



**PLANNING COMMISSION AGENDA  
CITY HALL AUDITORIUM, 250 NORTH 5TH STREET  
TUESDAY, JULY 24, 2012, 6:00 PM**

**Call to Order**

*Welcome. Items listed on this agenda will be given consideration by the City of Grand Junction Planning Commission. Please turn off all cell phones during the meeting.*

*If you wish to speak, please sign in prior to coming up to the podium. Sign in sheets are located at the back of the auditorium. In an effort to give everyone who would like to speak an opportunity to provide their testimony, we ask that you try to limit your comments to 3-5 minutes. If someone else has already stated your comments, you may simply state that you agree with the previous statements made. Please do not repeat testimony that has already been provided. Inappropriate behavior, such as booing, cheering, personal attacks, applause, verbal outbursts or other inappropriate behavior, will not be permitted.*

*Copies of the agenda and staff reports are located at the back of the auditorium.*

**Announcements, Presentations and/or Prescheduled Visitors**

**Consent Agenda**

*Items on the consent agenda are items perceived to be non-controversial in nature and meet all requirements of the Codes and regulations and/or the applicant has acknowledged complete agreement with the recommended conditions.*

*The consent agenda will be acted upon in one motion, unless the applicant, a member of the public, a Planning Commissioner or staff requests that the item be removed from the consent agenda. Items removed from the consent agenda will be reviewed as a part of the regular agenda. Consent agenda items must be removed from the consent agenda for a full hearing to be eligible for appeal or rehearing.*

**1. Minutes of Previous Meetings**

**[Attach 1](#)**

Approve the minutes of the May 22, June 12 and June 26, 2012 regular meetings.

**2. SBT-GJLC Telecommunications Tower – Conditional Use Permit** [Attach 2](#)

Request approval of a Conditional Use Permit for a Telecommunication Facility and Support Structure to allow a new 110' telecommunications tower with the potential for 5 additional collocations.

**FILE #:** CUP-2012-362  
**APPLICANT:** Rex Jennings – SBT Internet  
**LOCATION:** 400 23 Road  
**STAFF:** Senta Costello

**\* \* \* END OF CONSENT CALENDAR \* \* \***

**\* \* \* ITEMS NEEDING INDIVIDUAL CONSIDERATION \* \* \***

**Public Hearing Items**

*On the following item(s) the Grand Junction Planning Commission will make the final decision or a recommendation to City Council. If you have an interest in one of these items or wish to appeal an action taken by the Planning Commission, please call the Planning Division (244-1430) after this hearing to inquire about City Council scheduling.*

**3. Greater Downtown Plan and Overlay – Comprehensive Plan Text Amendment, Comprehensive Plan Future Land Use Map Amendments, Rezoning Properties within the Greater Downtown Plan area, Zoning Overlays for Corridors and the Downtown District** [Attach 3](#)**a. Comprehensive Plan and Future Land Use Map Amendments**

Request a recommendation of approval to City Council of the Greater Downtown Plan as an amendment to the Comprehensive Plan which includes Future Land Use Map amendments and a new Circulation and Trails Plan.

**FILE #:** CPA-2011-1067  
**APPLICANT:** City of Grand Junction  
**LOCATION:** Citywide  
**STAFF:** Kristen Ashbeck

**b. Comprehensive Plan Text Amendment**

Request a recommendation of approval to City Council to amend text of the Comprehensive Plan to allow the R-O (Residential Office) zone district in the Downtown Mixed Use land use designation.

**FILE #:** CPA-2012-216  
**APPLICANT:** City of Grand Junction  
**LOCATION:** Citywide  
**STAFF:** Kristen Ashbeck



**c. Rezone**

Request a recommendation of approval to City Council to approve City-initiated rezoning of properties within the Greater Downtown Plan area.

**FILE #:** RZN-2012-217  
**APPLICANT:** City of Grand Junction  
**LOCATION:** Citywide  
**STAFF:** Kristen Ashbeck

**d. Text amendments to Section 21.07 and Section 21.03.020(d) – Zoning Code Amendments**

Request a recommendation of approval to City Council to approve the Greater Downtown Plan Zoning Overlay as an amendment to Section 21.07 of the Zoning and Development Code, and as an amendment to Section 21.03.020(d) to add the R-O (Residential Office) zone district as a zone district that can implement the Downtown Mixed Use Comprehensive Plan land use designation.

**FILE #:** ZCA-2012-363  
**APPLICANT:** City of Grand Junction  
**LOCATION:** Citywide  
**STAFF:** Kristen Ashbeck

**General Discussion/Other Business****Nonscheduled Citizens and/or Visitors****Adjournment**

**Attach 1**  
**Minutes of Previous Meetings**

**GRAND JUNCTION PLANNING COMMISSION**  
**MAY 22, 2012 MINUTES**  
**6:00 p.m. to 6:08 p.m.**

The regularly scheduled Planning Commission hearing was called to order at 6:00 p.m. by Chairman Wall. The public hearing was held in the Mesa County Public Hearing Room.

In attendance, representing the City Planning Commission, were Reginald Wall (Chairman), Lynn Pavelka (Vice-Chairman), Ebe Eslami, Lyn Benoit, Gregory Williams, Loren Couch (Alternate) and Jon Buschhorn. Commissioners Pat Carlow and Keith Leonard were absent.

In attendance, representing the City's Public Works and Planning Department – Planning Division, were Lisa Cox (Planning Manager), Senta Costello (Senior Planner) and Brian Rusche (Senior Planner).

Also present was Jamie Beard (Assistant City Attorney).

Greg Moberg (Planning Supervisor) was present to record the minutes.

There were no interested citizens present during the course of the hearing.

**Announcements, Presentations and/or Visitors**

None.

**Consent Agenda**

**1. Minutes of Previous Meetings**

Approve the minutes of the March 27 and April 10, 2012 regular meetings.

**2. Fuoco Waterline Easement Vacation – Vacation of Easement**

Request a recommendation of approval to City Council to vacate a portion of a 15' public water line easement.

**FILE #:** VAC-2012-272

**PETITIONER:** Bob Fuoco – Fuoco Investments LLC

**LOCATION:** 2582 Highway 6 and 50

**STAFF:** Senta Costello

**3. Raser Special Permit – Special Permit – MOVED FROM PUBLIC HEARING**  
**ITEMS TO THE CONSENT AGENDA**

Request a recommendation of approval to City Council for a special permit on 0.527 acres in a C-2 (General Commercial) zone district.

**FILE #:** SPT-2012-265

**PETITIONER:** Joan Raser - Joan Raser LLC

**LOCATION:** 2476 and 2476 1/2 West Independent Avenue

**STAFF:** Brian Rusche

**4. Text Amendments to Sections 21.01.030, 21.02.070, 21.04.010, 21.04.020, 21.10.020 – Zoning Code Amendment – MOVED FROM PUBLIC HEARING ITEMS TO THE CONSENT AGENDA**

Request a recommendation of approval to City Council for a text amendment to Section 21.01.030, Application; Section 21.02.070(e), Change of Use Permit; Section 21.04.010(a), Use Categories and Principal Uses; and Section 21.10.020, Terms Defined – Change in Use – Principal Use.

**FILE #:** ZCA-2012-249  
**PETITIONER:** City of Grand Junction  
**LOCATION:** Citywide  
**STAFF:** Lisa Cox

Chairman Wall briefly explained the Consent Agenda and invited the public, planning commissioners, and staff to speak if they wanted any item pulled for additional discussion. Lisa Cox, Planning Manager, requested that Item 3, Raser Special Permit and Item 4, proposed Text Amendments, be moved to the Consent Agenda. Jamie Beard, Assistant City Attorney, advised that a motion would not be needed to move Items 3 and 4 to the Consent Agenda as long as there was no objection from the audience or the Commissioners. After discussion, there were no objections or comments received from the audience or Planning Commissioners on the Consent Agenda items.

**MOTION:(Commissioner Pavelka) “I move we approve the Consent Agenda as read with Items 1, 2, 3 and 4.”**

Commissioner Benoit seconded the motion. A vote was called and the motion passed unanimously by a vote of 6 - 0.

**Public Hearing Items**

**3. Raser Special Permit – Special Permit – MOVED TO CONSENT AGENDA**

**4. Area 1 Rezone – Rezone – MOVED TO CONSENT AGENDA**

**General Discussion/Other Business**

Lisa Cox, Planning Manager, mentioned that the annual Planning Commissioner and Zoning Board of Appeals training was scheduled for Wednesday, June 6<sup>th</sup> and would be held at the Stocker Stadium Hospitality Suite. The next Planning Commission workshop was scheduled for Thursday, June 7<sup>th</sup>.

**Nonscheduled Citizens and/or Visitors**

None.

**Adjournment**

With no objection and no further business, the Planning Commission meeting was adjourned at 6:08 p.m.

**GRAND JUNCTION PLANNING COMMISSION**  
**JUNE 12, 2012 MINUTES**  
**6:00 p.m. to 6:03 p.m.**

The regularly scheduled Planning Commission hearing was called to order at 6:00 p.m. by Chairman Wall. The public hearing was held in the Mesa County Public Hearing Room.

In attendance, representing the City Planning Commission, were Reginald Wall (Chairman), Lynn Pavelka (Vice-Chairman), Pat Carlow, Ebe Eslami, Lyn Benoit, Loren Couch (Alternate) and Jon Buschhorn (Alternate). Commissioners Lyn Benoit and Keith Leonard were absent.

In attendance, representing the City's Public Works and Planning Department – Planning Division, were Lisa Cox (Planning Manager) and Greg Moberg (Planning Supervisor).

Also present was Jamie Beard (Assistant City Attorney).

Lynn Singer was present to record the minutes.

There was 1 interested citizen present during the course of the hearing.

**Announcements, Presentations and/or Visitors**

None.

**Consent Agenda**

**1. Minutes of Previous Meetings**

None available at this time.

**2. Summer Hill Subdivision Amendment – Planned Development – Amendment**

Request a recommendation of approval to City Council to amend the existing (PD, Planned Development Ordinance and Preliminary Plan for Summer Hill Filing 6 and Future Filings to modify the minimum side yard building setback requirement from 7' to 5' for principal structures, to change the maximum lot coverage from 50% to 70% and to clarify the bulk requirements and allowed housing types. Request is also to approve a phasing plan of up to 6 years or until 2018 in order to obtain approval for Future Filings 7 & 8.

**FILE #:** PLD-2012-247

**PETITIONER:** Kevin Bray – Paradise Hills Properties

**LOCATION:** Summer Hill Court

**STAFF:** Scott Peterson

Chairman Wall briefly explained the Consent Agenda and invited the public, planning commissioners, and staff to speak if they wanted any item pulled for additional discussion. After discussion, there were no objections or revisions received from the audience or Planning Commissioners on the Consent Agenda items.

**MOTION:(Commissioner Pavelka) “I move we approve the Consent Agenda as read.”**

Commissioner Eslami seconded the motion. A vote was called and the motion passed unanimously by a vote of 6 - 0.

**Public Hearing Items**

None.

**General Discussion/Other Business**

None.

**Nonscheduled Citizens and/or Visitors**

None.

**Adjournment**

With no objection and no further business, the Planning Commission meeting was adjourned at 6:03 p.m.

**GRAND JUNCTION PLANNING COMMISSION  
JUNE 26, 2012 MINUTES  
6:00 p.m. to 6:21 p.m.**

The regularly scheduled Planning Commission hearing was called to order at 6:00 p.m. by Chairman Wall. The public hearing was held in the City Hall Auditorium.

In attendance, representing the City Planning Commission, were Reginald Wall (Chairman), Lynn Pavelka (Vice-Chairman), Pat Carlow, Ebe Eslami, Loren Couch (Alternate) and Jon Buschhorn (Alternate). Commissioners Lyn Benoit, Gregory Williams and Keith Leonard were absent.

In attendance, representing the City's Public Works and Planning Department – Planning Division, were Lisa Cox (Planning Manager), Greg Moberg (Planning Supervisor) and Senta Costello (Senior Planner).

Also present was Jamie Beard (Assistant City Attorney).

Lynn Singer was present to record the minutes.

There was 1 interested citizen present during the course of the hearing.

**Announcements, Presentations and/or Visitors**

None.

**Consent Agenda**

**1. Minutes of Previous Meetings**

None available at this time.

**2. Vodopich Subdivision – Subdivision – Extension**

Request approval of a one-year extension of the Preliminary Subdivision Plan for Vodopich Subdivision, a 10-lot subdivision on 3.22 acres in an R-4 (Residential 4 du/ac) zone district.

**FILE #:**PFP-2006-243

**PETITIONER:**Bill Nesheim – JBB Corporation

**LOCATION:**3023 F ½ Road

**STAFF:**Greg Moberg

**3. Ute Water Tank Tower #2 – SBT Internet – Conditional Use Permit**

Request approval of a Conditional Use Permit to allow the construction and maintenance of a telecommunications facility and support structure.

**FILE #:**CUP-2012-276

**PETITIONER:**Rex Jennings – SBT Internet

**LOCATION:**380 South Camp Road

**STAFF:**Senta Costello

Chairman Wall briefly explained the Consent Agenda and invited the public, planning

commissioners, and staff to speak if they wanted any item pulled for additional discussion. Lisa Cox, Planning Manager, said that staff had received information regarding the Conditional Use Permit on the SBT Internet application and asked Senta Costello to provide an overview of the information so the Commission could decide how to proceed.

Senta Costello, Senior Planner, Public Works and Planning Department, addressed the Commission and reported that they had received an email letter today from the representative of the property owner who owned the property surrounding the site where the applicant intended to place the tower. The representative had a couple of concerns, primarily the access to the property which had historically been accessed via an easement from Broadway up to the water tank and that Ute Water had been the grantor of that easement.

Ms. Costello stated that historically, the City had several applications come through to add facilities or to increase the height on the existing tower, and the access easement that Ute Water utilized had been used as the access for the project. That's how access had been viewed for those past applications. As far as the City was aware there was legal access for the existing water tank as well as the existing tower on the property. Chairman Wall asked if any documentation was needed for the applicant. Ms. Costello said the easement itself was the document that dictated how that easement could be used. She added that historically staff had based their review on the information provided by the property owner as far as what their legal rights were for use of the easement. She said that it had been used as the legal access for the other facilities that existed on the property. She noted that there had never been any question or contention as to whether or not the access was appropriate by the property owner surrounding the site that the easement crossed.

Chairman Wall said that it appeared to him that the landowner believed the easement was intended only for Ute Water. Ms. Costello said that was her understanding as well. Chairman Wall asked if there was additional documentation that the applicant needed to have the right to use the easement since Ute Water was not the applicant. Jamie Beard, Assistant City Attorney, suggested that the easement documents needed to be reviewed to determine whether or not there was access for the applicant based on what they were relying on. It appeared that Ute Water itself had an easement but was unclear on exactly what the terms of that easement were to cross the landowner's property who had made the complaint. She added that she was unable to access those particular documents to review and was unsure of whether or not there was any other easement, right or claim that the applicant might have to be able to cross the property. Ms. Beard said that there was not enough information at this time to say whether they did or didn't have the right to use the easement for access. She said that normally in order for an application to go forward, access for the use would be required; however, sufficient information was not available at this time to make that determination. Ms. Beard said the applicant relied on information that they thought was correct; however, it appeared that it was possible that they did not have the right information. Ms. Costello added that historically it had been the City's belief that there was legal access to the property not only Ute Water but the existing tower on the property. She thought it was an issue that could be resolved.



Commissioner Carlow asked about the discrepancy related to the height of the tower. Ms. Costello said the application that had been turned in was for a 110' lattice tower and the support ground facility associated with it. Commissioner Carlow said the gentleman building it said it would only be 30'. Chairman Wall asked if it could be 30' higher than the original tower. Ms. Costello said that Mr. Jennings was the applicant and would be able to clear up any issue with regard to the height. Chairman Wall said that he did not want to open it up for a full hearing right now as it appeared to him that there were still a lot of questions that needed to be answered for the adjacent property owner. Commissioner Couch believed the documents questioning access were generated by a management company and not the owner of the property. Assistant City Attorney Ms. Beard confirmed that the owner of the property where the tower would be placed was Ute Water Conservancy District; however, to get access to that property, one would have to cross someone else's property. Ms. Beard confirmed that the landowner was a limited liability company and one of the members of the LLC had raised the objection.

She outlined the Commission's choices as going forward with the request on the Consent agenda; deny the request on the Consent agenda; require the request be set for a full hearing; continue the request; or, after a full public hearing, if the Commission believed there was not enough information to make a decision, remand the request back to staff for further review. Chairman Wall asked if it was left on consent, could conditions be placed on the item. Ms. Beard said that if the Planning Commission action was anything more than approving the request as is, then there would have to be a full hearing because the reasons for the conditions would need to be identified. Commissioner Pavelka asked if the Commission could continue this item until the access was resolved, or could it be heard and approved with conditions so that the approval was conditioned upon access being resolved. Ms. Beard said that it could but the difficulty was that if it was determined that there wasn't sufficient access, staff would normally not make a recommendation to approve the request; staff had brought the request forward based on information different than what was now before the Commission. She added that legal access may make a difference as to what the final recommendation would be because with a Conditional Use Permit, factors such as compatibility and how the proposed use may affect surrounding properties would have to be considered.

Commissioner Carlow asked if this item should be denied and have the applicant resubmit the application to provide needed information. Ms. Beard said that would be a possibility with a full public hearing but with a continuance and remanding it back to staff for further review, the application would be kept alive while giving the applicant an opportunity to resolve this issue. Commissioner Carlow said that he would prefer to remand it back because if it was continued he assumed a date would have to be assigned to the continuance. Ms. Beard said that it was not necessary to continue the item to a date certain as staff could bring it back to the Commission when it appeared that it was ready to go forward again. Commissioner Couch asked if it was continued, how would the applicant recoup their expenses during the period of continuance. Ms. Beard said the expense to the applicant, as far as an issue between the applicant and the landowner, was really between the applicant and the landowner.

She advised that if the applicant could provide proof of an easement that gave them the right to cross the adjacent property, the Commission may be in a better position to go

forward with a conditional approval. She said that she was not aware that applicant had proved easement rights across the property at this time.

Commissioner Eslami asked if he was correct in that if this was approved tonight and they could not get access, then the tower could not be built. Ms. Beard said that staff's recommendation was made before the information from the adjacent property owner had been received.

**MOTION:(Commissioner Pavelka) "Mr. Chairman, I move we approve Item Number 2 on the Consent Agenda and with Item Number 3, I move that we continue it, based on information with respect to the access being resolved."**

Commissioner Carlow seconded the motion. A vote was called and the motion passed unanimously by a vote of 6 - 0.

**Public Hearing Items**

None.

**General Discussion/Other Business**

None.

**Nonscheduled Citizens and/or Visitors**

None.

**Adjournment**

With no objection and no further business, the Planning Commission meeting was adjourned at 6:21 p.m.

**Attach 2**  
**SBT-GJLC Telecommunications Tower**

**CITY OF GRAND JUNCTION**  
**PLANNING COMMISSION**

**MEETING DATE:** July 24, 2012  
**PRESENTER:** Senta Costello

**AGENDA TOPIC:** SBT Telecommunications Tower – CUP-2012-362

**ACTION REQUESTED:** Approval of a Conditional Use Permit (CUP)

BACKGROUND INFORMATION				
<b>Location:</b>		400 23 Road		
<b>Applicants:</b>		Owner: Grand Junction Land Company LLC Applicant: SBT Internet – Rex Jennings		
<b>Existing Land Use:</b>		Vacant residential		
<b>Proposed Land Use:</b>		New 110' lattice telecommunications tower with support structure		
<b>Surrounding Land Use:</b>	<b>North</b>	Vacant residential		
	<b>South</b>	Vacant residential		
	<b>East</b>	Vacant residential		
	<b>West</b>	Vacant residential		
<b>Existing Zoning:</b>		PD (Planned Development)		
<b>Proposed Zoning:</b>		PD (Planned Development)		
<b>Surrounding Zoning:</b>	<b>North</b>	PD (Planned Development)		
	<b>South</b>	PD (Planned Development)		
	<b>East</b>	PD (Planned Development)		
	<b>West</b>	PD (Planned Development)		
<b>Future Land Use Designation:</b>		Conservation		
<b>Zoning within density range?</b>		X	Yes	No

**PROJECT DESCRIPTION:** A request for approval of a Conditional Use Permit to construct a new 110' lattice telecommunications tower in a PD (Planned Development) zone district in accordance with Table 21.04.010 of the Grand Junction Municipal Code.

**RECOMMENDATION:** Approval of the Conditional Use Permit

## **ANALYSIS:**

### **1. Background**

The property was annexed in 1992 as part of the Ridges Majority Annexation #3 and zoned Planned Residential (PR). With the adoption of the Zoning and Development Code in 2000, all “Planned” zoned districts were given a designation of Planned Development (PD). The Planned Development zone exists without an associated plan for development of the property, but remains a residential use zone district and is undeveloped.

### **2. Consistency with the Comprehensive Plan**

The site is currently zoned PD (Planned Development) with the Comprehensive Plan Future Land Use Map identifying this area as Conservation. While the zoning is Planned Development, no active plan exists for the property.

The application is consistent with the Comprehensive Plan and implements the following Goals and Policies:

Goal 11: Public safety facilities and services for our citizens will be a priority in planning for growth.

This application provides for a current and future collocation on this facility, providing additional opportunities for communications growth and coverage in the community and the region for private and emergency communications needs.

Goal 12: Being a regional provider of goods and services the City and County will sustain, develop and enhance a healthy, diverse economy.

The tower will provide opportunities for wireless collocation to serve the Grand Junction area, as well as the applicant’s Vernal, Utah customers. The wireless industry has built more than 250,000 cell sites in the United States in the past 20 years, but many more cell sites are needed as iPhones, iPads and the like strain existing network capacity with data, email, computer and video applications. New cell sites and significant modifications to existing cell sites are also needed to further the federal government’s goal of using wireless to increase broadband speeds and coverage. The proposed new tower will provide additional opportunities for meeting the increasing demand for wireless services in our modern, growing community.

### **3. Section 21.02.110 the Grand Junction Municipal Code**

A conditional use permit shall be required prior to the establishment of any conditional use identified in Chapter [21.04](#) Grand Junction Municipal Code (GJMC) or elsewhere in

the Code. Requests for a Conditional Use Permit must demonstrate that the proposed development will comply with all of the following:

(1) *Site Plan Review Standards.*

*All applicable site plan review criteria in GJMC 21.02.070(g) and conformance with Submittal Standards for Improvements and Development (GJMC Title 22), Transportation Engineering Design Standards (GJMC Title 24), and Stormwater Management Manual (GJMC Title 26) manuals.*

Site plan review standards have been met.

(2) *District Standards.*

*The underlying zoning districts standards established in Chapter [21.03](#) GJMC, except density when the application is pursuant to [GJMC 21.08.020\(c\)](#).*

This area is zoned PD and there is no plan or underlying zone associated with the property. A cell tower is subject to use-specific standards which supersede and are more restrictive than any district standards in our Code, and therefore it is appropriate, in the limited case of a cell tower, to review and recommend approval of a development without an approved plan. Those use-specific standards are met by the proposal, except as discussed below.

(3) *Specific Standards.*

*The use-specific standards established in Chapter [21.04](#) GJMC. The use specific standards are discussed below.*

(4) *Availability of Complementary Uses.*

*Other uses complementary to, and supportive of, the proposed project shall be available including, but not limited to: schools, parks, hospitals, business and commercial facilities, and transportation facilities.*

The site is accessible by gravel road and electricity is available to support the project. Public emergency communication facilities will be allowed to collocate on the tower.

(5) *Compatibility with Adjoining Properties.*

*Compatibility with and protection of neighboring properties through measures such as;*

*Protection of Privacy. The proposed plan shall provide reasonable visual and auditory privacy for all dwelling units located within and adjacent to the site. Fences, walls, barriers and/or vegetation shall be arranged to protect and enhance the property and to enhance the privacy of on-site and neighboring occupants;*

The tower will have some visual effects which will be minimized by its design, including the lattice type and paint color (dull gray). The terrain blocks view from closer neighborhoods. Further away the tank and tower will be more visible but less noticeable.

*Protection of Use and Enjoyment. All elements of the proposed plan shall be designed and arranged to have a minimal negative impact on the use and enjoyment of adjoining property;*

Electromagnetic emissions will comply with federal law. Service providers will not be allowed to install facilities that interfere with emergency communications frequencies. The tower will have some visual effects which will be minimized by its design, including the lattice type. The structure will be left the dull gray that blends well with the changing color of the sky). The terrain blocks view from closer neighborhoods. Further away the tank and tower will be more visible but less noticeable. No outdoor storage will be allowed on the site.

*Compatible Design and Integration. All elements of a plan shall coexist in a harmonious manner with nearby existing and anticipated Development. Elements to consider include; Buildings, outdoor storage areas and equipment, utility structures, Buildings and paving coverage, Landscaping, lighting, glare, dust, signage, views, noise, and odors. The plan must ensure that noxious emissions and conditions not typical of land Uses in the same Zoning district will be effectively confined so as not to be injurious or detrimental to nearby properties.*

The building will blend with the surrounding terrain; all equipment will be stored inside the building. Electrical lines will be underground. The tower will be visible but as mentioned above is designed to minimize visual impact.

Staff has reviewed the project and finds that all applicable review criteria as listed above have been met. Specifically, the request meets the requirements of Section 21.04.030(q), Telecommunication Facilities/Tower, in the following ways:

- (vii) Location. Shared use/colocation of wireless communications facilities on existing structures, towers or buildings in a manner that precludes the need for the construction of a freestanding structure of its own is encouraged.

This application provides for a current and future co-locates on this facility, providing additional opportunities for communications growth and coverage in the community and the region for private and emergency communications needs,

- (i) Towers and telecommunications facilities shall be located to minimize any visual and other adverse impact to the neighborhood, especially residential areas and land uses.

The tank and proposed tower will be visible from several different angles, but the closer you are to the tank the less you can see, because the terrain blocks the view. The further away from the tank the more visible it is but less noticed. Also, the self supporting lattice tower has less visible light reflected from the surface area than a monopole tower. Even though the surface area may be the same as a monopole, the background color is seen through the lattice structure and allows the dull gray color to blend easier than the solid monopole. The structure will be left the dull gray that blends well with the changing color of the sky.

- (ii) Telecommunications facilities and towers shall be set back from all adjacent residentially zoned or used property by a minimum of 200 feet or 200 percent of the height of the proposed tower or facility, whichever is greater.

*The tower meets the setbacks except with respect to the Ute property which is interior to the entire property on which the tower site is located. The tower is approximately 50' from the Ute property line. This is the only location suitable, because if the tower were moved to meet setbacks, the loss in elevation would make the site unsuitable.* Federal law supersedes application of zoning and development restrictions on wireless communications facilities where the applicant can demonstrate a gap in coverage. (Telecommunications Act, 47 U.S.C. §332, and In re Cell Tower Litigation, 807 F. Supp.2d 928 (S.D. Cal. 2011).) In this situation, a coverage gap is evidenced by the application, including the following: “The City of Grand Junction needs better radio coverage for their police, fire and emergency response. Their current coverage is from Black Ridge; it shoots over the top of the Redlands area and they lose communication. This communication loss may be when entering buildings, or just in an area that has a dead spot.” And “The Redlands area also has issues with cell phone coverage. I received several reports that residents cannot use their cell phone in their homes.”

- (iii) All telecommunications facilities and towers shall be set back a minimum of 85 feet from the property line or at a 2:1 ratio (two feet of setback for every foot of tower height from the property boundary of the facility), whichever is greater, from non-residentially zoned or used property.

Federal law supersedes application of zoning and development restrictions on wireless communications facilities where the applicant can demonstrate a gap in coverage. (Telecommunications Act, 47 U.S.C. §332, and In re Cell Tower



Litigation, 807 F. Supp.2d 928 (S.D. Cal. 2011).) In this situation, a coverage gap is evidenced by “The City of Grand Junction needs better radio coverage for their police, fire and emergency response. Their current coverage is from Black Ridge; it shoots over the top of the Redlands area and they lose communication. This communication loss may be when entering buildings, or just in an area that has a dead spot.” And “The Redlands area also has issues with cell phone coverage. I received several reports that residents cannot use their cell phone in their homes.”

- (v) Monopole tower structures shall be separated from all other towers, whether monopole, self-supporting lattice or guyed, by a minimum of 750 feet.

Federal law supersedes application of zoning and development restrictions on wireless communications facilities where the applicant can demonstrate a gap in coverage. (Telecommunications Act, 47 U.S.C. §332, and In re Cell Tower Litigation, 807 F. Supp.2d 928 (S.D. Cal. 2011).) In this situation, a coverage gap is evidenced by “The City of Grand Junction needs better radio coverage for their police, fire and emergency response. Their current coverage is from Black Ridge; it shoots over the top of the Redlands area and they lose communication. This communication loss may be when entering buildings, or just in an area that has a dead spot.” And “The Redlands area also has issues with cell phone coverage. I received several reports that residents cannot use their cell phone in their homes.”

- (vi) Self-supporting lattice or guyed towers shall be separated from all other self-supporting lattice or guyed towers by a minimum of 1,500 feet.

The new tower will be at least 1500 feet from all other existing self-supporting lattice or guyed towers.

- (x) No new tower or facility shall be permitted unless the applicant demonstrates to the satisfaction of the Director that no existing tower, structure or utility facility can be used in lieu of new construction for the applicant's use.

The applicant has demonstrated to the Director's satisfaction that no existing facility can be used by the applicant for its purposes. The applicant has also demonstrated that the height of the proposed tower is the minimum necessary for the facilities it needs to accomplish its goals and to provide reasonable collocation potential. See attached General Project Report by SBT Internet.

## **FINDINGS OF FACT/CONCLUSIONS AND CONDITIONS:**

After reviewing the SBT-GJLC Telecommunications Tower application, CUP-2012-362 for a Conditional Use Permit, I make the following findings of fact, conclusions and conditions:

1. The requested Conditional Use Permit is consistent with and meets the goals and policies of the Comprehensive Plan.
2. The review criteria in Section 21.02.110 of the Grand Junction Municipal have all been met.
3. Applicable and enforceable use-specific standards of Section 21.04.030(q) have been met.
4. Approval of the project being conditioned upon the following:
  - The structures/facilities will be painted dull gray to blend with the sky.
  - No signage other than that required by applicable telecommunications laws will be allowed.
  - Tower must be designed and constructed to allow, include and support no fewer than five collocations (the applicant's proposed use plus four others).

**STAFF RECOMMENDATION:**

I recommend that the Planning Commission approve the requested Conditional Use Permit, CUP-2012-362 with the findings, conclusions and condition of approval listed above.

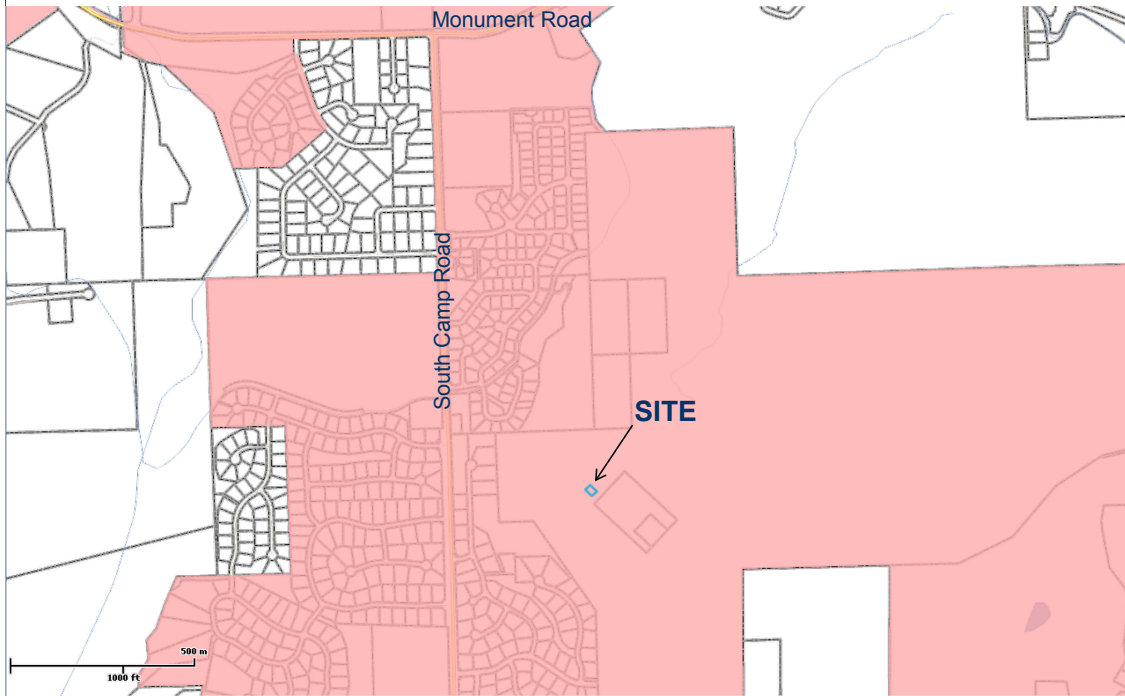
**RECOMMENDED PLANNING COMMISSION MOTION:**

Mr. Chairman, on the request for a Conditional Use Permit for SBT-GJLC Telecommunications Tower application, number CUP-2012-362 to be located at 400 23 Road, I move that the Planning Commission approve the Conditional Use Permit with the facts, conclusions and conditions listed in the staff report.

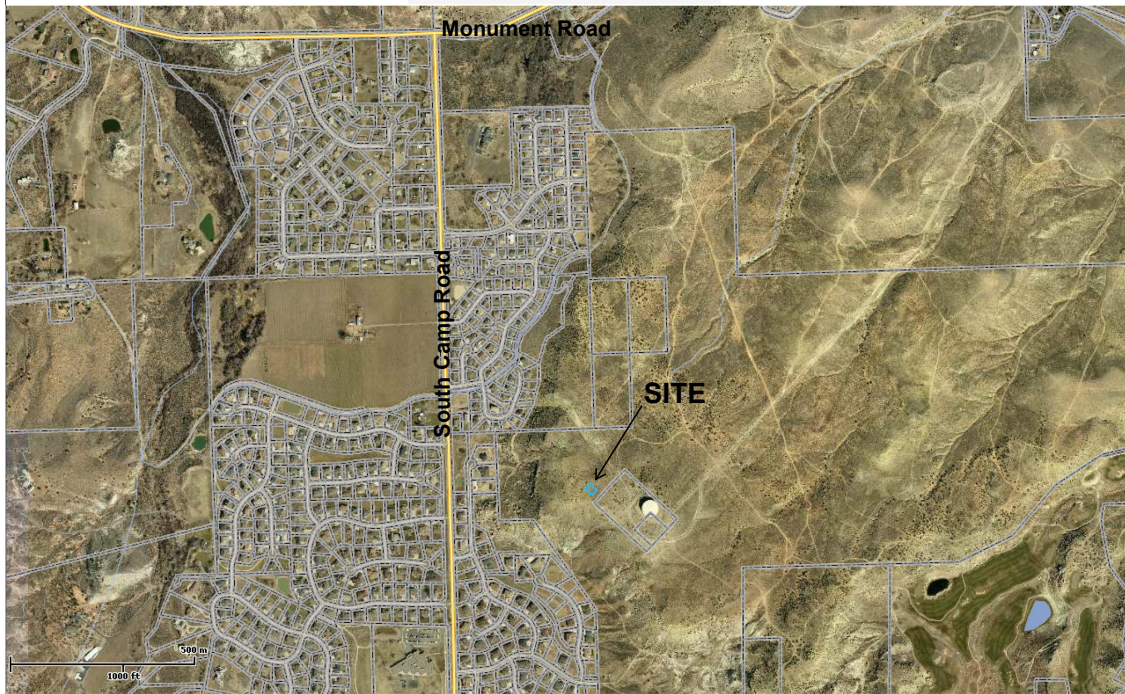
**Attachments:**

Site Location Map / Aerial Photo Map  
Future Land Use Map / Existing Zoning Map  
Letter from Grand Junction Regional Communication Center Board  
General Project Report  
Site Plans  
Tower elevations

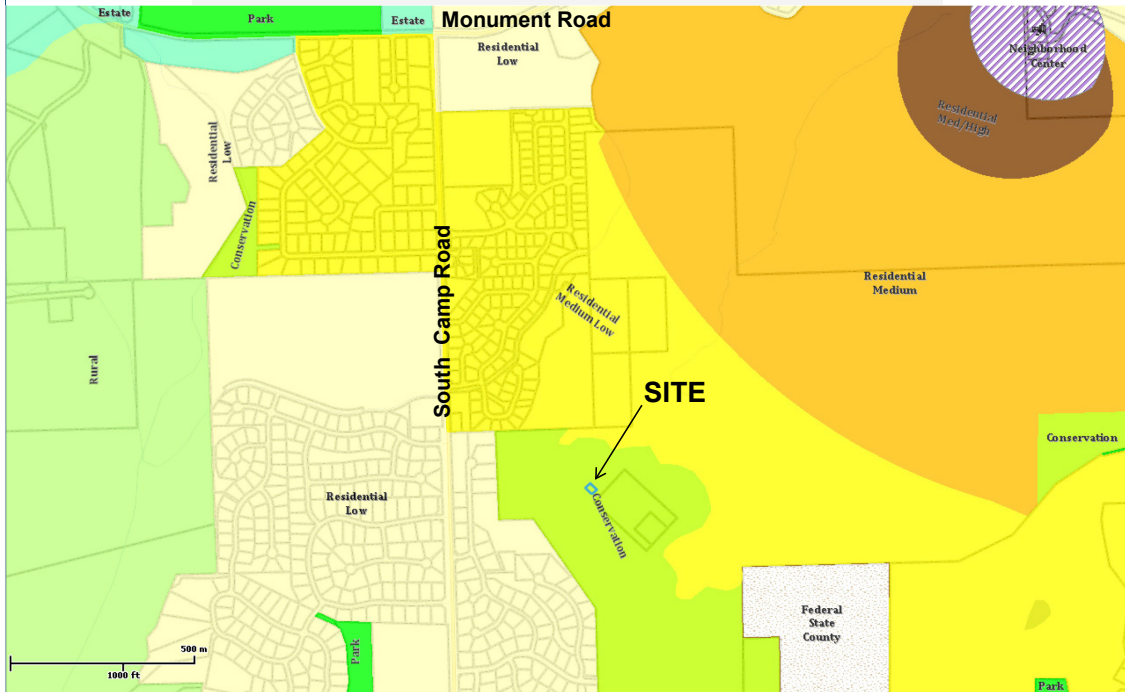
## Site Location Map



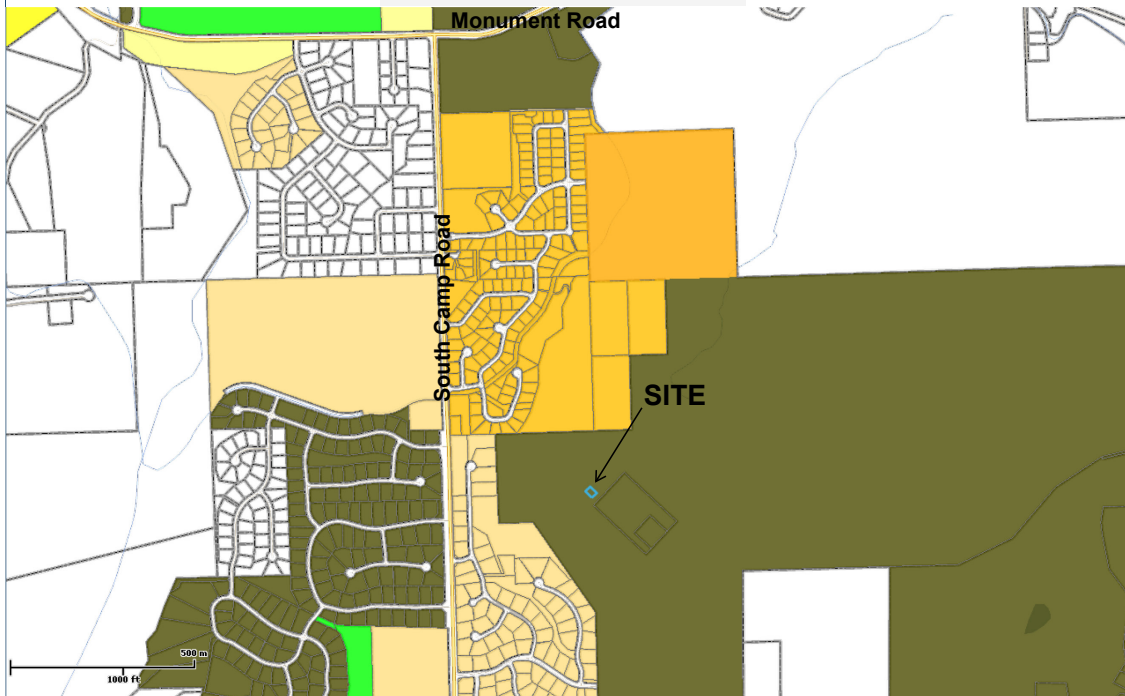
## Aerial Photo Map



## Comprehensive Plan Future Land Use Map



## Existing Zoning Map





June 8, 2012

To: City of Grand Junction Planning Department

RE: Ute Water Tank Communications Tower Site

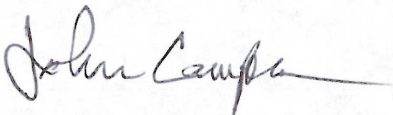
SBT Internet is interested in building a communications tower at the Ute Water Tank Site on the Redlands. SBT Internet has contacted the Grand Junction Regional Communication Center to inquire if the public safety communications network could benefit from using this tower to install additional communications infrastructure for its use. The Grand Junction Regional Communication Center Board met on June 1 and discussed this project. Public safety radio coverage in this area is currently spotty. Additional 800 MHz radio equipment in this location would be beneficial and provide better coverage for public safety agencies that respond to calls for service in that area.

The equipment and specifications that would be needed at this location to enhance the public safety radio communications system coverage are listed below:

- Tower height 40 feet
- Microwave link to the Water Plant radio site
- Building that would house radio equipment
- Backup power equipment

It is the Grand Junction Regional Communication Center Board's recommendation that SBT Internet be granted permission to build a tower at the Ute Water Tank Site on the Redlands. Further it is our understanding that SBT Internet has offered to allow the Communications Center to install its equipment on this tower, at no cost, for a period of at least five years. It is the intention of the communications center to move forward with the installation of its equipment, should this tower be approved and subsequently constructed.

I hope that this information assists you in making a decision regarding the permit and design process for this proposed radio communications tower. If you have any questions, please contact me or any member of the Communications Center Board.



John Camper, Police Chief, GJRCC Board Chair.

c: GJRCC Board

# General Project Report

## SBT Internet

### Introduction

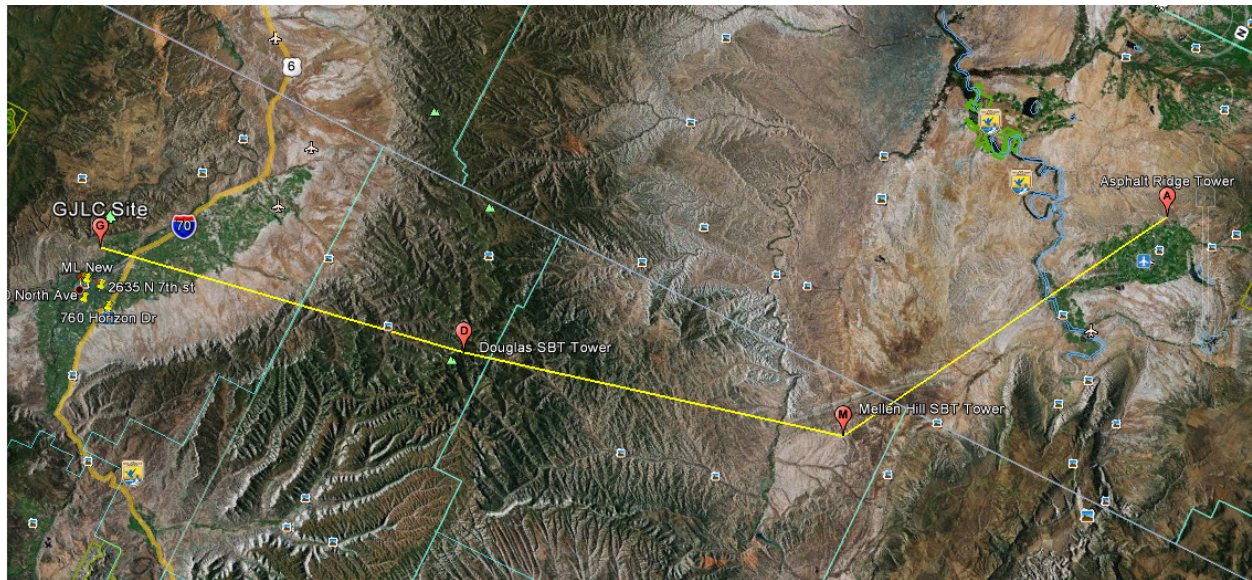
SBT Internet was founded in Vernal, Utah over 10 years ago and has always been family owned and operated. Over the years we have constructed over 14 towers with the largest at 195 feet tall. Our main business however is not building towers, rather it's to provide reliable and reasonable priced internet to the community of Vernal and its many outlying areas. A large portion of our customers are in rural areas and have no other options for accessing the internet; wireless is an ideal medium for connecting remote customers of this type.

