall not exceed the maximum permitted height for the wall or fence alone.

(c) Fence and wall~~The design will~~ shall not include ~~avoid~~ long, unarticulated street fences facing walks and shall prevent “fence canyons.”

~~(4) Objects such as communications towers, processing equipment, cooling towers, storage tanks, vents, vehicles, or any other auxiliary structures or equipment shall either be compatible with the building architecture or screened from adjacent nonindustrial properties, public right of way and common open space.~~

~~(5) In nonindustrial areas, screen fences or walls shall be at least one foot higher than the materials or equipment being screened, and at least six feet tall.~~

~~(6) Materials and colors for fences and walls shall be compatible with the building architecture.~~

(c) Guidelines.

~~(1) Where an alley or service lane abuts a public open space, special effort should be applied to ensuring that the alley has an attractive appearance. For example, additional landscaping should be provided along the alley to blend its appearance with the open space and all refuse/service areas should be screened from the open space or adjoining uses across the alley.~~

(d)(2) Chain link fencing in areas visible from nonindustrial properties, parking areas, public streets and pedestrian walkways shall be of a type and color that is aesthetically pleasing and complementary to the building and its surroundings.

~~(e) Chain link fencing and must shall be landscaped. While this type of fencing is not recommended, it may be used when no other solution is available.~~

**25.20.070 25.03.060 Service and storage areas. (Nonresidential and Multifamily Structures)**

~~(a) **Purpose.** The purpose of the service and storage area standards and guidelines is to ensure that service and storage areas are functional, yet do not visually impact views from adjacent properties, parking areas, common open space, public streets, pedestrian walkways and multi-use trails. It is also important that screening walls and fences match building architecture and design. .~~

~~(b) **Standards.**~~

~~(1) Service and emergency drive lanes shall be designed as part of the site circulation system. Circulation and parking for service areas shall be designed to minimize disruption to the flow of traffic.~~

~~(2) Service areas and storage areas shall not front onto streets and open spaces. Such areas shall be located to the rear or side of buildings, screened from view from the street and/or open space.~~

~~(3) Refuse storage and pick-up areas shall be combined with other service and loading areas to the extent practicable. All outdoor refuse containers shall be screened from view from adjacent properties and streets. All trash containers must be covered. Reinforced concrete aprons are required in front of trash storage areas to accommodate trucks.~~

To ensure that service and storage areas are functional while minimizing their visual impact on adjacent properties, parking and open space areas, public streets, walkways and trails, the following standards shall apply to nonresidential and multifamily structures in the 24 Road Corridor:

(a) Outdoor storage, loading and operations areas shall be attractively screened from adjacent parcels and streets.

(1) Outdoor storage, trash collection and/or compaction, loading or other such uses shall be located in the rear of the lot. If because of lot configuration the Director determines that such placement is not feasible, then the side yard may be used, but in no case shall such area be located within 20 feet of any public street, public sidewalk or on-site pedestrian way.

(2) Outdoor storage, HVAC equipment, trash collection, trash compaction and other service functions shall be designed so as to be visually incorporated into the overall site design and architecture of the building(s).

(3) Views of outdoor storage, trash collection and/or compaction, HVAC equipment, loading and other service facilities and functions shall be screened from visibility from all property lines and screened and separated from sidewalks and on-site pedestrian ways. Screening structures shall be made of the same materials as the principal structure.

~~(b) (4)~~ Rooftop mechanical equipment, including satellite dishes and antennas over 30 inches in diameter, shall be screened from the view of public streets and open space. Alternate structures housing such equipment or wall-mounted painted-to-match units in unobtrusive locations in lieu of traditional screening will be considered; In the discretion of the Director, visual simulation may be required to demonstrate visual impacts on surrounding properties, open spaces, trails, parks, streets or walkways requested.

~~(5) Screening enclosures shall be incorporated into building architecture and utilize the same materials as the principal building to the greatest degree practicable.~~

~~(6) Screening and fences shall be one foot higher than the object being screened, but not more than eight feet high, on all sides where access is not needed. A metal gate shall be included where required for complete screening.~~

~~(7) Utility appurtenances within the right-of-way shall be located behind the sidewalk and out of the tree lawn, or, where it must be in the tree lawn, equipment shall be centered on the tree line and aligned with but no closer than 42 inches from the face of curb. This includes switch boxes, telephone pedestals, transformers, meters, irrigation, and similar equipment. The use of alleys is encouraged to locate all such equipment to the extent possible.~~

~~(c) (8)~~ Utility connections to buildings, including but not limited to Switch switch boxes, wires and electrical, and gas or other meters, shall be screened or located out of view from the public ~~streets~~street. All utilities and their connections shall be underground where permitted by the utility provider and other regulations.

~~(9) Architectural screening of utility substations shall be required, including an architectural wall at least equal to the height of the equipment to be screened from view.~~

~~(10) All storage, loading, or service areas must be located in the side or rear yards of buildings.~~

~~(11) The outdoor storage of any goods, materials, machinery or equipment in nonindustrial sites requires screening.~~

~~(12) No service or storage area shall be visible from public streets or building entries. This may be accomplished through greenery or building design and location.~~

~~(d)(13)~~ Loading and servicing areas shall be designed so that the entire loading or servicing operations are conducted within the confines of the building site. In addition, these areas must be integrated into the building architecture. Loading doors shall be recessed from the building face to minimize their visual prominence.

(e) In non-industrial areas, screen fences or walls shall be at least one foot higher than the materials or equipment being screened, and at least six feet tall.



*Screening blocks views of loading and storage areas*



*Service areas incorporated into the architecture of the building*

~~(c) **Guidelines.**~~

~~(1) Where possible, utility equipment should be located to facilitate access and connection to multiple properties.~~

~~(2) Buildings within a development should share service areas to the extent practicable.~~

**Chapter ~~25.24~~ 25.04**

**SITE LIGHTING**

Sections:

- ~~25.24.010~~                      25.04.010 Pedestrian, accent and security lighting ~~Introduction.~~
- ~~25.24.020~~                      Street lighting – Public rights of way.
- ~~25.24.030~~                      Pedestrian lighting – Public rights of way.
- ~~25.24.040~~                      Parking area lighting.
- ~~25.24.050~~                      Accent and security lighting.

The purpose of the site lighting standards and guidelines is In order to create a well-balanced, integrated lighting plan for public and private properties that enhances ~~enhance~~ vehicular and pedestrian visibility while minimizing lighting glare and contrast, prevent undesired off-site glare, Lighting should emphasize both public and private ~~attractive~~ features, illuminate sites and attract visitors to and destinations by using ~~the~~ a minimum amount of light to meet these objectives needed to do so. ~~It should provide needed illumination of the site and,~~ at the same time, prevent undesired off-site glare. ~~the following standards in addition to the lighting regulations in the City of Grand Junction Zoning and Development Code (Title 21) shall apply to lighting within the 24 Road Corridor:~~

**~~25.24.020 Street lighting – Public rights of way.~~**

~~(a) Purpose.~~ To provide lighting consistent with the function and character of the street.

~~(b) Standards.~~

~~(1) The spacing, location, height, fixture style, light source and level of illumination shall be subject to the standards and review of the City of Grand Junction.~~

~~(2) All light fixtures shall be of a uniform design. Pole and fixture color shall be selected by the City of Grand Junction.~~

~~(c) Guideline.~~

~~(1) Placement of fixtures should provide a coordinated and organized appearance that works with placement of street trees, curb cuts, signage and other features to contribute to the overall continuity of the streetscape.~~

**~~25.24.030 Pedestrian lighting – Public rights of way.~~**

~~(a) **Purpose.** To provide consistent systems of pedestrian lighting that add to the character, aesthetic appeal, and safety, and thereby promote greater pedestrian activity.~~

~~(b) **Standards.**~~

~~(a) All new development shall provide pedestrian lighting along public streets and pedestrian/bicycle trails.~~

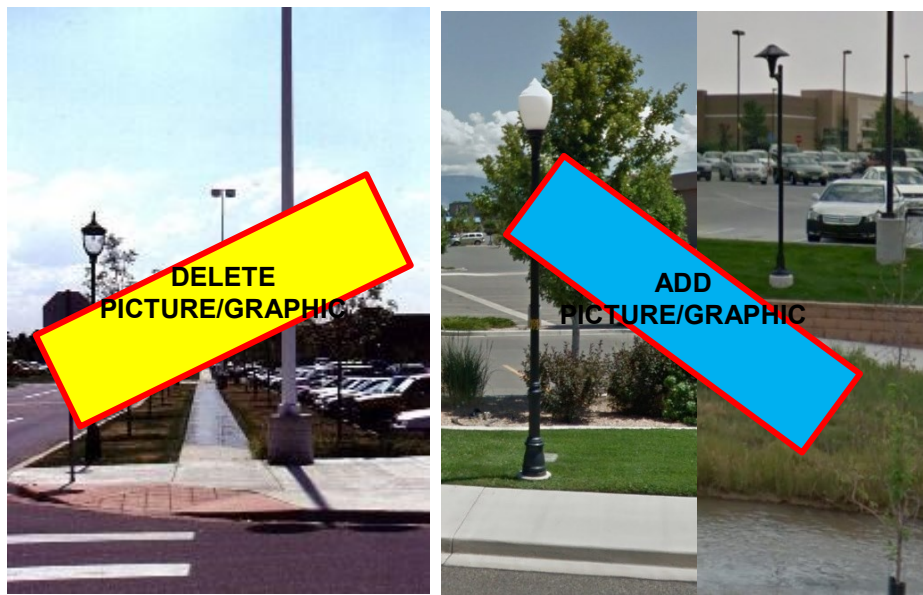
~~(b) (1) Lighting shall be designed to provide even and uniform light distribution without hot spots, dark spots, or glare.~~

~~(c) Lighting shall be designed to minimize dark areas that could pose a security concern near pedestrian areas.~~

~~(d) Pedestrian circulation systems shall be highlighted by visible light sources that clearly indicate the path of travel ahead.~~

~~(2) Pedestrian lighting shall use consistent fixtures, source colors, and illumination levels.~~

~~(3) Light fixtures shall be downcast or low cut-off fixtures to prevent glare and light pollution.~~



© **Guidelines.**

Examples of appropriate pedestrian lighting

~~(1) When pedestrian lighting is used in conjunction with street lighting, the pedestrian lighting should be clearly distinguishable from the ambient street lighting to clearly define the pedestrian path of travel.~~