Over the last few years internet has experienced a growth and demand for faster and more reliable speeds. This change is due to how we now use the internet; from on line classes in all age groups to watching Netflix. With all of these new demands for the internet, bandwidth is scarce and therefore cost a premium price in Vernal, Utah. Currently there is only one fiber line that runs into Vernal. Not only is this line very limited in its maximum capacity but it is also owned and controlled by one company, Strata. This Company can dictate the price and availability of the internet to the entire area.

The solution to our problem was finding reasonably priced quality bandwidth in Grand Junction. The challenge is transporting this bandwidth to Vernal over a microwave repeater backbone or a series of towers that link together to transport large amounts of data almost instantaneously.

### Inventory of Existing Sites

To date we have completed three of the four required towers for this project; a 160 foot Tower on Asphalt Ridge in Vernal, a 120 foot Tower on Mellen Hill in Rangley, CO and a 195 foot Tower on Douglas Pass between Rangley and Grand Junction. The only tower remaining for the completion of this project is the 110 foot tower in Grand Junction at the Grand Junction Land Company site.



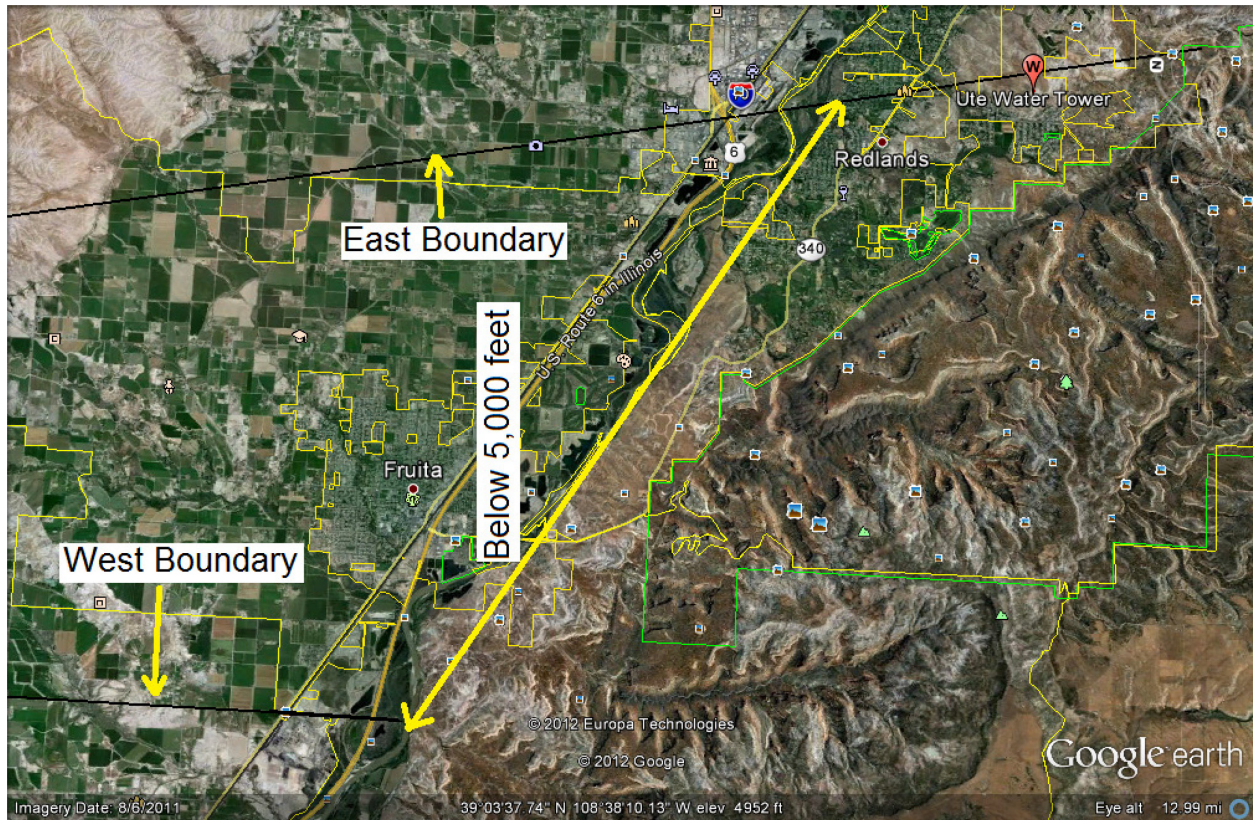
### Mapping & Description of Coverage Area

The location for this tower was not an easy one to find. This tower is the most important one because it is the beginning of the microwave link and will require four key elements to accomplish the task.

1. The path from Grand Junction to Douglas Pass must be a line of site shot for the microwave to work.
2. You must have a Fiber Optic connection to provide the volume of bandwidth needed.
3. Access to electrical power.
4. Land to install the tower and communication equipment.

What makes the Grand Junction Land Company site an essential one is that it meets all of these crucial items; but what is most important is the line of site. Finding a piece of land that can see Douglas Pass was the difficult issue. There are two ridges that block the view; one on the West side and one on the East side (see below). Not only are the ridges in the way, there is also a height issue. The elevation must be close to 5,000 feet or you cannot see over the distant terrain as you leave the valley.





The other locations we searched include the following: The established site on BLM land (Black Ridge) has over 15 towers with some producing too much interference to be a viable option. Also, Black Ridge is almost out of room with little or no new construction. BLM indicated there is a waiting list of about 2 to 3 years.

The Glade Park area has terrain problems of its own; and where you can see Douglas Pass, there is no land to rent. Glade Park is also too far from a reliable source of fiber optics to provide the bandwidth.

The Little Park area is high enough to see Douglas Pass, but there is no land to rent.

The SBA tower located on the water tank area is already loaded, and it cannot support a 10 foot dish.

In summary after searching for 4 months in about 40 different locations, the site above the Redlands area is not only the best location for SBT Internet, but the only one.

### Project Description

The repeater tower project for SBT internet is located next to 380 South Camp Road, Grand Junction, Colorado above the Redlands area. This location is on Grand Junction Land Company land. It is a piece of leased property that is 65 feet by 65 feet or 0.097 acres. This tower will be used for relaying internet bandwidth over a microwave radio licensed under the Federal Communication Commission (FCC). This tower will also be

used for Collocation for the 911 radios for emergency response, and other communication needs for the Redlands area. The collocation design for five carriers and a ten foot dish requires a 110 foot tall tower.

### Public Benefit

The City of Grand Junction needs better radio coverage for their police, fire and emergency response. Their current coverage is from Black Ridge; it shoots over the top of the Redlands area and they lose communication. This communication loss may be when entering buildings, or just in an area that has a dead spot. SBT Internet will provide a location on the tower free of charge for all emergency radio equipment, for the five year term of the contract. This is a value of \$120,000.

The Redlands area also has issues with cell phone coverage. I received several reports that residents cannot use their cell phone in their homes. This tower will accommodate collocation of cell phone companies that will alleviate the problem. The selection of this 110 foot tower was a result of understanding the needs of the city and the community for collocation and additional growth. This ultimately provides the best use of the land, by serving the public's needs now and for the future.

SBT Internet will provide collocation to qualified communication companies at the fair market value rate charged on similar tower sites. Qualified communication companies are ones that will not create interference with other facilities on the water tank site. They will use sectoral type antennas that will not diminish the loading capacity of the towers design. They will follow established Motorola tower and communication practices.

### Neighborhood Meeting

A neighborhood meeting was conducted on March 8<sup>th</sup> 2012 at the Liberty Baptist Church, 448 South Camp Road, Grand Junction, Co. starting at 5:30 p.m.

### Project Impacts

The only impact that we have found is a visual impact.

The site we have picked is 20 feet lower than the existing site at the Ute Water Tank area. This lower terrain will more effectively block the view of this tower than the current SBA tower in the same area. The further away from the tower the more visible it is but less noticed. The self supporting lattice tower has less visible light reflected from the surface area. Even though the surface area may be the same as the SBT monopole at the Ute Water tank area, the back ground color is seen through the lattice structure and allows the dull gray color to blend easier with the changing color of the sky, more so than the solid painted monopole.

### Project Compliance, Compatibility, and Impact

1. The adopted plans and policies for this project is a Conditional Use Permit. The above narrative has covered the explanation of why this site is the only one that is capable of providing all of the required elements.
2. Land use in the surrounding area is conservation.

3. Site access and traffic patterns. The Site access is a gravel road a little over a mile long, starting from Road 23 and South Broadway. This road has limited access with a locked gate permitting only authorized personnel.
4. At the water tank area there is a power transformer box that will provide power to the communication site. This is the only utility in this area. There are no fire hydrants on the site; the closest one is 900 feet away.
5. There are no special or unusual demands on utilities.
6. Effects on public facilities should be minimal. This site is in a remote area with locked gates, and consists of a concrete building with a steel tower. There is no sanitation, school, or irrigation needs; gravel roads have minimal authorized traffic.
7. (Not Applicable) Site soils and geology (such as Soils Conservation Service (SCS) soils mapping).
8. (Not Applicable) Impact of project on site geology and geological hazards, if any.
9. Operations for this facility do not fall under standard hours of business. It is an unmanned operation that runs continuously with periodic maintenance.
10. Number of employees on site will vary. Normal day to day operation requires no employees, periodic maintenance requires only one, and tower work as needed requires two employees.
11. Signage plans: The signage we will install is federally required. It will include FCC required license number and RF exposure warnings, OSHA required placards and warnings identifying all hazards, all emergency numbers, Site designation and owner contact information, FAA site designation numbers.
12. Must address the review criteria contained in the Zoning and Development Code for the type of application being submitted.

I have reviewed the Zoning and Development Code and have address all I could find in the above narrative.

#### E. Development Schedule and Phasing

##### Site Design and Structural pattern

Using the site plan, we will mark site structures on the ground. Staking out the area is not only for digging purposes but also to show Blue Stakes or One Call where we plan to dig.

Contact Xcel Power Company designer for estimate on power location and establish location for riser.

Contract a local electrical construction company for permitting with State or local authority and building the electrical distribution structure.

#### Excavating the site

Call Blue Stakes or One Call to mark the underground lines and hazards, then trench for underground electric supply.

Construct electrical riser mounting board for meter and distribution to communication building.

Dig foundations according to engineering design marked on grounds.

Dig depth using laser level for proper elevation.

#### Forming The Foundations

Form and install rebar and anchor bolts according to engineering drawings for the tower and building foundations.

Construct pier orientation jig to hold anchor bolts in place on the tower foundation, but do not install. This jig is light weight angle iron and three bolt templates.

Verify all elevations and locations with laser level and tape measure.

Call City Inspection for both foundations prior to pouring concrete.

#### Pouring The Concrete and Sampling

After all inspection are done and approved; call for concrete delivery. While contacting the already scheduled concrete company, also call your accredited scheduled quality testing company to pull samples of the concrete. This verifies the compressive strength of 3000 psi.

Have a crew on hand to handle the concrete, at least three people. Include: rubber boots, shovels, wheel barrel, concrete rakes, finishing trowel, 8 foot concrete vibrator (recommend two), bull float, and edger.

Pull at least two to three samples per each test. This gives you one to test and one to keep as a backup. Pull a test on the first truck arriving, one about midway of the pour and one out of the last or second to last truck delivering. You should have at least 6 samples or more. Store the samples on site for 24 hours, do not disturb until the next day. The tester will pick up the samples and store for testing on designated days.

Normal testing day intervals are at 7 days, 14 days and 28 days; keeping one sample past 28 days as a backup test.

Pour about a yard or two of concrete in each corner and center to stabilize the rebar matt and keep it from moving. Bring the level of the concrete up evenly throughout the pour, not loading anyone side at a time. This practice prevents any movement or skewing of the rebar matt and anchor bolts. Vibrate throughout the pour to insure all voids are gone and no holes or bubble spots remained. Do not over vibrate the concrete and separate the concrete from the sand and gravel. Vibrate each level of pour penetrating approximately 3 inches into the lower pour. This bonds the two layers together without disturbing the lower lay more than needed. Cover concrete with blankets if needed and let cure for a day, remove form boards if any.

#### Pier Pour

On the morning after the foundation pour, install your pier forms and anchor bolts with templates and jig. Verify level and measurements. Call city for inspection if needed.

Pour and sample concrete as before on each pier to specified height. Remove pier support and orientation jig about 3 days after pour.

#### Assemble The Tower

While foundation and piers are curing start assembling or finish assembling the tower sections, all but the base section. The concrete should reach 3000 psi in 7 to 10 days, if it was a good pour. Having extra samples allows you to call for a test early to see if the concrete is ready to stack the base section. While the engineering requirement is 3000 psi, we will use a 6 bag mix for the foundation and a 7 bag mix for the piers. When the concrete is fully cured it should yield well above the required amount and closer to a 4,000 psi which gives us added strength for our base. The base section will be assembled on the foundation. This gives us the ability to perfectly level the base section verifying its plum and symmetry. This is the bases for how well the entire tower turns out and its final completion. Tighten all bolts on the base section. The base section is set and needs tight bolts to keep it from moving. However, do not tighten the assembly bolts for the top sections. The bolts on braces to legs should only be finger tight. This allows the tower to settle after it has been stacked; some refer to it as shaking down the tower, before tightening the bolts. These bolts are tightened after the tower is standing. After all top sections are assembled, stage sections in order of crane lift. Bolt the separate sections together based on weight and in accordance to the crane's capacity, and crane operator's requirements, but connect no more than three tower sections together. More sections can be lifted but limiting the sections to three reduces the stress on the tower braces and members as they are lifted into position. The flange bolts that bolt the sections together are different from the bolts that bolt the braces to the legs. These flange bolts should be tightened with the sections still on the ground, before they are lifted into place.

#### Lifting The Tower and Building Into Place

Both the tower and building are in Grand Junction, the one that is lifted into place first depends on its orientation. Placing the communication building on the east side would put the building slightly behind the tower, so the building should be lifted into place first. The concrete pour will be at the same time for both of them so the concrete should cure the same time. The tower will have three climbers, one for each leg. They will use safety harnesses and fall and backup fall protection while climbing. Each climber will install the remaining flange bolts connecting the remaining sections together. One climber will use a bosun chair with backup fall protection. This winch operated chair will allow the climber to use an impact wrench to shake down and tighten the brace bolts up and down the inside of the tower. This completes the tower assembly.

#### Communication Building and Antennas

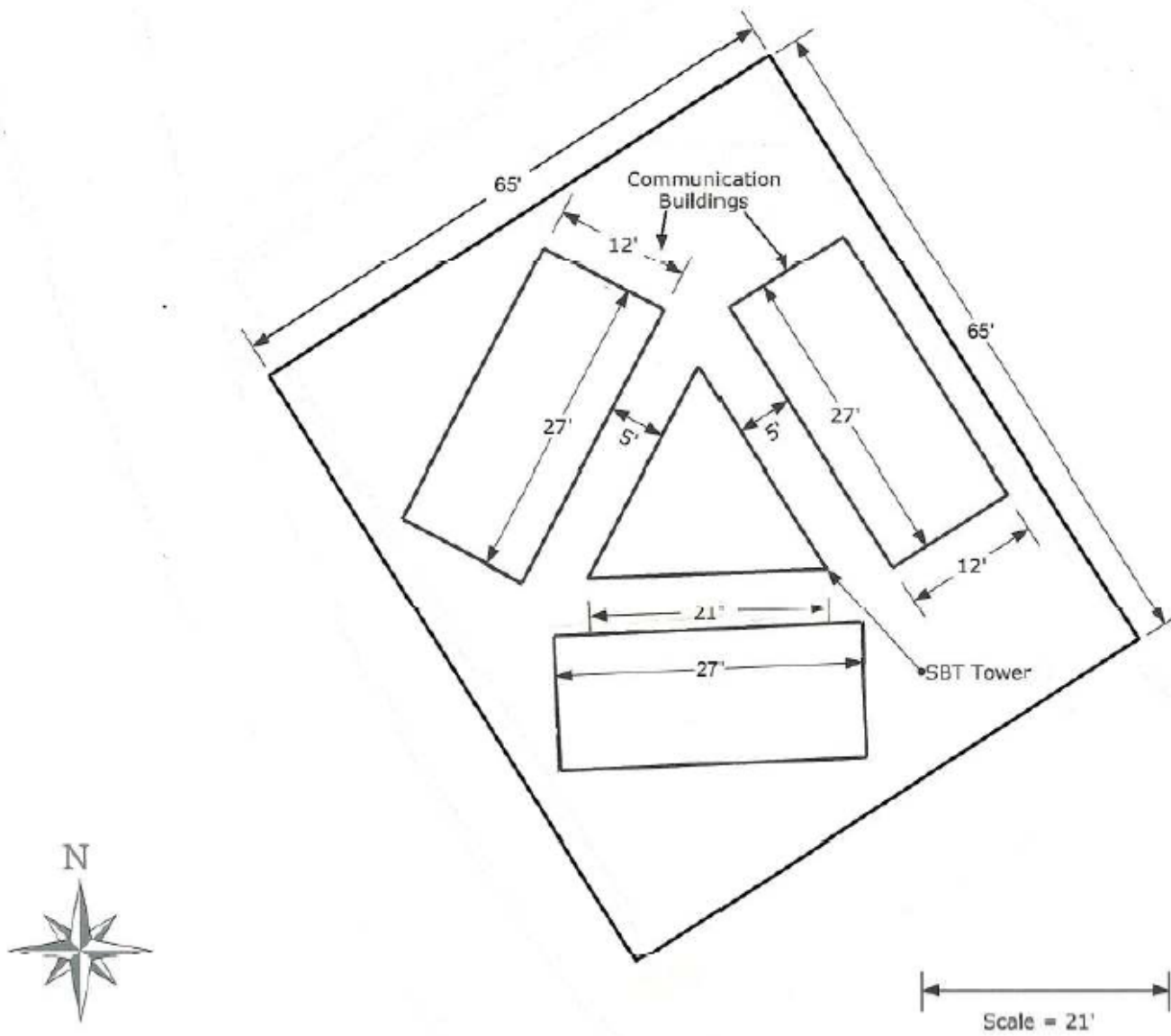
The communication building comes almost complete from the factory. We will connect the electrical power, mount the radios, mount the antennas, and run the waveguide (coaxial cable) to the antennas.

#### Fiber Optic Connection

We are waiting on Century Link for final plans on the fiber connection; if the cost is prohibitive we will microwave the bandwidth to the site.









**SECTION WEIGHT**

SECTION	WEIGHT (LBS)	SECTION	WEIGHT (LBS)	SECTION	WEIGHT (LBS)	SECTION	WEIGHT (LBS)
6425 OD X 280	5,463	OD X 224	2,956	OD-12019-S11	12,415	OD-12019-S10	1768
DIAGONALS	L 3.5 X 3.5 X .25	L 3.5 X 3 X .25 (SLV)	2 BAYS	L 2.5 X 2.5 X .1875	3 BAYS	2 BAYS	2 BAYS
SECTION WEIGHT	4037	3125	2335	2156	1245	1768	1768

**ELEVATION**

**PLAN VIEW**

**CONFIDENTIAL**

PROPERTY OF Sabre Communications  
NOT TO BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM, WITHOUT PERMISSION IN WRITING FROM Sabre Communications.

**MASTER 110 FT. MODEL SSTL-29**

**SECTION**

**CUSTOMER**

**JOB NO.** 06-12019 **SIZE** B **DRAWN BY** KAL **DATE** 12/28/05 **SCALE** 1/4" = 1'-0"

**PAGE** 0

[illegible]

**Attach 3**  
**Greater Downtown Plan Amendments**

**CITY OF GRAND JUNCTION**  
**PLANNING COMMISSION**

**MEETING DATE: July 24, 2012**  
**STAFF PRESENTATION: Kristen Ashbeck**

**AGENDA TOPIC/FILE:**

CPA-2011-1067 – Greater Downtown Plan and Future Land Use Map Amendments

CPA-2012-216 – Comprehensive Plan Text Amendment

RZN-2012-217 – Rezoning properties within the Greater Downtown Plan

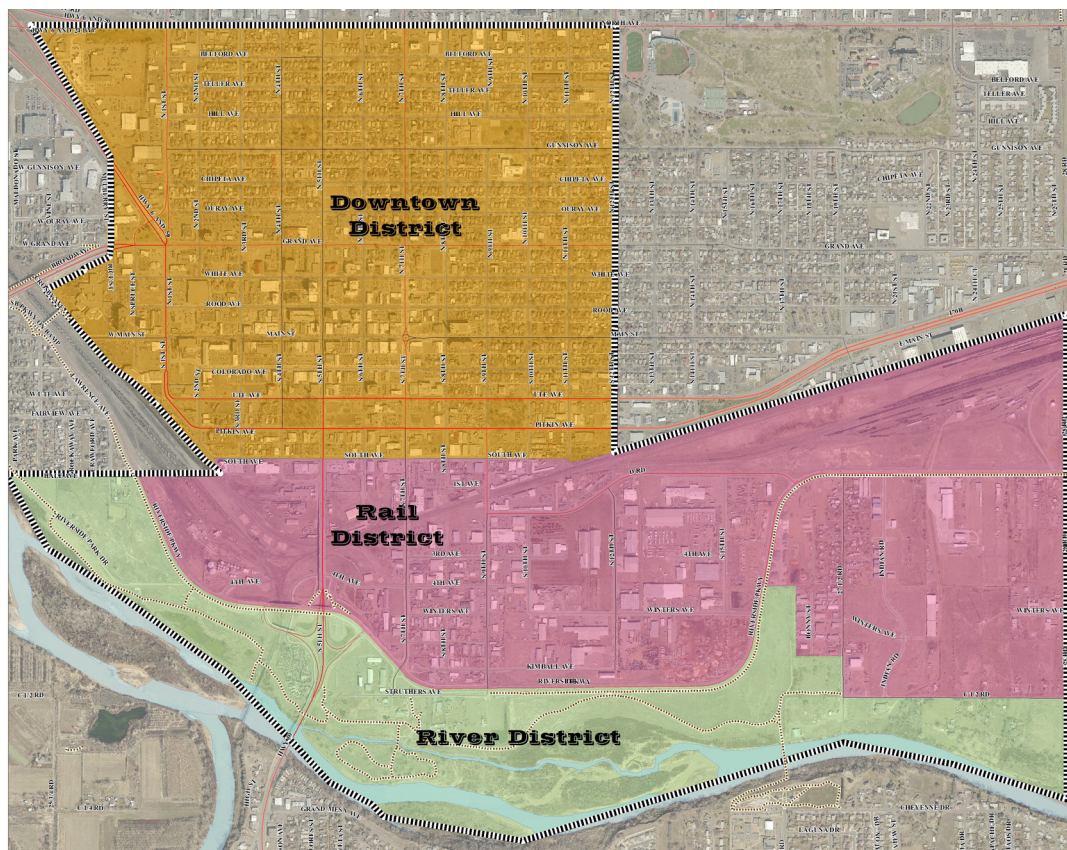
ZCA-2012-363 – Zoning Overlays for Corridors and Downtown District

**ACTION REQUESTED:**

Recommend approval of Greater Downtown Plan and its components as listed above to City Council.

**PROJECT DESCRIPTION:**

The Greater Downtown area generally encompasses the original square mile of the city and the area between the Riverside neighborhood to 28 Road and South Avenue to the Colorado River (see map below).



**RECOMMENDATION:**

Recommend approval to City Council of the proposed Greater Downtown Plan.

## 1. ANALYSIS

### Background

A *Strategic Downtown Master Plan* that encompassed the original square mile was developed through the Downtown Development Authority (DDA) and the guidance of a steering committee of interested downtown merchants, property owners and policymakers during 2007-2008. The Plan defined an overall vision and goals for downtown and included implementation strategies such as a zoning overlay.

The *Strategic Downtown Master Plan* was considered by City Council on September 14, 2009 but, due to pending adoption of the *Comprehensive Plan*, Council voted to continue the Plan to an unspecified future date.

The *South Downtown Neighborhood Plan* encompassed the area between the railroad tracks and the Colorado River and the Riverside neighborhood on the west to 28 Road on the east. A plan for the area was developed from 2006-2008 with 15 community focus group meetings, 3 public open houses with 80-100 people in attendance at each open house. The Plan included an existing conditions analysis, goals and implementation strategies including a circulation and trails plan, economic development strategies, rezoning some properties and a zoning overlay. The *South Downtown Neighborhood Plan* was considered by City Council on June 16, 2008 but was not adopted.

The *Greater Downtown Plan* integrates the two previous planning efforts and includes areas that had not previously been covered by either of those plans into a single plan for the downtown area. In addition, the *Greater Downtown Plan* incorporates elements of the Downtown Development Authority's potential projects in order to support the DDA's Downtown Plan of Development. For planning purposes, the Greater Downtown area has been divided into three subdistricts as shown on the map on the previous page: the Downtown, Rail and River Districts.

In accordance with Section 21.02.130(b), Comprehensive Plan Amendment, the Mesa County Planning Commission will be hearing the Greater Downtown Plan at a separate meeting on August 9, 2012 to take action on: 1) revisions to the Comprehensive Plan Future Land Use Map on several unincorporated parcels between 27-1/2 and 28 Roads and C-1/2 Road and the Colorado River; and, 2) a revision to the text of the Comprehensive Plan to include the R-O district as a zone district that can implement the Downtown Mixed Use land use category.

## **2. Planning/Public Process**

### **Technical Committee**

The Greater Downtown Plan technical committee was comprised of staff members from various public agencies including City Public Works and Planning, City Parks and Recreation, City Geographic Information Systems, Mesa County Planning, the Regional Transportation Planning Office, Mesa County Facilities and Parks, the Downtown Development Authority and the Mesa County Public Library District. The Committee met three times during the course of developing the Greater Downtown Plan and members attended public open houses to discuss concerns and proposals with participants.

### **Public Open Houses**

Two public open houses were held in December 2011 and February 2012 to present concepts and solicit input from property owners and interested citizens.

Notifications/invitations to both public open houses were mailed to all property owners within the Greater Downtown Plan area. Approximately 60 people attended the first open house and 40 attended the second open house.

### **Questionnaires and Comments**

A series of questionnaires were available at the December 2011 open house and on the City's web site that were used to solicit public comment and weigh community opinions on design concepts that might be proposed with the Plan. 130 questionnaires were returned. In addition, citizens could provide other written comments at both open houses. The results of the questionnaires and the written comments are included on following pages.

### **Letters/Meetings with Individual Property Owners**

City Public Works and Planning staff coordinated meetings with key individual property owners, businesses or others that contacted the City regarding the Greater Downtown Plan. In addition, individual letters were mailed to property owners along the corridors that may be impacted by the land use and zoning proposals of the Greater Downtown Plan. Follow up meetings or conversations were held with property owners as requested.

### **Community Presentations/Discussions**

Public Works and Planning staff conducted several presentations and discussions regarding the Greater Downtown Plan with community groups and businesses including the Chamber of Commerce, Bray and Company Realty, the Downtown Development Authority and Rail and River District corridor property owners.

## **City Council, City Planning Commission and Mesa County Planning Commission Workshops**

City Public Works and Planning staff attended several workshops with elected and appointed City and County officials to inform and solicit input on the Greater Downtown Plan during its development.

### **3. Greater Downtown Plan and Future Land Use Map Amendments (CPA-2011-1067 and CPA-2012-216)**

#### **Greater Downtown Plan**

The public participation process involved community evaluation of various design and planning concepts to determine which of these are most important to the community and should be addressed in greater detail in the Greater Downtown Plan. The concepts addressed four major topics relative to an area plan: land use, circulation, economic (re)development and visual character. The results, along with previous information for the CBD, show strong community support for ideas that were translated to the goals listed below for the Greater Downtown Plan.

#### A. Area-Wide Goals and Policies

Goal 1: Enhance the transportation system to accommodate automobiles, bikes and pedestrians, and provide adequate, convenient parking.

Goal 2: Establish and improve entry points into the Greater Downtown area.

Goal 3: Promote downtown living by providing a wide range of housing opportunities, primarily in the Downtown District.

Goal 4: Redefine the land use along key corridors to provide a mix that will offer the most opportunities for redevelopment and revitalization.

#### B. Downtown District Goals and Policies

Goal 1: Maintain and enhance the economic, cultural and social vitality of the Downtown District.

Goal 2: Require density/intensity in downtown as prescribed by the *Comprehensive Plan*, primarily within the Central Business District (CBD).

Goal 3: Develop a pedestrian-oriented, walkable downtown.

Goal 4: Stabilize and enhance the historic residential neighborhoods.

Goal 5: Recognize and promote opportunities to build sub-districts/ neighborhoods, each with a unique identity.



Goal 6: Jump-start the revitalization and reinvestment in the Downtown District with strategic catalyst projects.

### C. Rail District Goals and Policies

Goal 1: Preserve the opportunity for heavy industry and rail service that supports it.

Goal 2: Recognize distinction between “industrial” streets such as 9<sup>th</sup> and 12<sup>th</sup> Streets and “public” streets 7<sup>th</sup> Street and Riverside Parkway.

Goal 3: Promote higher quality, customer and pedestrian friendly development along 7<sup>th</sup> Street and Riverside Parkway.

Goal 4: Re-establish and improve a street grid in the Rail District.

### D. River District Goals and Policies

Goal 1: Create/maintain/enhance a green waterfront.

Goal 2: Create retail, general commercial and mixed use opportunities that complement the riverfront use.

Goal 3: Create/enhance redevelopment opportunities and partnerships.

## **Comprehensive Plan**

The Grand Junction Comprehensive Plan is based on extensive public input that identifies what kind of community we want to have and identifies ways to achieve our vision. It charts the course to help us become the most livable community west of the Rockies. It establishes a vision that focuses the community on what it should do to sustain the quality of life that all residents desire and expect. The Comprehensive Plan establishes the following guiding principles that will shape growth, all of which apply to development of the Greater Downtown area.

**A. Concentrated Centers** – The Plan calls for three types of centers; the City Center, Village Centers and Neighborhood Centers.

**B. Sustainable Growth Patterns** – Fiscal sustainability where we grow efficiently and cost-effectively. Encourage infill and redevelopment.

**C. Housing Variety** – Allow, encourage more variety in housing types that will better meet the needs of our diverse population.

**D. A Grand Green System of Connected Recreational Opportunities** – Take advantage of and tie together the exceptional open space assets of Grand Junction, including the Colorado River, our excellent park system, trails and our surrounding open spaces.

**E. Balanced Transportation** – Accommodate all modes of transportation including air, transit, freight, auto, bike and pedestrian.

**F. A Regional Center** – Preserve Grand Junction as a provider of diverse goods and services and residential neighborhoods.

Specific policies within the Comprehensive Plan further support the concepts of the Greater Downtown Plan as outlined below.

Goal 4. Support the continued development of the downtown area of the City Center into a vibrant and growing area with jobs, housing and tourist attractions.

Goal 5. To provide a broader mix of housing types in the community to meet the needs of a variety of incomes, family types and life stages.

Goal 6. Land use decision will encourage preservation of existing buildings and their appropriate reuse.

Goal 8. Create attractive public spaces and enhance the visual appeal of the community through quality development.

Goal 9. Develop a well-balanced transportation system that supports automobile, local transit, pedestrian, bicycle, air and freight movement while protecting air, water and natural resources.

### **Comprehensive Plan Text Amendment**

For some of the parcels in the Downtown District with an existing zoning of R-O (Residential Office), the existing Future Land Use Map shows a land use category of Downtown Mixed Use. Presently, the R-O district cannot be used to implement the Downtown Mixed Use category. However, the nature of these parcels is that they are small and on the periphery of the Central Business District so a rezone to a zone district that is acceptable in the Downtown Mixed Use Category (e.g. Downtown Business, B-2) would not provide a compatible transition to nearby residential areas. Consequently, the text of the Comprehensive Plan is proposed to be revised to include R-O (Residential Office) as an acceptable zone district to implement the Downtown Mixed Use land use category.

### **Future Land Use Map Amendments**

A component of the Greater Downtown Plan is a new Future Land Use Map for the area. The new map will amend the existing *Comprehensive Plan* Future Land Use Map and is the first step in implementing an overall vision for the Greater Downtown area. The land use categories and their application are further defined in the Greater Downtown Plan report document (attachment A). Future Land Use Map amendments in

the Greater Downtown area are proposed primarily to create better areas of transition between land uses, ensure that the future land use category is consistent with zoning and begin to define the intended character of development in some areas.

**Downtown District Future Land Use.** The Greater Downtown Plan within the Downtown District is formulated around seven general land use categories: Commercial, Downtown Mixed Use, Neighborhood Center Mixed Use, Urban Residential Mixed Use, Residential Medium High Density, Residential Medium Density and Parks and Open Space. Future Land Use Map amendments in the Downtown District are summarized below.

- North First Street Neighborhood Center – expands the North Avenue Neighborhood Center around the corner to the south and provides transition from the commercial corridor to the residential areas of the Downtown District.
- 500 Block of Ouray/Chipeta Avenue – revised from Downtown Mixed Use to Urban Residential Mixed Use in order to better reflect an intended lower intensity of land use on this block and correspond with existing zoning.
- Eastern periphery of the Central Business District – revised from Residential High Mixed Use to Residential Medium High to provide a better transition to the adjacent single family residential areas.

**Rail District Future Land Use.** The Greater Downtown Plan within the Rail District is formulated around five general land use categories: Business Park Mixed Use, Commercial, Commercial industrial, Industrial and Parks and Open Space. Future Land Use Map amendments in the Rail District are summarized below.

- Areas southwest of the railroad tracks to the Riverside Parkway on the west side of 5<sup>th</sup> Street – revised from Industrial to Business Park Mixed Use to be consistent with existing land uses but also reflect potential future redevelopment.
- Areas in the “wedge” between South Avenue and the railroad tracks – revised from Downtown Mixed Use to Industrial, Commercial/Industrial and Commercial to be consistent with existing zoning.
- South 7<sup>th</sup> Street just south of the railroad tracks from Commercial to Commercial/Industrial to be consistent with existing zoning.

**River District Future Land Use.** The Greater Downtown Plan within the River District is formulated around six general land use categories: Commercial, Commercial Industrial, Parks and Open Space, Conservation, Estate and Business Park Mixed Use. Future Land Use Map amendments in the River District are summarized below.

- Areas southwest of the Riverside Parkway to the riverfront trail on the west side of 5<sup>th</sup> Street (mostly City-owned properties) – revised from Industrial and



Commercial/Industrial to Business Park Mixed Use to be consistent with existing land uses but also reflect potential future redevelopment.

- Areas on the east and west side of 5<sup>th</sup> Street between the Riverside Parkway ramps and Struthers Avenue – revised from Park and Commercial to Commercial (west side) and Commercial/Industrial (east side) to be consistent with existing zoning and better conform to existing parcel boundaries.
- Areas east of 27-1/2 Road, between C-1/2 Road and the Colorado River – revised to suggest land use intensity on properties just east of Las Colonias Park (Commercial/Industrial), reflect ownership by Colorado State Parks (Park) and add Conservation area along the river to reflect the intentions to complete the Riverfront Trail in this area.

#### **4. Rezoning Properties within Greater Downtown (RZN-2012-217)**

Another component of the Greater Downtown Plan is a new zoning map for the area, although zoning changes from what presently exists are minimal. Similar to changes in the *Comprehensive Plan* Future Land Use Map, proposed zoning changes are also one of the first steps towards implementing an overall vision for the Greater Downtown area. The zone districts and their application are further defined in the Greater Downtown Plan report document (Attachment 1). Zoning changes in the Greater Downtown area are proposed primarily to create better areas of transition between land uses, ensure that the future land use category is consistent with zoning and begin to define the intended character of development in some areas.

**Downtown District Zoning.** Proposed zoning within the Downtown District is shown on maps and the zone districts further described in the Greater Downtown Plan report (Attachment 1). Generally, the zoning is proposed to remain the same as currently exists. The few zone changes are described below.

- North side of Grand Avenue corridor between 1<sup>st</sup> Street and the alley west of 7<sup>th</sup> Street – revised from R-O and B-1 to B-2 to be consistent with the Downtown Mixed Use land use category.
- Southeast corner of Chipeta Avenue and 5<sup>th</sup> Street – revised from R-8 to R-O to be consistent with the Urban Residential Mixed Use land use category.
- 1100 block of Colorado Avenue – revised from B-1 to B-2 to be consistent with the Downtown Mixed Use land use category.

**Rail District Zoning.** Proposed zoning within the Rail District is shown on maps and the zone districts further described in the Greater Downtown Plan report (Attachment 1). Generally, the zoning is proposed to remain the same as currently exists. The few zone changes are described below.

- Two parcels west of South 5<sup>th</sup> Street from I-2 to I-1 and I-O to be consistent with the Commercial/Industrial and Business Park Mixed Use future land use categories.
- City-owned parcels at the interchange of South 5<sup>th</sup> Street and Riverside Parkway from C-1 to CSR to be consistent with zoning of similar City-owned properties.
- Remnants of Las Colonias Park on the north side of Riverside Parkway from CSR to C-2 and I-2 to reflect existing and potential lease to adjacent property owners.

**River District Zoning.** Proposed zoning within the River District is shown on maps and the zone districts further described in the Greater Downtown Plan report (Attachment 1). Generally, the zoning is proposed to remain the same as currently exists. The few zone changes are described below, primarily impacting City-owned properties.

- Areas southwest of the Riverside Parkway to the riverfront trail on the west side of 5<sup>th</sup> Street (mostly City-owned properties) from I-O and I-1 to BP and I-O to be consistent with existing uses (private properties) as well as reflect potential future redevelopment of both the City and private properties in this area.
- Areas along the Colorado River near the Riverside Neighborhood from I-O to CSR to reflect existing riverfront trail use on City-owned property.
- City-owned parcels at the interchange of South 5<sup>th</sup> Street and Riverside Parkway from C-1 to CSR to be consistent with zoning of similar City-owned properties.
- City-owned parcels near the Botanical Gardens from C-2 to CSR to be consistent with zoning of similar City-owned properties.

## **5. Other Components of the Greater Downtown Plan (CPA-2011-1067)**

In addition to the Future Land Use and Zoning changes proposed, the City has a variety of other tools available through which the goals of the Greater Downtown Plan can be implemented so that the vision for Greater Downtown can materialize and eventually be realized. This Plan represents the first phase of implementation and includes the basic strategies of designating Future Land Use categories and zoning properties as needed as previously discussed, creating a circulation plan, creating a work plan for improvements to the public parks within Greater Downtown, establishing goals and policies for future phases of plan implementation such as economic development strategies (see below), and amending development standards of the zoning districts through a zoning overlay (see Section 6 on following page).

The Circulation and Trails Plans for Greater Downtown is shown in Appendix C of the Greater Downtown Plan report. The Plan identifies a street network that includes both existing and proposed streets and both major and minor streets and proposed on-street bicycle facilities and off-street trails. The Circulation and Trails Plans also identify potential enhancements within Greater Downtown that could be implemented over a number of years. The desired cross-sections and level of streetscape development

along the streets within Greater Downtown support the overall Plan goals, land uses and circulation to and from the area for pedestrian, bicycle and vehicles.

The Greater Downtown Plan report includes details for ongoing maintenance, improvements, redevelopment and in some cases new development within the Greater Downtown area parks. In addition, the plan summarized the work that has already been completed for potential redevelopment of the City-owned Jarvis property in the eastern portion of the Rail District.

**Downtown District Economic Redevelopment.** While the Downtown District is the heart of the community, it is but one subset of a larger market and has strengths which can be capitalized on and limitations which should be overcome. Downtown has a tremendous influence on the economic well-being of the entire region. Therefore, it is widely accepted that early projects in any revitalization effort should be publicly assisted until market conditions reach levels where new construction can support itself.

The *Grand Junction Strategic Downtown Master Plan* presented guiding principles which; while general in nature, were considered responsive to prevailing conditions, market opportunities, framework elements and stakeholder input. These guiding principles are still relevant to the Greater Downtown Plan and are listed below and described in greater Detail in the Greater Downtown Plan report (Attachment 1).

- Downtown is one submarket that competes with other submarkets in Grand Junction.
- Downtown must be market-responsive to changing conditions.
- Downtown infrastructure must be protected and retained.
- Downtown's "tool bag" must contain a variety of strategies and mechanisms that are comprehensive, flexible and creative in order to attract investment.
- Public investment must leverage private investment.
- Public policy must support downtown development.
- Public-private partnerships are essential.

**Rail and River District Economic Redevelopment.** The changes that have occurred in portions of Greater Downtown such as completion of the Riverside Parkway and planning for the future development of Las Colonias Park have already had a positive influence on the River and Rail Districts. Many properties have been renovated or redeveloped, new uses are relocating to the area and property values are generally on the rise. The Greater Downtown Plan envisions this trend continuing and being enhanced by the following redevelopment concepts:

- Allow existing heavy industry to remain, taking advantage of rail spurs within the area.
- Intensified commercial edge along the north side of the Riverside Parkway with opportunities for mixed use development.
- New general commercial, retail and residential uses will provide activity at the edge of the park after business hours to create a safe park environment that

gives “ownership” of the park to the adjacent local business owners and residents.

- New retail and commercial uses such as restaurants, shops and services along South 7<sup>th</sup> Street to serve the employees, recreational users and residents of the neighborhood.
- Commercial Industrial uses bridge the existing industrial and the commercial corridors.

In addition, discussions with the Grand Junction Economic Partnership, Business Incubator, Manufacturers’ Council and Chamber of Commerce during development of the Greater Downtown Plan brought to light many opportunities for the area, the majority of which is within the established Mesa County Enterprise Zone. The Plan outlines goals, policies and strategies that can be used to further the economic (re)development of the Rail and River Districts in Greater Downtown.

- Need for flex space for different types of small business.
- Opportunity to develop additional incentives for redevelopment.
- Allow for live-work opportunities.
- Opportunity to develop partnerships.

## **6. Greater Downtown Plan Zoning Overlay (ZCA-2012-363)**

The Greater Downtown Zoning Overlay (Attachment 2) is intended to provide guidance and criteria for the planning, design and implementation of public and private improvements in the Greater Downtown area. If properly administered and adhered to, the standards and guidelines should result in public and private development improvements (or a combination thereof) that achieve, as a minimum, a common level of quality in terms of site design, architectural design, landscaping and other site improvements.

The general purposes of the standards and guidelines are to support the overall goals of the Greater Downtown Plan.

- Maintain and enhance the economic, cultural and social vitality of Greater Downtown Promote downtown living by providing a wide range of housing opportunities in appropriate areas within Greater Downtown.
- Enhance the transportation system to accommodate automobiles, bikes and pedestrians, and provide adequate, convenient parking.
- Stabilize, preserve, protect and enhance the downtown residential neighborhoods.
- Promote and protect the unique identity of Downtown.

The standards and guidelines were developed upon an analysis of the existing character of the Greater Downtown area. The area was divided into subdistricts and the Downtown District was further divided into subareas (see map in Attachment 2) based on existing zoning, character of existing development and potential for redevelopment

opportunities. In addition, primary corridors were identified for which overlay guidance was created. The subareas and primary corridors are shown on the maps on the following page.

These standards supplement other development regulations such as the City of Grand Junction Zoning and Development Code and the City Transportation and Engineering Design Standards (TEDS). The standards identify design alternatives and specific design criteria for the visual character and physical treatment of private development and public improvements within Greater Downtown. They are adopted through an overlay zoning district, which will establish the means by which the standards are administered and enforced. The Director will make all decisions and appeals and variance requests will be heard by the City of Grand Junction Planning Commission. The Downtown Development Authority (DDA) shall be a review agency for all applications and shall also make recommendations for proposals in the Central Business District.

**Corridor Overlay.** The Greater Downtown Plan Zoning Overlay includes standards and guidelines for primary corridors in the River and Rail Districts to begin to implement goals of the Plan to: 1) improve the visual impact of development along the corridors; and 2) promote higher quality architectural treatment and site design as new development and redevelopment occurs along the corridors. The goals of the corridor guidelines and standards are to:

- Define a vision using examples of what is desired.
- Provide design flexibility on a site-by-site basis.
- Provide menus of design options so designer/builder can decide what works best for a particular project/site and the vision can be achieved without substantial cost.
- Provide design options that provide flexibility for trade-offs in building and site design.
- Clearly define what is required for new construction versus building remodels.

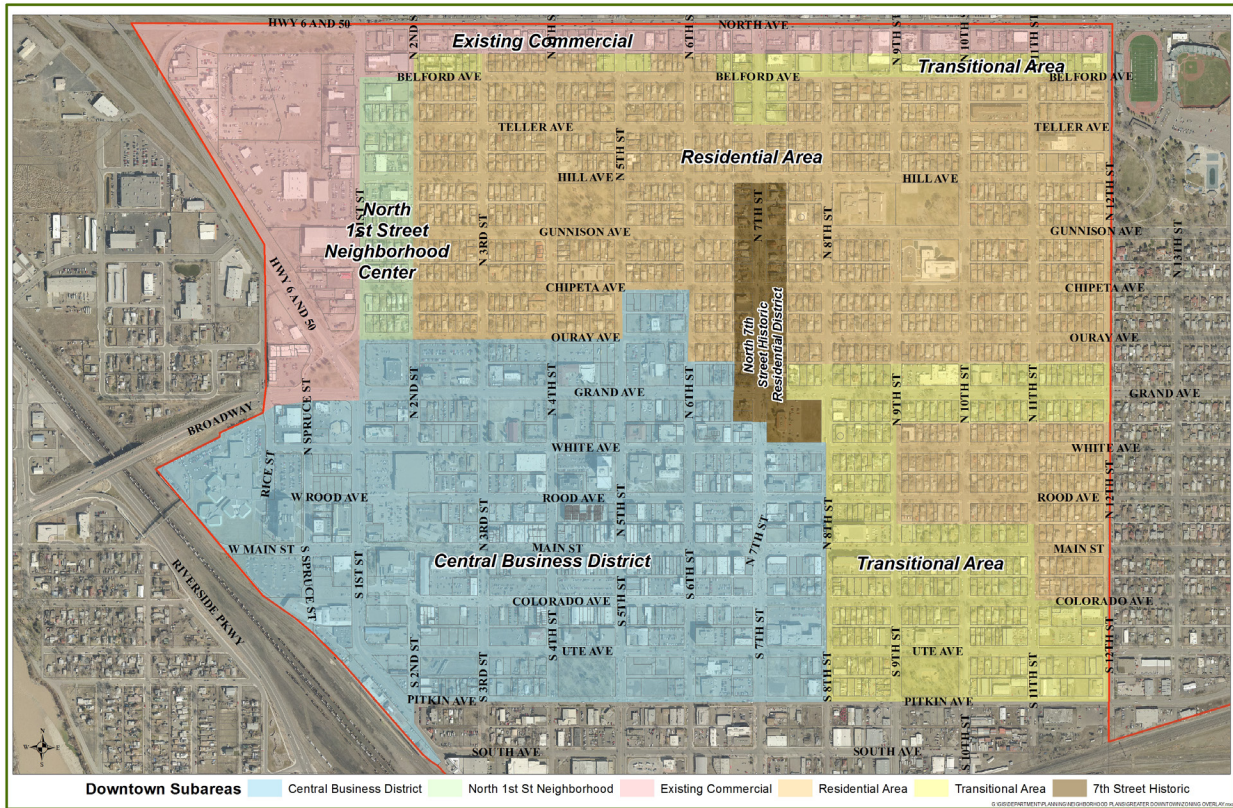
The corridor standards and guidelines are outlined in two areas: 1) Commercial Corridors; and 2) Industrial Corridors. For each type of corridor the standards and guidelines address Site Design, Architectural Design, Landscaping and Signage.

**Downtown District Subarea Zoning Overlay.** The Greater Downtown Plan includes zoning overlay standards for the subareas of the Downtown District as depicted on the map on page on the previous page. Application of the standards and guidelines will begin to implement goals of the plan to:

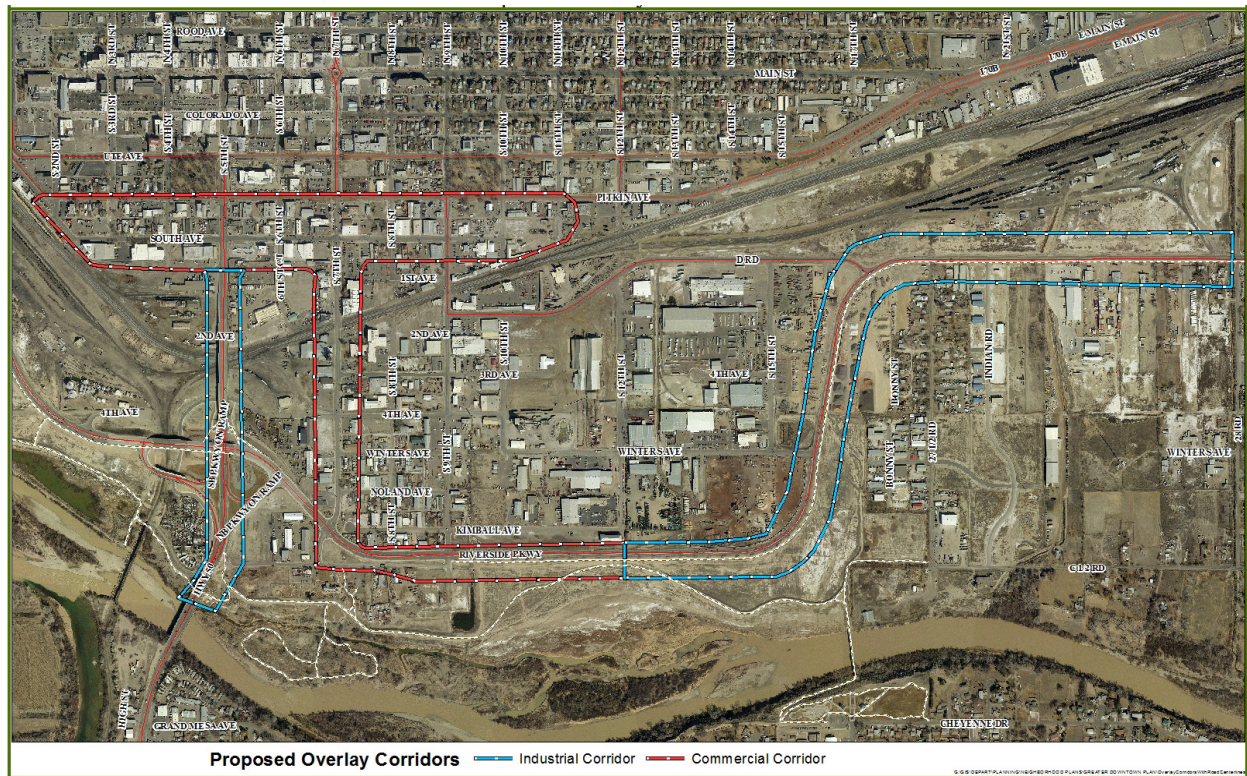
- Maintain and enhance the economic, cultural and social vitality of Downtown.
- Promote downtown living by providing a wide range of housing opportunities.
- Enhance the transportation system to accommodate automobiles, bikes and pedestrians, and provide adequate, convenient parking.
- Stabilize and enhance the historic residential neighborhoods.

- Establish and promote a unique identity.
- Preserve and restore significant historic structures.
- Activate the edges of the downtown parks with mixed use and programmed/active use of the parks as urban open space rather than passive green parks.





Downtown District Subareas



Industrial (Blue) and Commercial (Red) Overlay Corridors



The Downtown District subarea standards and guidelines are outlined in four areas and summarized below: 1) Area-Wide; 2) Central Business District; 3) Residential areas; and 4) Transitional areas.

### **Area-Wide Standards and Guidelines**

- Due to constraints of downtown properties, allows Director to make reasonable exceptions to the provisions of the Zoning and Development Code and the Greater Downtown Plan Zoning Overlay.
- Provide good, interconnected multimodal transportation choices.
- Requires traffic calming measures in public rights-of-way as properties redevelop or infrastructure is reconstructed.

### **Central Business District Standards and Guidelines**

#### **Overall Vision/Character**

- Activate the downtown core streets through emphasis on higher pedestrian traffic, businesses on the ground level that attract pedestrian traffic, and corner buildings that invite traffic on both streets.
- Encourage high quality, compatible design for all new buildings and establish a cohesive architectural character/theme that complements existing buildings.
- Use building materials that are traditional and weather well and provide a broad variety of appearance.
- Encourage high density, mixed-use development and structures.
- Encourage gradual scale transitions between the CBD and adjacent neighborhoods.

#### **Architectural Elements**

- Minimum building height in the CBD is three (3) stories; maximum building height 90 feet. Stories above 5 levels shall step back to ensure adequate air and light.
- Maximum building setback of 2 feet except on Ouray and Chipeta Avenues or there is a prescribed function for space in front of a building.
- Scale and massing of buildings along Ouray and Chipeta Avenues will be compatible with residential scale.
- Facades shall be visually interesting with varied materials, patterns, definition of bays or other building articulation, 50% minimum in windows on street level façade, façade cap/cornice that cast a shadow.

#### **Site Design Elements**

- Minimize single use, surface parking and encourage shared parking.
- Require parking located behind buildings to be accessed from the alley.
- Provide streetscape details and landscaping that compliment the architectural character of downtown and exhibit an urban character.



## **Residential Areas Standards and Guidelines**

- Do not allow further encroachment by non-residential uses, higher intensity/density or more intensive zoning but provide a diversity of housing types.
- Maintain and enhance the historic character of the streetscape including use and landscaping of the park strips.
- Maintain the existing character of the house styles. New construction and alterations shall be compatible with key architectural characteristics and site elements of the neighborhood including building mass and scale, setbacks, height, roof shape, window patterns, and exterior materials.
- Ensure accessory structures are subordinate to the primary structures on a site.
- Allow multifamily development where existing zoning allows but site and building design must be compatible with the scale and material finishes of single family residential structures.
- Do not allow off-street parking for multifamily development in the front yard or setback.

## **Transitional Areas Standards and Guidelines**

- Uses as allowed by the Zoning and Development Code but a mix of residential and nonresidential uses on the same lot shall be located in the same structure.
- Hours of operation of nonresidential uses restricted to 7:30 am and 8:00 pm.
- Maximum building size is 10,000 square feet.
- Outdoor storage and display are prohibited.
- New residential and non-residential construction shall be designed to have a single family residential character in building mass and scale, setbacks, height, roof shape, window patterns, location of entryways, and exterior materials.
- Signs for non-single family uses are restricted in type, size, location and lighting.
- Non-single family residential uses in the downtown Transitional areas shall be designed and operated not to increase on-street parking in front of single family dwellings in the neighborhood.
- Service entrances, loading areas and dumpsters shall be located only in the rear or side yard.
- Front yards shall be reserved for landscaping, sidewalks, driveway access to parking areas and signage.

### **REVIEW CRITERIA:**

Comprehensive Plan amendments and zoning changes must meet one or more of the following criteria for approval per sections 21.02.130 and 21.02.140 of the Municipal Code.

(1) *Subsequent events have invalidated the original premise and findings;*

(2) *The character and/or condition of the area has changed such that the amendment is consistent with the Plan;*

*(3) Public and community facilities are adequate to serve the type and scope of land use proposed;*

*(4) An inadequate supply of suitably designated land is available in the community, as defined by the presiding body, to accommodate the proposed land use;*

*(5) The community or area, as defined by the presiding body, will derive benefits from the proposed amendment.*

Primarily, criterion 5 applies to the proposed Greater Downtown Plan and the Greater Downtown Plan Zoning Overlay. The benefit derived by adoption of the Plan will help the community meet some of its long term goals as expressed in the Comprehensive Plan as discussed in greater detail on page 5 of the staff report.

#### **FINDINGS OF FACT/CONCLUSIONS:**

After reviewing the Greater Downtown Plan, files CPA-2011-1067, CPA-2012-216, RZN-2012-217 and ZCA-2012-363 for an amendment to the Comprehensive Plan Future Land Use Map and text, rezoning properties within Greater Downtown, and adopting a zoning overlay, staff makes the following findings of fact and conclusions:

1. The Greater Downtown Plan and Zoning Overlay are consistent with the Comprehensive Plan.
2. The review criteria in 21.02.130 and 21.02.140 of the Municipal Code have been met.

#### **STAFF RECOMMENDATION:**

Staff recommends that the Planning Commission forward a recommendation of approval to City Council of a Comprehensive Plan Amendment to adopt the Greater Downtown Plan and Zoning Overlay as an element of the Comprehensive Plan (files CPA-2011-1067, CPA-2012-216, RZN-2012-217 and ZCA-2012-363) with the findings and conclusions listed above.

#### **RECOMMENDED PLANNING COMMISSION MOTIONS:**

- 1) Mr. Chairman, on item CPA-2011-1067, request a recommendation of approval to City Council of the Greater Downtown Plan as an amendment to the Comprehensive Plan which includes Future Land Use Map amendments and a new Circulation and Trails Plans, I move that we forward to City Council with a recommendation of approval.
- 2) Mr. Chairman, on item CPA-2012-216, request a recommendation of approval to City Council for an amendment to the text of the Comprehensive Plan to allow the Residential Office (R-O) zone district in the Downtown Mixed Use land use designation, I move that we forward to City Council with a recommendation of approval.

3) Mr. Chairman, on item RZN-2012-217, request a recommendation of approval to City Council of City-initiated rezoning of properties within the Greater Downtown Plan area, I move that we forward to City Council with a recommendation of approval.

4) Mr. Chairman, on item ZCA-2012-363, request a recommendation of approval to City Council of the Greater Downtown Plan Zoning Overlay as an amendment to Section 21.07 of the Zoning and Development Code, and an amendment to Section 21.03.020(d) to add the R-O zone district as a zone district that can implement the Downtown Mixed Use Comprehensive Plan land use designation, I move that we forward to City Council with a recommendation of approval.

**Attachments:**

1. Ordinance to Amend Comprehensive Plan including:  
Exhibit A, Greater Downtown Plan Report  
Exhibit B, Future Land Use Map
2. Ordinance to Adopt Greater Downtown Plan Zoning Overlay and Map  
Exhibit A, Greater Downtown Plan Zoning Overlay Report
3. Ordinance to Adopt Greater Downtown Plan Zoning Map  
Exhibit A, Zoning Map

## ATTACHMENT 1

### CITY OF GRAND JUNCTION, COLORADO ORDINANCE NO. \_\_\_\_\_

#### **AN ORDINANCE ADOPTING THE GRAND JUNCTION GREATER DOWNTOWN PLAN AS AN ELEMENT OF THE COMPREHENSIVE PLAN FOR THE AREA GENERALLY INCLUDING THE ORIGINAL SQUARE MILE, SOUTH AVENUE TO THE COLORADO RIVER AND RIVERSIDE NEIGHBORHOOD TO 28 ROAD**

#### RECITALS.

A *Strategic Downtown Master Plan* that encompassed the original square mile was developed through the Downtown Development Authority (DDA) and the guidance of a steering committee of interested downtown merchants, property owners and policymakers during 2007-2008. The Plan defined an overall vision and goals for downtown and included implementation strategies such as a zoning overlay. The *Strategic Downtown Master Plan* was considered by City Council on September 14, 2009 but, due to pending adoption of the *Comprehensive Plan*, Council voted to continue the Plan to an unspecified future date.

The *South Downtown Neighborhood Plan* encompassed the area between the railroad tracks and the Colorado River and the Riverside neighborhood on the west to 28 Road on the east. A plan for the area was developed from 2006-2008 with 15 community focus group meetings, 3 public open houses with 80-100 people in attendance at each open house. The Plan included a circulation and trails plan, economic development strategies, rezoning some properties and zoning overlay. The *South Downtown Neighborhood Plan* was considered by City Council on June 16, 2008 but was not adopted.

The *Greater Downtown Plan* (Exhibit A) integrates the two previous planning efforts as well as includes areas that had not been covered by either of those plans into a single plan for the downtown area. In addition, the *Greater Downtown Plan* incorporates elements of the Downtown Development Authority's potential projects in order to support the DDA's Downtown Plan of Development. For planning purposes, the Greater Downtown area has been divided into three subdistricts: the Downtown, Rail and River Districts.

The public participation process involved community evaluation of various design and planning concepts to determine which of these are most important to the community and should be addressed in greater detail in the Greater Downtown Plan. The concepts addressed four major topics relative to an area plan: land use, circulation, economic (re)development and visual character. The results, along with previous information for the CBD, show strong community support for ideas that were translated to the goals listed below for the Greater Downtown Plan.

### Area-Wide Goals and Policies

Goal 1: Enhance the transportation system to accommodate automobiles, bikes and pedestrians, and provide adequate, convenient parking.

Goal 2: Establish and improve entry points into the Greater Downtown area.

Goal 3: Promote downtown living by providing a wide range of housing opportunities, primarily in the Downtown District.

Goal 4: Redefine the land use along key corridors to provide a mix that will offer the most opportunities for redevelopment and revitalization.

### Downtown District Goals and Policies

Goal 1: Maintain and enhance the economic, cultural and social vitality of the Downtown District.

Goal 2: Require density/intensity in downtown as prescribed by the *Comprehensive Plan*, primarily within the Central Business District (CBD).

Goal 3: Develop a pedestrian-oriented, walkable downtown.

Goal 4: Stabilize and enhance the historic residential neighborhoods.

Goal 5: Recognize and promote opportunities to build sub-districts/ neighborhoods, each with a unique identity.

Goal 6: Jump-start the revitalization and reinvestment in the Downtown District with strategic catalyst projects.

### Rail District Goals and Policies

Goal 1: Preserve the opportunity for heavy industry and rail service that supports it.

Goal 2: Recognize distinction between “industrial” streets such as 9<sup>th</sup> and 12<sup>th</sup> Streets and “public” streets 7<sup>th</sup> Street and Riverside Parkway.

Goal 3: Promote higher quality, customer and pedestrian friendly development along 7<sup>th</sup> Street and Riverside Parkway.

Goal 4: Re-establish and improve a street grid in the Rail District.

### River District Goals and Policies

Goal 1: Create/maintain/enhance a green waterfront

Goal 2: Create retail, general commercial and mixed use opportunities that complement the riverfront use.

Goal 3: Create/enhance redevelopment opportunities and partnerships

In addition to identifying goals and policies for the area, the Greater Downtown Plan does the following.

1. Includes amendments to the Comprehensive Plan Future Land Use Map (Exhibit B) and a text amendment to the Land Use Designations section of the Comprehensive Plan to read as follows:

**Downtown Mixed Use (DMU)**

Applicable Zones

R-16

R-24

R-O

B-2

C-1

MXR-3, 5, 8

MXG-3, 5, 8

MXS-3, 5, 8

2. Includes zoning changes as required to create better areas of transition between land uses, ensure that the zoning is consistent with the future land use category and begin to define the intended character of development in some areas.

3. Includes Circulation and Trails Plans that depict future street and trail systems for the area and outlines more specific multimodal transportation improvement concepts that serve as future guidance as development and redevelopment occurs in the area.

4. Includes a zoning overlay that provides guidance and criteria for the planning, design and implementation of public and private improvements in the Greater Downtown area. If properly administered and adhered to, the standards and guidelines should result in public and private development improvements (or a combination thereof) that achieve, as a minimum, a common level of quality in terms of site design, architectural design, landscaping and other site improvements.

5. Outlines other implementation tools such as economic development and redevelopment strategies and improvements to the public parks within the Greater Downtown area.

The Grand Junction Planning Commission is charged with the legal duty to prepare and consider and recommend action to City Council regarding master plans for the City.



The Greater Downtown Plan was heard in a public hearing by the Grand Junction Planning Commission on July 24, 2012 where the Planning Commission recommended that the City Council adopt the Plan.

**NOW THEREFORE, BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF GRAND JUNCTION:**

That the Greater Downtown Plan, City of Grand Junction, Colorado, in the form of the document attached hereto, and as recommended for adoption by the Grand Junction Planning Commission is hereby adopted.

The full text of the Ordinance, including the text of the Greater Downtown Plan, in accordance with paragraph 51 of the Charter of the City of Grand Junction, shall be published in pamphlet form with notice published in accordance with the Charter.

**INTRODUCED** on first reading the \_\_\_\_\_ day of \_\_\_\_\_, 2012 and ordered published pamphlet form.

**PASSED** and **ADOPTED** on second reading the \_\_\_\_\_ day of \_\_\_\_\_, 2012 and ordered published in pamphlet form.

\_\_\_\_\_  
President of City Council

ATTEST:

\_\_\_\_\_  
City Clerk

# Greater Downtown Plan

# DRAFT

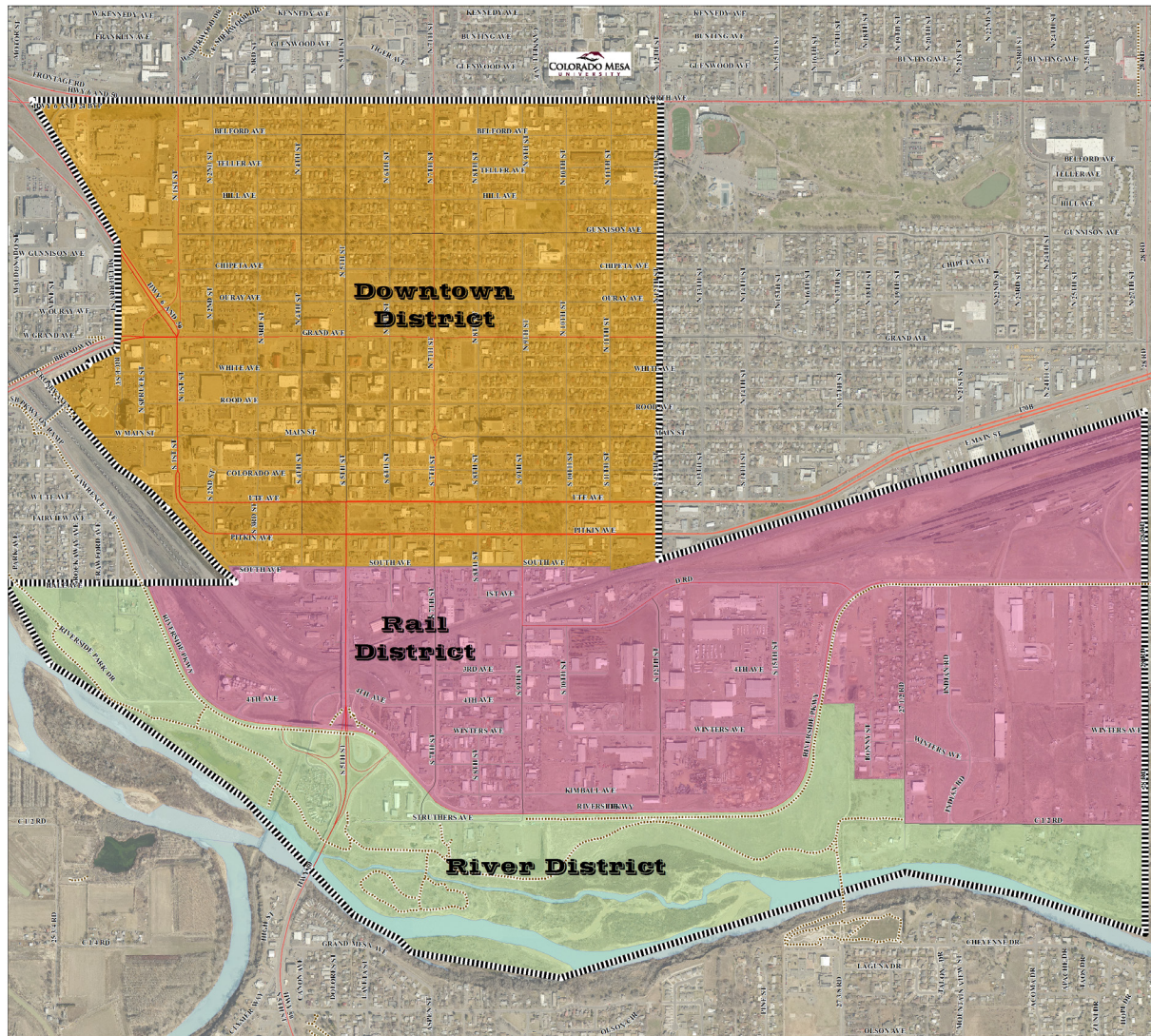


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## 1. STUDY AREA CONTEXT

The Greater Downtown planning area is comprised of three sub-districts: the Downtown, Rail and River Districts. The areas are distinctly different due to their location, influences and historic development but complement each other to form a developed area that is central to the community with uses that range from single family residential to rail-oriented heavy industrial.



## 2. PLANNING BACKGROUND

A *Strategic Downtown Master Plan* that encompassed the original square mile was developed through the Downtown Development Authority (DDA) and the guidance of a steering committee of interested downtown merchants, property owners and policymakers during 2007-2008. The Plan defined an overall vision and goals for downtown and included implementation strategies such as a zoning overlay.

The vision of the Plan addressed: the transportation network and other infrastructure, introduction of green treatments, creation of design standards and strategies for community marketing and promotion efforts. The goals of the plan were to:

- Maintain and enhance the economic, cultural and social vitality of Downtown
- Promote downtown living by providing a wide range of housing opportunities
- Enhance the transportation system to accommodate automobiles, bikes and pedestrians, and provide adequate, convenient parking
- Stabilize and enhance the historic residential neighborhoods
- Establish and promote a unique identity
- Jump-start the revitalization and reinvestment in the downtown area with strategic catalyst projects

The *Strategic Downtown Master Plan* was considered by City Council on September 14, 2009 but, due to pending adoption of the *Comprehensive Plan*, Council voted to continue the Plan to an unspecified future date.

A *South Downtown Neighborhood Plan* encompassed the area between the railroad tracks and the Colorado River and the Riverside neighborhood on the west to 28 Road on the east. A plan for the area was developed from 2006-2008 with 15 community focus group meetings, 3 public open houses with 80-100 people in attendance at each open house. The Plan included an existing conditions analysis, goals and implementation including a circulation and trails plan, economic development strategies, rezoning some properties and zoning overlay. The goals of the Plan were to:

- Create, maintain and enhance a green waterfront
- Recognize existing heavy industry and rail service that supports it
- Recognize the distinction between “industrial” streets and “public” streets
- Promote higher quality, “cleaner” uses in the area generally between 7<sup>th</sup> and 9<sup>th</sup> Streets
- Improve entry points and connections to downtown
- Increase light industrial opportunities
- Create areas of mixed uses to screen and transition to the heavy industry
- Create and enhance redevelopment opportunities and partnerships

The *South Downtown Neighborhood Plan* was considered by City Council on June 16, 2008 but was not adopted.

The *Greater Downtown Plan* is proposed to review, update as needed and integrate the two previous planning efforts as well as include areas that had not been covered by either of those plans into a single plan for the downtown area. In addition, the *Greater Downtown Plan* incorporates elements of the Downtown Development Authority’s potential projects in order to satisfy State statute and serve as the required Downtown Plan of Development, as well as incorporating elements of the *Comprehensive Plan* discussed below.

### **3. COMPREHENSIVE PLAN**

The Grand Junction Comprehensive Plan is based on extensive public input that identifies what kind of community we want to have and identifies ways to achieve our vision. It charts the course to help us become the most livable community west of the Rockies. It establishes a vision that focuses the community on what it should do to sustain the quality of life that all residents desire and expect. The

Comprehensive Plan establishes the following guiding principles that will shape growth, all of which apply to development of the Greater Downtown area.

**A. Concentrated Centers** – The Plan calls for three types of centers; the City Center, Village Centers and Neighborhood Centers.

**B. Sustainable Growth Patterns** – Fiscal sustainability where we grow efficiently and cost-effectively. Encourage infill and redevelopment.

**C. Housing Variety** – Allow, encourage more variety in housing types that will better meet the needs of our diverse population.

**D. A Grand Green System of Connected Recreational Opportunities** – Take advantage of and tie together the exceptional open space assets of Grand Junction, including the Colorado River, our excellent park system, trails and our surrounding open spaces.

**E. Balanced Transportation** – Accommodate all modes of transportation including air, transit, freight, auto, bike and pedestrian.

**F. A Regional Center** – Preserve Grand Junction as a provider of diverse goods and services and residential neighborhoods.

Specific policies within the Comprehensive Plan further support the concepts of the Greater Downtown Plan as outlined below.

**Goal 4.** Support the continued development of the downtown area of the City Center into a vibrant and growing area with jobs, housing and tourist attractions.

**Goal 5.** To provide a broader mix of housing types in the community to meet the needs of a variety of incomes, family types and life stages.

**Goal 6.** Land use decision will encourage preservation of existing buildings and their appropriate reuse.

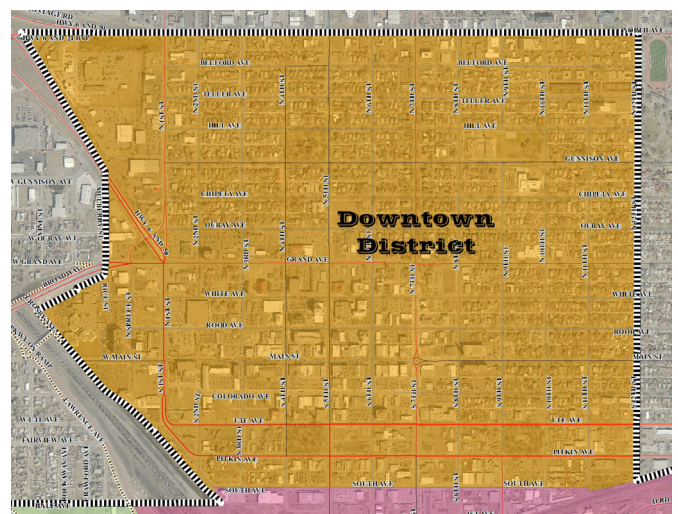
**Goal 8.** Create attractive public spaces and enhance the visual appeal of the community through quality development.

**Goal 9.** Develop a well-balanced transportation system that supports automobile, local transit, pedestrian, bicycle, air and freight movement while protecting air, water and natural resources.

#### 4. SITE ANALYSIS

##### A. Downtown District

The Downtown District encompasses the original square mile, the area south of North Avenue





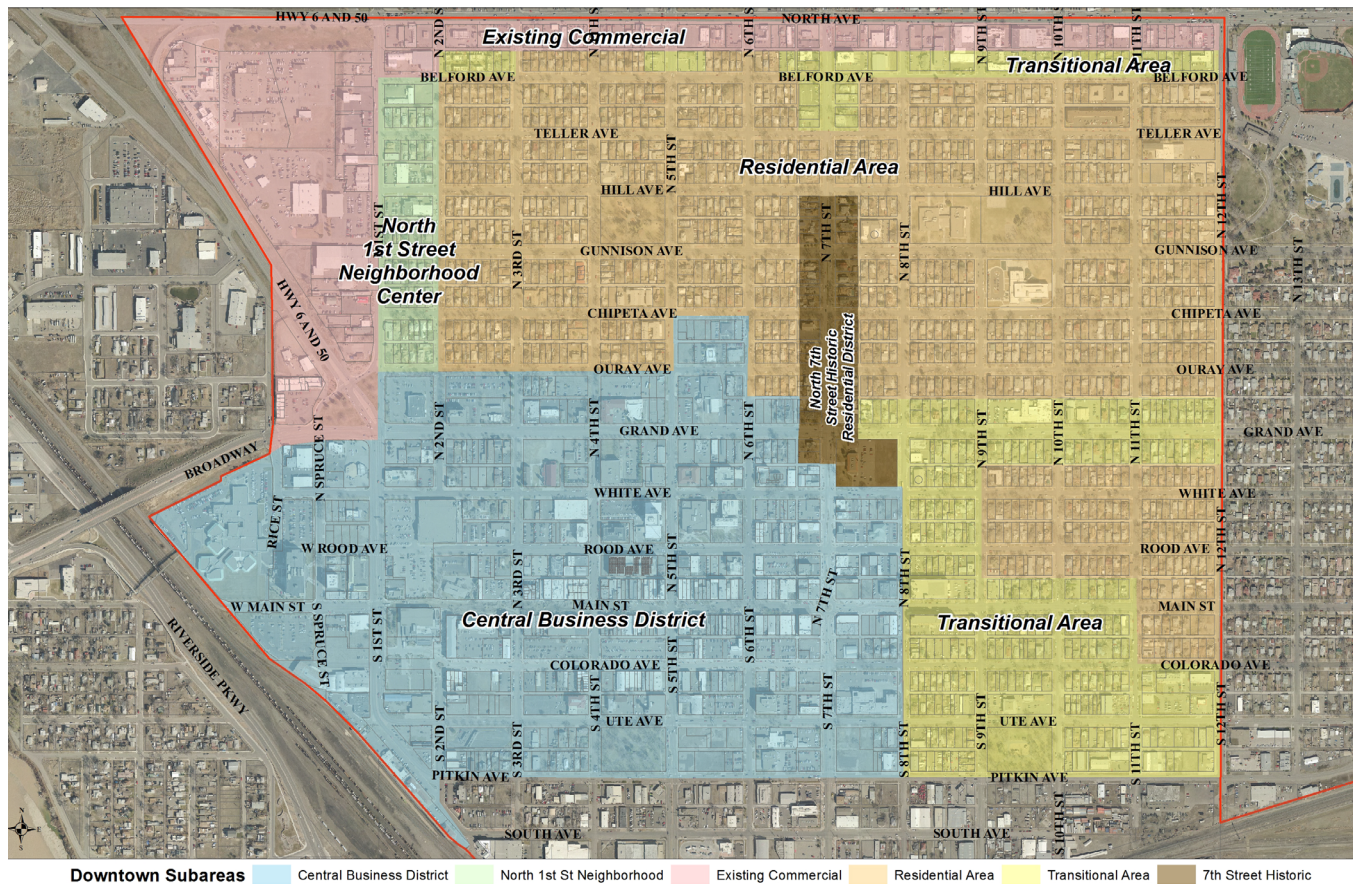
between 1<sup>st</sup> Street and Highway 6 & 50, and the area south of Grand Avenue between 1<sup>st</sup> Street and the railroad tracks.

The Downtown District should retain its role as the City's center and a regional destination. With a diverse mix of land uses, civic and public amenities, art and cultural facilities, the Downtown District offers a hub of activity that supports and reflects the regional demographics. Every effort should be made to keep and maintain existing public amenities and services in the Downtown District. These uses attract large numbers of residents and visitors to downtown which contributes to its economic vitality.

The District benefits from a number of characteristics that make it appropriate for development of retail, office, residential, institutional and community uses as described below.

- On average, urban residents spend a greater percentage of household income on retail expenditures, particularly on items such as apparel and food away from home. This indicates an opportunity for additional specialty retail and entertainment space in the Downtown District.
- Downtown housing has been and is expected to continue to increase in density with smaller households comprised of young and old, and moderate and lower-income residents. However, with a growing concentration of middle-aged, moderate- to high-income households in the City as a whole, there is an opportunity for Downtown to attract more diverse, higher-income resident base. The entirety of Grand Junction (urban and fringe) faces a growing shortage of quality affordable housing for its very low- and moderate-income residents, as well as working-wage families. This planning process emphasized the need for a set of strategies tailored specifically to the housing challenges present within the Downtown District.
- Downtown commercial vacancy and rental rates are approaching levels required to support new development and/or redevelopment. However, "seed" money will likely be necessary to leverage private investment in projects that will catalyze reinvestment activity throughout the Downtown District.
- Among the higher growth employment sectors in Mesa County are service industries often consisting of small businesses. This represents an opportunity for Downtown to develop not only additional live/work units, but also to promote the adaptive re-use of historically significant buildings and less traditional spaces including former church facilities.
- Forecasts indicate that more than one million square feet of employment space (office), more than 1.6 million square feet of retail space and nearly 1,100 residential units could be absorbed in the market over the next ten years, from which the Downtown District could benefit. The degree to which downtown is able to capture new demand within the trade area and beyond will be a function of the redevelopment process itself. Redeveloping key catalyst areas as residential, retail, employment and community destinations will necessarily increase its ability to capture not only a greater share of trade area demand, but also to reach beyond those boundaries.
- The Downtown District is comprised of many uses that result in a number of diverse neighborhoods. While some areas within downtown are comprised of one single use, such as residential or business/commercial, there are many areas that include a range of uses, such as

along Grand Avenue. In order to more fully understand specific issues facing the many, diverse neighborhoods within downtown, the area was divided into four primary sub-areas: Central Business District (CBD), Transitional, Residential and North 1<sup>st</sup> Street Neighborhood Center.



**Central Business District (CBD).** The CBD contains primarily commercial uses and is located south of Grand Avenue. The CBD also contains a significant number of public and civic facilities. While much of the CBD is zoned to permit a wide range of uses, there are several new projects that have integrated uses into one cohesive project. Pedestrian accessibility and amenities, public facilities, traffic patterns, building heights and commercial development densities combine to create an urban character that attracts local businesses, employees, residents and visitors to the CBD.

**Transitional Subarea.** The Transitional areas of downtown contain a mix of residential, commercial and institutional uses. Development densities and building heights are not as extreme as in the CBD and residential uses, which includes single family homes adapted to multifamily uses, are prevalent throughout the Transitional areas. With the exception of the main arterial Grand Avenue, traffic is less congested and parking is available, both on-street and in publicly- and privately-owned surface parking lots.





Existing homeowners in the Transitional areas, especially along Grand Avenue, expressed concern over the rapidly expanding commercial character of the area. Traffic congestion, parking issues, vehicle accessibility, lighting and signage were each cited as an adverse impact that existing single family residential homeowners were grappling with as more homes are converted to business uses.

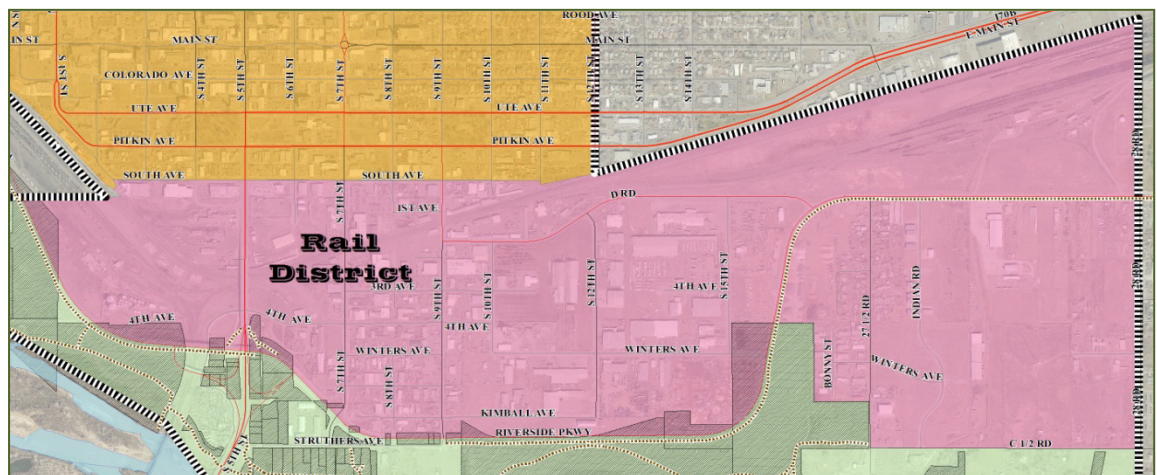


**Residential Subarea.** A significant amount of the Downtown Core contains single family residential uses. The large number of older, historic structures results in a definitive architectural character that local residents want preserved. The North Seventh Street Historic Residential District contains 34 historic homes with a high concentration of early Twentieth Century architectural styles and construction methods. The district covers five blocks of North Seventh Street with the most intact historical residential area in Grand Junction with a variety of Queen Anne, Colonial Revival and Mission style homes.

Along the edges of the downtown residential areas, commercial activity is encroaching on established single-family residential neighborhoods. Although existing residents expressed their satisfaction with existing development and uses along the North 1<sup>st</sup> Street and North Avenue, many participants expressed concern over future expansion and potential rezoning along these edges and wanted to see the Plan respect existing zone districts and development patterns and prohibit any further rezoning or variances.

**North 1<sup>st</sup> Street Neighborhood Center.** This subarea of the Downtown District is an extension of the Neighborhood Center defined along the North Avenue corridor. The intent of the Neighborhood Center is to provide for limited employment, residential, open space and limited retail uses that primarily provide convenience to the immediate neighborhood. While much of the area is already developed with commercial area, there is a need to create a better transition between the commercial uses facing North 1<sup>st</sup> Street and the side streets to the residential areas on the east side of North 2<sup>nd</sup> Street.

**B. Rail District**  
The Rail District is the area bounded by South Avenue and the railroad tracks to



the north, Riverside Parkway and C-1/2 Road along the west and south and 28 Road on the east. The man-made framework of the area consists of the buildings and infrastructure that already exist, some of which is very old, and some of which was recently constructed. The railroad tracks along the northern portion of the Rail District as well as the sidings that come into the area are very important considerations. Several spurs continue to be heavily used by the industry in the area, while others have been abandoned. The former Grand Junction Steel site, located in the center of the Rail District historically received three to four rail cars per week of raw material and finished products were then trucked from the site. Castings, Inc. handles approximately 480,000 pounds of materials and products in containers by rail each week. Thus, as in the past and as long as it continues to be used as a means of transport, the railroad is a valuable asset to this area and to the community as a whole.



The existing street network in the Rail District is incomplete. The plan for the area must address how the existing streets should be used and how new streets should be planned in the eastern and western ends of the District to continue to provide and enhance access for many modes of transportation to, from and through the area. Presently, 9<sup>th</sup>, 12<sup>th</sup> and 15<sup>th</sup> Streets are the primary north-south streets utilized by the heavy commercial and industrial uses in the area. 7<sup>th</sup> Street is generally perceived by the community as the “public” access to and from the Rail District and points south. Certainly, the completion of the Riverside Parkway through and along the southern edge of the Rail District has had a major impact on the area with a positive influence.

Part of the existing conditions of the built environment is the pattern of land ownership and use. In the central part of the Rail District there are numerous small parcels. Some have been aggregated into large holdings such as for the larger industries in the area including the former Grand Junction Steel site, Whitewater Building Materials Castings, Inc. and Munroe Pump. The railroad has large landholdings in the area as do various public entities. City-owned properties primarily include remnant parcels that were acquired for construction of the Parkway. Mesa County recently disposed of several small parcels and the State of Colorado has the Department of Transportation complex on the east side of 9<sup>th</sup> Street and D Road. To the east, the parcels are larger but some are not configured very conducive to development. There are also still remnants of the early days of this area. There are some remaining pockets of occupied single family residential homes.

There are also a few isolated commercial structures with historic significance, most notably the one pictured below which is a remnant of a sugar beet factory complex. The building was previously most visible from the riverfront trail and Orchard Mesa but it is now very visible from the Riverside Parkway. As this area becomes more familiar to people passing through on the trail and on the Parkway, perhaps some of the buildings like this one can become a more integral part of redevelopment.





It is estimated that there is a daytime population of over 1,000 employees in the Rail District that have very little available in the way of goods, services, restaurants and other commercial uses in close proximity. There appears to be a need and a desire to promote and develop uses that could not only service the daytime working population but also support the recreational and park users in evenings and on weekends.