~~(2) Light sources should generally be metal halide. Low wattage high-pressure sodium may be desirable in some residential settings where glare may be an issue.~~

~~(3) Placement of fixtures should provide a coordinated and organized appearance that facilitates uniform light levels and works with the placement of sidewalks, landscaping, signage, building entries and other features to contribute to the overall continuity of the streetscape and development. The use of a greater number of low fixtures in a well-organized pattern is preferred over the use of a minimum number of tall fixtures.~~

**25.24.040 Parking area lighting.**

~~(a) Purpose. To light parking areas in a consistent, attractive, and unobtrusive manner that minimizes off-site impacts.~~

~~(b) Standards.~~

~~(1) Parking and interior drives shall be lighted to provide functional, attractive, and unified lighting systems throughout the lot.~~

~~(2) The maximum height of parking lot light fixtures shall be 35 feet above the ground. Fixtures shall be of low cut-off design to minimize spill light and glare onto adjacent properties.~~

~~(3) Parking area lighting adjacent to residential development shall direct the light away from residential units and limit off-site light levels.~~

~~(4) Parking area lighting shall be extinguished one hour after the close of business, except as needed to provide for minimum security levels.~~

~~(c) Guidelines.~~

~~(1) Parking area lighting should complement the lighting of adjacent streets and properties and should use consistent fixtures, source colors and illumination levels. When adjacent to pedestrian-circulation and gathering areas, parking area lighting should not overpower the quality of pedestrian-area lighting.~~

~~(2) Poles should be placed to provide a unified, organized appearance throughout the parking area or development and should provide even and uniform light distribution. The use of a greater number of low fixtures in a well-organized pattern is preferred over the use of a minimum number of tall fixtures.~~

~~(3) At no point should lighting levels in parking and service areas, including service stations, exceed eight foot-candles when measured at the ground.~~

**25.24.050 Accent and security lighting.**

~~(a) Purpose. To light building architecture and site areas so as to accentuate design features and promote security in an attractive and understated manner that minimizes off-site impacts.~~



~~(b) Standards.~~

~~(1) Architectural accent lighting shall be limited to indirect lighting of architectural features only. No bare bulb or exposed neon lighting shall be used to accentuate building forms or details. Colored accent lighting is not permitted. Holiday lighting displays are exempted from restrictions on bare bulbs and colored accents. More prominent building lighting may be considered for buildings that adjoin I-70. The use of floodlights is not permitted.~~

(e) (2) Accent fixtures providing direct illumination shall be in character with the architectural and landscape design character of the development.

(f) Architectural lighting shall not be used to draw attention to or advertise buildings or properties. Architectural lighting may be used to highlight specific architectural, artistic or pedestrian features with the intent of providing accent and interest or to help identify entryways.

~~(3) Service area lighting shall be confined within the service yard boundaries and enclosure walls. No spillover shall occur outside the service or storage area. The lighting source shall not be visible from the street. Lights at service or exit doors shall be limited to low wattage downcast or low cut-off fixtures that may remain on throughout the night.~~

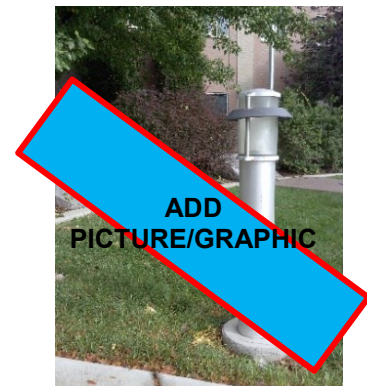
~~(c) Guidelines.~~

~~(1) Building lighting should only be used to highlight specific architectural features. Lighting of architectural features should be designed with the intent of providing accent and interest or to help identify entry and not to exhibit or advertise buildings or their lots.~~

(g)(2) Accent lighting of landscape and/or pedestrian areas shall ~~should be low level~~ be mounted close to the ground and use a low level of illumination, and background in appearance such as bollard lighting or similar low mount fixtures used for illuminating landscaping and pedestrian areas.

(h)(3) All accent and security lighting shall be shielded and downcast. ~~Outdoor storage areas including auto and truck parking and storage should be illuminated from poles similar to those used for parking lot lighting but at lower illumination levels.~~

~~(4) Security lighting should be limited to low intensity specialty fixtures. The light source should not be visible from the street or adjoining properties. Other wall-mounted security lighting is discouraged.~~



*Example of low level accent lighting*

Chapter ~~25.28~~ 25.05

SIGNS

Sections:

- ~~25.28.010~~ Introduction.
- ~~25.28.020~~ General sign criteria.
- ~~25.28.030~~ Site sign program.

~~25.28.010~~ Introduction 25.05.010 Sign standards.

In order to ensure that signs in the 24 Road Corridor ~~should~~ communicate information for property owners, tenants and users ~~while not without adding unnecessary to the~~ visual pollution ~~that is present in many road corridors~~ to the Corridor, to encourage signage that is on a pedestrian scale, and to preserve and accentuate the views of the natural beauty of the Colorado National Monument and Colorado River corridor, and to ensure continuity of signs within a given project, ~~Additional sign criteria are necessary to accomplish this that supplement the following standards, which include restrictions on sign size, height of freestanding signs and prohibition of billboards, shall apply to signage in the 24 Road Corridor, in addition to the sign regulations in the City of Grand Junction Zoning and Development Code (Title 21):~~

~~25.28.020~~ General sign criteria.

- (a) ~~Purpose.~~ These criteria include restrictions on temporary signs and billboards, as well as a requirement to develop a site sign program for individual projects.



*Signs should communicate information and not add to visual pollution*

- (a) Only the following sign types are permitted: freestanding signs, flush wall signs, exempt signs, and temporary signs as allowed/regulated by the Section 21.06.070, Zoning and Development Code, except as further restricted in this Chapter 25.05.
- ~~(b)~~ **Standards.** The following minimum criteria shall apply to all signs in the corridor:
  - (b)(4) Freestanding signs The height of a sign and support shall not exceed 12 feet in height from the finished site grade.

~~(c)(2) Sign face area for freestanding and flush wall signs shall not exceed 100 square feet per sign.~~

~~(d)(3) Temporary signs and exempt signs shall be permitted in accordance with Section 21.06.070 of the Zoning and Development Code. Signs shall not be located closer than 10 feet from the property line or right-of-way. (Directional signs may be located six feet from the curb. See guidelines in GJMC 25.28.030, Site sign program.)~~

~~(4) Temporary signs shall be permitted which identify the name of the proposed facility, the parties participating in its design, construction and financing, the anticipated date of occupancy, and leasing information. Temporary signs shall be limited to one eight-foot by four-foot freestanding project sign. All temporary signs shall be subject to time limitations established during the approval process.~~

~~(e) (5) No off-premises signs for outdoor advertising billboard(s) shall be permitted in the 24 Road Corridor. Billboard(s) that were installed in accordance with a permit or planning clearance from the City prior to November 1, 2000 may remain, except that they may not be expanded, enlarged, converted to illuminated or changeable copy signage, or replaced once removed within the corridor subarea.~~

~~(f) (6) Freestanding All information signage shall be placed perpendicular to approaching traffic and shall be positioned so there is a clear line-of-sight well before the point at which direction must be changed or action taken.~~

~~(7) Informational signage shall be positioned to avoid confusing backgrounds, particularly when directed to vehicular traffic.~~

~~(8) All traffic signs shall comply with the requirements of the State of Colorado Department of Transportation and the U.S. Manual on Uniform Traffic Control Devices.~~

~~(9) A licensed traffic engineer shall design the placement and type of regulatory signs.~~

~~(10) Regulatory signs may be necessary along some of the trails; in such cases the size and lettering shall be consistent with the design speed of the trail.~~

~~(11) If regulatory signage must communicate to vehicular traffic, it shall be placed so that it is visible.~~

~~(c) **Guidelines.**~~

~~(1) Signs within the corridor should be governed by similar restrictions relative to size, number, placement and illumination.~~

~~(2) The design of all signs should be coordinated to ensure a uniform appearance.~~

~~(3) Signs for similar purposes should be consistent in style and detail.~~

~~(4) The sign construction system should be flexible to easily permit changes in message without excessive cost.~~

~~(g) (5) Signs within a development project~~ Continuity of the sign system shall be designed and installed with continuity maintained by use of standard a coordinating scheme of color, typeface, materials, and construction details throughout the each project area.

#### **~~25.28.030 Site sign program.~~**

~~(a) Purpose. The site sign program is intended to be flexible and adaptable to different sites and will address sign location, layout, organization, and length of the message, the typeface, the design of the supporting structures and the compatibility with other signs in the system.~~

~~(b) Standard.~~

~~(1) A site sign program shall be prepared for each development project within the 24 Road Corridor and address building and wall signs. Each site sign program shall be tailored to the requirements of the development (residential, commercial, office, industrial, etc.) and can specify the use of identifying logos. It should specify the height of sign and support, sign face area, location, illumination, type and number of signs for the project. Types of signs shall include entrance and building identification signs, directional signs and regulatory signs. Both permanent and temporary signs shall be addressed.~~

~~(2) The entrance identification sign panel shall include the corporate name, logo, or signature and optional descriptive identifier.~~

~~(3) The street address number must appear on the sign. In the case of multiple tenants, all may be identified on the sign, up to a maximum of three tenants. Where there are more than three tenants, the building should be identified with a name and the tenants listed on a directory inside the building.~~

~~(4) The entrance identification sign shall be placed perpendicular to approaching vehicular traffic.~~

~~(5) Building identification signs provide for specific building identification viewed from the site or adjoining street. Letters may be painted on windows, or mounted on or routed out of the wall or fascia panel (commercial users only) designed specifically for signage.~~

~~(6) Directional signs serve to guide the motorist or pedestrian in, around, and out of the development site. Confine directional signs to a limited number of key decision points along the primary circulation system.~~

~~(7) Consolidate directional signs by “grouping” signs to various destinations within one sign frame.~~

~~(c) Guidelines.~~

~~(1) Entrance signs identify individual building tenants or the name of the building. Tenant entrance identification signs should provide a distinctive sign style that will complement a variety of architectural styles.~~

~~(h) (2) On freestanding signs, All entry identification signs should be either externally or internally illuminated. Only only graphics and typography are to be illuminated.~~

~~(i) (3) Freestanding Entrance identification signs should shall be single- or double-faced and constructed of a metal panel with stone or veneer base. The sign may be single or double-faced. If the sign is single-faced, the backside ~~should~~ shall be painted the same color as the cabinet and poles.~~

~~(4) No identification sign should be located closer than 10 feet to any property line.~~

~~(5) Generally, one tenant identification sign is sufficient. More than one may be used where a site has more than one vehicular entrance on different sides of the building, or when the nature of the site and adjacent streets requires more than one sign for proper identification. The sign should be placed so it does not obscure any other identification, information or vehicular control signs.~~

~~(j) (6) The owner or tenant of a building may elect to place the identification of the primary tenant on the surface of the building. Sign information should be limited to the display of the building name or the name of the business occupying the site. Only one building identification sign should be provided for each building. Secondary elements should be shown on the interior directory. The Flush wall signs sign shall not be illuminated externally; they may be either non-illuminated or internally illuminated.~~

~~(7) To minimize clutter, directional signs should identify only primary tenants within the development site.~~

~~(8) The positioning of directional signage is critical to its effectiveness. Each site requires careful analysis of vehicular and pedestrian traffic. Decision points must be identified and proper information and directional signage provided.~~

~~(9) Directional signage should be placed no closer than six feet from the curb of a street or drive.~~

~~(k)(10) Trail route identification signs ~~should~~ shall be placed at critical locations which shall be determined by the Director during site plan review.~~

INTRODUCED on first reading the \_\_\_\_\_ day of \_\_\_\_\_, 2016 and ordered published in pamphlet form.