There are surrounding influences that impact the Rail District such as the proximity of the area to the Downtown District. This plan should consider the main entrances and connections to the other Districts within the Greater Downtown area and how they can be improved as well as determine if or where there are barriers such as the railroad crossing and how those can be mitigated.

An inventory of existing land uses within the Rail District was completed as part of the planning process, identifying patterns of development and architectural character. Existing land use in the more developed central area of the District is and has historically been primarily heavy commercial and industrial with remnant pockets of residential. The area between 5<sup>th</sup> Street and 9<sup>th</sup> Street is characterized by smaller parcels with older structures. A few businesses have aggregated parcels into larger parcels which are more conducive to future improvement and/or redevelopment. This area presents the best opportunities within South Downtown for redevelopment that could be accomplished through creative incentives and partnerships.



The heavy industries are primarily located between 9<sup>th</sup> and 15<sup>th</sup> Streets, clustered around the existing railroad spurs on larger parcels. Since the rail and its users are valuable assets to the area and the community as a whole, the core of this area is unlikely to change. However, there appears to be some opportunity and community support to create transitional areas of varied land uses in a tier surrounding the core industrial area. These transitional areas can be used to create

compatibility between adjacent uses such as the park and the heavy industrial as well as help visually screen the industrial areas.



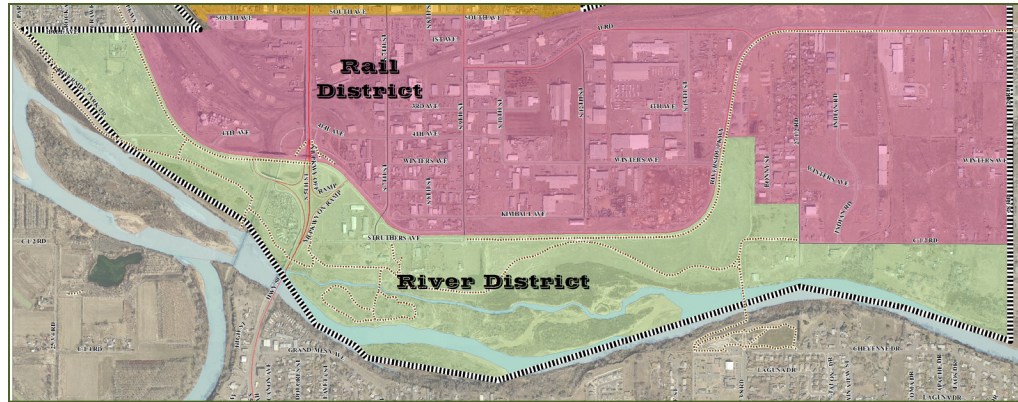
On the eastern side of the Rail District, much of



the property is held in larger ownerships and is vacant or underutilized. There is also a small pocket of existing residential use along 27-1/2 Road just south of the Riverside Parkway but which have been zoned for future commercial/industrial uses for many years. Much of the eastern area presents the greatest opportunity for increasing heavy commercial and industrial use within the Rail District and the Greater Downtown area as a whole.

### C. River District

The River District is located on either side of the confluence of the Gunnison and Colorado Rivers, roughly between the Riverside neighborhood to the northwest to 28 Road on the



east and the Riverside Parkway to the Colorado River. At this “grand junction”, the area transformed from an agricultural based community into the commercial and industrial area it is today. Over time, the area has been used to store mill tailings along the river, process sugar beets in the historic beet packing complex, and more recently the community has rediscovered its natural value for green space, trails and public uses such as the Botanic Gardens. The majority of the property in the River District is publicly owned including the Las Colonias Park site, the Botanic Gardens property, the Jarvis property, some remnants of land that were acquired for construction of the Parkway and State Parks properties on the east end of the District.

There is an existing trail through the area that must be recognized as the area develops and redevelops. The plan must consider that the trail will eventually be extended to the east along the river and that improved public access to the trail system throughout the area is a necessity.

Located within walking distance from the other downtown areas, the River District offers both easy access to recreational amenities along the river, as well as convenient access to shopping and businesses in the adjacent downtown core. It also functions as a gateway into downtown from Highway 50. Its location and context establishes it as one of the most important places in the City.

The River District is a critical area of the community. It virtually is THE Grand Junction. The Colorado River has a big influence on the area which presents both constraints and opportunities. The floodplain associated with the River has been altered by construction of a levee that protects a large part of the Railroad-River District from flood inundation but there are some areas that are still impacted by potential flooding of the River.





On the other hand, the River does present excellent opportunities to maintain and enhance amenities that have already been placed along the River including the Botanic Gardens, the Riverfront trail system, the Old Mill pedestrian bridge and the community investment of the Riverside Parkway

The proximity of the area to Orchard Mesa influences the plan both physically and visually. There are also recreation and open space uses within and nearby – Eagle Rim Park on Orchard Mesa and the

Botanic Gardens along the River. The Greater Downtown Plan should address how these spaces should be connected, enhanced and integrated into redevelopment of the area.



The topography of the site is also an important consideration. While the Railroad-River District area itself is flat, it is significantly lower than Orchard Mesa to the south. This makes it a very visible area as well as presents some unique opportunities for views and vistas.

In addition, the plan must consider planning efforts that have been completed for areas within the River District including the Botanical Gardens, Las Colonias Park and the City-owned Jarvis property. The Greater Downtown Plan must also integrate with the adjacent uses to the east that were included in the Pear Park Neighborhood Plan.

Several design concepts have been developed for the Las Colonias Park site which lies south of the Riverside Parkway from 9<sup>th</sup> Street to 27 Road. A trail has been constructed through the property and now that the Parkway has been in use, there is renewed interest in developing a more specific plan. Some amenities being considered are parking, restrooms, shelters, play area, open space, expanded walk/bike trails, open space for festivals, music, and educational presentations, various wetlands and arbor education opportunities, a kayak park, tree walk and a disc golf course.

The Jarvis property was historically used as a salvage yard until purchased and cleaned up by the City in the late 1980s. Since then, the property has been vacant with the exception of the recent trail construction through the site. Other uses on the west end of the South Downtown Neighborhood are industrial along the south side of the railroad tracks. A mixed use conceptual plan has been developed for the Jarvis property which was considered through the development of the Greater Downtown Plan.



There are areas of low density residential uses along the south side of C-1/2 Road just west of 28 Road. These uses are intended to remain, along with future development of the riverfront trail system from 27-1/2 Road to 28 Road. The Colorado State Parks has acquired several properties in this area in anticipation of continued trail development.

## **5. GREATER DOWNTOWN GOALS AND POLICIES**

City staff held a series of meetings with property owners, including elected City officials, representatives of large industries, economic redevelopment interests and owners of small businesses and properties. The meetings were conducted as informal brainstorming sessions in order to define more specific issues, constraints and opportunities and continue to discuss the community's vision for the Greater Downtown area. A more detailed outline of the public planning process is included as Appendix E.

In addition to these meetings, two public open houses were held (December 2011 and February 2012) and questionnaires and information were available on the City's web site for several weeks to invite similar input from other property owners, tenants and citizens at large. Participants were asked to evaluate various design and planning concepts to determine which of these are most important to the community and should be addressed in greater detail in the Plan. The concepts addressed four major topics relative to an area plan: land use, circulation, economic (re)development and visual character. The results of the evaluations and comments gathered on the concepts were tabulated as included in Appendix E. The results, along with previous information for the CBD, show strong community support for ideas that are translated to the goals and policies listed below for the Greater Downtown Plan.

### **A. Area-Wide Goals and Policies**

**Goal 1:** Enhance the transportation system to accommodate automobiles, bikes and pedestrians, and provide adequate, convenient parking.

**Policy 1a:** Street design will accommodate travel lanes, parking, bike lanes, medians, sidewalks, and street trees, appropriate to and complementary of the adjacent land use.

**Policy 1b:** Street design will achieve a balance between travel mobility, land use access and livability and improve connections to the Greater Downtown area and the connections between sub-districts.

**Policy 1c:** Emphasize "walkability" of the Downtown area through street design that is pedestrian friendly to provide a foundation for a safe, active and livable area, including sidewalks, accessibility improvements, bicycle facilities, off-street trail connections and safe crossings, where appropriate.

**Goal 2:** Establish and improve entry points into the Greater Downtown area (refer to the Wayfinding and Signage Map on page 35).

**Policy 2a:** Street and streetscape design will include signage, landscaping and other design elements to delineate appropriate entry points into Greater Downtown.

**Goal 3:** Promote downtown living by providing a wide range of housing opportunities, primarily in the Downtown District.

**Policy 3a:** Support a regional housing strategy with an emphasis on infill, downtown housing.

**Policy 3b:** Promote development patterns and regulations that accommodate vertical mixed-use development, primarily in the Central Business District (CBD).

**Goal 4:** Redefine the land use along key corridors to provide a mix that will offer the most opportunities for redevelopment and revitalization.

**Policy 4a:** Define sub-districts and corridor areas for groupings of land uses that are complimentary to the rest of the Greater Downtown area (refer to the Subareas Map on page 7).

**Policy 4b:** Mixed uses, including residential will be encouraged in appropriate subareas and corridors.

## **B. Downtown District Goals and Policies**

**Goal 1:** Maintain and enhance the economic, cultural and social vitality of the Downtown District.

**Policy 1a:** Define sub-districts and corridor areas for groupings of land uses that are complimentary to the rest of the Greater Downtown area (refer to the Subareas Map on page 7).

**Policy 1b:** Implement infill and redevelopment policies that support downtown.

**Policy 1c:** Encourage a wide mix of uses, offering retail and commercial services at ground level and business/office/residential on upper floors in all but residential areas.

**Policy 1d:** Maintain and expand public amenities and services in the Downtown District.

**Policy 1e:** Enhance and preserve Whitman and Emerson Parks to integrate the space into the downtown fabric and encourage use by the community.

**Policy 1f:** The City and Downtown Development Authority will explore the alternative street configuration to relocate the one-way couplet of streets that are currently Ute and Pitkin Avenues to utilize Pitkin and South Avenues for this purpose.

**Policy 1g:** The study of the one-way couplet will include an analysis of alternatives for 4<sup>th</sup> and 5<sup>th</sup> Streets including returning these streets to the 2-way grid system between Ute Avenue and North Avenue.

**Policy 1h:** Minimize surface parking in the CBD and develop new means of paying for shared parking (e.g. develop a fee in lieu of required on-site parking that will be used to fund shared parking structures).

**Goal 2:** Require density/intensity in downtown as prescribed by the *Comprehensive Plan*, primarily within the Central Business District (CBD).

**Policy 2a:** Strengthen means of implementation that promote vertically mixed-use structures, primarily within the CBD.

**Policy 2b:** Require minimums in height and density/intensity for new development in the CBD.

**Policy 2c:** Require minimal or no building setbacks within the CBD to maximize site intensity/density.

**Goal 3:** Develop a pedestrian-oriented, walkable downtown.

**Policy 3a:** Prohibit uses on ground level that do not support pedestrian activity.

**Policy 3b:** Require building façade details that activate the ground floor, particularly on corner buildings to activate north-south streets.

**Policy 3c:** Within the CBD, encourage shared parking and discourage single-use, surface parking.

**Goal 4:** Stabilize and enhance the historic residential neighborhoods.

**Policy 4a:** Discourage further encroachment of non-residential uses into the established residential neighborhoods.

**Policy 4b:** Establish design standards to address conservation and enhancement of the residential development patterns and streetscape.

**Policy 4c:** Establish design standards for the transitional areas to emphasize use and development compatibility with adjacent residential areas.

**Policy 4d:** Promote the establishment of neighborhood watch and neighborhood organizations.

**Goal 5:** Recognize and promote opportunities to build sub-districts/neighborhoods, each with a unique identity.

**Policy 5a:** Develop a set of guidelines within each sub-district to address building and façade design, streetscape, landscape and other elements of site development.

**Goal 6:** Jump-start the revitalization and reinvestment in the Downtown District with strategic catalyst projects.

**Policy 6a:** Plan and budget for strategic property acquisition for future development.

**Policy 6b:** identify locations for and promote concepts of catalyst projects, including public building/housing/mixed use, live/work units, mixed-use retail/residential and mixed-use retail/office.

## **C. Rail District Goals and Policies**

**Goal 1:** Preserve the opportunity for heavy industry and rail service that supports it.

**Policy 1a:** The City will maintain industrial zoning in those areas served by rail lines and sidings.

**Goal 2:** Recognize distinction between “industrial” streets such as 9<sup>th</sup> and 12<sup>th</sup> Streets and “public” streets 7<sup>th</sup> Street and Riverside Parkway.

**Policy 2a:** Develop street sections that reflect the differences in development patterns along and the use of the street.

**Policy 2b:** In as much as possible, encourage traffic generated from the eastern area of the Rail District to travel north and east rather than east through the low density residential areas.

**Goal 3:** Promote higher quality, customer and pedestrian friendly development along 7<sup>th</sup> Street and Riverside Parkway.

**Policy 3a:** Implement design guidelines and standards along corridors that will result in site and building design improvements along the corridors.

**Goal 4:** Re-establish and improve a street grid in the Rail District.

**Policy 5a:** The City will implement a Circulation Plan in the Rail District through construction of new streets as development occurs in the Industrial and Commercial/Industrial areas.

#### **D. River District Goals and Policies**

**Goal 1:** Create/maintain/enhance a green waterfront

**Policy 1a:** Take advantage of and create opportunities and partnerships to enhance the riverfront trail system.

**Policy 1b:** Take advantage of and create opportunities and partnerships to develop Las Colonias Park and open space areas within the Jarvis property.

**Goal 2:** Create retail, general commercial and mixed use opportunities that complement the riverfront use.

**Policy 2a:** Utilize zoning, overlay districts and incentives for development and redevelopment complimentary uses.

**Goal 3:** Create/enhance redevelopment opportunities and partnerships

**Policy 3a:** The City will work with the Downtown Development Authority (DDA) to expand its boundaries.

**Policy 3b:** The City will consider implementation of incentive strategies for redevelopment.

**Policy 3c:** The City will consider redevelopment opportunities for the Jarvis property including the potential for public-private partnerships.

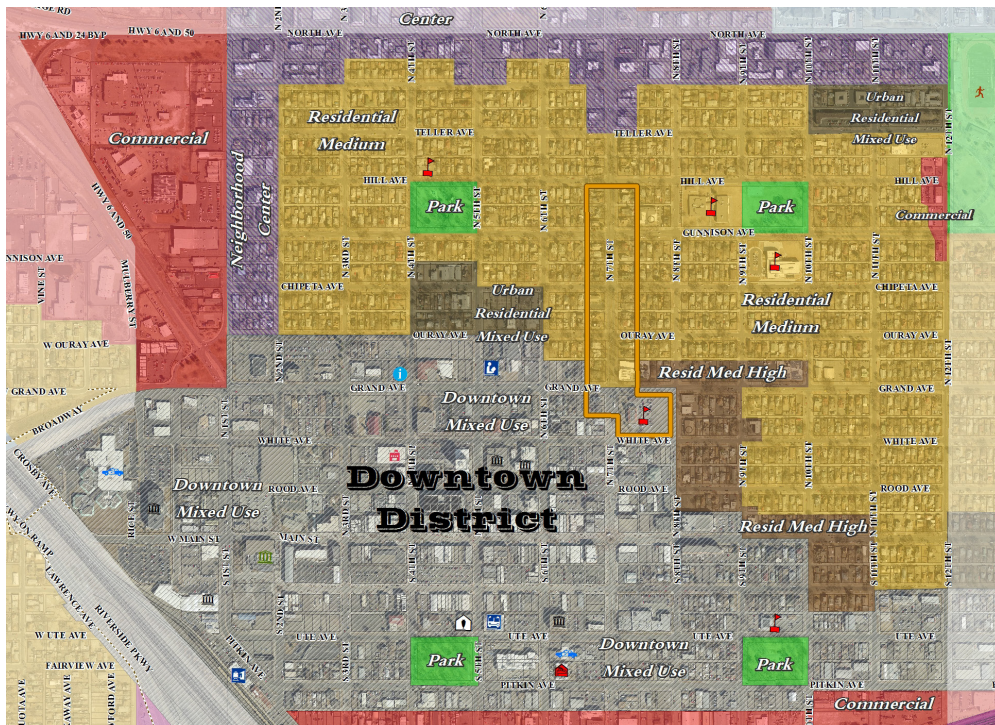


## 6. IMPLEMENTATION STRATEGIES

The City has a variety of tools available through which these goals can be implemented so that the vision for Greater Downtown can materialize and eventually be realized. This Plan represents the first phase of implementation and includes the basic strategies of designating Future Land Use categories, zoning properties accordingly as needed, amending development standards of the zoning districts through a zoning overlay, creating a circulation plan and establishing goals and policies for future phases of plan implementation such as economic (re)development strategies.

### A. Downtown District Future Land Use and Zoning

**Future Land Use.** The Greater Downtown Plan within the Downtown District is formulated around seven general land use categories: Commercial, Downtown Mixed Use, Neighborhood Center Mixed Use, Urban Residential Mixed Use, Residential Medium High Density, Residential Medium Density and Parks and Open Space. These categories correspond with those established in the City-County *Comprehensive Plan* and are illustrated on the map below. The future Land Use Plan for the Downtown District is shown below and the general categories are more specifically described on the following page.



- **Commercial (C)** – Permits a wide range of commercial development including office, retail, service, lodging and entertainment with outdoor storage or operations allowed in some locations. Mixed commercial and residential developments may be encouraged in some areas. Commercial areas in the Downtown District include the areas west of 1<sup>st</sup> Street north of Grand Avenue and the north side of the South Avenue corridor.
- **Downtown Mixed Use (DMU)** – Permits employment, residential, retail and office/business park uses. A mix of uses (2 to 8 stories), either horizontal or vertical is expected. Residential densities may exceed 24 units per acre.

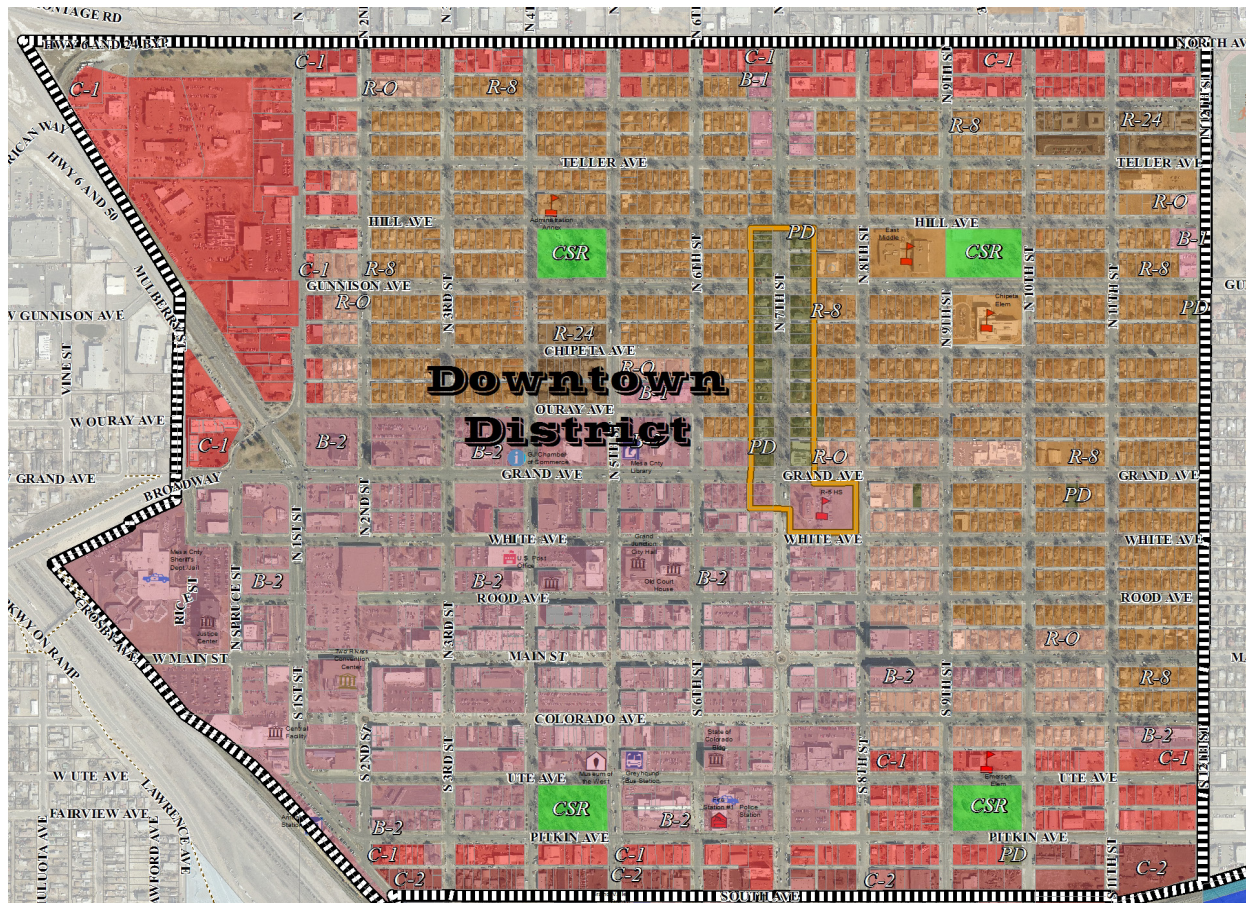


- **Parks and Open Space (Park)** – Public or private lands reserved for active park and recreation sites, open space, wildlife habitat, sensitive or hazardous land protection, and other environmental conservation purposes. In the Downtown District, the four original parks are designated Park.
- **Neighborhood Center Mixed Use (NCMU)** – Mixed use development to include employment, residential open space, retail commercial may be appropriate as a secondary use, integral to other uses and structures or as small village centers. These areas are focused on the North 1<sup>st</sup> Street and North Avenue corridors.
- **Urban Residential Mixed Use (URMU)** – Permits residential development with minor amounts of service-oriented and retail commercial use. Non-residential uses are not intended for more than 10% of a development. These areas of the Downtown District are located where there is an existing cluster of higher density residential uses or where property has been aggregated that could be redeveloped for similar types of development.
- **Residential Medium High Density (RMH)** – Permits a mix of residential development types with gross densities of 8 to 16 dwelling units per acre. Duplexes, manufactured home subdivisions and low intensity attached residential development will be integrated with townhomes and low-intensity, multifamily development. Larger multifamily development and/or small neighborhood retail/service may be permitted. In the Downtown District, these areas provide a transition on the northeastern perimeter of the central business district to the single family development of the established downtown neighborhoods.
- **Residential Medium Density (RM)** – Permits a mix of residential development types with gross densities of 4 to 8 dwelling units per acre. Single family development may be integrated with other dwelling types, including duplexes and low intensity attached residential development. The established neighborhoods within the Downtown District are designated RM.

**Zoning.** Proposed zoning within the Downtown District is shown on the map on the following page. Generally, the zoning is proposed to remain the same as currently exists. The zone districts within the Downtown District are described on the following pages.

- **Residential 8 units per acre (R-8)** – Provides for medium-high density attached and detached dwellings, two-family dwelling and multifamily. R-8 is a transitional district between lower density single-family districts and higher density multifamily or business development. A mix of dwelling types is allowed in this district.
- **Residential 24 units per acre (R-24)** – Provides for high density residential use. This district allows multifamily development within specified densities. R-24 may serve as a transitional district between single-family and trade zones. This district is intended to allow high density residential unit types and densities to provide a balance of housing opportunities in the community.
- **Residential Office (R-O)** – Provides low intensity, nonretail, neighborhood service and office uses that are compatible with adjacent residential neighborhoods. Development regulations

and performance standards are intended to make buildings compatible and complementary in scale and appearance to a residential environment.

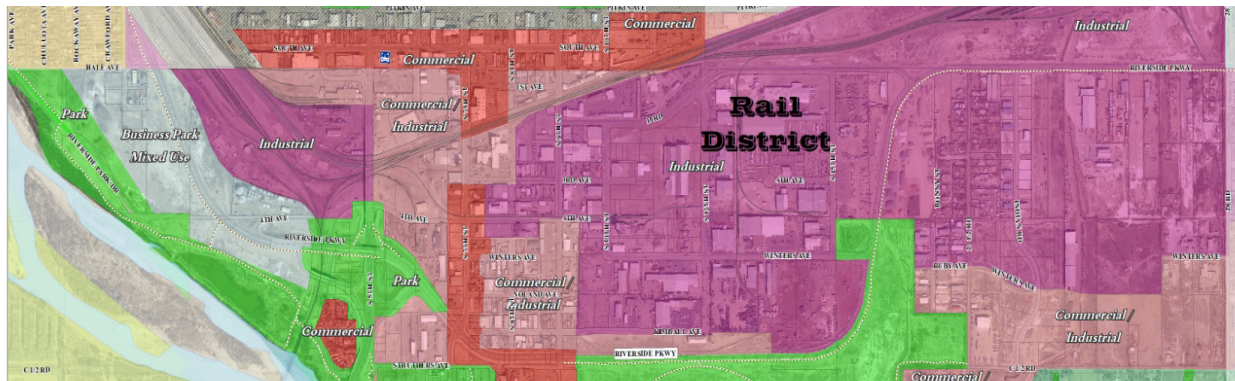


- **Neighborhood Business (B-1)** – Provides small areas for office and professional services combined with limited retail uses, designed in scale with surrounding residential uses; a balance of residential and nonresidential uses.
- **Downtown Business (B-2)** – Provides concentrated downtown retail, service, office and mixed uses not including major/regional shopping centers or large outdoor sales areas. The B-2 district promotes the vitality of the downtown area as provided by the Comprehensive Plan. Pedestrian circulation is encouraged as are common parking area.
- **Light Commercial (C-1)** – Provides indoor retail, service and office uses requiring direct or indirect arterial street access, and business and commercial development along arterials. The C-1 district should accommodate well-designed development on sites that provide excellent transportation access, make the most efficient use of existing infrastructure and provide for orderly transitions and buffers between uses.
- **General Commercial (C-2)** – provides for commercial activities such as repair shops, wholesale businesses, warehousing and retail sales with limited outdoor display of goods and outdoor operations.

- **Community Services and Recreation (CSR)** – Provides public and private recreational facilities, schools, fire stations, libraries, fairgrounds, and other public-institutional uses and facilities. The district includes open space areas to prevent environmental damage to sensitive areas and to limit development in areas where police or fire protection, flood protection, or other services or utilities are not readily available.

## B. Rail District Future Land Use and Zoning

**Future Land Use.** The Greater Downtown Plan within the Rail District is formulated around five general land use categories: Business Park Mixed Use, Commercial, Commercial industrial, Industrial and Parks and Open Space. These categories correspond with those established in the City-County *Comprehensive Plan*. The future Land Use Plan for the Downtown District is shown below and the general categories are more specifically described on the following page.

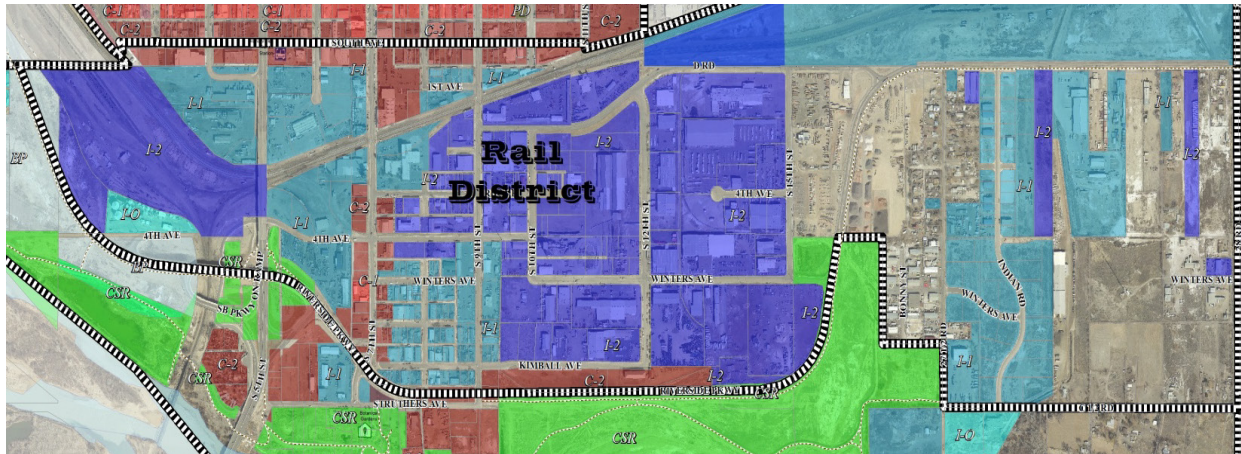


- **Business Park Mixed Use (BPMU)** – Permits business, light industrial, employment-oriented areas with the allowance of multifamily development. Within the Rail District, this land use category applies to the City-owned property along Riverside Parkway west of 5<sup>th</sup> Street.
- **Commercial (C)** – Permits a wide range of commercial development including office, retail, service, lodging and entertainment with outdoor storage or operations allowed in some locations. Mixed commercial and residential developments may be encouraged in some areas. Commercial areas in the Rail District are focused on the primary corridors of 5<sup>th</sup> Street, 7<sup>th</sup> Street, South Avenue and the north side of the Riverside Parkway between 7<sup>th</sup> and 12<sup>th</sup> Streets.
- **Commercial Industrial (CI)** – Permits heavy commercial, offices and light industrial uses with outdoor storage, with some outdoor operations. Yard operations may be permitted where adequate screening and buffering can be provided to ensure compatibility with existing and planned development in the vicinity of the proposed use. Residential uses are limited. Within the Rail District, Commercial Industrial land use provides transitional areas between Commercial and Industrial areas.
- **Industrial (I)** – Predominant uses are heavy commercial and industrial operations. Residential uses are not appropriate. Industrial areas in the Rail District are focused on existing infrastructure including rail and roadway corridors.



- **Parks and Open Space (Park)** – Public or private lands reserved for active park and recreation sites, open space, wildlife habitat, sensitive or hazardous land protection, and other environmental conservation purposes. In the Rail District, the Park areas correspond with City-owned property along 5<sup>th</sup> Street and the Riverside Parkway.

**Zoning.** Proposed zoning within the Rail District is shown on the map on the below. The majority of the zoning is proposed to remain the same as currently exists. The zone districts within the Rail District are described on the following pages.

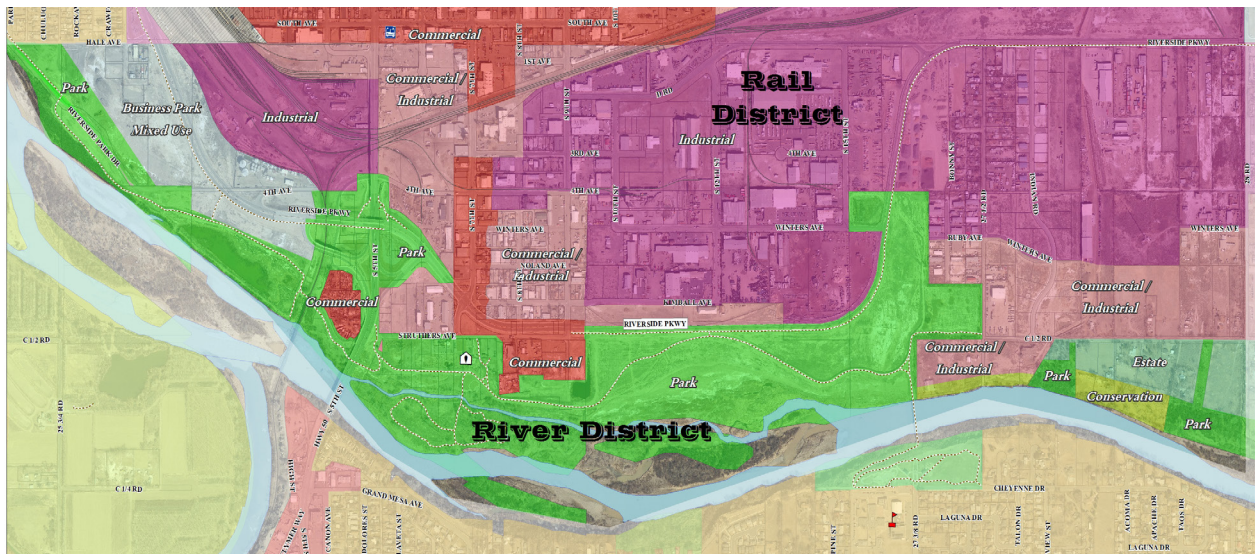


- **Light Commercial (C-1)** – Provides indoor retail, service and office uses requiring direct or indirect arterial street access, and business and commercial development along arterials. The C-1 district should accommodate well-designed development on sites that provide excellent transportation access, make the most efficient use of existing infrastructure and provide for orderly transitions and buffers between uses.
- **General Commercial (C-2)** – provides for commercial activities such as repair shops, wholesale businesses, warehousing and retail sales with limited outdoor display of goods and outdoor operations.
- **Community Services and Recreation (CSR)** – Provides public and private recreational facilities, schools, fire stations, libraries, fairgrounds, and other public-institutional uses and facilities. The district includes open space areas to prevent environmental damage to sensitive areas and to limit development in areas where police or fire protection, flood protection, or other services or utilities are not readily available.
- **Business Park Mixed Use (BP)** – Provides for a mix of light manufacturing and employment centers, limited commercial services and multifamily residential uses in a business park setting with proper screening and buffering, all compatible with adjoining uses.
- **Industrial/Office Park (I-O)** – Provides for a mix of light manufacturing uses, office park, limited retail and service uses in a business park setting with proper screening and buffering, all compatible with adjoining uses.

- **Light Industrial (I-1)** – Provides for areas of light fabrication, manufacturing and industrial uses which are compatible with existing adjacent uses, access to transportation and the availability of public services and facilities.
- **General Industrial (I-2)** – Provides areas of heavy and concentrated fabrication, manufacturing and industrial uses which are compatible with adjacent uses, easy semi-tractor trailer access to the State highway system and/or railroads and the availability of public services and facilities.

### C. River District Future Land Use and Zoning

**Future Land Use.** The Greater Downtown Plan within the River District is formulated around six general land use categories: Commercial, Commercial Industrial, Parks and Open Space, Conservation, Estate and Business Park Mixed Use. These categories correspond with those established in the City-County *Comprehensive Plan*. The following page shows the Future Land Use Plan for the River District and includes more detailed descriptions of the general categories.



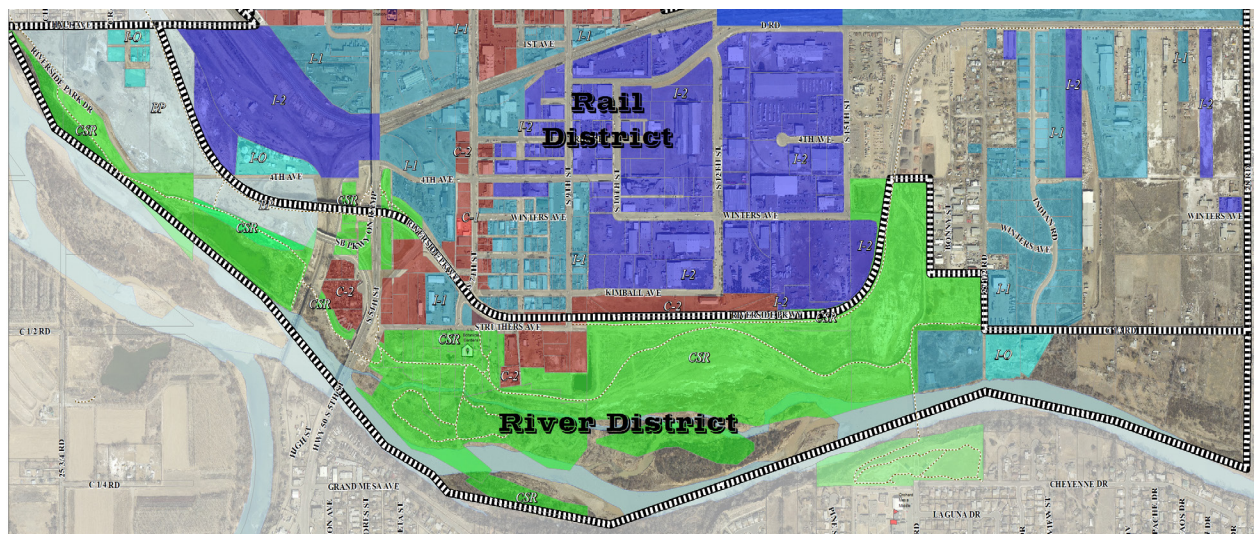
- **Business Park Mixed Use (BPMU)** – Permits business, light industrial, employment-oriented areas with the allowance of multifamily development. Within the River District, this land use category applies to the City-owned property along the river and the Riverside Parkway west of 5<sup>th</sup> Street.
- **Commercial (C)** – Permits a wide range of commercial development including office, retail, service, lodging and entertainment with outdoor storage or operations. Mixed commercial and residential developments may be encouraged in some areas. Commercial areas in the River District focus on private property along 5<sup>th</sup> Street and the south side of the Riverside Parkway east of the Botanical Gardens.
- **Commercial (C)** – Permits a wide range of commercial development including office, retail, service, lodging and entertainment with outdoor storage or operations. Mixed commercial and residential developments may be encouraged in some areas. Commercial areas in the River



District focus on private property along 5<sup>th</sup> Street and the south side of the Riverside Parkway east of the Botanical Gardens.

- **Commercial Industrial (CI)** – Permits heavy commercial, offices and light industrial uses with outdoor storage, with some outdoor operations. Yard operations may be permitted where adequate screening and buffering can be provided to ensure compatibility with existing and planned development in the vicinity of the proposed use. Residential uses are limited. Within the River District, Commercial Industrial land use corresponds with areas of existing heavy commercial/light industrial zoning.
- **Parks and Open Space (Park)** – Public or private lands reserved for active park and recreation sites, open space, wildlife habitat, sensitive or hazardous land protection, and other environmental conservation purposes. In the River District, the Park areas correspond with City-owned property along 5<sup>th</sup> Street, the Riverside Parkway and the Colorado River.
- **Estate** – Typical “estate” style single family homes on large lots of 1 to 3 acres. Centralized services might be needed depending on site conditions and proximity to existing services. Uses are residential. The Estate areas in the River District are limited to the large lot single family development along the eastern end of the Colorado River.
- **Conservation** – Public or private lands reserved for open space, wildlife habitat, sensitive or hazardous land protection and other environmental or conservation purposes. The areas shown as Conservation in the River District represent the community’s desire to continue the riverfront trail system east of 27-1/2 Road.

**Zoning.** Proposed zoning within the River District is shown on the map on the below. The majority of the zoning is proposed to remain the same as currently exists. The zone districts for properties currently within the City limits in the River District are described below and on the following pages.



- **Light Commercial (C-1)** – Provides indoor retail, service and office uses requiring direct or indirect arterial street access, and business and commercial development along arterials. The C-1 district should accommodate well-designed development on sites that provide excellent



transportation access, make the most efficient use of existing infrastructure and provide for orderly transitions and buffers between uses.

- **General Commercial (C-2)** – Provides for commercial activities such as repair shops, wholesale businesses, warehousing and retail sales with limited outdoor display of goods and outdoor operations.
- **Community Services and Recreation (CSR)** – Provides public and private recreational facilities, schools, fire stations, libraries, fairgrounds, and other public-institutional uses and facilities. The district includes open space areas to prevent environmental damage to sensitive areas and to limit development in areas where police or fire protection, flood protection, or other services or utilities are not readily available.
- **Business Park Mixed Use (BP)** – Provides for a mix of light manufacturing and employment centers, limited commercial services and multifamily residential uses in a business park setting with proper screening and buffering, all compatible with adjoining uses.
- **Industrial/Office Park (I-O)** – Provides for a mix of light manufacturing uses, office park, limited retail and service uses in a business park setting with proper screening and buffering, all compatible with adjoining uses.
- **Light Industrial (I-1)** – Provides for areas of light fabrication, manufacturing and industrial uses which are compatible with existing adjacent uses, access to transportation and the availability of public services and facilities.

#### **D. Development Standards**

The community desires to improve the visual character of the Greater Downtown areas that are most visible along major public corridors or from the major public spaces. Consequently, the Plan promotes a higher quality built environment through improved architectural character, reduced visual clutter and enhanced streetscape. These elements are addressed through the Greater Downtown Zoning Overlay detailed in a companion document to this Plan. The elements of the overlay are intended to augment the zoning district standards in the Zoning and Development Code.

#### **E. Greater Downtown Circulation Plan**

The Circulation Plan for Greater Downtown is shown in Appendix C. The plan identifies a street network that includes both existing and proposed streets and both major and minor streets. The Circulation Plan also identifies potential enhancements within Greater Downtown that could be implemented over a number of years. The desired cross-sections and level of streetscape development along the streets within Greater Downtown support the overall Plan goals, land uses and circulation to and from the area for pedestrian, bicycle and vehicles.

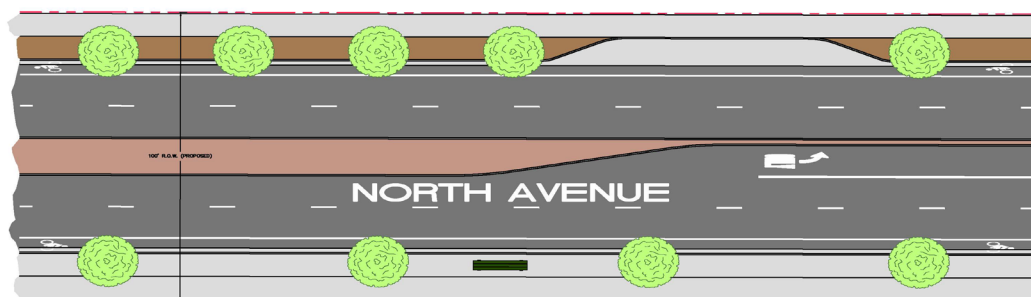
**Policy:** Conduct a more detailed traffic analysis of the area to determine needed intersection control and street cross-sections.

**Major Street Corridors.** Major streets in the Grand Junction urbanized area are classified according to their function in the transportation network. The two components of function are to provide access to

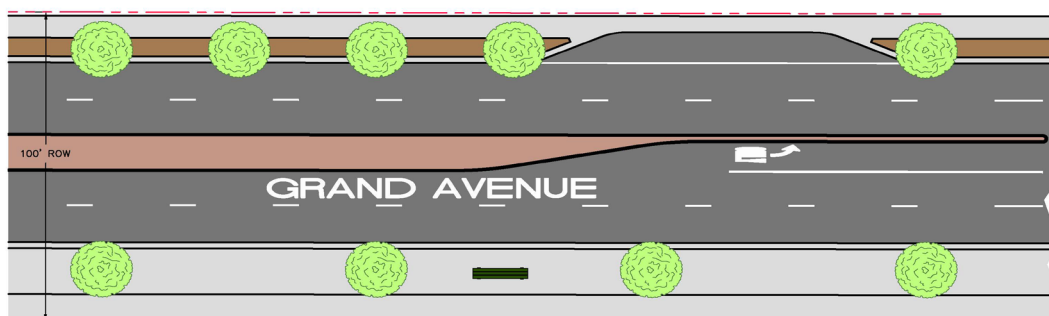
properties and to carry traffic from point to point. In order to preserve safety and capacity and enhance the quality of living, the relation of these two components should be inversely proportionate, with the busier streets having limited access and the quieter streets providing access to properties. The components of the major street system have been identified on a functional classification map, known as the Grand Valley Circulation Plan that has been adopted by the City of Grand Junction and accepted by Mesa County. The Greater Downtown Circulation Plan is intended to augment the Grand Valley Circulation Plan and provide more specific guidance on multimodal circulation improvements as development and redevelopment occurs in the Greater Downtown area.

As properties develop and redevelop within Greater Downtown, the Circulation Plan will be implemented through construction or improvement of streets when warranted by the proposed development. The specific design of each street is generally based on the land use and zoning of the properties along it. For example, in areas that will be zoned Commercial, the Commercial Street cross-section will apply, unless modified by suggestions in the Greater Downtown Plan. Similarly, streets within Industrial areas are to be developed/improved according to the City's adopted Industrial street cross-section, unless modified by suggestions in this Plan. The street classifications and proposed street sections for the major corridors in Greater Downtown are described below. The concept drawings included on the following pages further illustrate elements of the Circulation Plan.

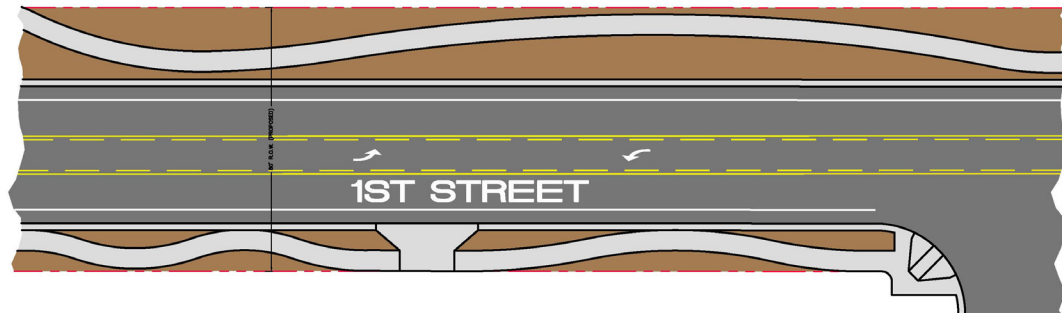
- **North Avenue – Arterial Street.** A potential layout for North Avenue between 1<sup>st</sup> and 12<sup>th</sup> Streets was adopted with the *North Avenue West Corridor Plan*. The layout is incorporated into the Greater Downtown Plan as depicted below.



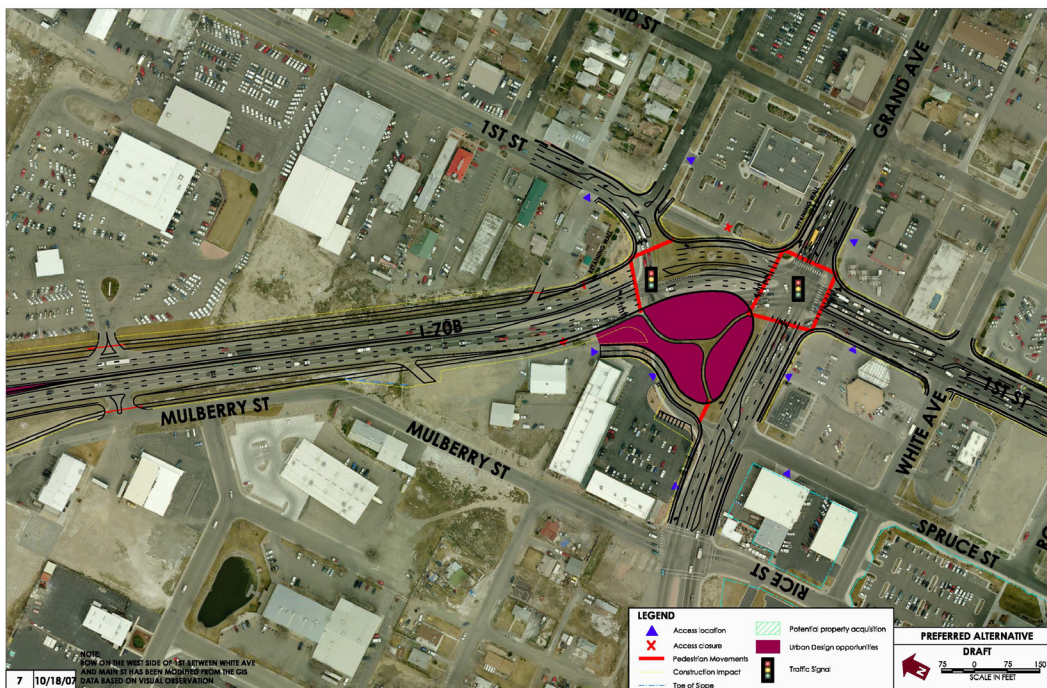
- **Grand Avenue – Arterial Street.** While Grand Avenue is an established street with improved streetscape elements the length of the segment within Greater Downtown (1<sup>st</sup> to 12<sup>th</sup> Streets), there could be improvements made in some blocks that would enhance traffic flow, pedestrian safety and visual aesthetics. A typical, potential median improvement to Grand Avenue is depicted below.



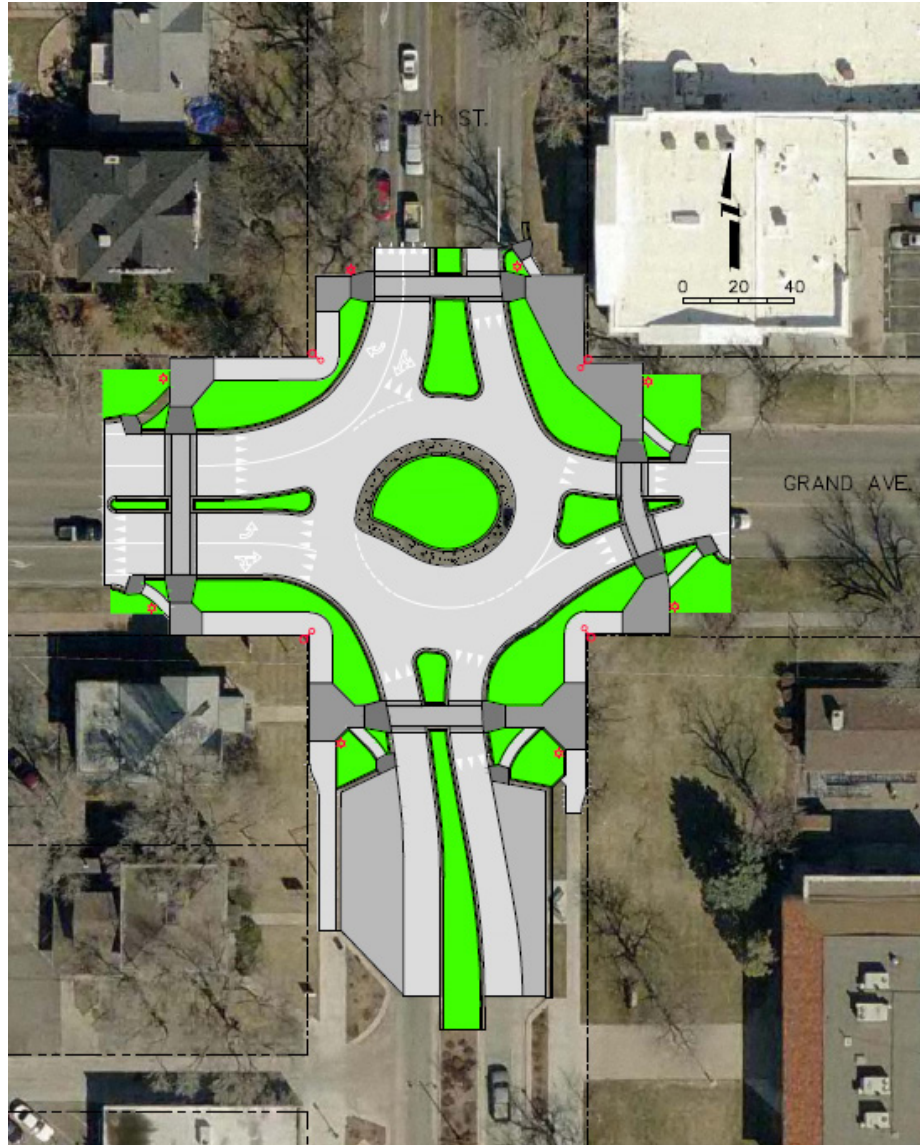
- North 1<sup>st</sup> Street – Arterial Street.** The segment of North 1<sup>st</sup> Street from Grand Avenue to North Avenue is deficient in street design and pedestrian amenities. The Greater Downtown Plan envisions eventual redevelopment of this street to improve safety, drainage, pedestrian circulation, access and improve on-street parking. A potential layout is depicted below.



- 1<sup>st</sup> Street and Grand Avenue Intersection –** The Colorado Department of Transportation (CDOT) completed an Environmental Assessment for the I-70B/6 & 50 corridor which recognized circulation needs at the 1<sup>st</sup> and Grand intersection. A potential design is shown below. The Greater Downtown Plan incorporates these ideas as well as potential improvements on 1<sup>st</sup> Street from Grand Avenue to Pitkin Avenue.

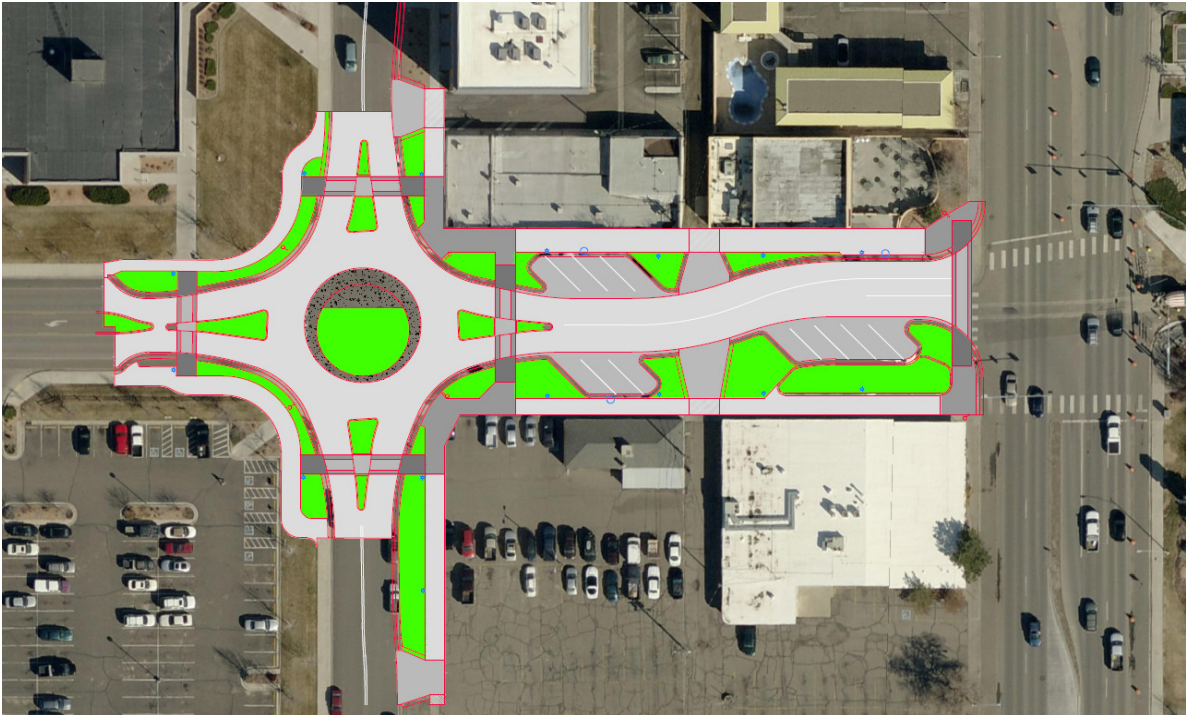


- **7<sup>th</sup> Street and Grand Avenue Intersection** – A design for a roundabout was included with the plans for 7<sup>th</sup> Street improvements that were completed in 2007-2008. The roundabout was not constructed at that time but the intersection still warrants vehicular and pedestrian improvements. The plan as originally proposed is shown below.





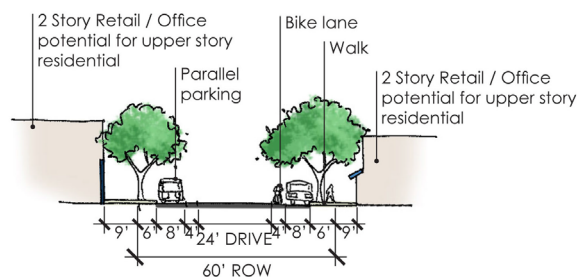
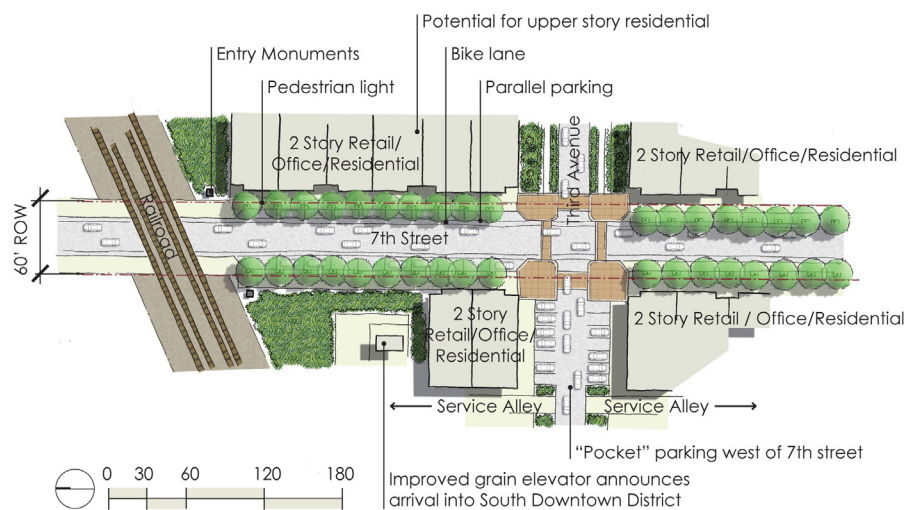
- West Main Street and Spruce Street Intersection** – Vehicular and pedestrian traffic has increased on the West Main Street corridor due to activity at the County Justice Center and the Mesa County Central Services building. To ease traffic flow through the area and improve pedestrian safety, this potential project for a roundabout at Spruce and West Main Streets was originally conceived during the visioning of Main Street in 2008. The roundabout would serve as a visual anchor to the west end of Main Street just as the one at 7<sup>th</sup> Street serves the east end. The roundabout would also allow for a circulator bus to serve the Main Street corridor from Spruce Street to 7<sup>th</sup> Street.



- Riverside Parkway** – Arterial Street. The right-of-way width varies; multi-lane; bike lanes; detached walk on the south side; no on-street parking. Completed in 2007.



- South 7<sup>th</sup> Street** – Collector Street. 60-foot right-of-way width; 2 lanes; bike lanes; on-street parking both sides; detached walks with landscaping. The streetscape plan for South 7<sup>th</sup> Street should be enhanced with a similar design as what is currently under construction on 7<sup>th</sup> Street south of Grand Avenue. Generally, the design would continue the 7<sup>th</sup> Street boulevard treatments from downtown, Ute and Pitkin to the Botanical Gardens and riverfront area with additional street trees, historic street lights, street furniture and public art. This design would create a more consistent visual character to connect the South Downtown Neighborhood with the traditional downtown area and improves the visual design of the corridor and emphasizes its use as the primary public north-south corridor through the neighborhood. The design features enhanced pedestrian facilities with colored concrete, pedestrian safe zones at the “bulb-outs” for easier crossing and additional landscaping.

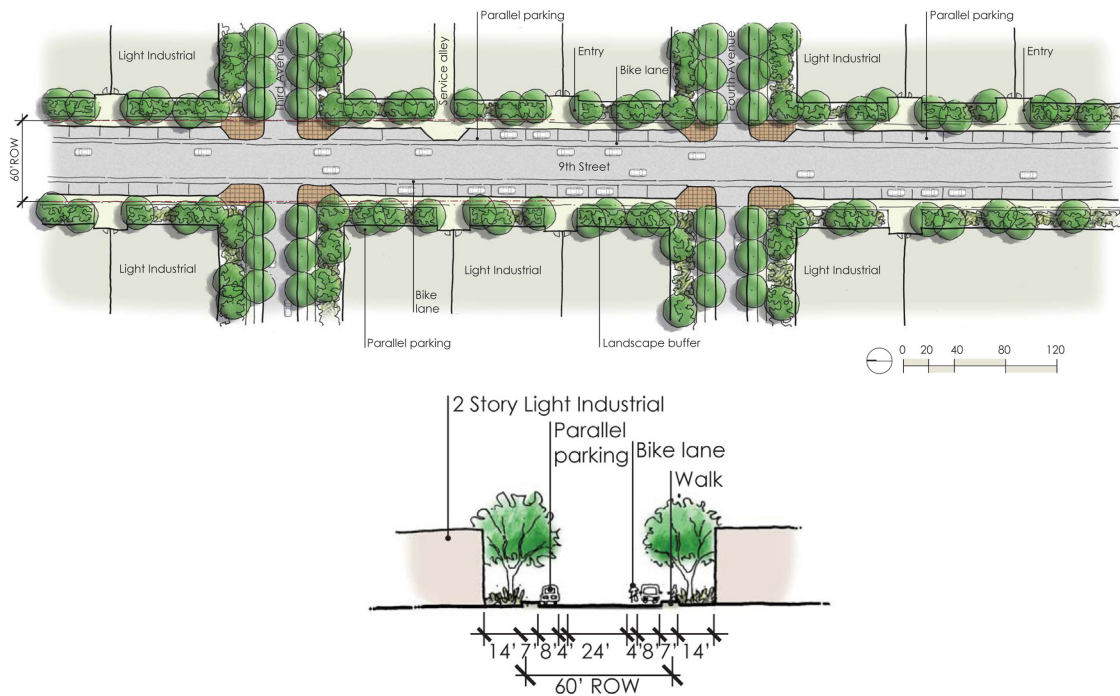


**7th Street Section**

- South 9<sup>th</sup> Street and 27-1/2 and C-1/2 Roads** – Collector Street. 60-foot right-of-way width; 2 lanes; bike lanes; on-street parking both sides; detached sidewalk preferred where possible. The streetscape plan for South 9<sup>th</sup> Street is to develop similar to what presently exists along South 7<sup>th</sup> Street with a more defined hardscape of curb and gutter, enhanced pedestrian facilities and street trees. This design improves the visual quality of the corridor without requiring improvements on private property or compromising adjacent uses. The design also allows the street to function for the commercial/light industrial traffic that it carries as well as



provides for a more comfortable pedestrian or bicycle environment since South 9<sup>th</sup> Street may be used by the public to access business/commercial areas and Las Colonias Park.



**9th Street Section**

- **Kimball Avenue** – Collector Street. 60-foot right-of-way width; 2 lanes; on-street parking on one side; detached walks with landscaping.



- **D Road (from 9<sup>th</sup> Street east to the Riverside Parkway)** – Arterial Street. Section yet to be determined.

**Local Streets.** The Local Street network provides access to individual parcels and serves short length trips to and from collector and higher order streets. Trip lengths on local streets should be short with a lower volume of traffic along with slower speeds. Design of local streets occurs through the development process and will be in accordance with the City's adopted Transportation Engineering Design Standards (TEDS). It is important in the design process to provide connections to adjacent parcels and subdivisions for efficient vehicle travel and a safe network for pedestrians and bicycles.

**Trails.** The Grand Valley Trails Plan is a planning document that shows the location of future bicycle facilities, trails and pedestrian paths. Implicit in the plan is the construction of sidewalks in accordance with the adopted street cross-sections as previously illustrated. One of the major purposes of the City's Urban Trails Committee is facilitating linkages from the riverfront trail system to the urban area. As development or redevelopment occurs, construction of trails, paths, bike lanes and pedestrian facilities in accordance with the adopted plan either occurs with the development or the City constructs the same with the collection of the Transportation Capacity Payment (TCP) as part of a more comprehensive capital improvement project.

The Grand Valley Trails Plan shows the following proposals within Greater Downtown.

- Extension of Riverfront Trail from Las Colonias Park east to 28 Road (and beyond).
- Bike Routes (signs but no separate lane) on Grand Avenue from 7<sup>th</sup> Street east and 28 Road between the Riverside Parkway and C-1/2 Road.
- Sharrow Route (bikes share lane with cars) on Grand Avenue between 1<sup>st</sup> and 7<sup>th</sup> Streets.

**Riverside Parkway Pedestrian Overpass.** It is envisioned that eventually there may be need for one or more pedestrian overpasses from the commercial areas of the Rail District to the riverfront areas and Las Colonias Park. Development, activities and uses in the future park and types of development along the north side of the Parkway will dictate where these may be needed based on the level of pedestrian traffic. An overpass on the western end of the area in the vicinity of 7<sup>th</sup> or 9<sup>th</sup> Street could also serve as an entrance feature to the neighborhood as further discussed in section F. below.

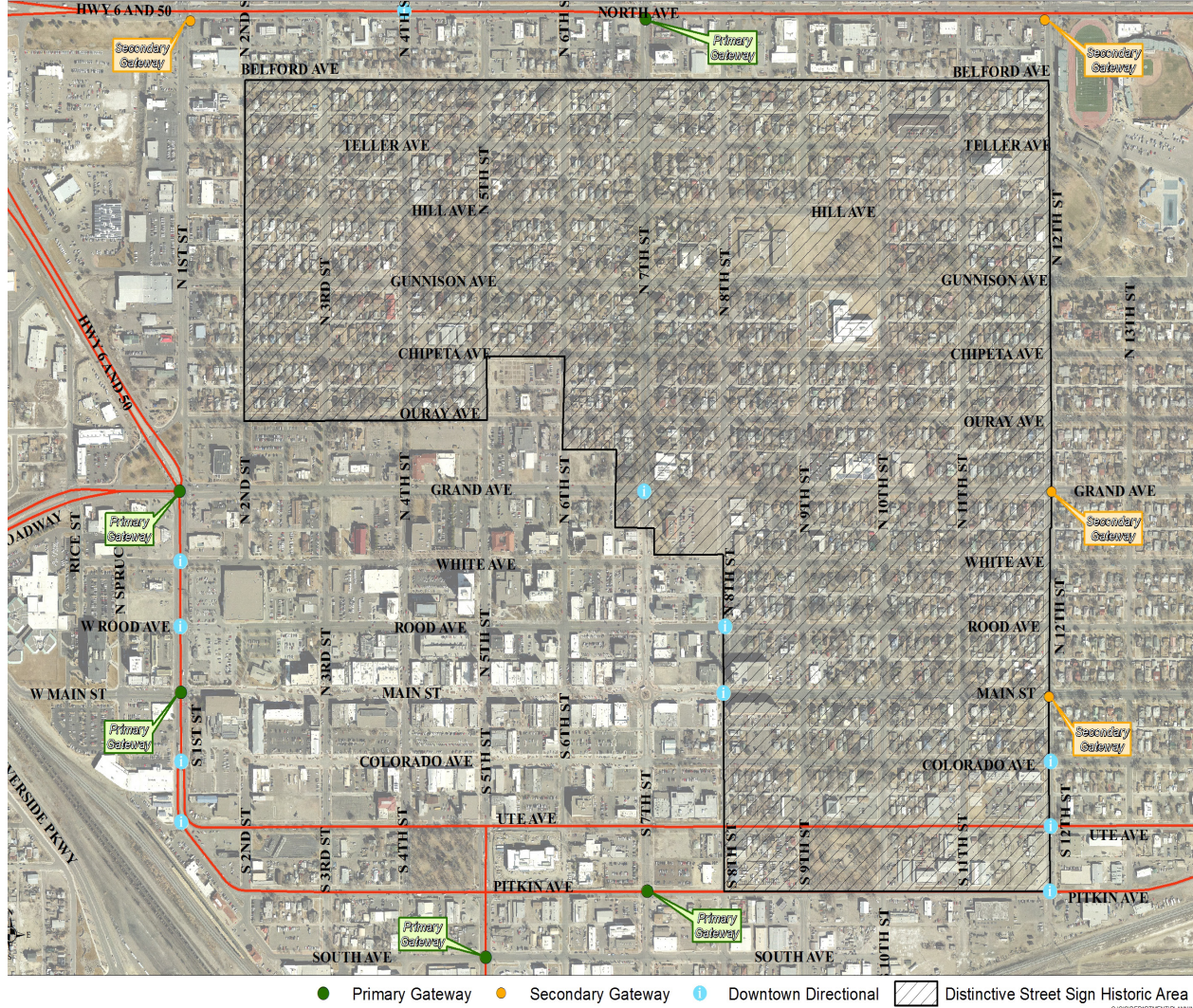
**Public Transportation (GVT).** Grand Valley Transit (GVT) presently serves the Downtown District but does not provide service in the Rail and River Districts. Future transit needs within the Rail and River Districts will need to be monitored as more areas are developed or redeveloped and as Las Colonias Park becomes more developed and active.

## **F. Entryways and Signage**

Important intersections in the street network offer opportunities to develop a unique theme and identity for the Greater Downtown area. The primary intersections vary in scale and include entries to the Downtown, Rail and River Districts. Each of these should be developed according to general concepts and criteria that are appropriate for their scale, function and importance.



## Downtown District Wayfinding and Signage



### Downtown District Entryways and Signage

While the Downtown District has a strong base of local and regional users, approaches to the District offer no distinguishing features or directional signage that orient first time visitors. When approaching or traversing perimeter streets, visitors have no sense that they are within eyesight of the historic town site or even that they are headed in the right direction toward Downtown. Creating gateways at key entry points, as well as simple perimeter identifiers, will orient visitors, as well as strengthen and celebrate the heritage of the Downtown Districts. Because of the diversity of uses and neighborhoods within the Downtown District, the design of entryways should include a range of sizes, poles and ornamentation of signage and varied landscape elements, lighting and other features that are integrated with the signage. The primary gateways to the Downtown District and between Downtown and the Rail District include the following intersections.



- 7<sup>th</sup> Street and North Avenue
- 1<sup>st</sup> Street and Grand Avenue
- 1<sup>st</sup> Street and Main Street
- 5<sup>th</sup> Street and South Avenue
- 7<sup>th</sup> Street and Pitkin Avenue

In addition, the distinctive public sign palette already started in the Downtown District should be continued and expanded to include street signs and directional signs that have recognizable poles, ornamentation, colors, fonts and logos. The DDA will further develop sign standards and guidelines for private signage placed on buildings or as freestanding signs.

### **Rail and River District Entryways**

**5<sup>th</sup> Street/Riverside Parkway.** There are opportunities to celebrate the entry into Grand Junction and the Rail and River Districts at the 5<sup>th</sup> Street bridge and Struthers Avenue area in conjunction with the Western Colorado Botanical Gardens with attractive low scale signage and sculpture. In addition, there are smaller monuments at various points along the Riverside Parkway that indicate to motorists that they are approaching or traveling on the Riverside Parkway (shown below). Due to the scale of the 5<sup>th</sup> Street/Riverside Parkway intersection and the publicly-owned area around it, this intersection affords the opportunity to create a monument/sculpture of a much larger scale to mark the entrance and give identity to the Rail and River Districts and/or to this “Grand Junction”.

Another possibility in this vicinity is if a pedestrian overpass is desired/needed near the 7<sup>th</sup> Street/Riverside Parkway intersection it could serve several purposes: pedestrian access across the Parkway, include design elements that give a distinct character to the Rail and River Districts and integrate with surrounding open space, pocket parks and/or water features at the landings on each side of the Parkway.

**South 7<sup>th</sup> Street/Railroad Tracks.** There is an existing silo on the Mesa Feed property that identifies the Rail District as you travel south on 7th Street. It is of a scale that is visible from the southern perimeter of downtown and represents the historical agriculture and industrial base upon which the Rail and River Districts have developed. Such a structure could be enhanced and/or replicated to become an even stronger element at this major entrance to the area.

**28 Road/Riverside Parkway.** This intersection is a smaller scale than the others but a neighborhood entry could be created, particularly along the north side. The sense of arrival at this location could be created through a water feature, public art, an architectural feature with signage paving patterns and/or landscaping. The design of the entry feature should be of the same character of those that might be created at the other major entry points.

## **G. Economic Redevelopment**

### **Downtown District Economic Redevelopment**

While the Downtown District is the heart of the community, it is but one subset of a larger market and has strengths which can be capitalized on and limitations which should be overcome. Downtown has a tremendous influence on the economic well-being of the entire region. Therefore, it is widely accepted that early projects in any revitalization effort should be publicly assisted until market conditions reach levels where new construction can support itself.

The *Grand Junction Strategic Downtown Master Plan* presented guiding principles which; while general in nature, were considered responsive to prevailing conditions, market opportunities, framework elements and stakeholder input. These guiding principles are still relevant to the Greater Downtown Plan and are listed and described below.

- **Downtown is one submarket that competes with other submarkets in Grand Junction.** The downtown environment, while presenting tremendous opportunity for investment in a setting uniquely positioned to offer both heart and history, carries with it certain limitations, particularly for land-intensive non-destination-oriented land uses. Several market sectors, however, not only survive, but also thrive in a downtown setting. Recognize the obstacles associated with downtown development and encourage regulatory and financial solutions including public subsidies and creative financing mechanisms.
- **Downtown must be market-responsive** to changing conditions, with implementation tools and mechanisms in place to both offset competitive disadvantages and capitalize on competitive assets. Market conditions should be continually monitored and information distributed to a broad audience including developers, business and property owners, lenders, city staff and elected and appointed officials.
- **Downtown infrastructure must be protected and retained** including physical features, service organizations, a mix of employers, historic residential neighborhoods and community attitudes toward Downtown. Unlike many communities across the country, Downtown Grand Junction's existing infrastructure is more than sufficient to promote itself as a downtown neighborhood. These assets, which provide the impetus for investment, need to be protected and promoted.
- **Downtown's "tool bag" must contain a variety of strategies and mechanisms to attract investment.** These tools can be financial, physical, market, or organizational in nature and can be used independently or in various combinations. Given the obstacles associated with downtown development, it is imperative that whatever mix of tools is put in place it be comprehensive, flexible and creative.
- **Public investment must leverage private investment.** Historically, the planning, financing and implementation of projects in the downtown market were the primary responsibility of public sector entities such as the City and the DDA. However, while the public sector continues to play a significant role in most downtown efforts, a critical component to the success of any revitalization strategy today is participation by both the public and private sectors. Leveraging of resources is key, as no one entity, public or private, has sufficient resources alone to sustain a long-term downtown improvement effort.
- **Public policy must support downtown development.** Experience has proven that main street or downtown development will best succeed if regional growth management programs reward efficient development patterns. If growth is allowed to occur in a land extensive, inefficient way that effectively subsidizes lower densities, main street development will operate at a competitive disadvantage. Given Grand Junction's existing land use patterns, downtown is susceptible to continued dilution of its role as the community's central business and shopping district.



- **Public-private partnerships are essential.** Under any investment strategy, local government needs to have strong involvement, a visible presence, perhaps be the entity that provides continuing leadership, and always provide regulatory incentives and seed capital for early projects. Not only does government have the legal responsibility to address many of the implementation components, but it is also the logical conduit to local, regional, state and federal funding sources.

#### **Rail and River District Economic Redevelopment**

The changes that have occurred in portions of Greater Downtown such as completion of the Riverside Parkway and planning for the future development of Las Colonias Park have already had a positive influence on the River and Rail Districts. Many properties have been renovated or redeveloped, new uses are relocating to the area and property values are generally on the rise. The Greater Downtown Plan envisions this trend continuing and being enhanced by the following redevelopment concepts:

- Allow existing heavy industry to remain, taking advantage of rail spurs within the area.
- Intensified commercial edge along the north side of the Riverside Parkway with opportunities for mixed use development.
- New general commercial, retail and residential uses will provide activity at the edge of the park after business hours to create a safe park environment that gives “ownership” of the park to the adjacent local business owners and residents.
- New retail and commercial uses such as restaurants, shops and services along South 7<sup>th</sup> Street to serve the employees, recreational users and residents of the neighborhood.
- Commercial Industrial uses bridge the existing industrial and the commercial corridors.

In addition, discussions with the Grand Junction Economic Partnership, Business Incubator, Manufacturers’ Council and Chamber of Commerce during development of the Greater Downtown Plan brought to light many opportunities for the area, the majority of which is within the established Mesa County Enterprise Zone. The Plan outlines goals, policies and strategies that can be used to further the economic (re)development of the Rail and River Districts in Greater Downtown.

- Need for flex space for different types of small business – new to area or graduating from the Business Incubator. Opportunity for these businesses to serve employee base, residents and recreational users in the area.
- Opportunity to develop additional incentives for redevelopment that has taken advantage of partnerships and/or assembled parcels of land totaling a minimum of ½ acre or more
- Allow for live-work opportunities
- Opportunity to develop partnerships

#### **H. Greater Downtown Area Parks**

The four downtown parks were included on the original town plat created by the Grand Junction Town Company in 1882. All four parks still exist as downtown open space. The City acquired Las Colonias Park from the State Parks Department in 1997. It encompasses the mill tailings clean-up site.





**1. Hawthorne Park – 400 Gunnison Avenue**

- New restroom/shelter constructed 2010
- South picnic shelter requires new roof; scheduled 2013
- Playground surfacing replaced in 2012
- Playground equipment on a 5 to 7 year replacement schedule
- Irrigation is good, 10-15 year replacement

**2. Washington Park – 10<sup>th</sup> Street and Gunnison Avenue**

- No restrooms or shelter at this park
- Playground equipment scheduled for replacement in 2013
- Playground surface scheduled for replacement every 2 years
- Irrigation will be replaced every 5 to 7 years

**3. Whitman Park – 5<sup>th</sup> Street and Pitkin Avenue**

- Restroom scheduled for replacement in 2013
- Development of a new master plan for the park recommended (see possible concepts below)

#### 4. Emerson Park – 9<sup>th</sup> Street and Pitkin Avenue

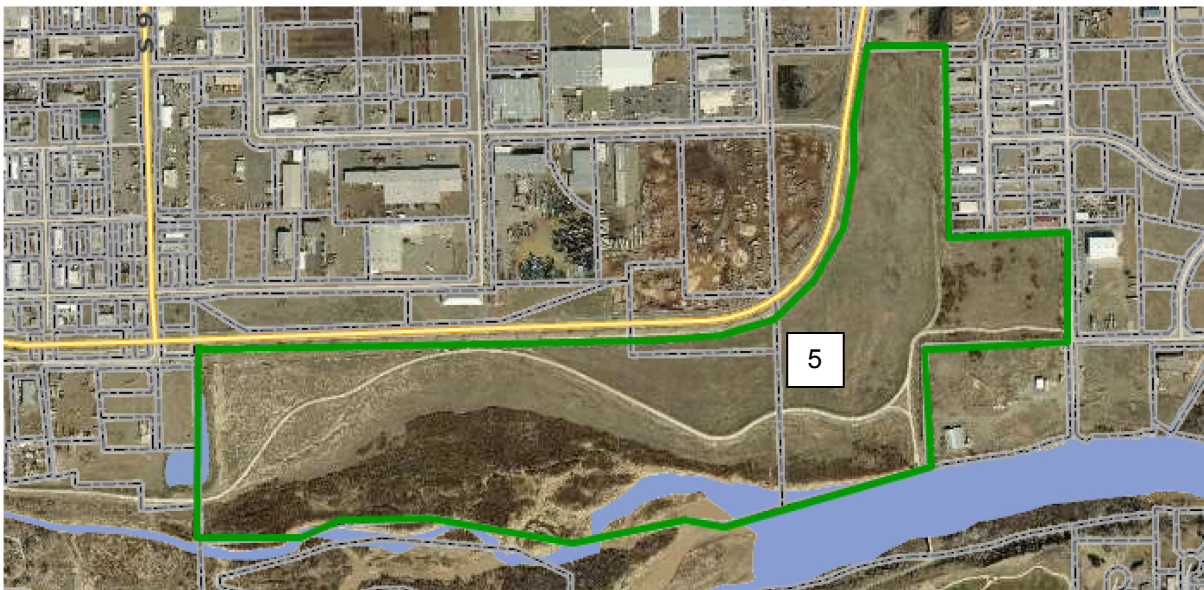
- Restroom scheduled for replacement in 2013
- Playground area for ages 2-5 is good
- Playground for ages 5-15 scheduled for replacement in 5 to 7 years
- Playground surfacing will be replaced in 2012
- Development of a new master plan for the park recommended (see possible concepts below)

#### Potential Concepts for Whitman and Emerson Parks

- Provide for a diversity of uses to create a higher level of utilization.
- Minimize the impact of adjacent streets and automobile traffic.
- Activate the edges of the parks with mixed use.
- Contemplate programmed, active use.
- Redevelop as more active, flexible urban open space rather than as passive, green parks.

#### 5. Las Colonias Park – Struthers Avenue and 7<sup>th</sup> Street

- A master planning process for the park will occur in 2012-2013
- Some amenities being considered are parking, restrooms, shelters, play area, open space, expanded walk/bike trails, open space for festivals, music, and educational presentations, various wetlands and arbor education opportunities, kayak park, tree walk and a disc golf course.



#### I. City-Owned Jarvis Property

The City completed an initial planning analysis for the City-owned property on the west side of 5<sup>th</sup> Street between the Colorado River and the Riverside Parkway known as the Jarvis Property. The property is constrained by natural features and the encroachment of the Parkway, but does have approximately 43 acres of developable land.

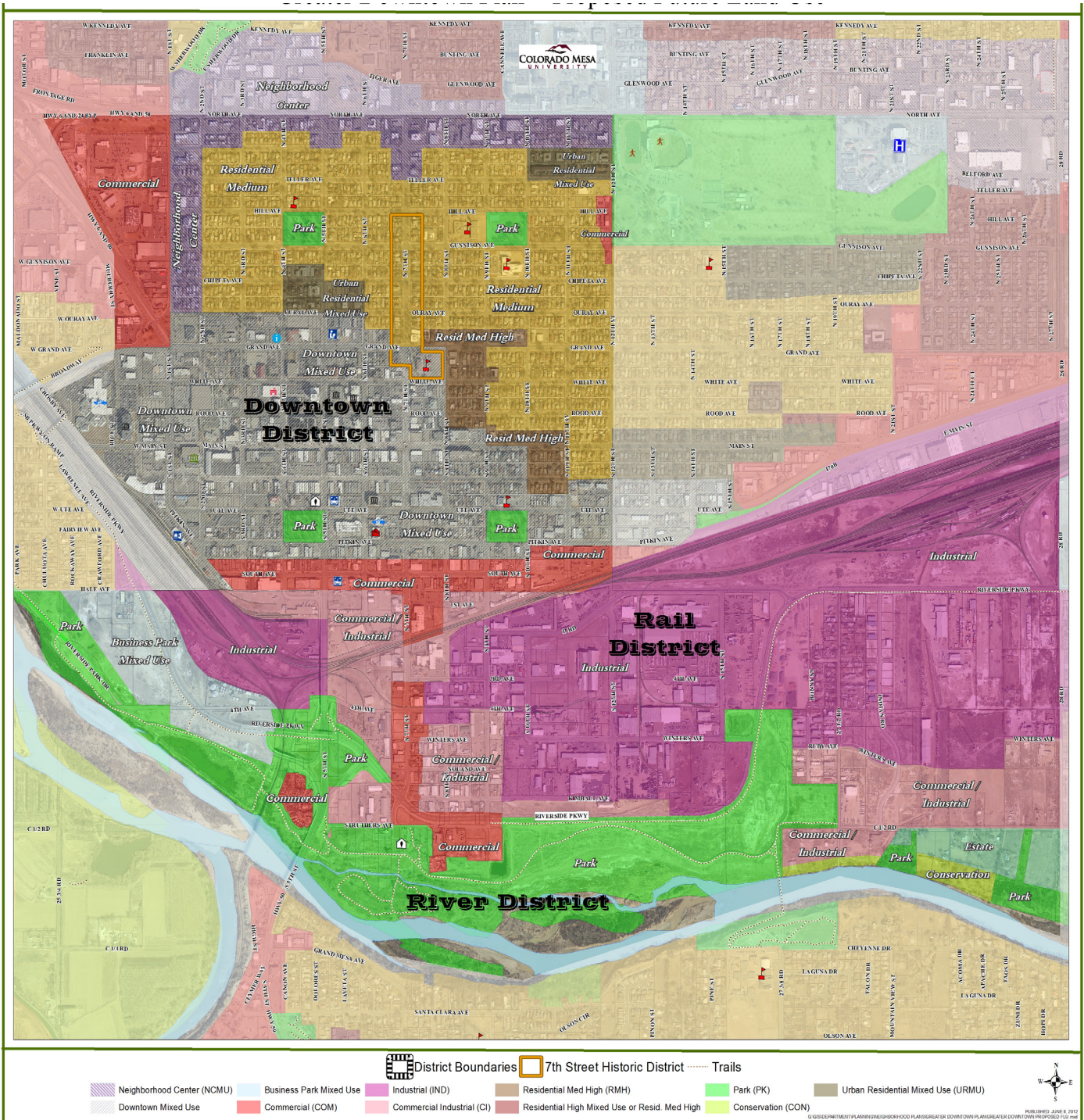
The initial study was to chart a direction for revitalization of the property. It summarized the key assets, identified some important issues and potential impediments to development, analyzed current market conditions and outlined a concept for organizing potential development of the property. The property and potential project are viewed as a unique opportunity for the City to chart the future of a rare property type, a place where it may be possible to provide a mix of uses, including residences, along the bank of the Colorado River. The major opportunities and constraints identified for potential development of the Jarvis Property were to: continue the riverfront greenway through the property, restore habitat, relocate the high voltage power facilities that run through the site, and provide flood protection for the property.