PASSED and ADOPTED on second reading the \_\_\_\_ day of \_\_\_\_\_, 2016 and ordered published in pamphlet form.

\_\_\_\_\_  
President of the Council

ATTEST:

\_\_\_\_\_  
City Clerk

# **Clean Copy of Proposed Text Changes**

**Title 25**

**24 ROAD CORRIDOR DESIGN STANDARDS**

**Chapters:**

**25.01 Introduction**

**25.02 Site Development**

**25.03 Architectural Design**

**25.04 Site Lighting**

**25.05 Signs**



## Chapter 25.01

### INTRODUCTION

#### 25.01.010 Background and Intent.

The 24 Road Corridor Design Standards provide development standards for public and private improvements in the 24 Road Corridor in order to implement the goals and objectives of the 24 Road Corridor Subarea Plan and to:

- (a) establish a practical, interconnected system of streets, parks, and parkways that allows easy orientation and convenient access for all modes of transportation;
- (b) utilize natural open spaces, such as creeks, and developed public spaces, streets, parks and parkways, to organize and coordinate development;
- (c) accommodate a broad mix of development types that encourage alternative transportation, especially walking, and transit use;
- (d) provide common usable open space that is of mutual benefit to surrounding property owners, businesses, and residents; and
- (e) establish a pattern and character for the long-term evolution of the corridor.
- (f) Adherence to these standards and guidelines will ensure that public and private improvements in the 24 Road Corridor will be well planned and executed in a high quality manner, which were important goals established through the 24 Road Corridor Subarea Plan.

These standards supplement other development regulations in the Grand Junction Municipal Code. Where there is a conflict between this Title 25 and the Zoning and Development Code (Title 21), the more restrictive standard shall apply.



*View corridor along 24 Road highlights natural features*

## Chapter 25.02

### SITE DEVELOPMENT

#### Sections:

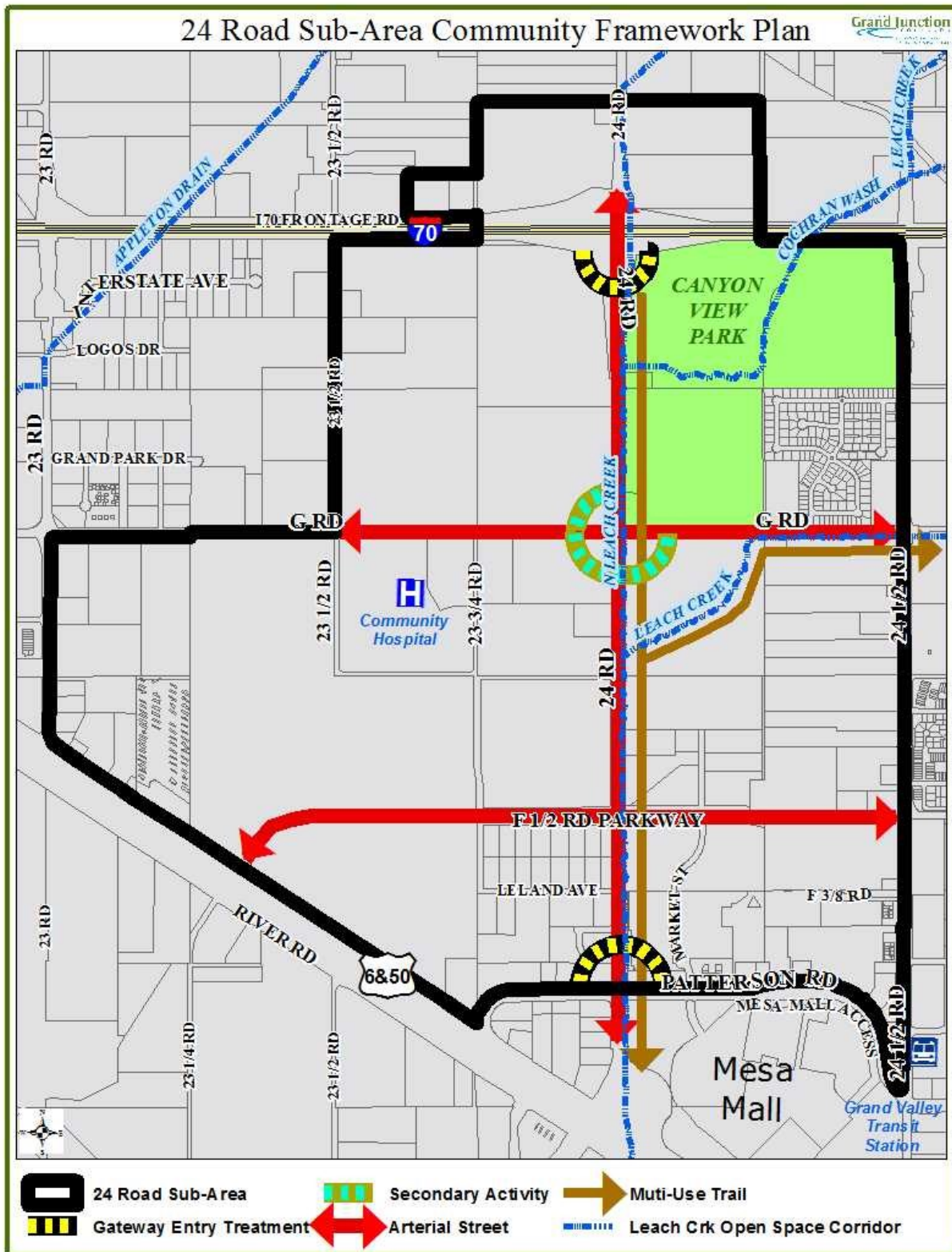
25.02.010	Purpose
25.02.020	Community Framework
25.02.030	On-Site Open Space
25.02.040	Organizing Features.
25.02.050	Site Grading and Drainage.
25.02.060	Building and Parking Setbacks.
25.02.070	Building Location and Orientation.
25.02.080	Parking, Access, and Circulation.
25.02.090	Auto-Oriented Uses.
25.02.100	Pedestrian and Bicycle Circulation.
25.02.110	Sidewalks.
25.02.120	Bicycle Circulation.

#### **25.02.010 Purpose**

The location and design of buildings and site improvements are key to establishing the overall character and function of the 24 Road Corridor. Important site development considerations include large area development planning and coordination, the placement of buildings on the lot, relative amounts of area devoted to open space, buildings and parking areas, and vehicular and pedestrian circulation. With large undeveloped parcels such as those in the 24 Road Corridor, there is an opportunity to implement a “big picture” vision. The vision for the Corridor is to increase pedestrian movement and encourage a mixture of development types integrating housing, commercial uses, neighborhood centers, shared parking, and other appropriate land uses within neighborhoods and within buildings in a flexible pattern of lots and blocks to allow site planning flexibility and consistency of development patterns.

#### **25.02.020 Community Framework**

The overall planning concept for the 24 Road Corridor includes a community framework (Figure 2.1) that provides a distinctive image and organizing element for public and private development. The community framework includes public streets, parks, open spaces, natural drainages and future stormwater management facilities that serve and connect part or all of the corridor.



**25.02.030 On-Site Open Space**

On-site open space could be provided within areas that are also set aside for stormwater drainage, landscaped setbacks and other landscape areas associated with on-site buffer landscaping and amenities. Open space shall be provided on-site so as to implement and comply with the following:

- (a) Utilize well-defined natural and developed open spaces as features to serve as the focus of block, lot, and circulation patterns.
- (b) Supplement public open space such as parks and drainage corridors with privately developed open space that helps complete linkages and organize development. (Refer to open space requirements of zoning and development code.)
- (c) Open space, such as Leach Creek and developed parks and plazas, shall be used as a positive planning tool to organize and focus lot, block, and circulation patterns. Public access shall be provided to all public open space, natural and developed, directly from the public street/sidewalk system or through a public facility. Natural open space corridors and naturalized drainage ways (with trails) shall be publicly accessible at not less than 800-foot intervals.
- (d) Buildings with frontage on open space shall provide windows, doors, plazas, or other amenities that encourage pedestrian activity and provide views onto and/or are oriented toward the open space.



*Examples of privately developed and publicly developed open space amenities*



*Example of development oriented to open space*

*Organizing features provide a focus for development*

**25.02.040 Organizing Features**

(a) **Purpose.** An organizing feature is a public open space around which development is focused. Organizing features can utilize natural open space, creeks, formal public spaces, streets, parks, and parkways to organize and coordinate development patterns. Organizing features should reinforce the pattern and orientation of streets and buildings through orderly arrangements of landscaping, pedestrian circulation and amenities, such as might be typical of a town square or campus quadrangle. Prominent amenities could be developed within open space to link building groups.

(b) **Standards.**

(1) At least one central feature or gathering place shall be located within a geographically distinct neighborhood, e.g., a convenient outdoor open space or plaza with amenities such as benches, monuments, kiosks, or public art. These places may be located on “civic blocks,” and may include buildings such as libraries, government offices, or public meeting places.

(2) Buildings shall not orient rear, blank, or service dominated facades toward an organizing feature and shall include an entry that is visible, convenient to use, and connected to a public sidewalk by a direct route. Facades facing an organizing feature shall be of at least comparable architectural quality to other primary building facades.