## **APPENDICES**

- A. Future Land Use Map
- B. Zoning Map
- C. Circulation and Trails Plans
- D. Development Concepts
- E. Summary of Public Process and Questionnaires/Comments

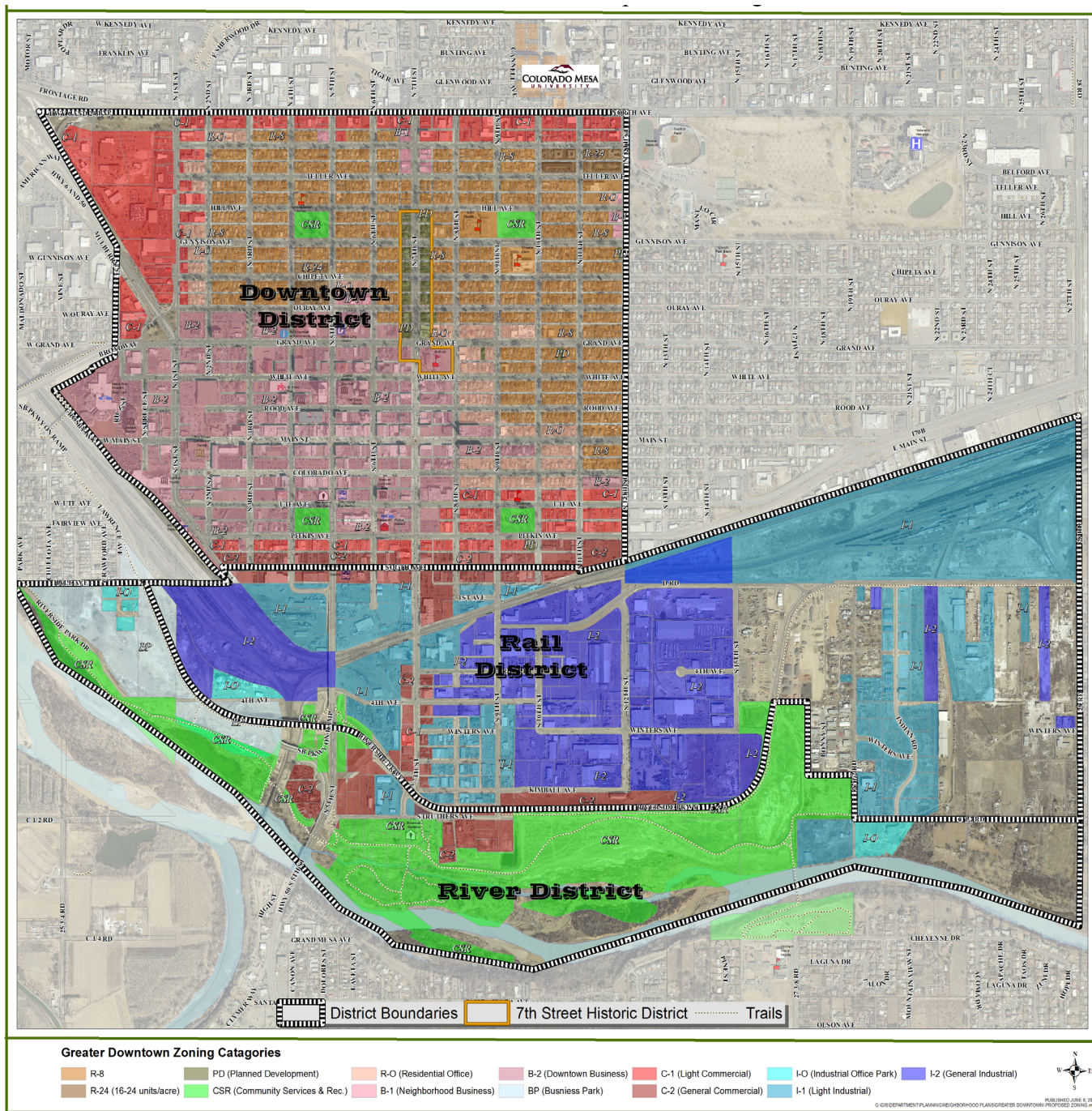


# Appendix A – Future Land Use Map



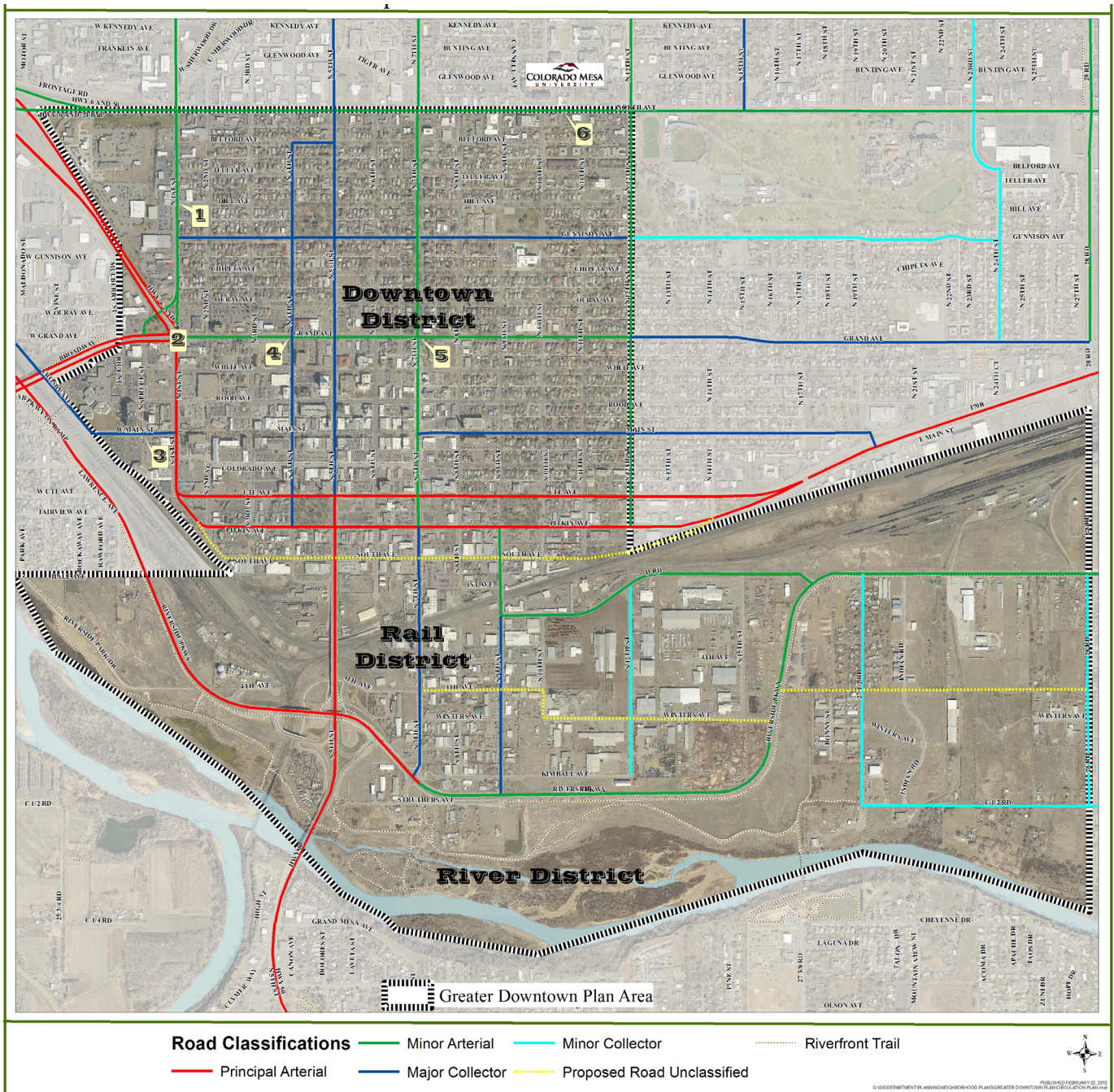


## Appendix B – Zoning Map





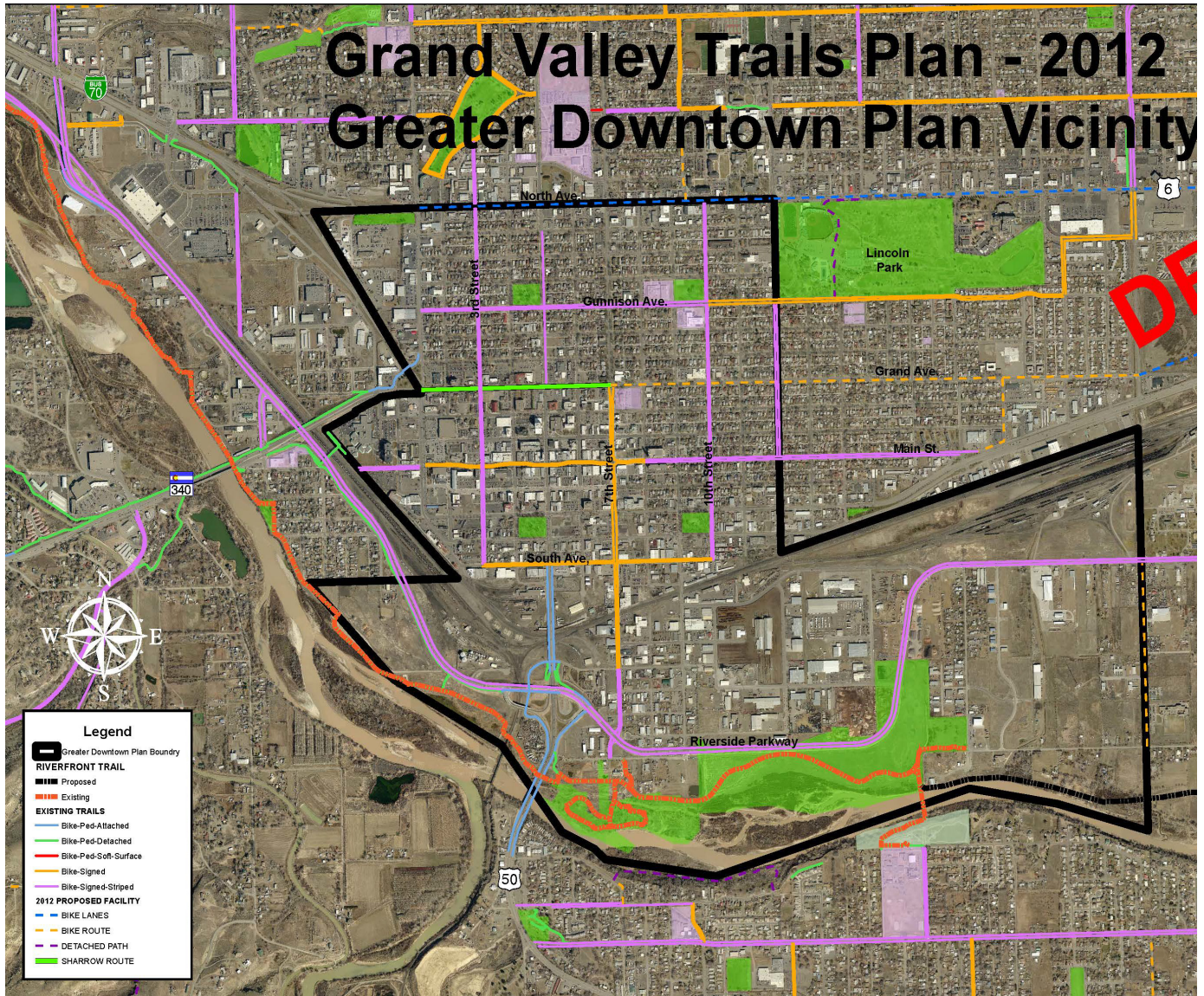
## Appendix C – Circulation and Trails Plans





# Grand Valley Trails Plan - 2012

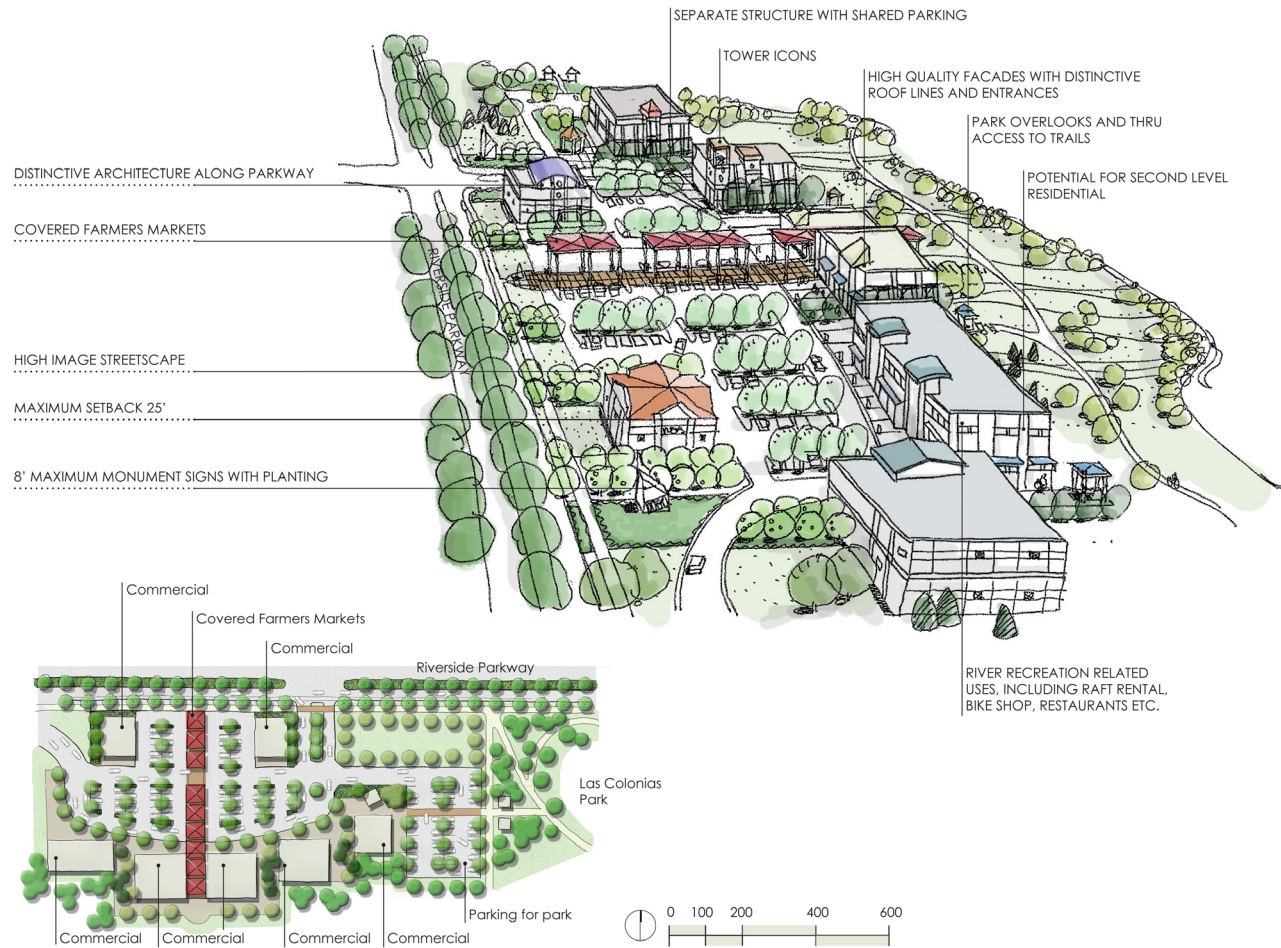
## Greater Downtown Plan Vicinity



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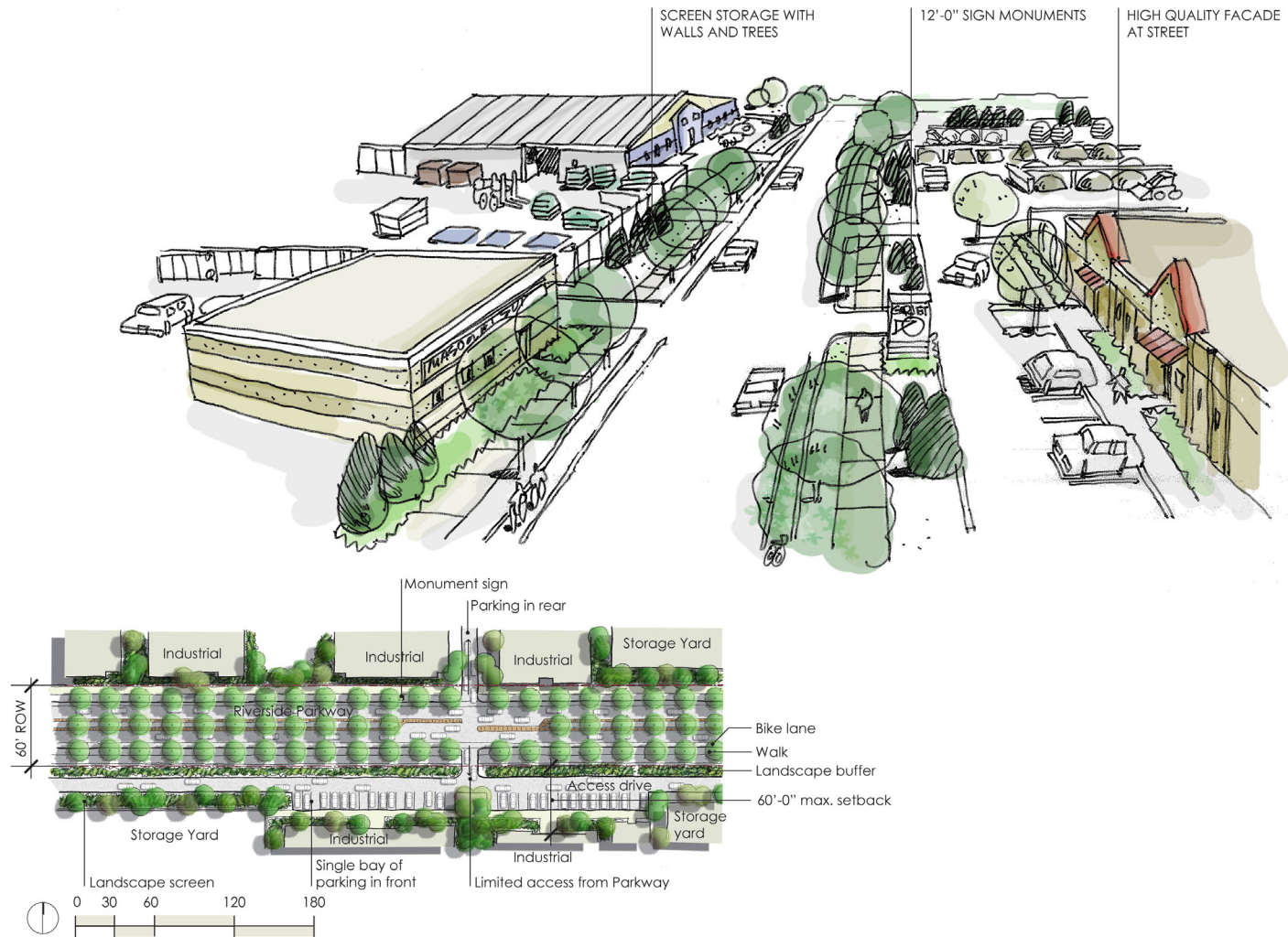
## Appendix D – Development Concepts



**7th and Struthers Redevelopment**

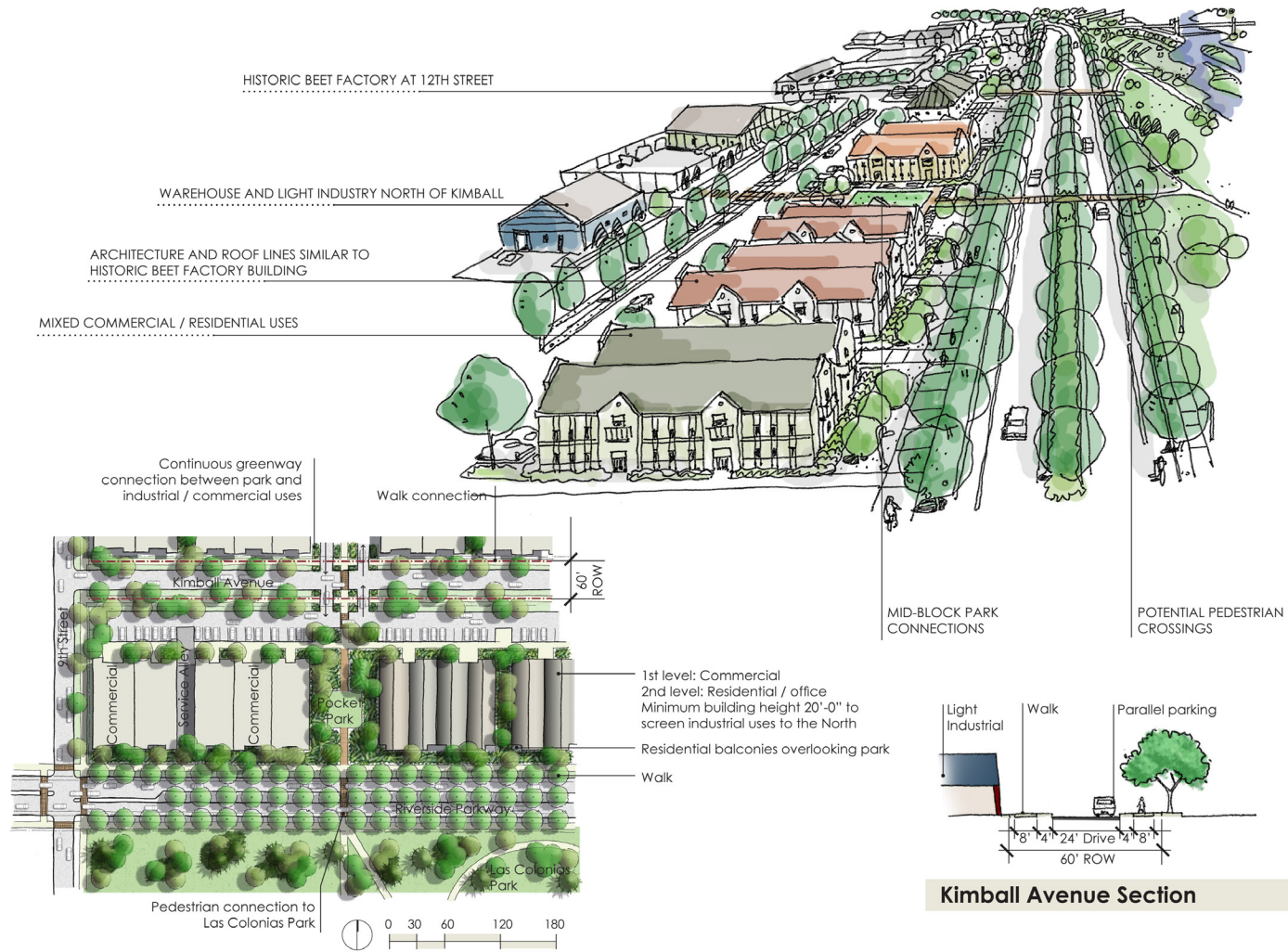
**SOUTH DOWNTOWN DESIGN GUIDELINES**

EDAW | AECOM



## Riverside Parkway Between 27 ½ and 28 Road

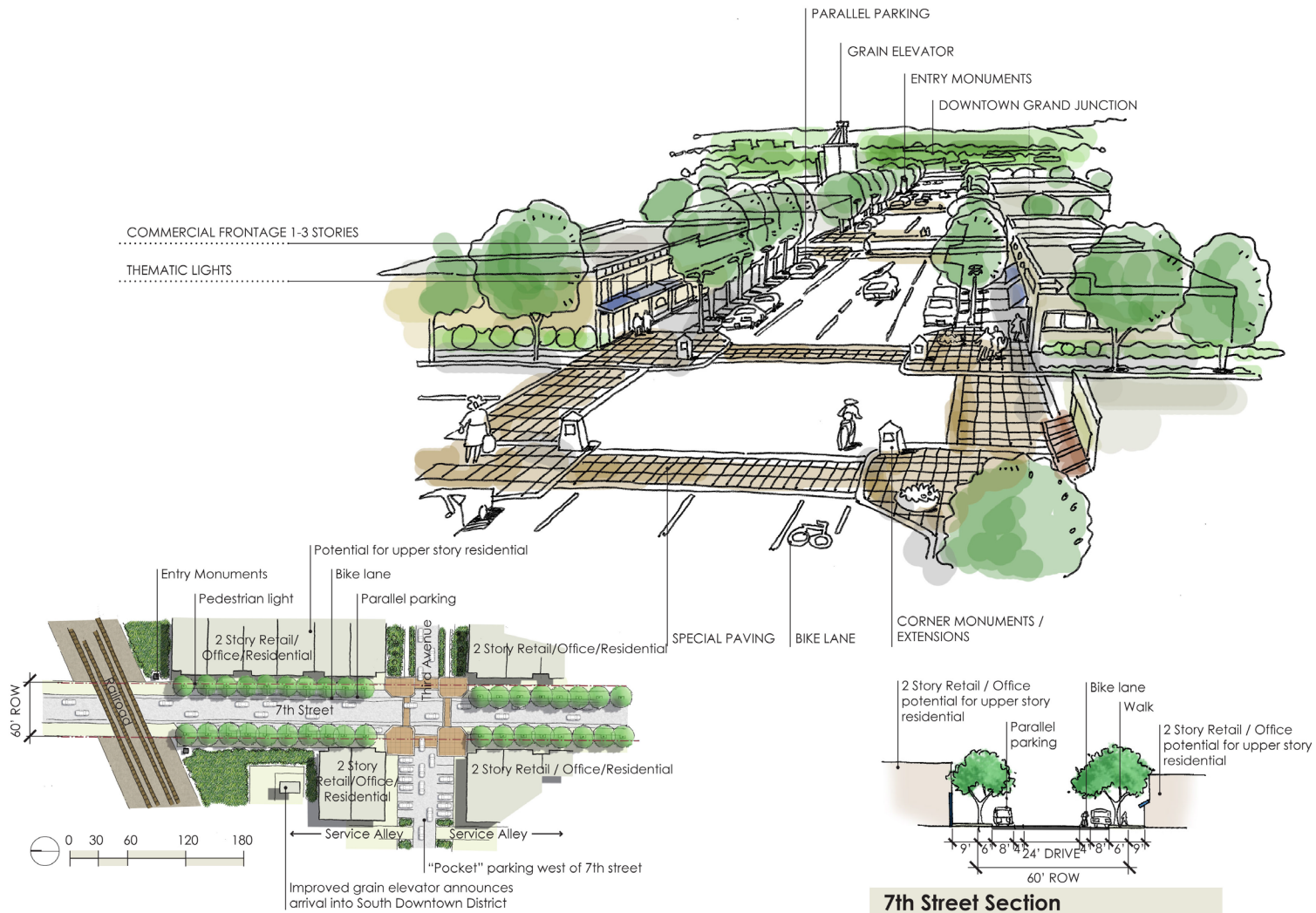
### SOUTH DOWNTOWN DESIGN GUIDELINES



## Riverside Parkway Between 5th Street East and 12th Street

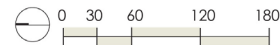
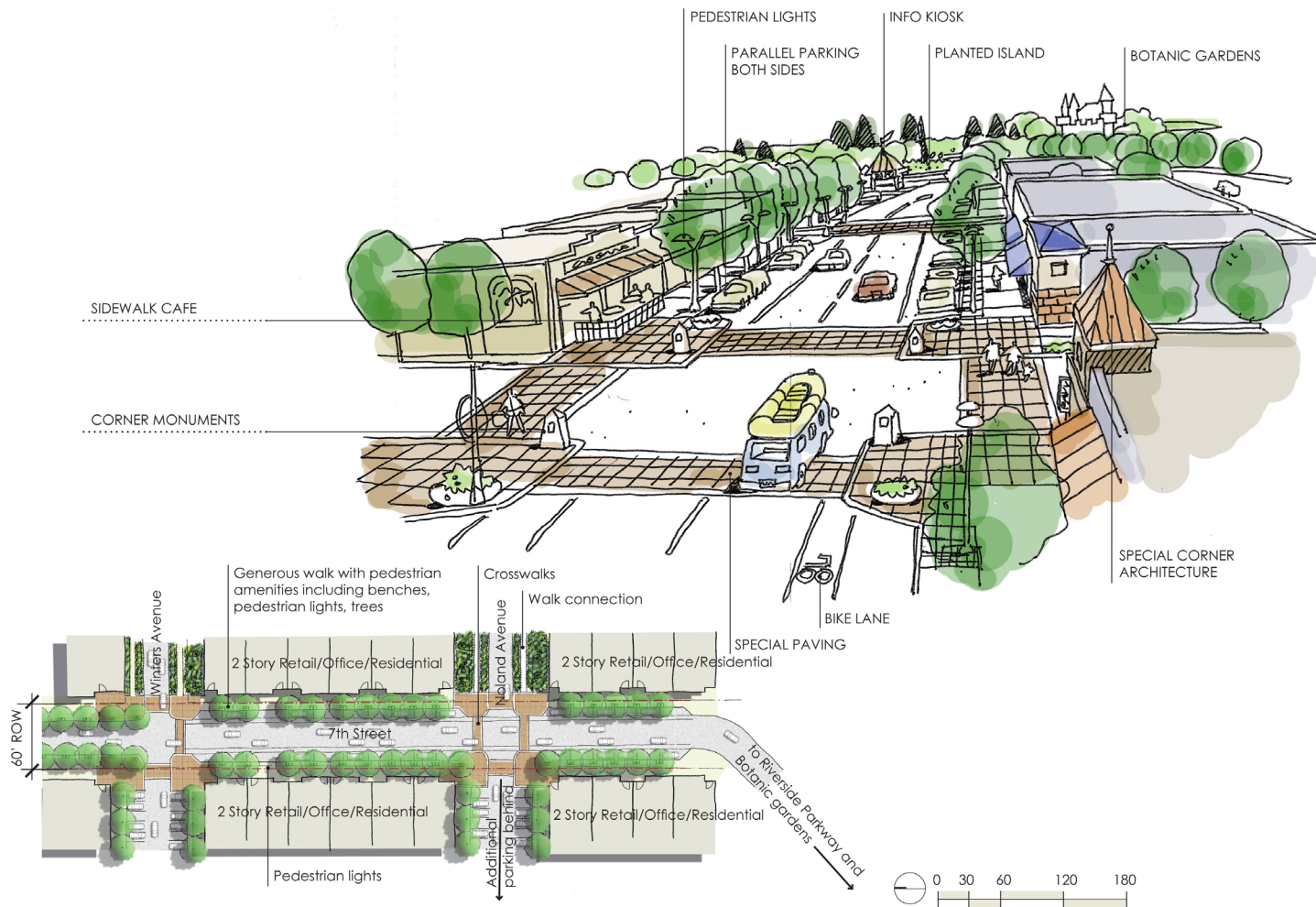
SOUTH DOWNTOWN DESIGN GUIDELINES





7th Street Looking North





## 7th Street Looking South

# SOUTH DOWNTOWN DESIGN GUIDELINES



## Existing Conditions

FRONT YARD PATIO

TREE LAWN

NO PARKING ON SOUTH SIDE

GARDEN GATES

NEW BUILDING  
OF COMPATIBLE  
STYLE AND SCALE

IRON FENCE

6'-0" GARDEN STRIP

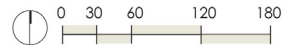
NEW SIDEWALK, CURB  
AND GUTTER

REHABILITATE EXISTING  
STRUCTURES



ON STREET PARKING  
NORTH SIDE

RAISED BRICK CROSSWALK  
WITH CORNER PLANTING



## Noland Avenue

## SOUTH DOWNTOWN DESIGN GUIDELINES

## **APPENDIX E: PUBLIC PARTICIPATION PROCESS**

### **TECHNICAL COMMITTEE**

The Greater Downtown Plan technical committee was comprised of staff members from various public agencies including City Public Works and Planning, City Parks and Recreation, City Geographic Information Systems, Mesa County Planning, the Regional Transportation Planning Office, Mesa County Facilities and Parks, the Downtown Development Authority and the Mesa County Public Library District. The Committee met three times during the course of developing the Greater Downtown Plan and members attended public open houses to discuss concerns and proposals with participants.

### **PUBLIC OPEN HOUSES**

Two public open houses were held in December 2011 and February 2012 to present concepts and solicit input from property owners and interested citizens. Notifications/invitations to both public open houses were mailed to all property owners within the Greater Downtown Plan area. Approximately 60 people attended the first open house and 40 attend the second open house.

### **QUESTIONNAIRES AND COMMENTS**

A series of questionnaires were available at the December 2011 open house and on the City's web site that were used to solicit public comment and weigh community opinions on design concepts that might be proposed with the Plan. 130 questionnaires were returned. In addition, citizens could provide other written comments at both open houses. The results of the questionnaires and the written comments are included on following pages.

### **LETTERS/MEETINGS WITH INDIVIDUAL PROPERTY OWNERS**

City Public Works and Planning staff coordinated meetings with key individual property owners, businesses or others that contacted the City regarding the Greater Downtown Plan. In addition, individual letters were mailed to property owners along the corridors that may be impacted by the land use and zoning proposals of the Greater Downtown Plan. Follow up meetings or conversations were held with property owners that responded to the letter. The meetings/conversations including the following individuals or businesses: Marie Ramstetter, Jim Golden, Woodstove Warehouse, 4NR Properties, Struth LLC, Peggy Cox, Kathy Ziola, John Crouch, the Redstone Group (Sugar Beet building), Butch Jarvis, VanGundy's, Castings, Inc., KelMac Industries (GJ Steel site), and Whitewater Sand and Gravel.

### **COMMUNITY PRESENTATIONS/DISCUSSIONS**

Public Works and Planning staff conducted several presentations and discussions regarding the Greater Downtown Plan with community groups and businesses including

the Chamber of Commerce, Bray and Company Realty, the Downtown Development Authority and Rail and River District corridor property owners.






**CITY COUNCIL, CITY PLANNING COMMISSION AND MESA COUNTY PLANNING COMMISSION WORKSHOPS**

City Public Works and Planning staff attended several workshops with elected and appointed City and County officials to inform and solicit input on the Greater Downtown Plan during its development.







## Central Business District – Questionnaire Results






1. Establish a cohesive character/theme that harmonizes new structures with the existing buildings through common materials, scale and architectural details

#	Answer		%
1	Strongly Disagree		7%
2	Disagree		7%
3	Neither Agree nor Disagree		15%
4	Agree		37%
5	Strongly Agree		33%
	Total		100%






2. Promote high density, vertically mixed use structures (e.g. retail at street level and residential or office above)

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		4%
3	Neither Agree nor Disagree		4%
4	Agree		37%
5	Strongly Agree		56%
	Total		100%




3. Building height restrictions in the CBD should be based on guidelines that address compatibility with surrounding uses.

#	Answer		%
1	Strongly Disagree		7%
2	Disagree		26%
3	Neither Agree nor Disagree		19%
4	Agree		30%
5	Strongly Agree		19%
	Total		100%





4. Taller buildings should be located in the center of the CBD, with lower buildings on the edges of the CBD.

#	Answer		%
1	Strongly Disagree		4%
2	Disagree		12%
3	Neither Agree nor Disagree		31%
4	Agree		31%
5	Strongly Agree		23%
	Total		100%





5. Preserve and restore significant historic structures

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		0%
3	Neither Agree nor Disagree		13%
4	Agree		30%
5	Strongly Agree		57%
	Total		100%




6. Promote infill development that is compatible with the existing downtown character.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		9%
3	Neither Agree nor Disagree		13%
4	Agree		43%
5	Strongly Agree		35%
	Total		100%




7. The streetscape will be dominated by buildings rather than surface parking lots.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		9%
3	Neither Agree nor Disagree		13%
4	Agree		35%
5	Strongly Agree		43%
	Total		100%



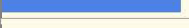
8. Shared parking and parking structures should be encouraged in the CBD to reduce the amount of surface parking lots.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		0%
3	Neither Agree nor Disagree		17%
4	Agree		39%
5	Strongly Agree		43%
	Total		100%

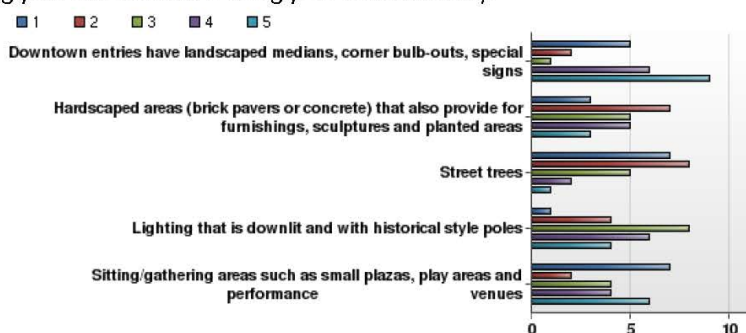
9. Explore new ways to pay for public parking.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		0%
3	Neither Agree nor Disagree		27%
4	Agree		36%
5	Strongly Agree		36%
	Total		100%

10. Provide streetscape details that complement the architectural character of downtown Grand Junction.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		0%
3	Neither Agree nor Disagree		9%
4	Agree		52%
5	Strongly Agree		39%
	Total		100%

11. The most important streetscape characteristics to me are (Please rank the options below in order of your most preferred with 1 being your favorite and 6 being your least favorite):



12. Please provide any additional comments you may have.

#### Text Response

#2 if you consider parking #4 Keep very tall buildings - like the Alpine Bank Building OUT of downtown.

#11 Xeroscape, dark sky!!, sitting/gathering along rail district as well

The focus within the CBD should be less on specific architectural styles and/or materials, but on those necessary elements to create a streetscape (location of entrances, location of parking, uses of the sidewalk, etc). The collection and range of architectural styles is what illustrates the evolution of a place and brings unique character to that place.

I think there should be a legitimate concrete Skatepark in the downtown park area as an anchor for destination based year round usage... feel free to contact me for any info or questions... (info@bin707.com)






The CBD has a core of historical structures on Main Street and Colorado, but the modern buildings on 7th and what the new Marriott adds is a varied and interesting streetscape with both modern and historic structures. This diversity of design allows for creativity, originality of builders and not a cookie cutter approach to the CBD dictated by code. That said, a 20 story medical center next to the transitional housing areas makes no sense so some type of restrictions should apply. The removal of trees on Main was necessary given the 50 year life cycle, but better planning needs to be done to assure we maintain a natural canopy of vegetation and the City also needs to do a better job of maintaining the fountains assuring they are functioning and serviced as needed. The one located across from the Avalon has malfunctioned much of the time since installation. Also winterization and maintenance of these wonderful outdoor water features must be planned operationally. Parking will always be an issue and the City needs to be more in touch with the reality of what they are approving as evidenced by the latest "VET" housing development done by Catholic Outreach which was totally lacking substantive evidence for the staff justification of parking. The result will be loss of parking for downtown businesses and there is no law that says the facility has to be used for VA occupants forever.

#### Downtown Area-Wide Concepts – Questionnaire Results





1. Examine the possibility of making 4th and 5th Streets both 2-way streets between Grand and North Avenues

#	Answer	%
1	Strongly Disagree	32%
2	Disagree	18%
3	Neither Agree nor Disagree	14%
4	Agree	18%
5	Strongly Agree	18%
	Total	100%









2. Establish a distinctive public sign palette for the original square mile to include street signs and directional signs that have recognizable poles, ornamentation, colors, fonts and logos.

#	Answer		%
1	Strongly Disagree		9%
2	Disagree		5%
3	Neither Agree nor Disagree		32%
4	Agree		27%
5	Strongly Agree		27%
	Total		100%





3. Emphasize “walkability” of Downtown through ongoing improvements to pedestrian crossings, bicycle facilities (e.g. bike lanes on streets, bike racks at strategic locations).

#	Answer		%
1	Strongly Disagree		4%
2	Disagree		0%
3	Neither Agree nor Disagree		4%
4	Agree		30%
5	Strongly Agree		61%
	Total		100%

4. Establish and improve gateways to the Downtown Area (please select your top 4 choices).

#	Answer		%
1	7th Street and North Avenue		37%
2	1st Street and Grand Avenue		84%
3	12th Street and Grand Avenue		16%
4	1st and Main Streets		84%
5	12th and Main Streets		21%
6	5th Street and South Avenue		68%
7	7th Street and Pitkin Avenue		58%
8	Other – please describe		32%

Other – please describe	
River front trail	
1st - NORTH	
4th & North	
14th & I-70B (Desert Vista Park)	
7th and Main	

2	Disagree		10%
3	Neither Agree nor Disagree		25%
4	Agree		40%
5	Strongly Agree		20%
	Total		100%



5. Please provide any additional comments you may have.

#### Text Response

Make the accessibility to the river and riverfront trails from downtown area more inviting.

#### #1 Why?

Make user-friendly ways to get to river trails

#4 improve how? They are all fine now Making 4th & 5th only partly 2-way would be a disaster. There would be auto accidents - people driving in the wrong lanes No more roundabouts!






In time pedestrian traffic will increase yet travel today in the next 20 years most travel will be via cars

#1 but make transition from 1-way to 2-way very clear with islands & other street features Main & Gunnison are ideal 7th from downtown to River is already bike-friendly ok






The directional signage beginning on Horizon Drive and leading to the Downtown along 7th Street already has a distinctive color scheme. However, this scheme was compromised at the intersection of 7th Street and Grand Avenue when the sign pole was painted black. If a color scheme is created, it should build on this work and must not be compromised.

### North Central Business District – Questionnaire Results






1. Establish a cohesive character/theme that harmonizes new structures with the existing buildings through common materials, scale and architectural details

#	Answer		%
1	Strongly Disagree		10%
2	Disagree		10%
3	Neither Agree nor Disagree		25%
4	Agree		45%
5	Strongly Agree		10%
	Total		100%


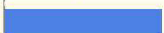


2. Promote vertically mixed use structures (e.g. retail or office at street level and residential or office above)

#	Answer		%
1	Strongly Disagree		5%
2	Disagree		5%
3	Neither Agree nor Disagree		10%
4	Agree		50%
5	Strongly Agree		30%
	Total		100%



3. The maximum building height of 65 feet shall only be allowed on parcels that front Grand Avenue. The buildings should “step down” so that the front of buildings that are directly across the street from residential buildings or uses are only 35 feet in height.

#	Answer		%
1	Strongly Disagree		5%
2	Disagree		10%
3	Neither Agree nor Disagree		25%
4	Agree		40%
5	Strongly Agree		20%
	Total		100%

8. Where available, some parking for non-residential uses may be on the street but only in front of the actual use, not in front of other adjacent uses.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		16%
3	Neither Agree nor Disagree		37%
4	Agree		32%
5	Strongly Agree		16%
	Total		100%

9. Should similar guidelines and standards be applied to the 1st Street Corridor from Grand Avenue to Belford Avenue?




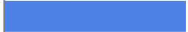

#	Answer		%
1	Yes		78%
2	No		22%
	Total		100%

10. Please provide any additional comments you may have.





Text Response
limit mixed-use intrusion into residential areas
#1 but not exactly alike #8 there's just no space on 4th St
#1 existing bldgs are crap #6 I don't notice anything about Grand west of 7th worth harmonizing with.
Keep it simple. Trees are such a gift. Keep it "big western town" rather than a city. Really, we have a choice western town here that offers relief from cities. May we manage to preserve & maintain that western "wide open spaces" air.
The one-block depth of this subarea creates a challenge. I agree that a maximum setback is necessary, both along Grand and 1st Street. However, this will result in the relocation of parking, loading, etc to the rear of the block, assuming the properties within the block are assembled. I believe that it would be a mistake to severely limit the height and/or building locations along Ouray and 2nd Street. The width of these streets and the streetscape itself would be better used as the transition. This would allow greater utilization of the properties and provide a better defined "edge" between the business and the residential area.

### Transitional Areas – Questionnaire Results






1. Front yards of transitional uses shall be reserved for landscaping, sidewalks and driveway access to parking areas and signage to maintain the residential character.

#	Answer		%
1	Strongly Disagree		5%
2	Disagree		11%
3	Neither Agree nor Disagree		16%
4	Agree		42%
5	Strongly Agree		26%
	Total		100%






2. Keep signs for the non-residential uses subordinate to the residential character.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		5%
3	Neither Agree nor Disagree		16%
4	Agree		58%
5	Strongly Agree		21%
	Total		100%





3. Regulate maximum building size in transitional corridors.

#	Answer		%
1	Strongly Disagree		11%
2	Disagree		21%
3	Neither Agree nor Disagree		16%
4	Agree		42%
5	Strongly Agree		11%
	Total		100%






4. Regulate hours of operation for transitional uses.

#	Answer		%
1	Strongly Disagree		11%
2	Disagree		33%
3	Neither Agree nor Disagree		28%
4	Agree		17%
5	Strongly Agree		11%
	Total		100%






5. Regulate building, site and signage lighting for transitional uses to minimize impact on adjacent residential core.

#	Answer		%
1	Strongly Disagree		5%
2	Disagree		0%
3	Neither Agree nor Disagree		5%
4	Agree		58%
5	Strongly Agree		32%
	Total		100%





6. Regulate parking and screening on non-residential sites to minimize impact on adjacent residential core.

#	Answer		%
1	Strongly Disagree		6%
2	Disagree		6%
3	Neither Agree nor Disagree		11%
4	Agree		39%
5	Strongly Agree		39%
	Total		100%






7. Transitional uses should not be allowed to have outdoor storage areas.

#	Answer		%
1	Strongly Disagree		5%
2	Disagree		16%
3	Neither Agree nor Disagree		37%
4	Agree		16%
5	Strongly Agree		26%
	Total		100%





8. Where available, some parking for non-residential uses may be on the street but only in front of the actual use, not in front of other adjacent uses.

#	Answer		%
1	Strongly Disagree		6%
2	Disagree		0%
3	Neither Agree nor Disagree		35%
4	Agree		41%
5	Strongly Agree		18%
	Total		100%

9. Reuse of residential structures and new construction in the transitional corridors shall retain residential character.

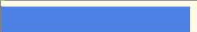






#	Answer		%
1	Strongly Disagree		6%
2	Disagree		12%
3	Neither Agree nor Disagree		6%
4	Agree		47%
5	Strongly Agree		29%
	Total		100%

10. Regulate the spacing of non-traditional residential uses (e.g., service organizations, group homes) so as to equitably disburse them throughout the downtown area.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		20%
3	Neither Agree nor Disagree		10%
4	Agree		50%
5	Strongly Agree		20%
	Total		100%



11. The most important residential characteristics to me are (check all that apply):

#	Answer		%
1	Maintain landscaped front yards (no parking in the front yard)		68%
2	Setback of building from street		32%
3	Small, low signage		53%
4	Maintain or construct building forms that are typical of residential architecture (e.g. 1-1/2 to 2 stories, sloping roofs, window pattern, porches)		53%
5	Use materials that are similar in color and texture as those in the residential neighborhood (e.g. roofing, siding)		37%
6	Minimize the visual impact of parking provided for the transitional uses		58%
7	Other – please describe		16%

Other – please describe

Cleanliness and general appearance. In general, the current businesses in this area look nicer than the residences.

blend old & new (modern) architecture thru committee approval/aesthetic guidelines ie: Bozeman, MT

Feel free to tear it up and start over

12. Please provide any additional comments you may have.

Text Response

I am a part owner of a business at 9th and Rood. My main concern is traffic control. There are regularly accidents at 9th and Main, we hear each one and watch them out our windows. In my opinion, having watched this intersection for over a decade, you could drastically reduce the number of these accidents by reducing or eliminating the on-street parking on Main street in the vicinity of this intersection. I'm convinced most of these accidents are caused by visual error. People travelling either way on 9th will stop, look each way, see traffic on Main, and PULL OUT ANYWAY because they think, in a quick glance, the vehicle they saw was parked on Main. In a quick glance, drivers sometimes cannot differentiate between a parked and a travelling vehicle. The City put blinking lights around the stop signs for a while, I see they're gone now, I suspect they statistically did not help reduce the number of accidents. Please try my idea. I'm sure the property owners will complain. But it could be you or your family that is in the next accident at 9th and Main.