(3) Parking lots and parking spaces shall not be permitted within organizing features.

(4) Multi-building developments shall use an organizing feature to create an internal campus-like arrangement of buildings and open space; provided, that the organizing feature is bounded along at least one side by a public street.

(5) Organizing features shall utilize natural open space, creeks, formal public spaces, streets, parks, and parkways, where such are available on or adjacent to the site, to organize and coordinate development patterns.



*Water features invite interaction*



*Fountain as an organizing feature*

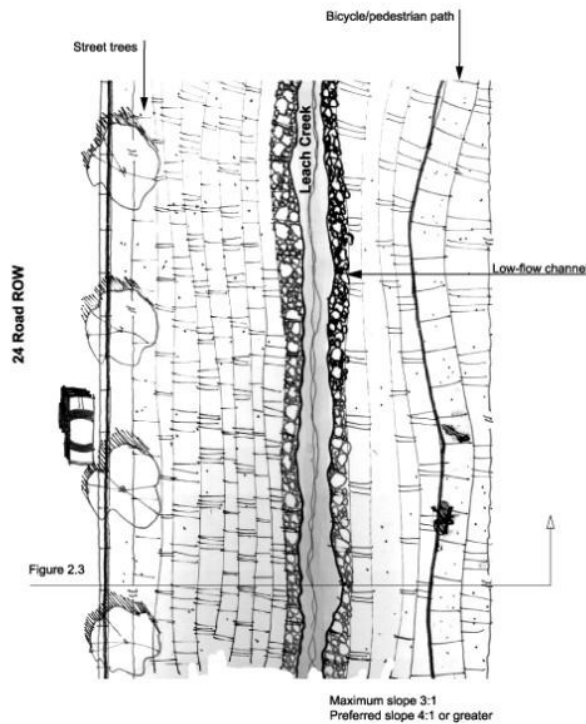
*Open space incorporates drainage facility and amenities*

**25.02.050 Site Grading and Drainage**

The site grading and drainage standards ensure that development fits within existing topography, reinforces the community open space framework, and effectively diverts and retains stormwater. In addition to other site grading and drainage requirements of the Municipal Code, the following standards shall apply in the 24 Road Corridor:

- (a) Unless precluded by soil conditions, graded slopes shall not be steeper than 3:1 slope. Where space limitations demand, terracing with retaining walls is the preferred solution.
- (b) Existing riparian areas and drainages, such as Leach Creek and its tributaries, shall be developed and maintained as natural open space corridors that provide surface drainage and developed pedestrian trails. The treatment of surface drainages shall be an open channel with gently sloping sides and naturalized landscape, except for the west bank of Leach Creek which is constrained by the 24 Road improvements. (See Figures 2.2 & 2.3.)

**Figure 2.2: Natural Corridor Plan**



*Open space incorporates drainage facility and amenities*

**Figure 2.3: Natural Corridor Section**



**25.02.060 Building and Parking Setbacks**

Building and parking setback standards establish a coordinated streetscape image and provide sufficient space between buildings, roads and parking areas for adequate light, privacy, views, sound control and landscaping. In order to establish additional open space and maintain views, building and parking setbacks shall be provided as specified in Table 3.1.

**Table 3.1: Supplemental Building and Parking Setbacks**

Frontage	Building Setback	Parking Setback
24 Road – west ROW	50'	25'
24 Road – east ROW	35' from edge of Leach Creek corridor	10' from edge of Leach Creek corridor
Patterson Road (F Road)	Per zoning	10'
F 1/2 Road	Per zoning	10'
G Road	Per zoning	10'

Notes:

- (a) Width of Leach Creek drainage corridor to be determined and will include multi-use trail. (See Figure 2.3.)
- (b) All measurements are from the right-of-way.



*Coordinated landscaping in the building setback complements ROW landscaping*

*Buildings at the edges of project site with pedestrian connections through parking areas*

**25.02.070 Building Location and Orientation**

To encourage the orientation of buildings toward the street, reinforce the character and quality of public streets and sidewalks, maximize useful interconnections, and enhance the appearance of properties from the street, the following standards shall apply:

- (a) No development shall be permitted to place or orient buildings, parking, circulation, or service facilities on a lot in such a way as to treat primary street frontage(s) as a rear lot line. "Rear" shall be defined to mean a portion of the property lacking public access and containing a predominance of service functions and/or service facilities that significantly diminish the architectural or landscape quality of the development.
- (b) All sides of a building that are visible from a street, residential area, public park or organizing feature shall have the equivalent architectural treatment of the primary building façade.
- (c) All service and loading facilities that are visible from a street, public park, residential area or organizing feature shall be completely screened from view.
- (d) Nonresidential uses located in the same block with residential uses shall be located on higher traffic streets and at the periphery or the end of each block having both uses and be effectively screened of light, noise, and pollution from service area or other incompatible activities.
- (e) Large retail buildings (over 20,000 gross square feet) shall be located to minimize the impact of windowless walls and service areas on public streets. On sites that include large retail buildings, smaller buildings in-line or on pads shall be located to form edges that frame and reinforce the space and appearance of public streets. Pad buildings shall locate at least one facade including windows and similar architectural features within 35 feet of the public right-of-way. Pad buildings shall be located at site corners and entries.
- (f) All sites abutting 24 Road shall treat 24 Road as a primary frontage with regard to the quality and orientation of buildings, site design, architectural features, and parking area design. This shall be in addition to any other required access and orientation. Primary frontage is intended to include landscape and building design that conveys the project identity and character and is of equal or superior quality to any other frontage of the project. No truck docks or service areas shall be permitted to face 24 Road.

**25.02.080 Parking, Access, and Circulation**

In order to provide for safe and convenient movement of pedestrians and motor vehicles, limit vehicular/pedestrian conflicts, reduce paved areas, provide screening for paved areas and soften the visual impact of parking lots by providing interior planting, the following standards shall apply:



- (a) No more than two double-loaded bays of parking, with a maximum length of 125 feet, shall be allowed in front of buildings smaller than 20,000 square feet in size.
- (b) For retail buildings 20,000 to 30,000 square feet in size, no more than 50 percent of the total surface parking area shall be located in the *restricted area* between the street and a building. For retail buildings larger than 30,000 square feet, no more than 60 percent of the total surface parking area shall be located in the *restricted area*. The restricted area is located between the front facade of the principal structure and the primary abutting street. The restricted area shall be determined by drawing a line from the front corners of the building to the nearest property line as shown in the example.



*Example of Restricted Area*

- (c) Service entrances, service yards and loading areas shall not be located on a side of any building that faces 24 Road, any arterial street, I-70, or residential uses.
- (d) Opportunities for future auto, pedestrian, and bicycle connections to adjoining sites shall be considered in all site design. Such connections shall be provided where reasonably practicable.



*Examples of pedestrian circulation systems within parking lots*



*Sidewalks and landscaping break up and soften the visual impact of large parking lots*

**25.02.090 Auto-oriented Uses**

To minimize impacts of auto circulation, queuing, drive-up facilities (including speaker systems and similar activities) and to promote street-oriented building design and pedestrian amenities, the following standards shall apply:

- (a) Drive-up and drive-through facilities (order stations, pick-up windows, bank teller windows, money machines, car drop-off areas for auto service or rental, etc.) shall be located on the side or rear of a building and away from residential uses.
- (b) For buildings greater than 100 feet from the street and with no intervening buildings, drive-through windows may be allowed to face a perimeter street, and drive-through lanes may be allowed with adequate landscaping buffer from the right-of-way line.

**25.02.100 Pedestrian and Bicycle Circulation**

To promote efficient and safe movement of pedestrians and bicyclists through the 24 Road Corridor, sidewalk and/or multi-use trail links shall be provided between development sites and open space including but not limited to an off-street multi-use trail connecting Canyon View Park and the Colorado River Trail.



*Direct pedestrian access from sidewalk*



*Pedestrian circulation in retail area*

**25.02.110 Sidewalks**

To provide opportunity for continuous pedestrian movement through the 24 Road Corridor, the following standards shall apply:

- (a) A direct pedestrian connection to the building entry shall be provided from the public sidewalk.
- (b) Development shall provide pedestrian circulation from public walks to parking areas, building entries, plazas, and open spaces. Walkways shall be provided to separate pedestrians and vehicles, and shall link ground level uses. Primary walks shall be a minimum of five feet wide.
- (c) Clear and safe pedestrian routes shall be defined through parking areas to provide pedestrian access between buildings with minimum conflicts with vehicles. Walkways crossing drive aisles shall be clearly marked with signage, special paving, landscaping or other similar means of demarcation.

- (d) Sidewalks at least eight feet wide shall be provided along all building facades that abut public parking areas and feature a public entrance.
- (e) Pedestrian walks incorporating bicycle lanes shall be not less than 10 feet in width.
- (f) All parking areas located between a street and building shall include a landscaped pedestrian walk linking the building with a public sidewalk.
- (g) Physical barriers between contiguous development shall be prohibited unless necessary for safety or the mitigation of adverse impacts.
- (h) Development shall provide pedestrian linkages between the Leach Creek trail corridor and other public ways and open space areas.



*Shared plaza at building entries*



*Landscaped public sidewalk*

**25.02.120 Bicycle Circulation**

To provide a safe, direct, and attractive system of interconnected public bikeways and bike routes throughout the corridor and defined routes to primary building entries, the following standards shall apply:

- (a) Bicycle parking shall be located in visible, active, and well-lit areas, near building entries, convenient to primary bicycling access, and not encroaching on pedestrian walkways. If possible, bike parking shall be located so it is visible from the inside of adjacent buildings.
- (b) Bicycle circulation shall connect and align with pre-existing and planned off-site bicycle routes. Crossings at intervening streets shall be located where safe means for crossing can be provided.