Bozeman, MT!!!

#9 the transitional area has no redeeming qualities.





The Transitional Area needs to be redefined. It encompasses existing commercial uses between 7th and 12th Street, many with frontage on North Avenue, which are identified as part of a student commercial/entertainment district. It also encompasses the Ratekin and Qwest towers, both zoned Downtown Business, which should be included in the CBD.

## Residential Areas – Questionnaire Results




1. No large-scale redevelopment projects should be allowed within the downtown residential core.

#	Answer		%
1	Strongly Disagree		10%
2	Disagree		20%
3	Neither Agree nor Disagree		20%
4	Agree		20%
5	Strongly Agree		30%
	Total		100%




2. The downtown residential core should be preserved for residential uses only with no further encroachment of non-residential uses.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		24%
3	Neither Agree nor Disagree		29%
4	Agree		10%
5	Strongly Agree		38%
	Total		100%





3. Maintain the existing character of the house styles within the downtown residential core neighborhood – new construction or alteration must be compatible with key architectural characteristics and site elements of the neighborhood.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		15%
3	Neither Agree nor Disagree		0%
4	Agree		45%
5	Strongly Agree		40%
	Total		100%






4. Maintain and enhance the pattern of landscaped front yards that gives the downtown residential core neighborhood a distinctive, friendly appearance.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		0%
3	Neither Agree nor Disagree		14%
4	Agree		48%
5	Strongly Agree		38%
	Total		100%






5. Regulate the scale of accessory structures to maintain their character as subordinate to the primary residence.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		14%
3	Neither Agree nor Disagree		10%
4	Agree		43%
5	Strongly Agree		33%
	Total		100%





6. New construction of accessory structures may be allowed to be built at historic setbacks (e.g. there could be a zero foot setback from the alley and only 3 feet from neighboring property line).

#	Answer		%
1	Strongly Disagree		10%
2	Disagree		20%
3	Neither Agree nor Disagree		25%
4	Agree		35%
5	Strongly Agree		10%
	Total		100%





7. Where existing residential zoning allows, provide a diversity of housing types through development of multi-family housing that is in keeping with the character of the neighborhood.

#	Answer		%
1	Strongly Disagree		5%
2	Disagree		19%
3	Neither Agree nor Disagree		14%
4	Agree		29%
5	Strongly Agree		33%
	Total		100%




8. Discourage tearing down existing historic homes in order to construct new residential structures.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		10%
3	Neither Agree nor Disagree		19%
4	Agree		38%
5	Strongly Agree		33%
	Total		100%




9. Regulate the spacing of non-traditional residential uses (e.g. service organizations, group homes) so as to equitably disburse them throughout the downtown residential area.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		6%
3	Neither Agree nor Disagree		24%
4	Agree		59%
5	Strongly Agree		12%
	Total		100%







10. Enhance access to and improvements within existing public open spaces (e.g. parks and school grounds) within the downtown residential core.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		0%
3	Neither Agree nor Disagree		18%
4	Agree		47%
5	Strongly Agree		35%
	Total		100%

11. Maintain and enhance the historic character of the streetscape.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		0%
3	Neither Agree nor Disagree		6%
4	Agree		63%
5	Strongly Agree		31%
	Total		100%

12. The most important streetscape characteristics to me are (check all that apply):

#	Answer		%
1	Street Trees		94%
2	Landscaping rather than parking or other uses in park strips (area between sidewalk and curb)		71%
3	Distinctive street signs for downtown residential core		24%
4	Distinctive street lighting for downtown residential core		41%
5	Minimally maintained landscaping in front yards		6%
6	Detached sidewalks (space between the sidewalk and the curb)		47%
7	Other – please describe		0%
Other – please describe			





13. Please provide any additional comments you may have.

Text Response
<p>Allow for Bed &amp; Breakfast establishments to operate within original square mile, provided that signage, parking areas and lighting can blend in with neighboring properties.</p> <p>#8 depending on the existing quality of the building #11 evergreens!</p> <p>#2 small-scale mixed use would be ok #8 "historic" does not necessarily mean "good" The trees and lawns are pleasant, but GJ could afford to mount a significant effort toward xeriscape concepts. Honestly it seems criminal how folks in this town gush the Colorado River all over the place with little or no thought toward making a more rational truce with the desert.</p> <p>I'd love to see softer lighting, going upward. For good ideas, check out <a href="http://www.darksky.org">www.darksky.org</a>. Xcel says people need only a 60 watt bulb (or equivalent on houses). I recommend (&amp; long for) banishment of motion-sensor lighting... My plea is keep it simple. Preserve the "big western town" feel of this town. No more narrowing of streets. Please avoid turning into a city (like Boulder). THANKS!</p> <p>Prohibiting "non-residential" uses can create unintended outcomes. For example, should a church be allowed to expand and offer a coffee shop or other gathering place open to the public? This sort of amenity within the residential area is what adds value to the area. It is important to permit multi-family (re)development within the original square mile for two reasons. One, it has excellent infrastructure to service the population and proximity to services. Two, it is the only area within Grand Junction that has lost population in the 2010 Census (Tract 2 and 3), suggesting a loss of families from the area and/or an aging population of smaller households.</p>







## Corridor Overlays – Questionnaire Results






1. Create a mix of higher quality, new uses along South Avenue.

#	Answer		%
1	Strongly Disagree		9%
2	Disagree		0%
3	Neither Agree nor Disagree		18%
4	Agree		9%
5	Strongly Agree		64%
	Total		100%





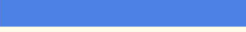
2. Create a more pedestrian-friendly commercial and service corridor along South Avenue/South 7th Street that complements the street improvements that have already been completed.

#	Answer		%
1	Strongly Disagree		9%
2	Disagree		0%
3	Neither Agree nor Disagree		18%
4	Agree		18%
5	Strongly Agree		55%
	Total		100%





3. Do not allow billboards on the South Avenue/South 7th Street corridor.

#	Answer		%
1	Strongly Disagree		5%
2	Disagree		5%
3	Neither Agree nor Disagree		27%
4	Agree		9%
5	Strongly Agree		55%
	Total		100%






4. Promote higher quality architecture and screening of outdoor uses for sites that face South Avenue and South 7th Street.

#	Answer		%
1	Strongly Disagree		10%
2	Disagree		5%
3	Neither Agree nor Disagree		14%
4	Agree		14%
5	Strongly Agree		57%
	Total		100%






5. Should similar guidelines and standards be applied to the 1st Street corridor from South Avenue to Grand Avenue?

#	Answer		%
1	Strongly Disagree		14%
2	Disagree		0%
3	Neither Agree nor Disagree		24%
4	Agree		14%
5	Strongly Agree		48%
	Total		100%






6. Limit expansion and construction of any new outdoor uses and operations (such as outdoor operation of rail yards, salvage yards, etc.) on properties that abut the South 5th Street right-of-way.

#	Answer		%
1	Strongly Disagree		14%
2	Disagree		14%
3	Neither Agree nor Disagree		14%
4	Agree		9%
5	Strongly Agree		50%
	Total		100%






7. Provide screening on the South 5th Street bridge that limits views to adjacent uses but still provides longer vistas to the east (Grand Mesa) and west (Colorado National Monument and Uncompahgre Plateau).

#	Answer		%
1	Strongly Disagree		23%
2	Disagree		5%
3	Neither Agree nor Disagree		23%
4	Agree		9%
5	Strongly Agree		41%
	Total		100%






8. Promote higher quality architectural features on building facades that directly face or can be seen from the 5th Street bridge on the east and west sides of the bridge.

#	Answer		%
1	Strongly Disagree		9%
2	Disagree		9%
3	Neither Agree nor Disagree		18%
4	Agree		18%
5	Strongly Agree		45%
	Total		100%





9. Do not allow billboards on the South 5th Street corridor.

#	Answer		%
1	Strongly Disagree		9%
2	Disagree		18%
3	Neither Agree nor Disagree		23%
4	Agree		5%
5	Strongly Agree		45%
	Total		100%






10. Create a mix of higher quality, new uses along South 7th Street and Riverside Parkway.

#	Answer		%
1	Strongly Disagree		9%
2	Disagree		5%
3	Neither Agree nor Disagree		27%
4	Agree		18%
5	Strongly Agree		41%
	Total		100%





11. Create a more pedestrian-friendly commercial core for area employees, residents and visitors.

#	Answer		%
1	Strongly Disagree		9%
2	Disagree		0%
3	Neither Agree nor Disagree		23%
4	Agree		14%
5	Strongly Agree		55%
	Total		100%






12. Integrate new commercial and mixed use with park, trail, recreation and open space uses.

#	Answer		%
1	Strongly Disagree		10%
2	Disagree		5%
3	Neither Agree nor Disagree		14%
4	Agree		19%
5	Strongly Agree		52%
	Total		100%






13. Do not allow billboards along the South 7th Street/Riverside Parkway business park mixed use corridor.

#	Answer		%
1	Strongly Disagree		10%
2	Disagree		0%
3	Neither Agree nor Disagree		19%
4	Agree		14%
5	Strongly Agree		57%
	Total		100%






14. Promote commercial and mixed uses along the north side of the Riverside Parkway that would transition to and screen industrial areas behind to the north.

#	Answer		%
1	Strongly Disagree		5%
2	Disagree		15%
3	Neither Agree nor Disagree		20%
4	Agree		30%
5	Strongly Agree		30%
	Total		100%






15. Use the remnant building of the historic sugar beet factory as a guide for scale, height and architectural elements for potential new development along Riverside Parkway.

#	Answer		%
1	Strongly Disagree		10%
2	Disagree		5%
3	Neither Agree nor Disagree		19%
4	Agree		19%
5	Strongly Agree		48%
	Total		100%

16. Promote higher quality architectural treatments for building facades that face the Riverside Parkway.






#	Answer		%
1	Strongly Disagree		14%
2	Disagree		5%
3	Neither Agree nor Disagree		5%
4	Agree		36%
5	Strongly Agree		41%
	Total		100%

17. Regulate screening of outdoor storage in the front yard area to minimize visual impact of these uses along the Riverside Parkway.






#	Answer		%
1	Strongly Disagree		14%
2	Disagree		9%
3	Neither Agree nor Disagree		5%
4	Agree		27%
5	Strongly Agree		45%
	Total		100%



18. Promote higher quality streetscape with minimal building setbacks, use of front yard space, signage, fencing and landscaping regulations.

#	Answer		%
1	Strongly Disagree		5%
2	Disagree		10%
3	Neither Agree nor Disagree		15%
4	Agree		40%
5	Strongly Agree		30%
	Total		100%

19. Do not allow billboards along the Riverside Parkway commercial/industrial corridor.

#	Answer		%
1	Strongly Disagree		5%
2	Disagree		5%
3	Neither Agree nor Disagree		33%
4	Agree		5%
5	Strongly Agree		52%
	Total		100%

20. Please provide any additional comments you may have.

#### Text Response

#### NO BILLBOARDS

I think your survey is biased - where do you say you want NO OVERLAYS

Encouraging progressive developers/landowners to make a few "1st moves" needs to be a kick-start for these standards. Once a few are on board this plan can be used as an incentive to get businesses to locate and upgrade in these districts. I envision that as the riverfront and other areas develop, GJ may be able to attract Denver businesses that cannot afford the downtown real estate they want there.

Looks good!

#7 waste of \$;#8 If you do this, no need for a screen; #9 I kinda like them; #10 Keep good reliable businesses in place; #12 if it will help our economy & Bring more businesses downtown; #14 Waste of \$. What is, is. I like seeing the rock yard. Keep it simple. Keep it real. Keep night lighting subtle, friendly, rather than glaring & blinding. Go for quality (which it sounds like you are doing). Yet please make it possible for "old-timers" to maintain their business without undue expense for dubious purpose. Keep our town Western! (that's my plea) open & friendly & not too fancy THANKS

I agree w/ all. The "strongly agree" s are of greatest importance. #15 that and lower

Mistake to not allow residential in rail district. ie: LODO in Denver, has the potential to be urban atmosphere that would attract young professionals to work & reside ie: living/working lofts. Densely populated "downtown" area can prevent urban sprawl, lessen carbon footprint and make this area a desirable place to live & work

1) This is a convoluted process. 2) Driving for answers you want 3) We need jobs, business & honor private property rights

With respect to the South Avenue corridor, is there still discussion about rerouting the I-70B corridor to Pitkin/South? If so, then that changes the type of character along that corridor, though pedestrian access should still be provided. South 5th Street (bridge) does serve as an entry point into the downtown; however, since it essentially "flies over" the existing industrial areas, it is not necessary to completely restrict those uses. The main focal point should be the end of the bridge and the forward vista into downtown, not what is underneath. The orientation of the Riverside Parkway paralleling Las Colonios Park presents something of a challenge for future commercial development, since direct access is only available at 7th St, 9th St, and Winters Avenue. These intersections should be the focus of the integration of commercial to open space concept. The areas along Kimball Avenue adjacent to the sugar beet factory are perhaps too far away from these nodes to be viable for mixed-use development

## Greater Downtown Plan Open House

### December 8, 2011

### Citizen Comments

- I don't want changes in zoning OR overlays – leave it alone for now – that's the best way to stimulate job growth.
- # one priority should be emphasis on maintaining and enhancing the character of the downtown residential and business to provide character, charm and livability within the core area. Increased bike lanes/racks, pedestrian crossings and a strong control of architectural features will serve to ensure an attractive downtown for years to come.
- Would love to see better use of Whitman Park connected to the Museum, Renaissance Fair, Farmer's Market etc. An East/West traffic corridor south of Ute would make the area more pedestrian-friendly to downtown visitors.
- Must have strict sign codes & restrictions; NO lighted signs in residential district; Same for R-O zones.
- R-O must provide parking for residents & employees, clientele during business hours. Parking is a big problem in R-O areas where renters & employees are not allowed to park during business hours and take parking from residents. Sometimes R-O residents tie up parking for days at a time leaving homeowners without parking and must carry groceries and other items from alley or further from home. Limit the amount of families living in existing homes. Some homes have multiple families living there (with numerous vehicles).
- NO parking of vehicles on parkway.
- Support for the Ute/South shift of one-ways.
- Encourage continued support for the "Arts" and Museum in downtown.
- Support for safety for bicyclists and pedestrians. WALKABILITY!
- Support for parks and active recreation in the area.
- Agree with – no billboards on South Ave/S 7<sup>th</sup> Street/S 5<sup>th</sup> Street corridors.
- Promote higher architecture and screening for site South Ave.
- Set minimum height requirements for downtown – Build Taller.
- I like the corridor concepts connecting downtown to the river. I live &\* work downtown, bike the river and only need a couple connections to get there, so 5<sup>th</sup> Street & 7<sup>th</sup> Street corridors bike-friendly are important to me personally; and I think, good for the area in general.
- With proposed rezone of block between 5<sup>th</sup> & 6<sup>th</sup>, Ouray & Chipeta – would a credit union or bank be a compatible use? – Eve Tallman 683-2424.
- I agree that building sizes need to blend as they approach the residential districts from Grand Ave.
- We need better ways to notify owners of the meetings. I didn't know of the Chipeta Elementary meeting.
- Clean tamarisk, Russian Olive (tents, dogs) between S 5<sup>th</sup> Street and Railroad Bridge.
- Improve bike, pedestrian route Main to River trail.
- Put soft path along river thru Los Colonias (something easy to replace if floods).
- Put pedestrian bridge over cut in Jarvis pond from soft path loop.
- Improve pedestrian, bike route W. Main to River (by Dual Immersion Academy school, etc).
- I would like to see more development along the river. Has city planning ever taken a "field trip" to Littleton to see what they have done with their river trail? I grew up there and saw it transform from trash to a treasure.
- Since this town is named after the confluence of 2 rivers, why not have a confluence park? Buy all the trashed area down at the confluence.
- River District – Concerned with Cities priority on present & future Riverfront Trail maintenance. Does not appear to be a priority. Also concerned with City "real" concern & commitment to the River District development for the Park & Future Trail to 29 Road.
- The industrial use of the 2 blocks south of Pitkin should remain due to the already constructed infrastructure i.e. – rail yards, holding tanks, etc. to re-set this area into another location would be very cost prohibited.

Regarding the workshop at City Hall 09.19.2011 regarding  
Zoning and Future Land Conflicts

1) South downtown area;

1) South Downtown, the OPTION 3 submitted by Kathy Portner was the plan she said matched current use and that she would propose. The City Council concurred. That the "triangle on 6th Street should be I-1, historical use of rail; take change North to South side of South Ave.; West side of 7th to accommodate current users. Council agreed with one dissenting vote.

There was talk of "conditional use" and "over-lays".

The Mayor's remarks were that both of these 'conditions' on zoning creates unpredictability. Both should be avoided whenever possible. Council concurred.

At the work shop, there was also concern that the South Downtown should be considered separate from the Downtown Plan. However, we note this has been put as one.

Meeting 12.08.2011 at Whitman Center

No property owners have been for a change of zoning in South Downtown. The Council and Planning Department discussed this at length at 9.19.2011 workshop and felt uses should be kept for current users and zoning should remain the same.

- 1) Why are the same meetings necessary – over and over?
- 2) Why are minutes not kept of previous meetings?

Please consider keeping the zoning the same for the South Downtown so businesses have predictability and continue operating – instead of wondering what the City is going to do to them.

Thank you.

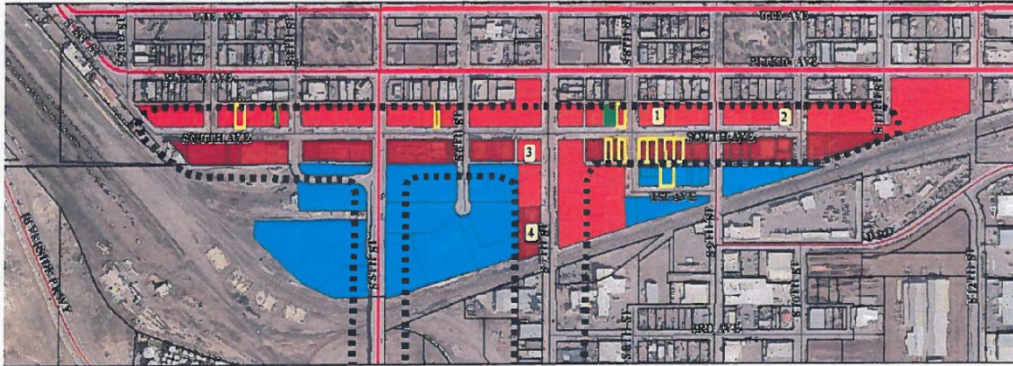


Lois Dunn  
PO box 1889  
Grand Junction CO 81502  
970-243-8843  
loisgdunn@gmail.com

Attached: Proposal 3 as presented at the 9.19.2011 workshop



## OPTION 3: Omitted Area South Ave to RR Tracks Potential Zoning



### Potential Zoning

- C-1 (Light Commercial)
- C-2 (General Commercial)
- I-1 (Light Industrial)

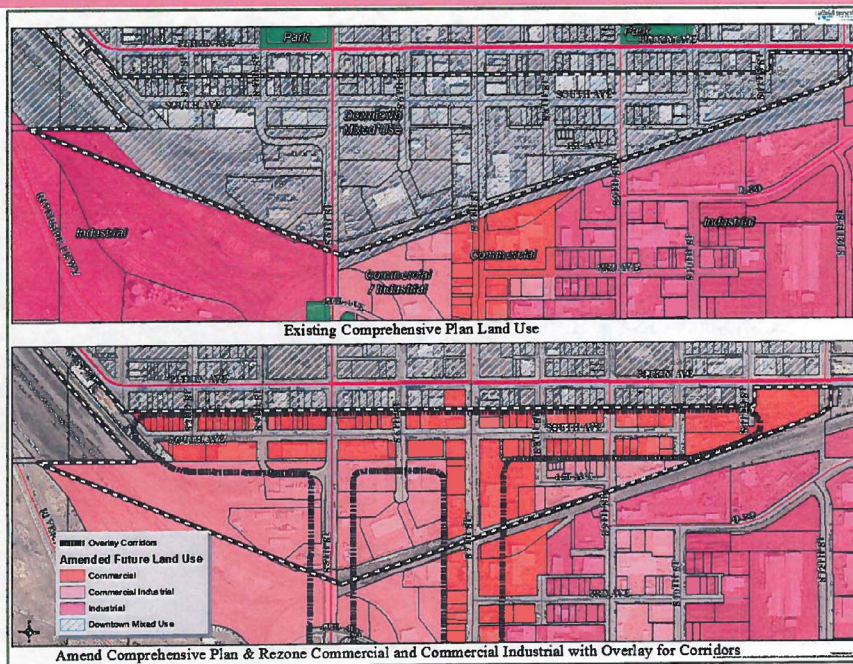
### Non-Conforming Use in Potential Zone

- 1 Warehouse - conforms in C-2, not C-1
- 2 Private Gas Pumps - conforms in C-2, not C-1
- 3 Flea Market - conforms in C-1, no outdoor operation
- 4 Dible Oil - conforms in C-2, not C-1

### Overlay Corridors

- Non-Conform Use Only with Existing Zoning
- Non-Conforming Use with Existing & Potential Zoning

## OPTION 3: Omitted Area South Ave to RR Tracks Extend Concepts from South Downtown Plan





## Greater Downtown Plan Open House

### February 23, 2012

### Citizen Comments

#### DOWNTOWN OVERLAYS

- Transitional Areas – Need parking guidelines for business & residential uses. What about parking guidelines for rentals? Currently property owners do not have any requirements to have renters park on or in front of units. What about off street parking requirements? We need them.
- Transitional Area – Text seems to primarily be geared to business discussion. We need guidelines & standards to be inclusive of the residential aspects!! All readers need to understand we are an inclusive neighborhood comprised of residential and business interests. Current zoning states “Residential/Office”.
- 1<sup>st</sup> Street from Main St south to the depot – slow traffic/Parkway/landscaped median, shift of Ute/Pitkin to Pitkin/South.
- On street parking should be encouraged to slow traffic and act as a buffer on collector streets (e.g. Grand Ave) to buffer traffic from residential yards. Parked cars area a safety buffer for children playing in yards.
- No uses earlier than 7:30 am or after 8 pm. The Yoga Collective would immediately be in violation of this on 10<sup>th</sup> & Main.

#### PROPOSED LAND USE MAP

- It doesn't seem to make sense putting a Commercial/Industrial area on the river, breaking up the continuity of the park areas.
- Train Depot – Why not encourage Amtrak, GVT & Greyhound Bus Co. to make a true intermodal center @ 1<sup>st</sup>/2<sup>nd</sup> and Pitkin? Then encourage higher density housing and mixed use for this area.
- Isn't the Neighborhood Center supposed to serve clients who “walk” from the immediate neighborhood? On 1<sup>st</sup> Street there are barely sidewalks to serve the NC. Isn't this commercial area being used by the “Driving” community more than the walking neighbors? Do you expect this to change?

#### PROPOSED ZONING MAP

- The infamous Brady property will be surrounded by parks on 3 sides according to the FLU. How does the I-1 zoning make any sense? Why not get Brady to trade for some vacant land to it's north and east.
- Rail industrial zone inhibits residential uses & remodels – should be mixed use zone. This area is full of houses – not just industrial uses.
- County zoning missing from maps; any changes to County zoning considered?
- Brady Trucking need to be moved to the vacant industrial land to the east of where they are now and the land by the River needs to be part of the Park.
- Winters/Kimball – 7<sup>th</sup>/9<sup>th</sup> – There area residences there that should be retained for that use! Why can't we have residential blended use in ANY work areas? It makes not sense to segregate them and force their use change when we supposedly are trying to create a walkable city.
- Why is the Industrial use STILL specified by the Riverfront Trail? OM elevation looks right down on that area. Retain our Riverfront for a beautiful recreational feature, please!!!
- BP – doesn't allow museums; concerned that some retail sales are not allowed in the BP. Compare BP to C-2 uses.
- S 7<sup>th</sup> Street – look at potential of leaving the C-2 zoning and utilize the overlay zone for design standards.
- Would like to do mixed use, but business – residence requires owner or employee live there.
- Suggest incentives for new uses or upgrades we'd like to see – 1111 S 7<sup>th</sup> Street.
- Attached letter from John Crouch.
- Attached letter from Margaret Cox.

## **CIRCULATION/TRAILS/BUS**

- NO MORE ROUNDABOUTS. THEY DO NOT AID IN TRAFFIC FLOW! Courtesy is not practiced, yield signs are not followed. They increase traffic congestion!
- The homes that area between 7<sup>th</sup> & 9<sup>th</sup> and Winters to Noland should be preserved as residential. This area needs residential if you want such things as a brew pub & other business to make this area a beautiful thriving area – Not a home for the homeless – Thank you, Kathy Jordan.
- 1<sup>ST</sup> & Grand – Uh.... Roundabout! It's a perfect intersection; DCOT & the City already own the land.
- Main Street east of 8<sup>th</sup> Street is too wide. Install some central medians or something to slow traffic, increase interest, create more neighborhood identity.
- 7<sup>th</sup> Street needs a sign that keeps large 53 feet long trucks off of it between Ute and Grand.
- 7<sup>th</sup> and Grand roundabout may cause vehicle/pedestrian conflict. Have walk signs/lights now. Traffic exiting roundabout does not expect or look for pedestrian traffic. These conflicts exist at 7<sup>th</sup> and Main roundabout.
- #2 – Spruce St @ Grand Ave – needs to be a right-in/right-out only – unclear if it is planned that way.
- #3 – Spruce & Main improvements look great - & should be a priority with new and increased traffic on South Spruce – Mesa County Central Services. In short term remove diagonal parking spaces closest to the intersection – poor sight distance now.
- Will a pedestrian connection from Ute Ave to Pitkin Ave be built where 6<sup>th</sup> St is now closed for the Fire & Police Facilities?
- More marked bike lanes would improve safe traffic flow.

## **CORRIDOR OVERLAYS**

- 1101 Kimball owner Bryan Wiman – We support the corridor overly that affects our property “Sugar Beet Factory”. We understand that the existing use is not affected and that is important to us. We also respect the significant beauty of the Colonias Park area and we support re-development if market demands.
- Indian Road plans contradict this?? Follow up on – 396/398 & 397/399 Indian Road
- Moving Pitkin/Ute one way traffic to Pitkin/South should not occur. The Grand Valley Transit facility would be very negatively impacted – both pedestrian users and bus ingress-egress from such a drastic increase in traffic. – John Heidernan.
- A traffic light at S 5<sup>th</sup> and South Ave would back up north bound traffic significantly worse on the overpass. The one at Pitkin already does. Moving it one block south – not a good idea. - John Heidernan.
- The proposal to move Pitkin & Ute one way traffic to Pitkin & South is of concern to us. That would essentially put our 2 businesses, Enterprise & All Pets Center in the median strip of I-70B. Our access is already a huge problem for our clients and this would make it worse.

## **PARKS**

- Brady trucking should expand to the east NOT on the riverfront.

## **GENERAL**

- Please, no more parking meters. – Rob Rubin

City of Grand Junction  
City Neighborhood Services  
Attn Kristen Ashbeck, Senior Planner  
250 North 5<sup>th</sup> Street  
Grand Junction, CO 81501

05/15/2012

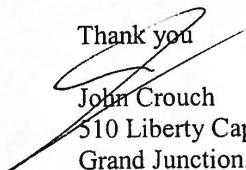
Dear Kristen:

Thank you for speaking with me regarding the proposed rezone from General Commercial (C-2) to Light Commercial (C-1).

The area of concern is commercial and best suited for the highest zoning available.. The railroad, junk yards, metal processors etc. and decades of investment in the area below Pitkin is not conducive to spotty zoning. Keep it all the highest zoning available.

To do what is suggested will force businesses that need higher zoning to move to Fruita or to the County.

Thank you



John Crouch  
510 Liberty Cap Court  
Grand Junction, Co 81507

Rec'd 4/25/12

Margaret Cox  
P.O. Box 2422  
Grand Junction, CO 81502

Grand Junction City Council  
City Hall  
250 N. 5<sup>th</sup> St.  
Grand Junction, CO 81501  
April 24, 2012

**RE: Re- Zone and Over-Lay Plan for South Downtown Grand Junction**

Dear City Council members,

I have recently been made aware of proposed changes to the zoning of south downtown that would negatively affect my commercial property at 805 Struthers Avenue. This 2 acre parcel is presently zoned C-2 and borders Riverside Parkway between 8<sup>th</sup> and 9<sup>th</sup> street on the south side. It is adjacent to city land that will someday become Los Colonias Park. Long range city plans include possible commercial development of privately owned land in my specific area. I am not opposed to the gradual development of the area for recreation, retail and even some residential uses. I have reviewed the proposals over the years with some interest. The only consistent conclusion I draw of these proposals is that they are projected endlessly into the future. There is never a start date.

The Planning Department has not been able to give any specific time-line on the creation of the park, saying it will take way more funding than is now in the budget. Re-zoning of that area should not take place until much of the park is completed and can demonstrate a draw for both the public and investors.

One compromise could be to change the zoning to C-1 only for individual parcels as requested by their owners. I would have no objection to that change, but would not want 805 Struthers rezoned to C-1 at this time.

My property is a non-conforming site with non-conforming use since the zoning changed from Industrial to C-2. In years past I have sacrificed the flexibility of industrial usage. Should I decide to change my use or expand my facilities the C-2 designation already requires a major investment to meet city building codes, screening, landscaping and parking, etc. If the zoning were to change to C-1 I would no longer be allowed to use the land for storage and other outdoor uses if I changed my activities or have a building expansion over 20 percent, or if a potential buyer envisioned another use outside the C-1 zoning specifications. This limits my property's potential. It devalues my property. Quibbling about which zoning is more valuable is not productive at this point. I value my land based on the revenue it generates and its potential to provide me an income far into the future. This is based on the flexibility zoning allows.



Although the argument may someday be made that the potential use will be more valuable as a C-1 property, I see no evidence at all of this being so at this time. There is almost no retail anywhere in the area. There is no city park as a destination for the public. Efforts at upgrading the river corridor have been minimal, with the exception of the parkway itself and are not visible to the traveler on that parkway. There is no entrance to the park, no parking lot or access to the river or the pedestrian bridge to the east from the Parkway. No business has invested in a C-1 business on Struthers or anywhere near. I feel this is not the time to make changes to zoning that effect the possibilities for land owners to find tenants, have a variety of commercial functions, or ultimately to sell the real estate.

One exception, in particular, to the "grandfathering" of my outdoor usage I find unfair and would like addressed. If a property is vacant for more than one year the zoning requirements will kick in and protection as a "non-conforming" site will be lost. This does not accommodate the economic downturn that both Grand Junction and our national economy have experienced. Coming years could even be more challenging. This would represent a "taking" of sorts to force a revision of that non-conforming status. Commercial properties often times cannot be occupied consistently and, unfortunately, can remain vacant for well over twelve months. Owners already have taxes and other related expenses and should not have to lose their effective zoning status because of the misfortune of a vacancy. I would ask that this provision be removed from any Over-Lay or Re-Zoning considerations.

Please leave my property at 805 Struthers Avenue at the current zoning of C-2.  
Thank you for your consideration.

Sincerely,



Margaret Cox

970-778-9000

>>> Marie Ramstetter <ramstet@gmail.com> 3/14/2012 3:46 PM >>>  
I am still opposed to the down zone

On Wed, Mar 14, 2012 at 12:23 PM, Kristen Ashbeck <[kristena@ci.grandjct.co.us](mailto:kristena@ci.grandjct.co.us)>  
wrote:

Hi Marie,

Thank you for your comment regarding the Greater Downtown Plan. We understand your concerns and would like to provide you with additional information concerning the proposed zone change of your property from C-2 to C-1. If you review the attached information, it outlines the differences between the two zones. You will see that there is not a great difference between the uses that you might consider for your property, especially since the building already exists and there is very little room on the site for expansion or for outdoor uses. The zone change certainly would not impact a current use of the building or the building itself.

If after reviewing the information (or if you are unable to open the attachment), you have further questions or comments, please do not hesitate to contact me.

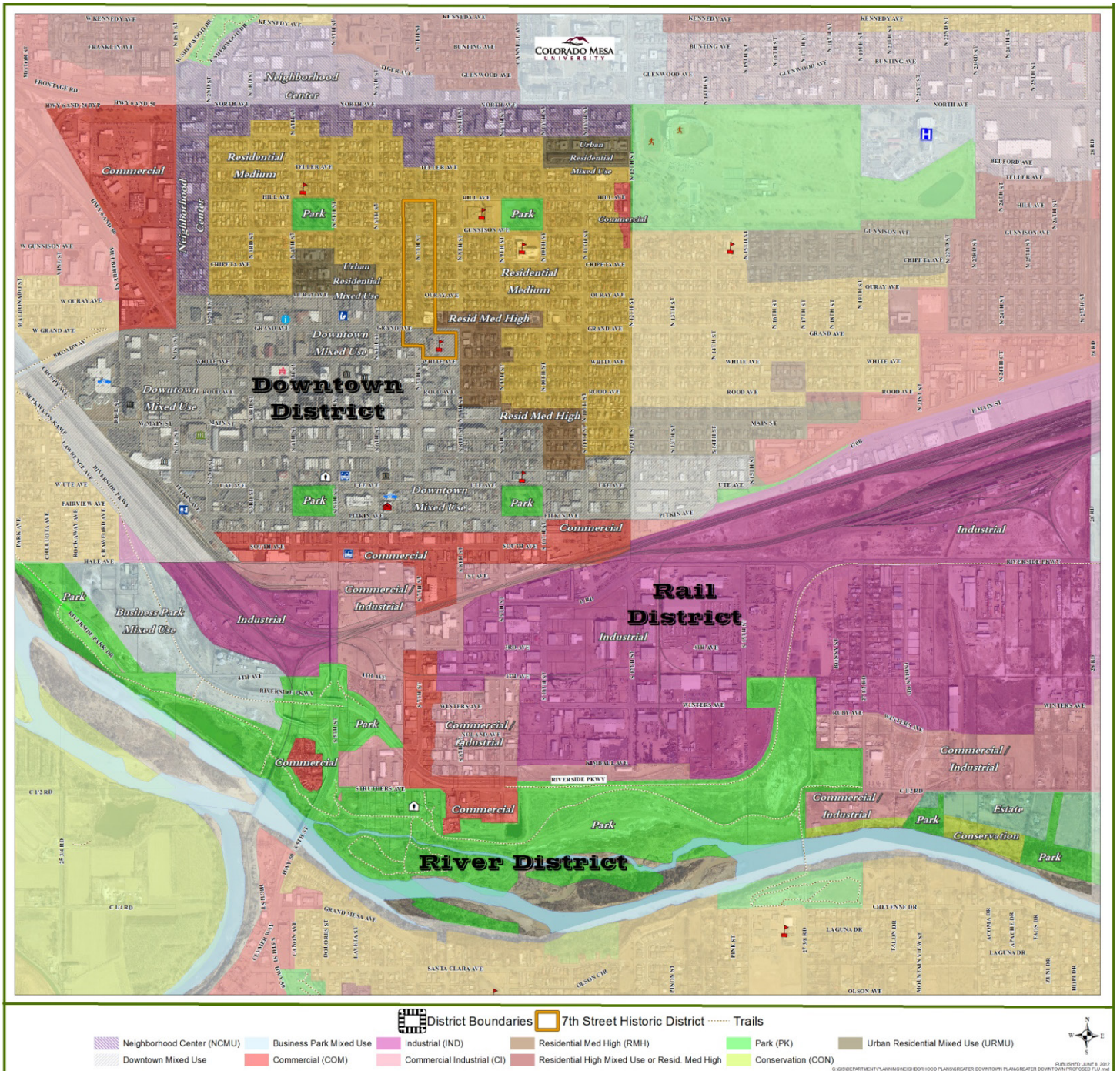
Kristen Ashbeck  
Neighborhood Services / CDBG  
[970.244.1491](tel:970.244.1491)  
[kristena@gjcity.org](mailto:kristena@gjcity.org)  
[970.256.4114](tel:970.256.4114) fax  
City of Grand Junction  
250 North 5th Street  
Grand Junction CO 81501

>>> Marie Ramstetter <[ramstet@gmail.com](mailto:ramstet@gmail.com)> 3/13/2012 2:19 PM >>>

I am absolutely opposed to your attempt at down zoning my property, tax id 2945-231-00-008 located at 803 S 7th Street. Consider this a formal protest to the City.



## EXHIBIT B – FUTURE LAND USE MAP



## ATTACHMENT 2

### CITY OF GRAND JUNCTION, COLORADO ORDINANCE NO. \_\_\_\_\_

#### **AN ORDINANCE AMENDING THE ZONING AND DEVELOPMENT CODE TO AMEND SECTION 21.07 TO ADD THE GREATER DOWNTOWN PLAN ZONING OVERLAY AND AMEND SECTION 21.03.020(d), COMPREHENSIVE PLAN LAND USE DESIGNATION**

##### RECITALS.

One of the recommendations of the Greater Downtown Plan was to create standards and guidelines as a first step for implementing the Plan. The Plan recommends adoption of these standards and guidelines as a zoning overlay to apply to portions of the Greater Downtown area as specified in the Plan.

Overlay zoning is one way to create a more flexible and discretionary alternative to traditional zoning. An overlay zone is defined as “an overlay district superimposed on one or more established zoning districts which may be used to impose supplemental regulations on development in these districts, permit uses otherwise disallowed, or implement other forms of incentives”.

An overlay zone supplements the underlying zone with additional standards, guidelines and/or incentives while generally leaving the underlying zoning regulations in place. Examples might include different setbacks, increased height allowance or varied allowed uses. A parcel within the overlay zone area will thus be simultaneously subject to two sets of zoning regulations: the underlying and the overlay zoning standards and guidelines.

Overlay zone boundaries are not restricted by the underlying zoning district's boundaries. An overlay zone may or may not encompass the entire underlying zoning district. Likewise, an overlay zone can cover more than one zoning district, or even portions of several underlying zone districts.

The Greater Downtown Plan Zoning Overlay (Exhibit A) is being proposed to cover the Central Business District (CBD), Residential and Transitional subareas of the Downtown District as well as those properties that have frontage on defined street corridors including South 5<sup>th</sup> and 7<sup>th</sup> Streets, Pitkin and South Avenues and the Riverside Parkway.

The Grand Junction Planning Commission is charged with the legal duty to prepare and consider and recommend action to City Council regarding amendments to the Zoning and Development Code for the City.



The Greater Downtown Plan Zoning Overlay was heard in a public hearing by the Grand Junction Planning Commission on July 24, 2012 where the Planning Commission recommended that the City Council adopt the Zoning Overlay.

**NOW THEREFORE, BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF GRAND JUNCTION:**

The Zoning and Development Code is hereby amended to amend Section 21.07 to add a section entitled "Greater Downtown Plan Zoning Overlay" to be applied to the areas described in the Greater Downtown Plan Zoning Overlay (Exhibit A) and generally described above.

The Zoning and Development Code is hereby amended to amend Section 21.03.020(d), Comprehensive Plan Land Use Designation to indicate that the R-O zone district may be used to implement the Downtown Mixed Use (DT-MU) Land Use Designation.

That the Greater Downtown Plan Zoning Overlay, City of Grand Junction, Colorado, in the form of the document attached hereto, and as recommended for adoption by the Grand Junction Planning Commission is hereby adopted.

The full text of the Ordinance, including the text of the Greater Downtown Plan Zoning Overlay, in accordance with paragraph 51 of the Charter of the City of Grand Junction, shall be published in pamphlet form with notice published in accordance with the Charter.

**INTRODUCED** on first reading the \_\_\_\_ day of \_\_\_\_, 2012 and ordered published pamphlet form.

**PASSED** and **ADOPTED** on second reading the \_\_\_\_ day of \_\_\_\_, 2012 and ordered published in pamphlet form.

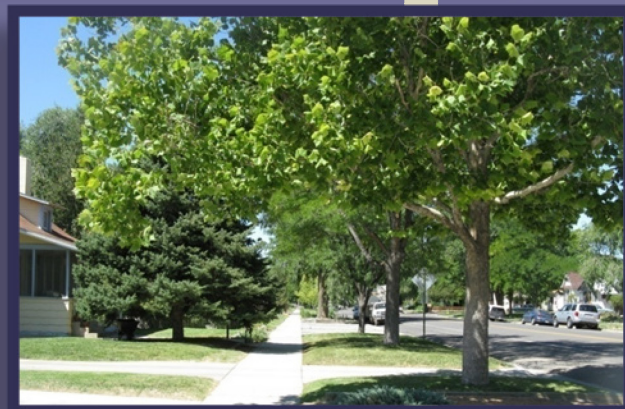
\_\_\_\_\_  
President of City Council

ATTEST:

\_\_\_\_\_  
City Clerk

# Greater Downtown Plan Zoning Overlay

# DRAFT



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## INTRODUCTION

The Greater Downtown Zoning Overlay is intended to provide guidance and criteria for the planning, design and implementation of public and private improvements in the Greater Downtown area. If properly administered and adhered to, the standards and guidelines should result in public and private development improvements (or a combination thereof) that achieve, as a minimum, a common level of quality in terms of site design, architectural design, landscaping and other site improvements.

The general purposes of the standards and guidelines are to support the overall goals of the Greater Downtown Plan.

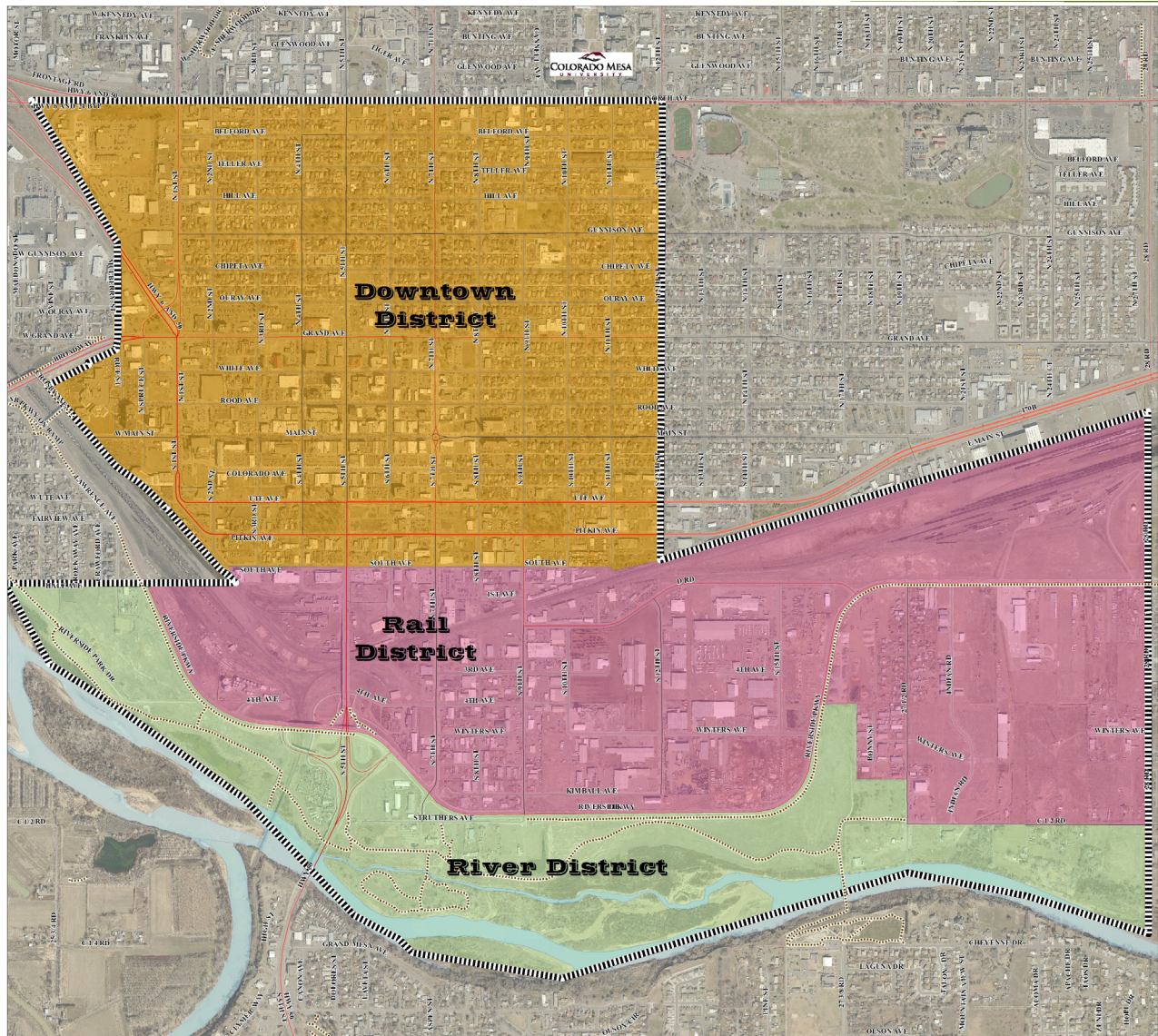
- Maintain and enhance the economic, cultural and social vitality of Greater Downtown Promote downtown living by providing a wide range of housing opportunities in appropriate areas within Greater Downtown.
- Enhance the transportation system to accommodate automobiles, bikes and pedestrians, and provide adequate, convenient parking.
- Stabilize, preserve, protect and enhance the downtown residential neighborhoods.
- Promote and protect the unique identity of Downtown.