*Public bikeway provides an interconnected system*



*On-street bicycle route designation separate from the roadway*

**Chapter 25.03**

**ARCHITECTURAL DESIGN**

Sections:

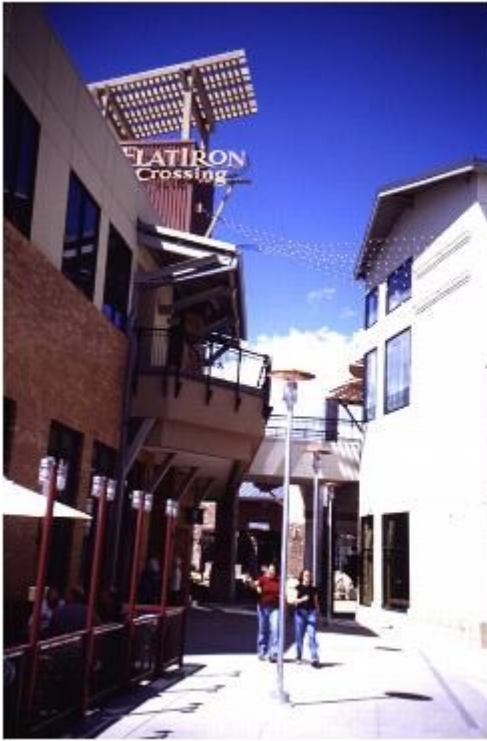
- 25.03.010 Purpose
- 25.03.020 Building Form and Scale
- 25.03.030 Building Materials
- 25.03.040 Multi-Unit Residential Development
- 25.03.050 Fencing and Walls
- 25.03.060 Service and Storage Areas

**25.03.010 Purpose**

These architectural design standards are intended to:

- (a) Encourage a consistent level of architectural quality throughout the 24 Road Corridor;
- (b) Create a unified architectural character at the project level, addressing building form, scale and materials, including for walls, fencing and service and storage areas;
- (c) Reinforce the urban character of streets and open spaces;
- (d) Provide human scale and orientation;
- (e) Define the base, body and top of building elevations through the use of color, materials and details;
- (f) Ensure that the highest level of architectural detail occurs adjacent to areas of pedestrian activity;
- (g) Ensure that service and storage areas are functional yet do not visually impact views from adjacent uses and public areas;
- (h) Provide flexibility to allow for a number of design alternatives for different building types and to encourage design creativity.

Note: Additional standards for architectural design are included in Title 21, Grand Junction Zoning and Development Code.



*Buildings in a variety of forms reinforce the pedestrian scale*

**25.03.020 Building Form and Scale (Nonresidential and Multifamily Structures)**

The following standards shall apply to all building facades and exterior walls that are visible from adjacent public streets and other public spaces. These standards are intended to reduce the massive scale of large buildings, which, without application of these standards, may be incompatible with the 24 Road Corridor Overlay desired character.

- (a) Facades greater than 150 feet in length, measured horizontally, shall incorporate wall plane projections or recesses having a depth of at least three percent of the length of the facade and extending at least 20 percent of the length of the facade. No uninterrupted length of any facade shall exceed 150 horizontal feet.
- (b) Ground floor facades that face public streets shall have display windows, entry areas, awnings, or other such features along no less than 60 percent of their horizontal length. If the facade of the building facing the street is not the front, it shall provide the same features and/or landscaping in scale with the facade.
- (c) Variation in roof lines/roof materials, in order to add interest to and reduce the massive scale of large buildings, is required. Roofs shall have no less than two of the following features:
  - (1) Parapets concealing flat roofs and rooftop equipment, such as HVAC units, from public view. Parapets shall not exceed one-third of the height of the supporting wall and shall not be of a constant height for a distance of greater than 150 feet;

- (2) Overhanging eaves, extending no less than three feet past the supporting walls, for no less than 30 percent of the building perimeter;
  - (3) Sloping roofs that do not exceed an average slope greater than or equal to one foot of vertical rise for every three feet of horizontal run and less than or equal to one foot of vertical rise for every one foot of horizontal run; and
  - (4) Three or more roof slope planes.
- (d) Primary building entries shall be connected to the public street sidewalk by the most direct route practical. Corner buildings need only provide public entry on one street-oriented facade.
- (e) Ground floor retail shall have direct pedestrian entries onto public streets, parks, or plazas. Primary building entries must be easily and directly accessible from a street and shall be either oriented to or easily visible from the street.
- (f) Entryway design elements and variations should give orientation and aesthetically pleasing character to the building. The following standards identify desirable entryway design features. Each principal building on a site shall have clearly defined, highly visible main entrances featuring no less than three of the following:
- (1) Canopies or porticos;
  - (2) Overhangs;
  - (3) Recesses/projections;
  - (4) Arcades;
  - (5) Raised corniced parapets over the door;
  - (6) Peaked roof forms;
  - (7) Arches;
  - (8) Outdoor patios;
  - (9) Display windows;
  - (10) Architectural details such as tile work and moldings which are integrated into the building structure and design; and
  - (11) Integral planters or wing walls that incorporate landscaped areas and/or places for sitting.
- (g) To the greatest extent possible, mechanical appurtenances shall be located within the structure. External mechanical appurtenances such as heating and air conditioning equipment shall be screened and finished to match the colors and materials of the building.



*Primary building entrances present inviting facades along arterial streets*



*Retail development includes pedestrian-scale open space.*

### **25.03.030 Building materials (Nonresidential Structures)**

The following standards shall apply to nonresidential structures:

- (a) All primary buildings shall use materials that are durable, economically maintained, and of a quality that will retain their appearance over time including but not limited to stone, brick, stucco, pre-cast concrete, and architectural metals.
- (b) Facade colors shall be nonspecular, neutral or earth tone colors. Use of high intensity, metallic, black or fluorescent color is prohibited.
- (c) Building trim and accent areas may feature brighter colors, including primary colors.
- (d) Predominant exterior building materials shall not include smooth-faced concrete block, smooth-faced tilt-up concrete panels, or prefabricated steel panels.
- (e) Ancillary buildings, enclosures and projected building elements shall be designed as an integral part of their primary facility. Where detached buildings are necessary, they shall be compatible to the main building in design, form, use of materials, and color.
- (f) Building materials should be used in a manner that achieves a coordinated design on all building facades (“360-degree design”).
- (g) High quality, durable materials (brick and concrete masonry, pre-cast concrete, stone) should be the dominant treatment on all building facades facing 24 Road, Patterson Road/F Road, F 1/2 Road, and G Road.

### **25.03.040 Multi-Unit Residential Development (Multifamily Structures)**

To ensure that multifamily residential architecture provides human scale and orientation and reinforces the urban character of streets and open spaces, the following standards shall apply to multifamily residential development in the 24 Road Corridor:

- (a) Buildings shall be designed to provide human scale, interest, and variety. The following techniques may be used to meet this objective:
  - (1) Variation in the building form related to the scale of individual dwelling units or rooms such as recessed or projecting bays, shifts in massing, or distinct roof shapes.
  - (2) Diversity of window size, shape, or patterns that relates to interior functions.



- (3) Emphasis of building entries through projecting or recessed forms, detail, color, or materials.
- (4) Variations of material, material modules, expressed joints and details, surface relief, color, and texture to break up large building forms and wall surfaces. Such detailing could include sills, headers, belt courses, reveals, pilasters, window bays, or similar features. Changes in materials should generally occur at inside corners or where the transition is accommodated through an architectural detail such as a cap or belt course.
- (b) Portions of buildings that are functionally limited from including significant window areas shall either be oriented away from public streets or shall make extensive use of the scaling methods defined above.
- (c) Garages and carports shall either be integrated into the primary building form or shall be constructed of the same materials as the primary buildings.



*Examples of multifamily housing that provides variety and human scale*

**25.03.050 Fencing and Walls (Nonresidential and Multifamily Structures)**

To provide for security, to screen unsightly areas, and to provide visual relief and buffers, the following standards shall apply to multifamily and non-residential structures in the 24 Road Corridor:

- (a) The design and materials for walls and fences shall be coordinated with the design and materials of the principal buildings in terms of color, quality, scale and detail. This is not intended to require identical materials and design.
- (b) The combined height of walls and fences on top of an earth berm shall not exceed the maximum permitted height for the wall or fence alone.
- (c) Fence and wall design shall not include long, unarticulated street fences facing walks and shall prevent “fence canyons.”
- (d) Chain link fencing in areas visible from nonindustrial properties, parking areas, public streets and pedestrian walkways shall be of a type and color that is aesthetically pleasing and complementary to the building and its surroundings.
- (e) Chain link fencing shall be landscaped.

**25.03.060 Service and Storage Areas (Nonresidential and Multifamily Structures)**

To ensure that service and storage areas are functional while minimizing their visual impact on adjacent properties, parking and open space areas, public streets, walkways and trails, the following standards shall apply to nonresidential and multifamily structures in the 24 Road Corridor:

- (a) Outdoor storage, loading and operations areas shall be attractively screened from adjacent parcels and streets.
  - (1) Outdoor storage, trash collection and/or compaction, loading or other such uses shall be located in the rear of the lot. If because of lot configuration the Director determines that such placement is not feasible, then the side yard may be used, but in no case shall such area be located within 20 feet of any public street, public sidewalk or on-site pedestrian way.
  - (2) Outdoor storage, HVAC equipment, trash collection, trash compaction and other service functions shall be designed so as to be visually incorporated into the overall site design and architecture of the building(s).
  - (3) Views of outdoor storage, trash collection and/or compaction, HVAC equipment, loading and other service facilities and functions shall be screened from visibility from all property lines and screened and separated from sidewalks and on-site pedestrian ways. Screening structures shall be made of the same materials as the principal structure.
- (b) Rooftop mechanical equipment, including satellite dishes and antennas over 30 inches in diameter, shall be screened from the view of public streets and open space. Alternate structures housing such equipment or wall-mounted painted-to-match units in unobtrusive locations in lieu of traditional screening will be considered. In the discretion of the Director, visual simulation may be required to demonstrate visual impacts on surrounding properties, open spaces, trails, parks, streets or walkways.
- (c) Utility connections to buildings, including but not limited to switch boxes, wires and electrical, gas and other meters, shall be screened or located out of view from public streets. All utilities and their connections shall be underground where permitted by the utility provider and other regulations.
- (d) Loading doors shall be recessed from the building face to minimize their visual prominence.
- (e) In non-industrial areas, screen fences or walls shall be at least one foot higher than the materials or equipment being screened, and at least six feet tall.



*Screening blocks views of loading and storage areas*



*Service areas incorporated into the architecture of the building*

**Chapter 25.04**

**SITE LIGHTING**

**25.04.010 Pedestrian, Accent and Security Lighting**

In order to enhance vehicular and pedestrian visibility while minimizing lighting glare and contrast, prevent undesired off-site glare, emphasize attractive features, illuminate sites and attract visitors to destinations using the minimum amount of light needed to do so, the following standards in addition to the lighting regulations in the City of Grand Junction Zoning and Development Code (Title 21) shall apply to lighting within the 24 Road Corridor:

- (a) All new development shall provide pedestrian lighting along public streets and pedestrian/bicycle trails.
- (b) Lighting shall be designed to provide even and uniform light distribution without hot spots, dark spots, or glare.
- (c) Lighting shall be designed to minimize dark areas that could pose a security concern near pedestrian areas.
- (d) Pedestrian circulation systems shall be highlighted by visible light sources that clearly indicate the path of travel ahead.
- (e) Accent fixtures providing direct illumination shall be in character with the architectural and landscape design character of the development.
- (f) Architectural lighting shall not be used to draw attention to or advertise buildings or properties. Architectural lighting may be used to highlight specific architectural, artistic or pedestrian features with the intent of providing accent and interest or to help identify entryways.
- (g) Accent lighting of landscape and/or pedestrian areas shall be mounted close to the ground and use a low level of illumination, such as bollard lighting or similar low mount fixtures used for illuminating landscaping and pedestrian areas.
- (h) All accent and security lighting shall be shielded and downcast.



*Examples of appropriate pedestrian lighting*

*Example of low level accent lighting.*

Chapter 25.05

SIGNS

25.05.010 Sign Standards

In order to ensure that signs in the 24 Road Corridor communicate information for property owners, tenants and users without adding unnecessary visual pollution to the Corridor, to encourage signage that is on a pedestrian scale, and to preserve and accentuate the views of the natural beauty of the Colorado National Monument and Colorado River corridor, and to ensure continuity of signs within a given project, the following standards, which include restrictions on sign size, height of freestanding signs and prohibition of billboards, shall apply to signage in the 24 Road Corridor, in addition to the sign regulations in the City of Grand Junction Zoning and Development Code (Title 21):

- (a) Only the following sign types are permitted: freestanding signs, flush wall signs, exempt signs, and temporary signs as allowed/regulated by the Section 21.06.070, Zoning and Development Code, except as further restricted in this Chapter 25.05.
- (b) Freestanding signs shall not exceed 12 feet in height from finished grade.
- (c) Sign face for freestanding and flush wall signs shall not exceed 100 square feet per sign.
- (d) Temporary signs and exempt signs shall be permitted in accordance with Section 21.06.070 of the Zoning and Development Code.
- (e) No billboards shall be permitted in the 24 Road Corridor. Billboards that were installed in accordance with a permit or planning clearance from the City prior to November 1, 2000 may remain, except that they may not be expanded, enlarged, converted to illuminated or changeable copy signage, or replaced once removed.
- (f) Freestanding signage shall be placed perpendicular to approaching traffic.
- (g) Signs within a development project shall be designed and installed with continuity by use of a coordinating scheme of color, typeface, materials, and construction details throughout the project area.
- (h) On freestanding signs, only graphics and typography are to be illuminated.
- (i) Freestanding signs shall be single- or double-faced and constructed of a metal panel with stone or veneer base. If the sign is single-faced, the backside shall be painted the same color as the cabinet and poles.
- (j) Flush wall signs shall not be illuminated externally; they may be either non-illuminated or internally illuminated.
- (k) Trail route identification signs shall be placed at critical locations which shall be determined by the Director during site plan review.



*Signs should communicate information and not add to visual pollution*



Date: June 22, 2016  
 Author: Scott D. Peterson  
 Title/Phone Ext:  
Senior Planner/1447  
 Proposed Schedule:  
July 12, 2016  
 File #: CPA-2016-252 &  
RZN-2016-253

**Attach 3**

**PLANNING COMMISSION AGENDA ITEM**

<b>Subject:</b> Redlands Hollow Rezone, Located at 508 22 ¼ Road
<b>Action Requested/Recommendation:</b> Forward a recommendation of approval to City Council to Rezone 2.88 acres from R-2 (Residential – 2 du/ac) to R-4 (Residential – 4 du/ac).
<b>Presenters Name &amp; Title:</b> Scott D. Peterson, Senior Planner

**Executive Summary:**

A request to rezone 2.88 acres from R-2 (Residential -2 du/ac) to R-4 (Residential 4 du/ac) zone district.

**Background, Analysis and Options:**

The subject property (Lot 2, Krause Subdivision), located at 508 22 ¼ Road, currently contains a single-family detached home and detached garage on 2.88 +/- acres. The applicant, Redlands Investment Properties, LLC, is in negotiations to purchase the property and is requesting to rezone the property to R-4 in anticipation of developing a residential subdivision.

The site was annexed into the City in 1999 as part of the Krause Annexation No. 1 and No. 2. The annexed property was zoned R-2 (Residential – 2 du/ac) which was in conformance with the Residential Low designation of the City’s Growth Plan.

In 2010 the City and County adopted the Comprehensive Plan which included the Future Land Use Map and the Blended Residential Land Use Categories Map (“Blended Map”). The new Future Land Use Map continued to designate the area where the property is located as Residential Low. The following zone districts are listed as appropriate zone districts to implement the Residential Low future land use category: RR, R-E, R-1, R-2, R-4 and R-5. The Blended Map as applied to this property allows up to five dwelling units per acre.

Therefore the proposed R-4 zone is compatible with (1) the Comprehensive Plan Future Land Use Map; (2) the Blended Map; (3) the surrounding R-2 (City) and RSF-4 (County) zoning; and (4) the surrounding single family uses.

**Neighborhood Meeting:**

A Neighborhood Meeting regarding the proposed zone change and subdivision application was held on April 19, 2016 with 11 citizens along with the applicant, applicant's representative's and City Project Manager in attendance. Area residents in attendance voiced concerns regarding increased traffic on 22 ¼ Road as a result of the proposed subdivision, increase in the overall density on the property and also some residents would like to keep a rural setting and not become part of a more urban environment.

**How this item relates to the Comprehensive Plan Goals and Policies:**

The proposed rezone meets the following Comprehensive Plan goals and policies:

**Goal 3:** The Comprehensive Plan will create ordered and balanced growth and spread future growth throughout the community.

**Policy B:** Create opportunities to reduce the amount of trips generated for shopping and commuting and decrease vehicle miles traveled thus increasing air quality.

**Policy C:** Increasing the capacity of housing developers to meet housing demand.

**How this item relates to the Economic Development Plan:**

The purpose of the adopted Economic Development Plan by City Council is to present a clear plan of action for improving business conditions and attracting and retaining employees. The proposed Rezone provides additional residential housing opportunities for residents of the community, located within the highly desirable Redlands area and near neighborhood commercial centers, elementary and junior high schools, which could contribute positively to employers' ability to attract and retain employees.

**Board or Committee Recommendation:**

There is no other committee or board recommendation.

**Other issues:**

There are no other issues identified.

**Previously presented or discussed:**

This has not been previously discussed by the Planning Commission.

**Attachments:**

1. Background information
2. Staff report
3. Site Location Map
4. Aerial Photo Map

5. Comprehensive Plan Future Land Use Map
6. Blended Residential Land Use Categories Map
7. Existing Zoning Map
8. Ordinance



BACKGROUND INFORMATION				
<b>Location:</b>		508 22 ¼ Road		
<b>Applicant:</b>		Barbara Krause, Owner Redlands Investment Properties LLC, Applicant		
<b>Existing Land Use:</b>		Single-family detached home		
<b>Proposed Land Use:</b>		Residential single-family detached subdivision		
<b>Surrounding Land Use:</b>	<b>North</b>	Single-family detached		
	<b>South</b>	Single-family detached		
	<b>East</b>	Single-family detached		
	<b>West</b>	Single-family detached		
<b>Existing Zoning:</b>		R-2 (Residential – 2 du/ac)		
<b>Proposed Zoning:</b>		R-4 (Residential – 4 du/ac)		
<b>Surrounding Zoning:</b>	<b>North</b>	County RSF-4 (Residential Single-Family – 4 du/ac)		
	<b>South</b>	R-2 (Residential – 2 du/ac)		
	<b>East</b>	R-2 (Residential – 2 du/ac)		
	<b>West</b>	County RSF-4 (Residential Single-Family – 4 du/ac)		
<b>Future Land Use Designation:</b>		Residential Low (0.5 – 2 du/ac)		
<b>Zoning within density/intensity range?</b>		X	Yes	No

**Sections 21.02.130 & 140 of the Grand Junction Zoning and Development Code:**

The City may rezone property if the proposed changes are consistent with the vision (intent), goals and policies of the Comprehensive Plan and meets one or more of the following criteria:

*(1) Subsequent events have invalidated the original premise and findings; and/or*

The property was annexed and zoned R-2 in 1999. In 2010 the City of Grand Junction and Mesa County jointly adopted a Comprehensive Plan, replacing the Growth Plan and establishing new land use designations. The Comprehensive Plan includes a Future Land Use Map and a Blended Residential Land Use Categories Map (“Blended Map”). The Blended Map blends compatible residential densities into three categories (Low, Medium and High), allowing overlapping of zones to provide flexibility to accommodate residential market preferences and trends, streamline the development process and support the Comprehensive Plan’s vision. The overlap of zones allows an appropriate mix of density for an area without being limited to a specific land use designation and does not create higher densities than what would be compatible with adjacent development.

The adoption of the Blended Map is a subsequent event or change that allows the property to be rezoned to a higher zone and therefore this criteria has been met.

*(2) The character and/or condition of the area has changed such that the amendment is consistent with the Plan; and/or*

The residential character within the immediate vicinity of the proposed rezone has not changed since the area developed in the 1980's. Within a larger area several residential developments have occurred since 2004. These developments were annexed and zoned R-4 and include Redlands Valley Subdivision (Swan Lane), Schroeder Subdivision (2 lots adjacent to Reed Mesa Drive), D & K Lucas Subdivision (Lucas Court) and Boulders Subdivision (Milena Way).

There are approximately 130 parcels within 1,000 feet of the proposed rezone. These parcels range from .20 acres to 9.8 acres or a density of 5 dwelling units per acre to 1 dwelling unit per 9.8 acres. The parcels within the immediate vicinity range from .32 acres to 8.3 acres or 3.1 dwelling units per acre to 1 dwelling unit per 8.3 acres.

Though the character and/or condition of the immediate vicinity of the property has not changed significantly within the last 30 years, the broader area has seen growth since the property was annexed and zoned in 1999. Also, given that the criterion includes that "the amendment is consistent with the Plan," and the requested zone is compatible with the surrounding single family uses/densities and consistent with the Comprehensive Plan, this criteria has been met.

*(3) Public and community facilities are adequate to serve the type and scope of land use proposed; and/or*

Adequate public and community facilities and services are available to the property and are sufficient to serve the residential land uses allowed in the R-4 zone district. Ute Water and City sanitary sewer are presently located within 22 ¼ Road. Property is also being served by Xcel Energy electric and natural gas. To the northeast, is a neighborhood commercial center that includes an office complex, veterinary clinic, convenience store, car wash and gas islands. Further to the east is another car wash, bank and medical clinic. Within walking distance are Broadway Elementary School, Redlands Middle School and area churches, located north of Broadway (Hwy 340). Less than a mile from the property is Grand Junction Redlands Fire Station No. 5.

Therefore, this criterion has been met.

*(4) An inadequate supply of suitably designated land is available in the community, as defined by the presiding body, to accommodate the proposed land use; and/or*

There is an adequate supply of suitably designed land available in the community as the R-4 zone district comprises the second largest amount of residential acreage within the City limits behind the R-8 zone district (Over 1,862 acres within the City limits is zoned R-4).

Therefore, this criterion has not been met.

*(5) The community or area, as defined by the presiding body, will derive benefits from the proposed amendment.*

The proposed R-4 zone would implement Goal 3 of the Comprehensive Plan by creating an opportunity for future residential development which will provide additional residential housing opportunities for residents of the community, located within the highly desirable Redlands area and near neighborhood commercial centers, elementary and junior high schools, which could contribute positively to employers' ability to attract and retain employees.

Therefore, this criterion has been met.

Alternatives: The following zone districts would also be consistent with the Future Land Use designation of Residential Low for the subject property:

- a. R-R (Residential – Rural)
- b. R-E (Residential – Estate)
- c. R-1 (Residential – 1 du/ac)
- d. R-5 (Residential – 5 du/ac)

In reviewing the other zoning district options, the residential zone districts of R-R, R-E and R-1, would have a lower overall maximum density than what the property is currently zoned (R-2), and the R-5 zone district would allow more density. The applicant has requested an R-4 zone.

If the Planning Commission chooses to recommend one of the alternative zone designations, specific alternative findings must be made as to why the Planning Commission is recommending an alternative zone designation the City Council.

## **FINDINGS OF FACT AND CONCLUSIONS**

After reviewing the Redland Hollow Rezone, RZN-2016-253, a request to zone 2.88 acres from R-2 (Residential – 2 du/ac) to R-4 (Residential – 4 du/ac) zone district, the following findings of fact and conclusions have been determined:

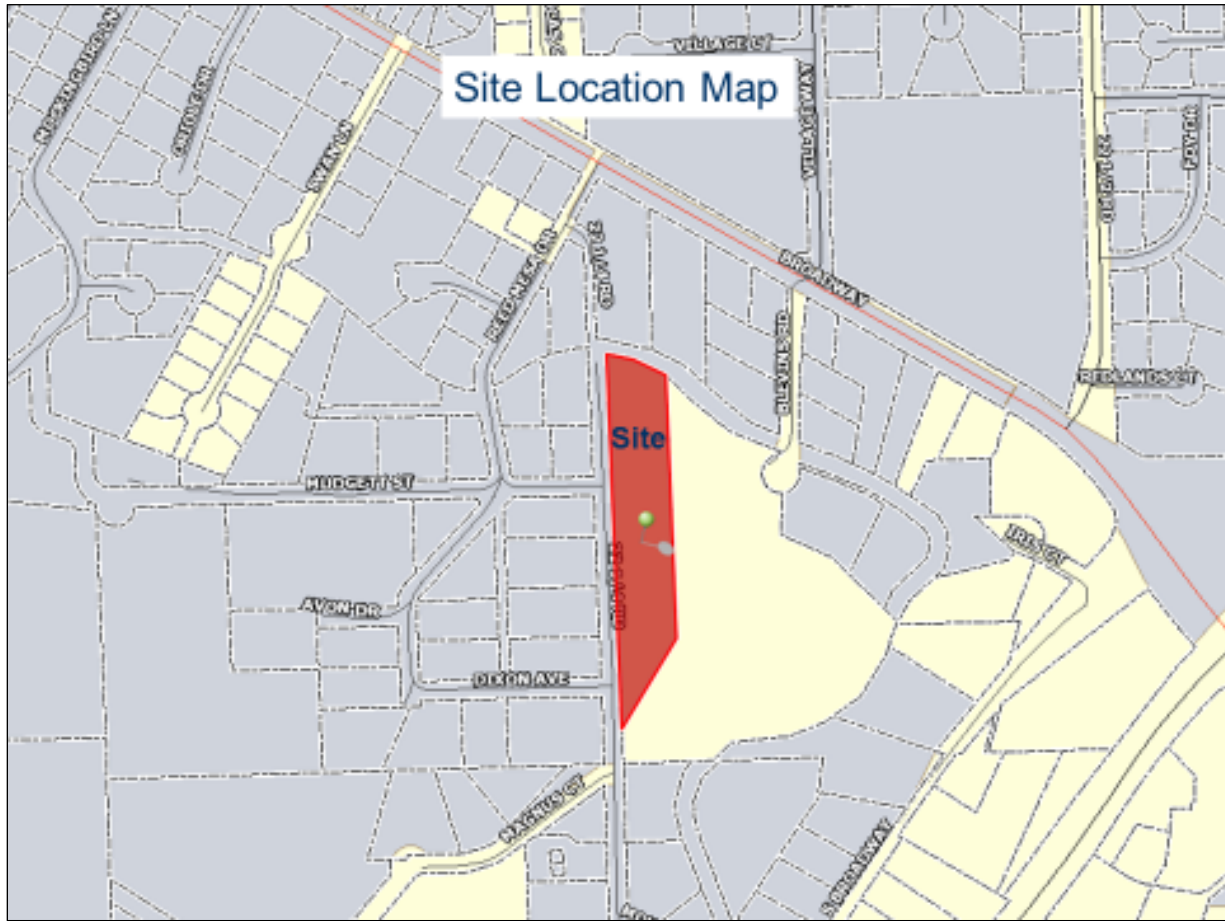
1. The requested zone is consistent with the goals and policies of the Comprehensive Plan;
2. All review criteria Section 21.02.140 of the Grand Junction Municipal Code, except for criterion 4, have been met.

## **STAFF RECOMMENDATION:**

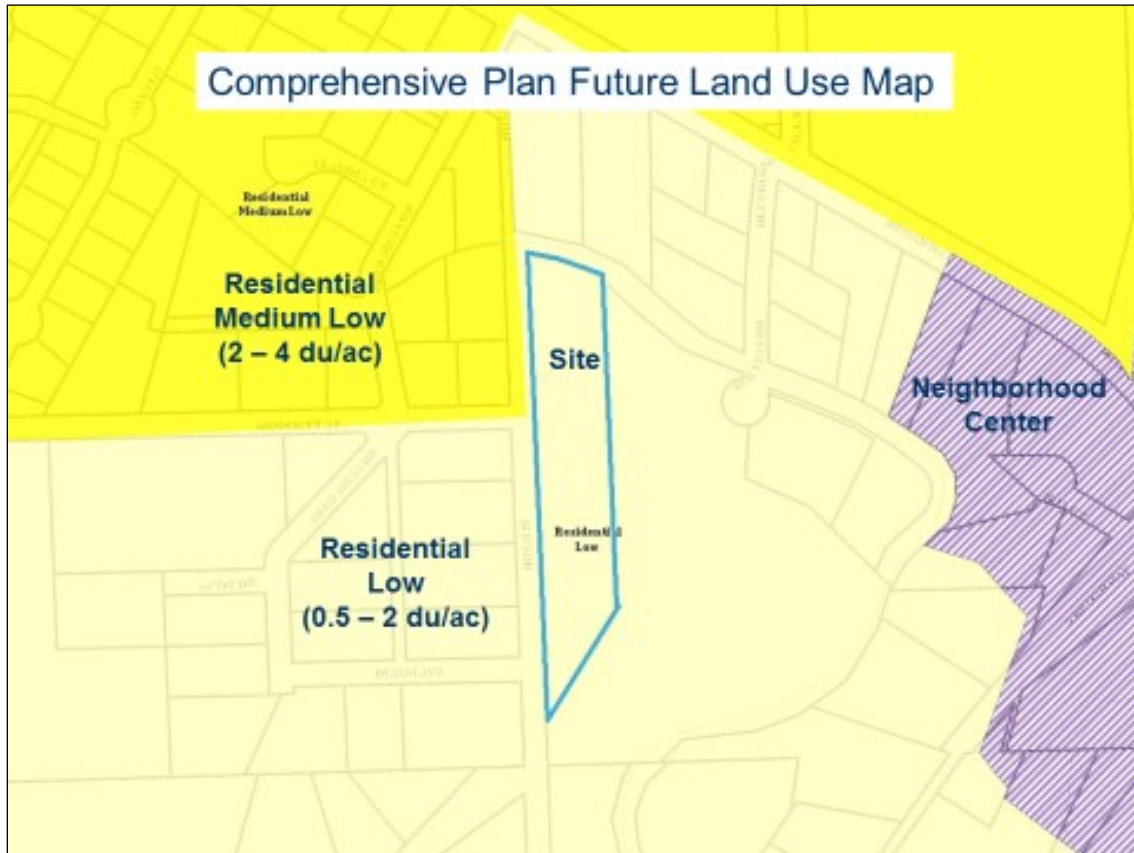
I recommend that the Planning Commission forward a recommendation of approval of the requested rezone from R-2 (Residential 2 du/ac) to an R-4 (Residential – 4 du/ac) zone district for RZN-2016-253, to the City Council with the findings and conclusions listed above.

## **RECOMMENDED PLANNING COMMISSION MOTION:**

Madam Chairman, on the Rezone request RZN-2016-253, I move that the Planning Commission forward a recommendation of approval for the Redlands Hollow Rezone from an R-2 (Residential 2 du/ac) to an R-4 (Residential – 4 du/ac) zone district with the findings of fact and conclusions listed in the staff report.













**From:** "John F. Whitcomb" <jfwhitcomb@gmail.com>  
**To:** <scottp@gjcity.org>  
**Date:** 5/13/2016 6:29 PM  
**Subject:** Redlands Hollow Subdivision

Dear Mr Peterson,

My name is John Whitcomb and I live at 484 22 1/4 Rd. My wife and I are very concerned about the proposed re-zoning of the land on 22 1/4 Rd to allow six residential homes to be built. We have a quiet neighborhood here, with homes built on large lots. You could fit 3 of these proposed homes on our lot alone. We don't need to crowd these homes on to tiny lots. Traffic is already a problem on our street where many vehicles turn from Broadway on to Reed Mesa or South Broadway as a shortcut to Redlands Pkwy or Broadway to avoid the intersection. With the scheduled construction of a roundabout at that intersection. Adding the traffic that these houses would bring would only exacerbate the already steady flow of vehicles through what was once a quiet area. Please do not over crowd our little streets and further complicate our traffic problems.

The builder states he wants "High End" homes but there is no guarantee of that. High end homes aren't built on 0.3 acres lots. We also have a water runoff problem here from the Riggs hill area down towards these lots.

In summary, if homes must be built on this property they should be restricted to 3 homes on 1/2 acre lots not 6 houses crammed into a small space.

Sincerely,

1stSgt John F. Whitcomb USMC Ret.

**From:** <keibo21@aol.com>  
**To:** <scottp@gjcity.org>  
**CC:** <Keibo21@aol.com>, <SPGarden@aol.com>  
**Date:** 5/17/2016 9:21 AM  
**Subject:** Proposed Redlands Hollow Subdivision

Hello Scott,

(A previous, incomplete version may have been sent to you, if that is so, then this is the complete email we intended to send you)

Our names are Campbell and Susan Stanton and we have lived at 503 Reed Mesa Drive for nearly 25 years. We reside approximately 1 block west of the proposed "Redlands Hollow Subdivision" which would consist of six small lots/houses tentatively planned to be located along the east side of 22 1/4 Road contingent on rezoning of the property. Our neighborhood is unique in that it has homes on fairly good sized lots that are not crammed right up to each other thus providing an open, country feel that we all enjoy and savor. This was one of the principal reasons we and others moved here. Although some traffic utilizes our streets as a shortcut around the Hwy 340/Redlands Parkway intersection, it is relatively quiet and traffic is tolerable.

While we're not opposed to development of vacant land, we are opposed to the integrity of our neighborhood being compromised by overdevelopment. We oppose the rezoning of the lots from R-2 to R-4. We feel that no more than three or four homes should be permitted on larger parcels within the proposed subdivision if the project were to proceed.

We thank you for your consideration and appreciate the opportunity to voice our opinion on this matter.

Sincerely,

Campbell & Susan Stanton

**From:** Sharon Sigrist <sharonsigrist@gmail.com>  
**To:** <scottp@gjcity.org>  
**Date:** 5/12/2016 10:34 AM  
**Subject:** Redlands Hollow Subdivision

To whom it concerns,

My name is Sharon Sigrist I live at 2215 Dixon Ave. My husband, myself and our neighbors are very concerned over the proposed rezoning of the land on 22 1/4 rd. to allow 6 residential homes to be built there. Our concerns are traffic, and over crowding in one little area. There is plenty of open land in this area, we do not need to crowd one street, on block one neighborhood. Traffic is a problem already with people using Reed Mesa to South Broadway as a short cut around the Redlands Parkway - Broadway intersection. This short cut is probably uncontrollable but adding 30+ more vehicle trips a day by adding 6 homes can be prevented. Please do not over crowd our neighborhood and further complicate the traffic problems in these few little streets.

The builder states he wants to build high end homes, but what if he doesn't? We will have low rent transient renters that are not healthy for an established family neighborhood. He also, does not have to put in curb & gutter. I realize the rest of the neighborhood does not have nor want, but I am not convinced that is the right way to proceed. There is a huge water run off problem already in this neighborhood, yes it only happens once or twice a year but non the less I do not think just a retention pond is the answer and could open a can of worms.

It is quiet here, we have had lots of new homes built in this area, they have ALL been built on large lots EXCEPT what is being asked for here!

Sharon Sigrist

**From:** Sharon Sigrist <sharonsigrist@gmail.com>  
**To:** Scott Peterson <scottp@ci.grandjct.co.us>  
**Date:** 5/24/2016 8:36 AM  
**Subject:** Re: Redlands Hollow Subdivision

Scott, Please keep me up dated as to the Redlands Hollow Subdivision. Has the builder submitted a plan? How many homes does he PLAN to build. Some workers were out 2 weeks ago and they were very rude to the home owners around the lot. They parked their equipment blocking driveways. One of my neighbors came home with a new born & could not get to her drive. I do not know where this lack of consideration is coming from but I hope that changes, too. DO NOT CHANGE THE ZONING PLEASE!

**From:** Ruth Reed <ruthreed2221@gmail.com>  
**To:** <scottp@gjcity.org>  
**Date:** 5/16/2016 3:07 PM  
**Subject:** Redlands Hollow Subdivision

We do not need to change the zoning in our little enclave. I agree with all of my neighbors for all of the reasons stated in their correspondence that the zoning is fine just the way it is. No changes please.

**From:** Audrey Mullis <yerdua6454@hotmail.com>  
**To:** "scottp@gjcity.org" <scottp@gjcity.org>  
**Date:** 5/31/2016 11:46 AM  
**Subject:** Redlands Hollow Subdivision

Dear Mr. Peterson,

My name is Audrey Mullis, 2208 Mudgett St, GJ, CO 81507. I spoke with you by phone last week. You requested that I put my concerns about the Redlands Hollow Subdivision in writing. Although I now live west of the area on Mudgett St, I formerly lived at 517 22 1/4 Road for 25 years and am very familiar with the Reed Mesa Subdivision in general and thus interested in preserving the rural atmosphere. My concerns are:

1 - Traffic...due to a full scale commercial Auto Repair operation that has somehow developed (I assume illegally) at 519 Reed Mesa Drive, the traffic coming and going from Broadway has increased substantially. There are as many as 20+ vehicles in the yard at any given time, with trailers delivering and picking up these units on a daily basis. With only Reed Mesa Drive and Mowry Drive for ingress & egress, the traffic situation is obviously already strained. The addition of six more homes/families would add that much more.

2 - Property Values: The Reed Mesa Subdivision has grown considerably since we moved here in 1969, but it has managed until recently to retain the rural atmosphere. Adding six closely-packed homes would not only change that concept but in the process would also lower property values. It might be reasonable to add 3 or 4 homes on that entire property with lots similar in size to those adjacent to the property, but certainly not the development as proposed.

Thank you,

Audrey Mullis

970-243-5184

**From:** "Debbie Moesser" <dmoesser@bresnan.net>  
**To:** <scottp@gjcity.org>  
**Date:** 5/15/2016 12:35 PM  
**Subject:** Redlands Hollow Subdivision

May 15, 2016

To Scott or Whom It May Concern,

My name is Debbie Moesser and I live at 2220 Claudia Ct. My neighbors and I are VERY concerned over the purposed rezoning of the land on 22 ¼ Rd, allowing 6 residential homes to be built there. Our concerns are traffic (there are too many SPEEDING down Reed Mesa now) and over crowing in one little area. There is plenty of open land in this area. We do not need to crowd one street in this neighborhood.

Traffic is a problem already with people using Reed Mesa, once again SPEEDING, with kids playing, to South Broadway as a short cut around Redlands Parkway – Broadway intersection. This short cut is probably uncontrollable but adding 30+ more vehicle trips a day by adding 6 homes can be prevented.

PLEASE do not over crowd our neighborhood and further complicate the traffic problems on the little streets.

The builder says he wants to build high end homes, but what if he does not? We will have low rent transient renters that are not healthy for an established family neighborhood.

It is a quiet neighborhood. The homes that have been built in the area have been built on LARGE lots EXCEPT what is being asked for here!

PLEASE DO NOT LET HIM BUILD!

Thank you,

Debbie Moesser

2220 Claudia Ct.



**From:** Sarah Livingston <sarah.m.livingston@gmail.com>  
**To:** <scottp@gjcity.org>  
**Date:** 7/2/2016 6:09 PM  
**Subject:** Redlands hollow subdivision-rezoning

To whom it may concern,

My name is Sarah Livingston and I live at 517 22 1/4 at the end of the cul-de-sac, directly across the street from the proposed Redlands Hollow Subdivision. I am in opposition to the request to rezone 2.88 +/- acres from an R-2 to R-4 zone district. I am in opposition of this rezoning for a few reasons:

1. The lot sizes will be significantly smaller therefore changing the fabric & historical character of the neighborhood.
2. There is already a lot of traffic that turns around and my personal driveway because they do not realize it is not a "thru street". I do not want six houses because this will cause even more traffic.
3. The narrowness of our street will not allow for people to turn their cars around except in my driveway.
4. Our view of the Book Cliffs will be obstructed.
5. It is highly likely that property tax will raise much higher due to the value of these homes proposed (we were told they would be in the \$400,000 range).

I understand that I cannot to stop the development of four homes, but I would like to stop the development of six. These are the reasons that I oppose the rezoning of the Redlands hollow subdivision.

Respectfully,  
Sarah Livingston

Sent from my iPhone

**CITY OF GRAND JUNCTION, COLORADO**

**ORDINANCE NO.**

**AN ORDINANCE ZONING THE PROPOSED REDLANDS HOLLOW REZONE TO R-4 (RESIDENTIAL – 4 DU/AC)**

**LOCATED AT 508 22 1/4 ROAD**

Recitals:

After public notice and public hearing as required by the Grand Junction Zoning and Development Code, the Grand Junction Planning Commission recommended approval of zoning the proposed Redlands Hollow Subdivision to the R-4 (Residential – 4 du/ac) zone district, finding that it conforms to and is consistent with the Future Land Use Map designation of Residential Low Density and the Blended Residential Land Use Map category of Residential Low Density of the Comprehensive Plan and the Comprehensive Plan’s goals and policies and is generally compatible with land uses located in the surrounding area.

After public notice and public hearing, the Grand Junction City Council finds that the R-4 (Residential – 4 du/ac) zone district is in conformance with at least one of the stated criteria of Section 21.02.140 of the Grand Junction Zoning and Development Code.

**BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF GRAND JUNCTION THAT:**

The following property shall be zoned R-4 (Residential – 4 du/ac):

Lot 2, Krause Subdivision as identified in Reception # 1902961 in the Office of the Mesa County Clerk and Recorder.

Introduced on first reading this \_\_\_\_\_ day of \_\_\_\_\_, 2016 and ordered published in pamphlet form.

Adopted on second reading this \_\_\_\_\_ day of \_\_\_\_\_, 2016 and ordered published in pamphlet form.

ATTEST:

\_\_\_\_\_  
City Clerk

\_\_\_\_\_  
Mayor