The standards and guidelines were developed upon an analysis of the existing character of the Greater Downtown area. The area was divided into subdistricts and the Downtown District was further divided into subareas based on existing zoning, character of existing development and potential for redevelopment opportunities. In addition, primary corridors were identified for which overlay guidance was created. The subdistricts, subareas and primary corridors are shown on the maps on the following pages.

These standards supplement other development regulations such as the City of Grand Junction Zoning and Development Code, which includes detailed criteria by zone district, planned development regulations, design and improvement standards, supplemental use regulations, sign regulations and the City Transportation and Engineering Design Standards (TEDS). In the instance the following standards are silent on a development concern, the existing regulations shall apply.

The standards identify design alternatives and specific design criteria for the visual character and physical treatment of private development and public improvements within Greater Downtown. They are adopted through an overlay zoning district, which will establish the means by which the standards are administered and enforced. The Director will make all decisions and appeals and variance requests will be heard by the City of Grand Junction Planning Commission. The Downtown Development Authority (DDA) shall be a review agency for all applications and shall also make recommendations for proposals in the Central Business District.

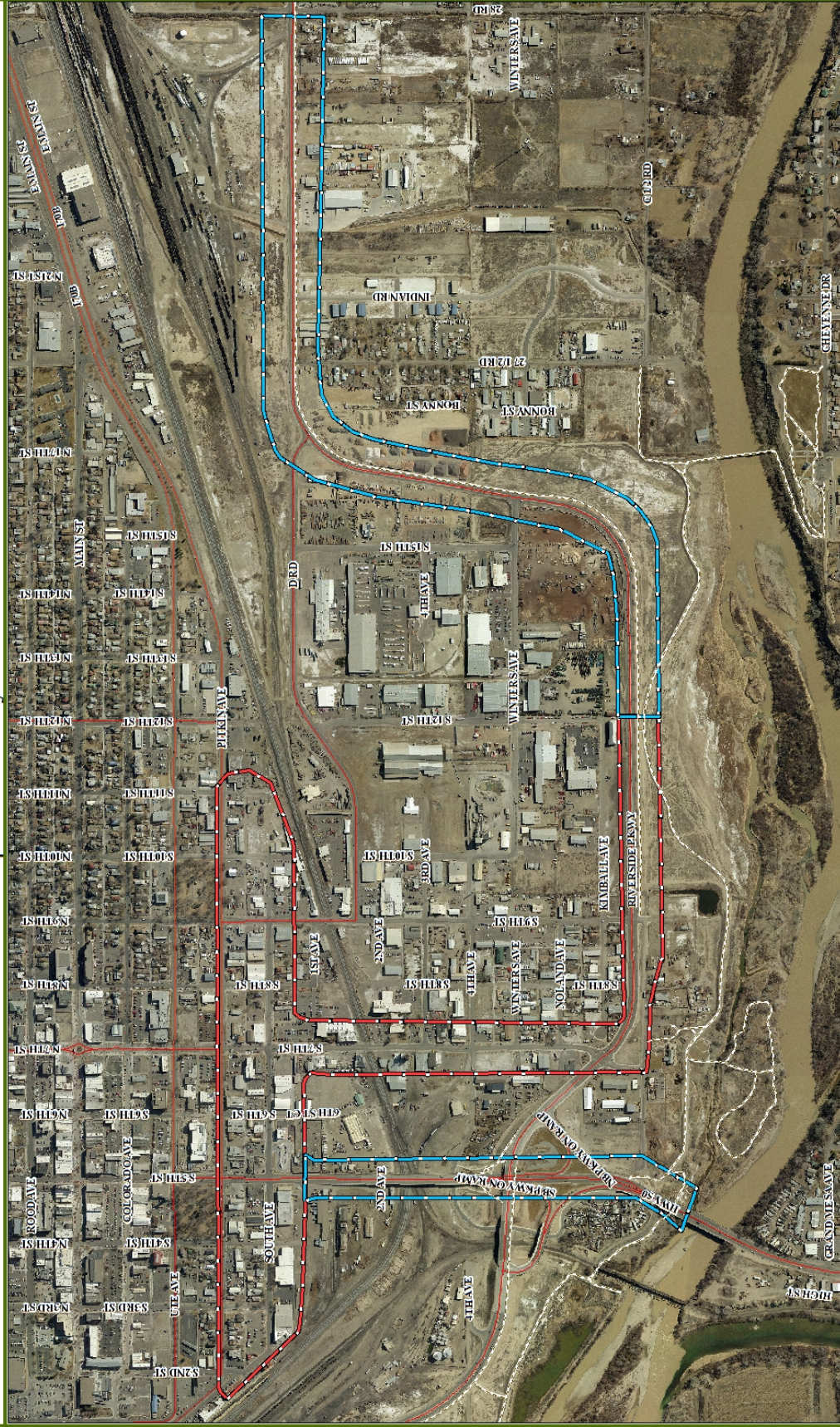




Greater Downtown Plan Subdistricts

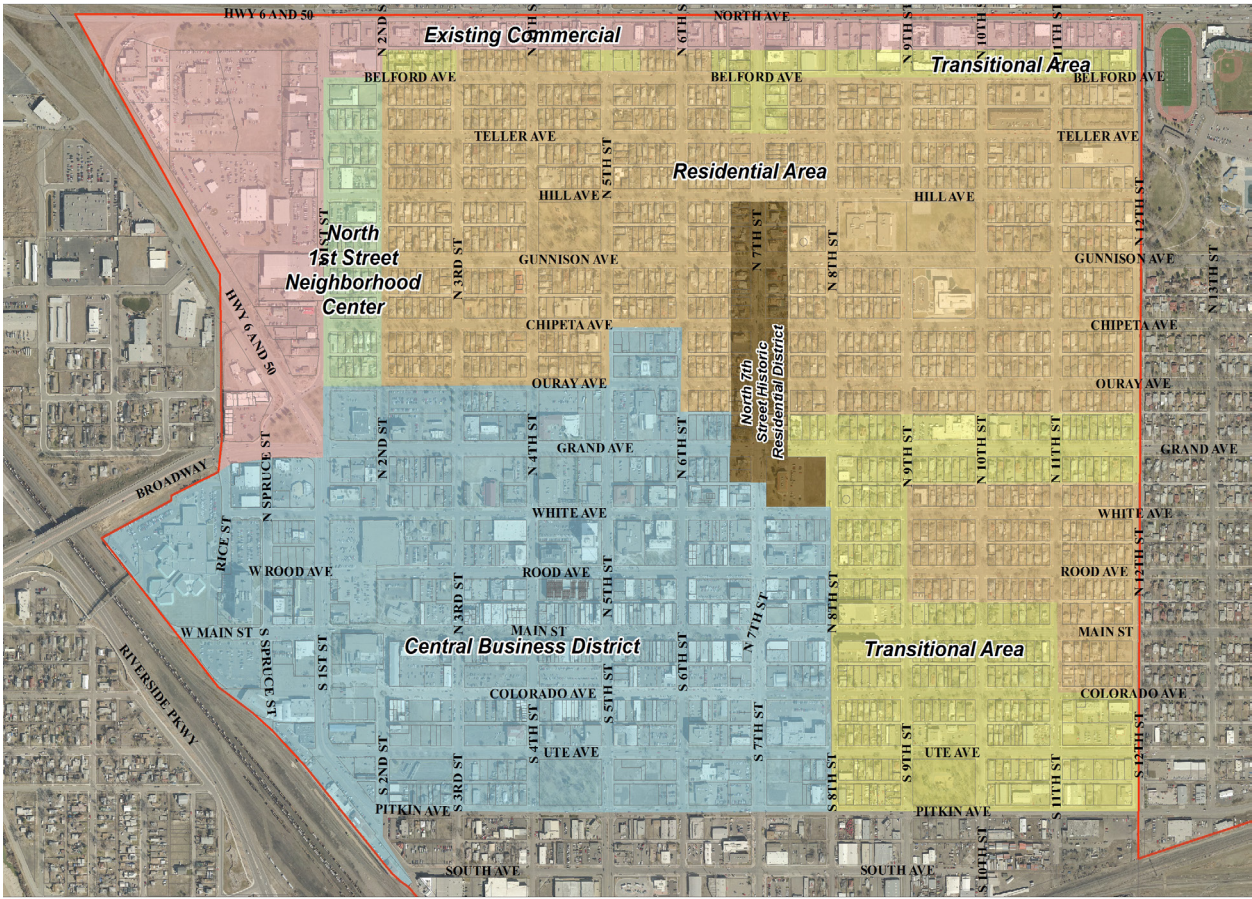


## Proposed Overlay Corridors





# Downtown District Subareas



**Downtown Subareas**    Central Business District    North 1st St Neighborhood    Existing Commercial    Residential Area    Transitional Area    7th Street Historic

## CORRIDOR STANDARDS AND GUIDELINES

### 1. OVERALL CORRIDOR VISION/CONCEPTS

The Greater Downtown Plan includes zoning overlay standards for primary corridors in the River and Rail Districts to begin to implement goals of the plan to 1) improve the visual impact of development along the corridors; and 2) promote higher quality architectural treatment and site design as new development and redevelopment occurs along the corridors. The visual features identified below define the concepts proposed along the corridors and, as implemented as new development and redevelopment occurs, will shape the desired character of the built environment. The features are grouped into three categories: Architectural Features; Building Materials; and Streetscape and Site Design.

#### A. Architectural Features

1. Façade modulation and roofline variations. The corridors can be improved through use of visually interesting architectural features that are designed to reduce mass and scale, including variation in the building form with recessed or projecting bays and variation in the rooflines.



2. Façade design. Design details can be used to emphasize architectural features such as the modulation and roof line changes discussed above or other features such as entryways and windows. Façade design details that break up a façade and add visual interest include:

- a. Columns and pilasters that help break up a horizontal plane of a building or other site feature.
- b. Change of material such as on an exterior wainscoting panel.
- c. Accent colors that help define and/or accentuate architectural features.

3. Defined entry. Many of the other architectural features above can be used to accentuate and define the primary entrance to a building, add architectural interest, as well as make the building more pedestrian- or customer-friendly.



4. Window sizes and shapes. The shape and sizes of windows used on a building break up the façade, provide visual variety and provide a pedestrian- and customer friendly character.

5. Awnings and porticos. Awnings and porticos are encouraged to help provide architectural interest.

## **B. Building Materials**

1. Mix of materials. Variations of materials used on exterior facades break up large building forms and walls.



2. Different textures, colors and tones. Use of different textures, colors and tones provide visual interest and can be used to accentuate architectural features.



## **C. Streetscape and Site Design**

Streetscape features along the primary corridors within the Rail and River Districts in Greater Downtown also help establish the visual character of the corridors. Examples of the desired characteristics are illustrated below.

1. Building Placement. Buildings are encouraged to be brought forward on a site to the street or sidewalk.

2. Landscaping. Many of the streets within the Rail and River Districts do not have, nor are planned to have, a sidewalk. However, streetscape interest can be created through landscaping, encouraging use of xeriscape concepts.



3. Streetscape Design. An urban streetscape is desired that includes hard surfaces, tree grates and street trees. This design concept exists along some of the corridors in the Rail and River District. The urban streetscape along the street can be blended with the site design of the adjacent parcel so that on-site landscaping complements and enhances the existing streetscape.



4. Detached sidewalks . Where possible, detached sidewalks are desired along primary corridors and already exist along some streets. The park strip between the curb and sidewalk can provide the landscape relief for an adjacent developed property.



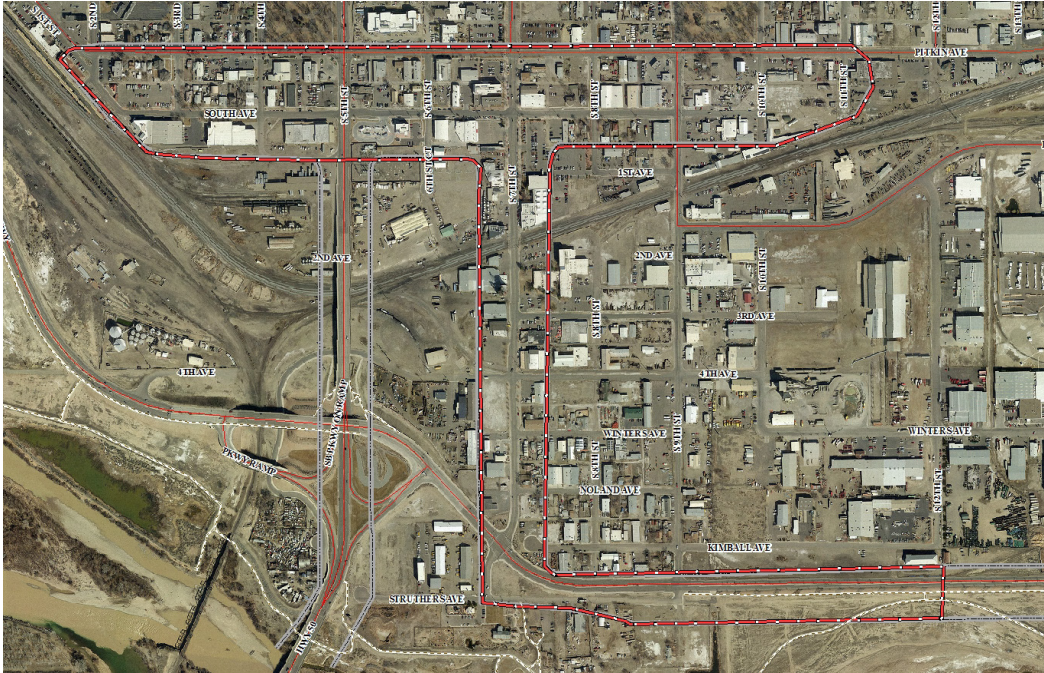
5. Outdoor storage, display and operations. Minimize the visual impact of outdoor storage, display and operations areas through placement on a site behind a building, to the rear of the property or otherwise screened.





## 2. COMMERCIAL CORRIDOR STANDARDS AND GUIDELINES

The following Commercial Corridor Standards and Guidelines apply to the south side of Pitkin Avenue, South Avenue, South 7<sup>th</sup> Street and the Struthers Avenue/Riverside Parkway area from 7<sup>th</sup> Street to the alignment of 12<sup>th</sup> Street.



### A. New Site Development or Redevelopment

#### Standards

1. In order to prevent parking from dominating the visual setting of the commercial corridors, front yards shall be primarily reserved for landscaping, sidewalks, driveway access to parking areas and signage.
2. Outdoor storage and permanent display areas shall only be allowed in the rear half of the lot, beside or behind the principal structure. Portable display of retail merchandise may be permitted subject to the provisions of the Zoning and Development Code.

#### Guidelines

1. For properties within the Commercial Corridors, residential uses may be allowed per the Light Commercial (C-1) zone district in the Zoning and Development Code, regardless of the zone district on the property that is within the corridor.
2. Wherever possible, parking is not to be located in the front yard – all parking should be located behind or to the side of the building.



## **B. Architectural Design of New Buildings/Additions or Substantial Exterior Remodel**

### **Standards**

1. For construction of new buildings and additions, or substantial exterior remodel, the façade(s) on all primary buildings that face the Greater Downtown Commercial Corridors shall use exterior materials that are durable, economically maintained, and of a quality that will retain their appearance over time. Buildings shall be designed to relate directly to and reinforce the pedestrian scale and quality of the adjacent streets, civic and open spaces.

2. The façade(s) of a new building, addition or substantial exterior remodel that face the Commercial Corridor rights-of-way or that face the public Riverfront Trail, shall have visually interesting architectural features and patterns that are designed to reduce mass and scale and reflect the desired vision for the corridors as described in Section 1 on pages 8 through 11. In order to do so, the façade(s) of a new building, addition or substantial remodel that face the Commercial Corridor rights-of-way shall exhibit a minimum of three (3) of the following architectural design elements.

- a. Variation in materials, material modules, expressed joints and details, surface relief and texture to break up building forms and wall surfaces. Such detailing could include sills, headers, belt courses, reveals, pilasters, window bays or similar features.
- b. Façade articulation/variation such as recessed or projecting bays or pilaster/column projections a minimum of every 30 feet.
- c. Variation in color.
- d. Facade feature that emphasizes the primary building entrance through projecting or recessed forms, detail, color or materials.
- e. Variation in roof lines/roof materials in order to add interest to and reduce the scale of buildings or expanses of blank wall. This can be accomplished through design elements such as overhangs/eaves, recesses/projections, raised cornice parapets over doors or bays and peaked roof forms.
- f. Screening of mechanical equipment either located on the roof or on the ground.
- g. Windows.
- h. Window or entry awning.
- i. Other.

## **C. Landscaping for New Site Development or Redevelopment**

### **Standards**

1. On-site landscaping shall be required per the Zoning and Development Code. The total amount of required landscaping may be reduced by the Administrator if a minimum of two (2) of the following conditions exist or are proposed.

- a. A higher quality of architectural design is achieved with greater than three (3) of the architectural design elements listed in B. above are included in the building design.
- b. Fifty percent (50%) or more of the building façade has a maximum setback of 15 feet.
- c. Street trees and other finished landscaping or hardscape exist within the adjacent public right-of-way.
- d. All parking is located to the side and/or rear of the building.
- e. All outdoor storage and operations are located behind the building.

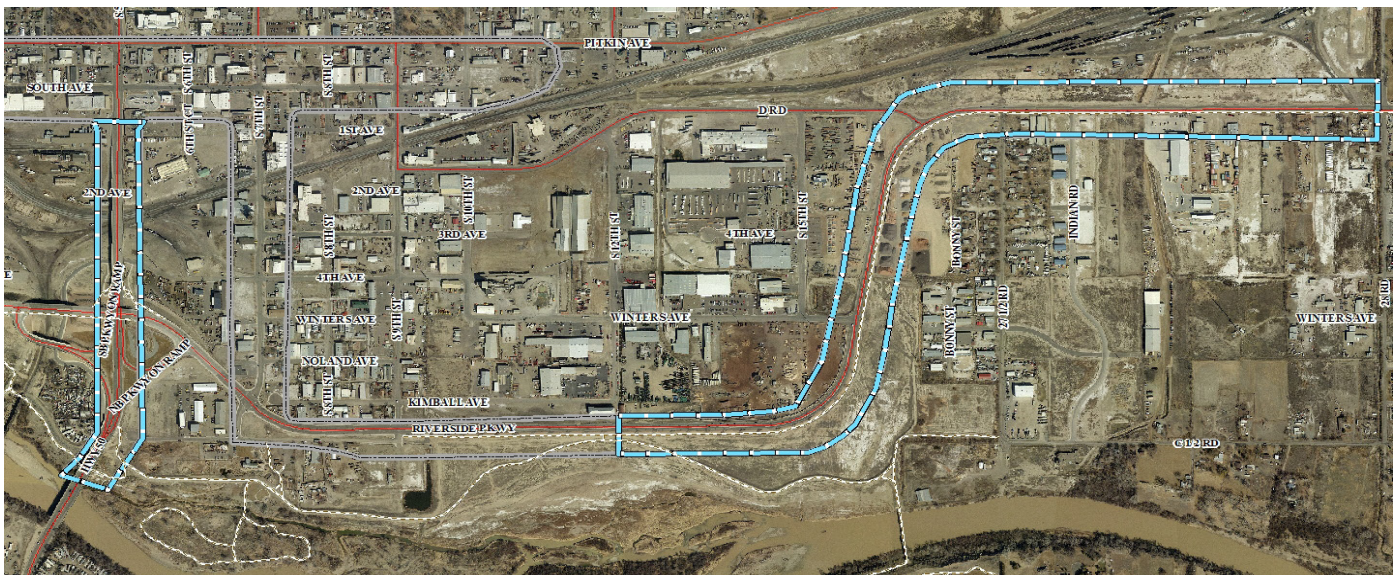
## **D. Signage**

### **Standards**

1. Only flush wall and monument style signs are allowed on the Greater Downtown Commercial Corridor properties. Flush wall signs are allowed per the Zoning and Development Code. Monument signs shall be located at least five (5) feet behind the front property line, shall not be located in the sight distance triangle and shall not exceed eight (8) feet in height.
2. Signs shall not be internally illuminated. External illumination is allowed. Illumination shall comply with the City of Grand Junction Zoning and Development Code.
3. New off-premise signs and billboards as defined by the City of Grand Junction Zoning and Development Code are not allowed on properties within the Greater Downtown Commercial Corridors or on properties that are within 600 feet from the centerline of the Riverside Parkway right-of-way. Any existing off-premise signs and billboards shall be subject to all other applicable provisions of the Zoning and Development Code.

## **3. INDUSTRIAL CORRIDOR STANDARDS AND GUIDELINES**

The following Industrial Corridor standards and guidelines apply to all properties with frontage on 5<sup>th</sup> Street south of South Avenue to the River and on Riverside Parkway from the 12<sup>th</sup> Street alignment east to 28 Road.



#### **A. 5<sup>th</sup> Street Industrial Corridor Standards**

1. No new outdoor uses and operations shall be allowed on parcels that have frontage on the corridor right-of-way. Maximum expansion and construction of any new outdoor uses and operations (such as outdoor operation of rail yards, salvage yards, etc.) on properties that abut the 5<sup>th</sup> Street right-of-way shall be 25 percent.
2. Screening on the 5<sup>th</sup> street bridge over the railroad tracks is required in order to limit views to adjacent uses but still provide longer vistas to the east (Grand Mesa) and west (Colorado National Monument and Uncompahgre Plateau).
3. New off-premise signs and billboards as defined by the City of Grand Junction Zoning and Development Code are not allowed on properties within the South 5<sup>th</sup> Street Industrial Corridor or on properties that are within 600 feet from the centerline of the South 5<sup>th</sup> Street right-of-way. Any existing off-premise signs and billboards shall be subject to all other applicable provisions of the Zoning and Development Code.

#### **B. Riverside Parkway Industrial Corridor Standards**

##### 1. New Site Development or Redevelopment

- a. Service entrances, service yards and loading areas shall primarily be located only in the rear or side yard. Along the Riverside Parkway right-of-way, a six-foot (6') high solid fence or wall of stone, wood or masonry shall screen: each service yard or area from adjoining single family residential zones and uses which are not separated by a street (not counting an alley or any easement).
- b. Outdoor storage and permanent display areas shall be allowed per the zone district of the property. Any storage in the front yard adjacent to the Riverside Parkway right-of-way shall be screened with a six-foot (6') high solid architectural wall constructed of stone, wood, masonry or combination thereof with a minimum 14-foot landscape buffer provided outside of the wall unless modified per the landscaping section below.

##### 2. Architectural Design of New Buildings or Substantial Exterior Remodel

- a. For construction of new buildings and additions, the façade(s) that face the Riverside Parkway on all primary buildings shall use exterior materials that are durable, economically maintained, and of a quality that will retain their appearance over time.
- b. The façade(s) of a new building that face the Riverside Parkway shall exhibit a minimum of three of the following architectural design elements:
  - (i) Variation in materials.
  - (ii) Façade modulation/articulation a minimum of every 30 feet.
  - (iii) Variation in color.
  - (iv) Facade feature that emphasizes the primary building entrance.
  - (v) Variation in roofline.
  - (vi) Windows.
  - (vii) Window or entry awning.

(viii) Other

### 3. Landscaping for New Site Development or Redevelopment

a. On-site landscaping shall be required per the Zoning and Development Code. The total amount of required landscaping may be reduced by the Administrator if a minimum of two (2) of the following conditions exist or are proposed:

- (i) A higher quality of architectural design is achieved with greater than three (3) of the architectural design elements listed in 2. above are included in the building design.
- (ii) Fifty percent (50%) or more of the building façade has a maximum setback of 20 feet.
- (iii) Street trees exist within the adjacent public right-of-way.
- (iv) All parking is located to the side and/or rear of the building.
- (v) All outdoor storage and operations are located behind the building.

### 4. Signage

a. Only flush wall and monument style signs are allowed on the Greater Downtown Riverside Parkway Industrial Corridor properties. Flush wall signs are allowed per the Zoning and Development Code. Monument signs shall be a maximum of twelve (12) feet in height with a maximum total of 100 square feet per sign face allowed per parcel. Signs shall not be internally illuminated. External illumination is allowed.

b. New off-premise signs and billboards as defined by the City of Grand Junction Zoning and Development Code are not allowed on properties within the Riverside Parkway Industrial Corridor or on properties that are within 600 feet from the centerline of the Riverside Parkway right-of-way. Any existing off-premise signs and billboards shall be subject to all other applicable provisions of the Zoning and Development Code.

## **DOWNTOWN DISTRICT STANDARDS AND GUIDELINES**

### **1. OVERALL DOWNTOWN DISTRICT VISION/CONCEPTS**

The Greater Downtown Plan includes zoning overlay standards for the subareas of the Downtown District as depicted on the map on page 7. Application of the standards and guidelines will begin to implement goals of the plan to:

- Maintain and enhance the economic, cultural and social vitality of Downtown.
- Promote downtown living by providing a wide range of housing opportunities.
- Enhance the transportation system to accommodate automobiles, bikes and pedestrians, and provide adequate, convenient parking.
- Stabilize and enhance the historic residential neighborhoods.
- Establish and promote a unique identity.
- Preserve and restore significant historic structures.
- Activate the edges of the downtown parks with mixed use and programmed/active use of the parks as urban open space rather than passive green parks.

### **2. AREA WIDE STANDARDS AND GUIDELINES**

The following standards and guidelines apply to all sub-areas of the Downtown District.



## A. GENERAL

**Policy:** Due to the constraint of many downtown properties and the City's desire to promote improvement and redevelopment in the Downtown Area, the Director may make reasonable exceptions to the provisions of the *Zoning and Development Code* and the *Greater Downtown Plan Zoning Overlay* for purposes of bulk standards (except for building height), landscaping, parking or other use-specific special regulations.

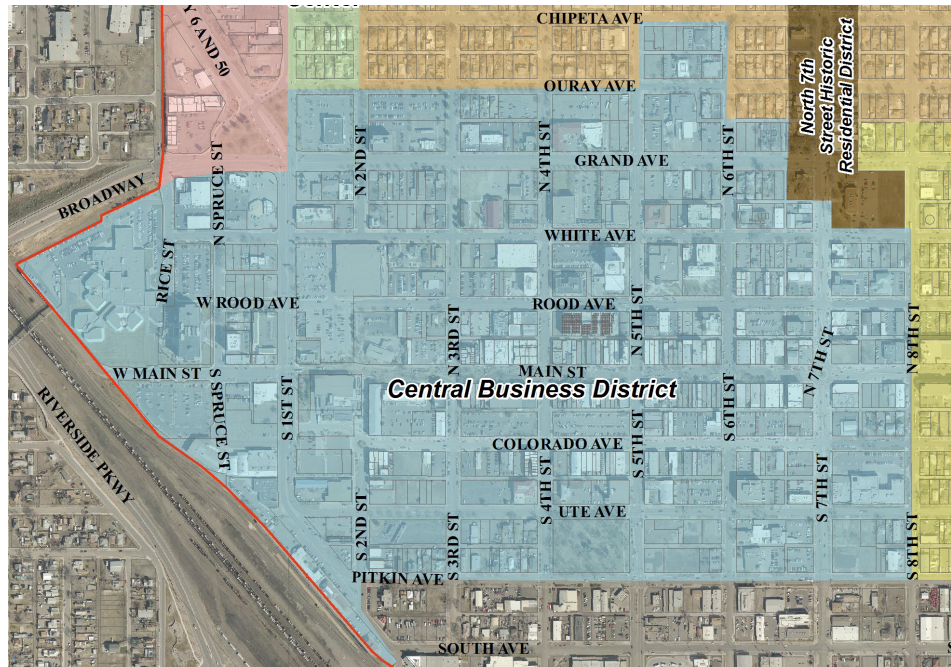
**Policy:** Provide good, interconnected transportation choices for better access and better health.

### Standards

1. Provide traffic calming measures where appropriate, including pedestrian refuge areas, medians, landscaping and corner bulb-outs.

## 3. CENTRAL BUSINESS DISTRICT (CBD) STANDARDS AND GUIDELINES

The following standards and guidelines apply to the CBD area shown on the map on page 5 (blue area). Further development and implementation of these concepts will be done in coordination with the DDA. The standards and guidelines are intended to apply to new development or substantial redevelopment within the area.



## A. LAND USE / ARCHITECTURE

### 1. Overall Vision/Concepts/Policies

- a. Activate the downtown core streets through emphasis on higher pedestrian traffic, businesses on the ground level that are oriented towards attracting higher pedestrian volumes, and buildings that “turn the corner” (invite activity on both the primary and cross streets).

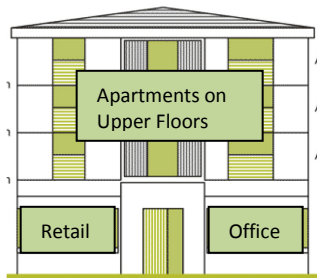




b. Encourage high quality, compatible design for all new buildings and establish a cohesive architectural character/theme that harmonizes new structures with the existing buildings through common materials, scale and basic architectural details as outlined in greater detail in the following standards and guidelines.

c. Typical building materials found in the CBD materials are traditional and weather well, allow a broad variety in appearance and ensure buildings are of high quality. To facilitate the creation of a cohesive architectural character/theme for new buildings, additions or exterior remodels in the CBD, the following exterior finishes are most appropriate: brick, sandstone, stucco, metal cladding, tiles, wood, glazing and decorative CMU.

d. Encourage high density, mixed-use development and structures (e.g. retail at street level and residential or office above).



e. Encourage gradual scale transitions between the CBD and adjacent neighborhoods.

## 2. Standards

### a. Building Height

- Minimum building height in the CBD shall be three (3) stories. Variances to this requirement may be considered and approved by the Grand Junction Planning Commission with a formal recommendation from the Downtown Development Authority (DDA) if the applicant establishes that strict



adherence to the Standard will result in practical difficulties or unnecessary hardships because of site or business characteristics that are not applicable to most properties in the same zoning district. The following criteria shall be used to consider variances to the minimum height requirement of three (3) stories.

- (i) Type of business not conducive to a vertical organization of operational space;
  - (ii) Exceptional conditions/constraints of the property;
  - (iii) The variance request is the minimum necessary to make possible the reasonable use of the proposed building; or
  - (iv) The variance will render the new use/building greater compatibility with a neighboring site, building or use.
- Maximum building height in the CBD shall be 90 feet. An increase of up to 25 percent may be considered by the Grand Junction Planning Commission.
  - Taller buildings will be located in the center and southern and western tiers of the CBD, with lower buildings on the northern and eastern edges of the CBD.
  - Upper floors of buildings taller than the traditional three to five stories in the CBD shall step back in order to enhance the traditional scale of downtown and ensure adequate air and light at the sidewalk level.

b. Building Setbacks , Scale and Massing

- Maximum building setback from the adjacent street public right-of-way shall be 2 feet except on Ouray and Chipeta Avenues.
- Buildings shall be set back a minimum of 20 feet from the rights-of-way on Chipeta and Ouray Avenues.
- Building setbacks of up to ten (10) feet from the adjacent right-of-way may be allowed if there is a prescribed function for the space such as limited outdoor display, seating, outdoor dining areas or a small street park.
- For the CBD areas north of Grand Avenue, the maximum building height of 90 feet shall only be allowed along the Grand Avenue corridor. The buildings will “step down” so that the front of the buildings that are directly across Ouray Avenue from residential buildings or uses are no taller than allowed by the corresponding residential zone district.
- Scale and massing of buildings or portions of buildings along Ouray and Chipeta Avenues will be compatible with residential scale.



c. Building Facades/Windows/Doorways – The façade(s) of a new building, addition or substantial exterior remodel within the Central Business District (CBD) shall have visually interesting architectural features and patterns that are designed to reflect the desired vision for the CBD described in Section 1 on pages 18 and 19. In order to do so, the façade(s) of a new



building, addition or substantial remodel within the CBD shall exhibit a minimum of six (6) of the following architectural design elements.

- On corner parcels, façade design of ground floors shall “turn the corner” to induce activity and interest in the streetscape on the north-south streets within the CBD.
- Facades shall be visually interesting. Ornamentation such as varied brick patterns, stone accents and window headers are acceptable ornamentation.
- The front façade of the building shall be “active” with at least 50 percent of the façade in windows, with doors spaced no more than 50 feet apart. Side facades shall meet a minimum of 50 percent of this requirement.
- Building facades shall be articulated on any street-facing side. Larger buildings shall be articulated in a hierarchy of smaller volumes and masses that better relate to other buildings and the scale of streets. This shall be accomplished through establishment of building bays that are distinguished by recessed or protruding elements or a variation in materials or color to break up the façade and reduce the overall scale of large buildings.
- Higher levels of fenestration shall be required for buildings along the high pedestrian use streets between 3<sup>rd</sup> and 7<sup>th</sup> Streets and Ute and Grand Avenues.
- Fenestration shall exhibit a block style commercial building including windows on the street level that are larger than those above, with window size decreasing as the floor level increases.

d. Roofs and Parapets

- All buildings shall include a façade cap. The cap shall be defined by a distinct roof line or parapet. The design of every new building shall use ornamentation of these features to enhance the building’s identity and support the architectural character of the CBD.





- The façade cap must be in three dimensions that projects and casts a shadow and shall relate proportionately to the overall building design.



- Building design shall minimize the visual impact of mechanical equipment located on the roof as viewed from the adjacent street.

### 3. Guidelines

a. Façade detailing should be compatible with, but not be identical to, that of a neighboring historic building. New facades should have their own, unique design. To create continuity, horizontal lines should be in alignment with neighboring buildings.

b. Awnings may overhang windows and shall be constructed of canvas or heavy cloth or metal (no plastic), utilizing primarily neutral colors.



c. Other upper level architectural features may encroach on the public right-of-way, provided all safety considerations have been met.

d. Entrances are often the primary focal point of a building and, as such, should be designed to fit with the overall character of the area.

e. Doorways may be finished with paints, stains, metal and aluminum cladding set to match the existing trim colors.

f. Single, double, revolving and corner doorways are acceptable in new construction.

g. Doorways can be recessed a maximum of 4 feet from the plane of the façade.

### B. SITE / AREA IMPROVEMENTS

#### 1. Parking

**Policy:** Maintain streetscapes dominated by buildings with parking located behind.

#### **Standards**

- a. Single use, surface parking shall be minimized.

- b. Parking located behind buildings on private property shall be accessed from the alley.
- c. Maximize opportunities for on-street parking by minimizing curb cuts along the street rights-of-way.

#### **Guidelines**

- a. Encourage shared parking.
- b. Available parking in the entire CBD area may be used towards meeting the parking required for new development in the CBD.
- c. The Director may make reasonable exceptions to the provisions of the *Zoning and Development Code* and the Downtown Strategic Master Plan Overlay for purposes of determining parking requirements.

## **2. Streetscape and Landscape**

**Policy:** Provide streetscape details and landscaping that compliment the architectural character of downtown Grand Junction and exhibit urban character.

#### **Standards**

- a. All pedestrian level lighting shall be downlit and shall be in a historical style light poles.
- b. The streetscape along Grand Avenue and 4<sup>th</sup> and 5<sup>th</sup> Streets north to Ouray Avenue will continue in a design compatible with the existing improvements along Grand Avenue (e.g. decorative pavement and street trees).
- c. The Director shall determine landscaping requirements for new development or redevelopment, considering existing and proposed streetscape and/or the urban design character of the area.

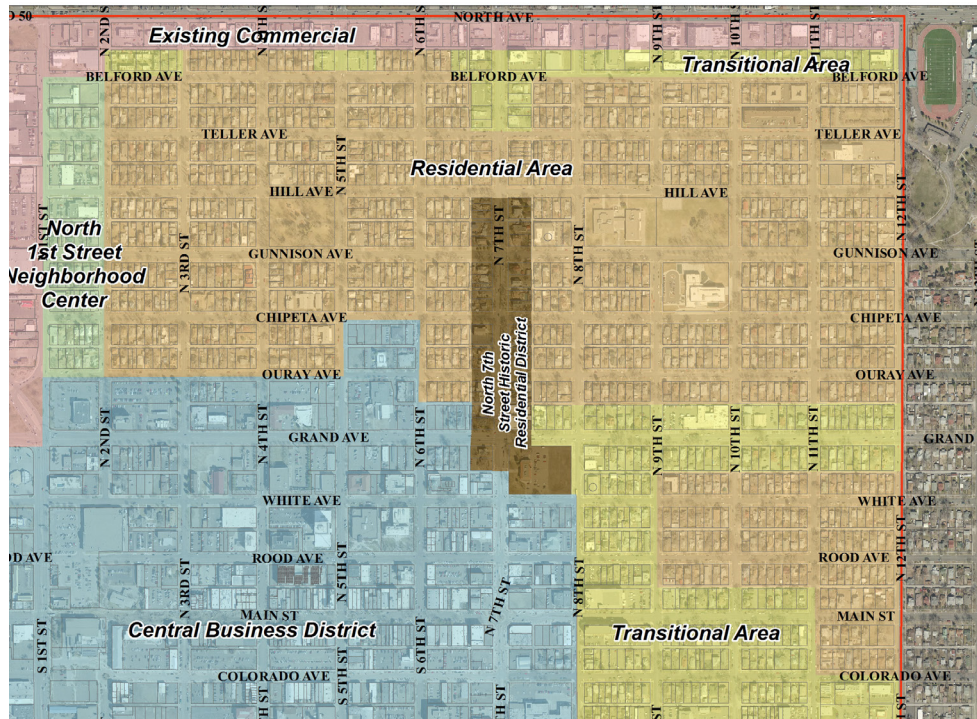
#### **Guidelines**

- a. Create downtown entries at strategic locations as shown on the Wayfinding and Signage Map in Appendix A. Enhancements may include landscaped medians, corner bulbs and special signs.
- b. The streetscape along 5<sup>th</sup> and 6<sup>th</sup> Streets north of Ouray Avenue to Chipeta Avenue should transition between the urban hardscape and a more residential streetscape character (e.g. detached sidewalk, landscaping in park strip between curb and sidewalk and street trees).



#### 4. RESIDENTIAL AREAS STANDARDS AND GUIDELINES

The following standards and guidelines apply to the Residential areas shown on the map on the map below (orange areas). The standards and guidelines are intended to apply to new development or substantial redevelopment within the area.



**A. Policy:** The existing downtown historic residential neighborhoods will be stabilized and enhanced.

#### **B. Standards**

##### **1. Land Use and Intensity/Density of Development**

- a. The downtown residential core will be preserved for residential uses, with no further encroachment by non-residential uses, higher intensity/density uses or more intensive zoning.
- b. Where existing residential zoning allows, provide a diversity of housing types through development of multifamily housing that is in keeping with the character of the neighborhood (refer to Multifamily Development section on page 26).

##### **2. Streetscape and Street/Pedestrian Systems**

- a. Enhance access to and improvements within existing public open spaces (e.g. parks and school grounds) within the downtown residential core such as enhanced pedestrian crossings and lighting for safety.



- b. Maintain and enhance the historic character of the streetscape with emphasis on the following elements: street trees, landscaping rather than parking or other uses in the park strip between sidewalk and curb, distinctive street signs and lighting and detached sidewalks.



### 3. Architectural Considerations

- a. Maintain the existing character of the house styles within the downtown residential core neighborhood. New construction and alterations shall be compatible with key architectural characteristics and site elements of the neighborhood.

- b. Each new building and addition shall be located so that it aligns with existing neighborhood buildings. "Aligns" means elevation (e.g. horizontal lines of peaks of roofs, cornices and window sills) and plan (e.g. setbacks from the street and rear property lines and spacing between structures/setbacks from side property lines).



- c. Building Orientation/Style. Main entrances shall open onto a street and shall align with those of adjacent residential buildings. For example, on many of the downtown homes, raised foundations and steps that define the main entrance are prevailing characteristics. Door styles shall be similar to those found on residential buildings within the area.

- d. Building Mass/Scale and Proportion. New buildings or additions to existing buildings shall be visually compatible with the area. Visually compatible means compatible with adjacent and neighboring buildings including mass and scale, shape, windows, doors, openings, roof shape, roof pitch and orientation.

- e. Building Height. New buildings and additions shall have the same number of stories and a height which is compatible with those of nearby dwellings. Two and one-half (2-1/2) stories shall be the maximum subject to the maximum height of thirty-five (35) feet.

- f. Roof Shape. The roofs of new buildings shall be visually compatible with nearby dwellings. Roof pitch shall be at least 4:12.



g. Fenestration. Structures shall be visually compatible with surrounding residential structures. Visually compatible includes the relationship of width to height, and the spacing of windows and doors. For example, tall evenly-spaced rectangular windows are typical of many of the residential styles in the downtown area.

h. Materials. The exterior materials of all new buildings, additions and alterations shall be similar in size and appearance to nearby dwellings.



#### **4. Accessory Structures**

- a. Accessory structures shall be no taller than the highest eave line of the principal structure.
- b. The footprint size of an accessory structure shall be a maximum of 35 percent of the footprint of the principal structure.

#### **5. Front Yards/Park Strips/Parking**

- a. Vehicular parking in the park strip area between the curb and detached sidewalk is not allowed.
- b. Parks strips will be landscaped in a traditional style, including street trees, grass, and low plantings or a combination thereof. Park strip landscaping shall include some live material – use of all non-living material such as rock is discouraged. Use of drought-tolerant plants is encouraged.

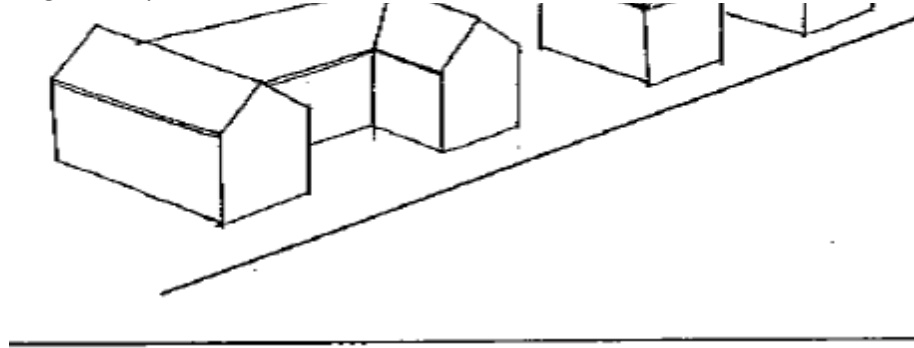


#### **6. Multifamily Development**

Infill of new multifamily buildings may occur where zoning allows within the downtown residential core. However, the site design and structures for this type of development must maintain a scale and character compatible with the area. In addition to the Architectural

Considerations listed in 3. above, multifamily development shall follow the standards and guidelines below.

- a. Incorporate forms typical of the single family residential architecture of downtown including sloping roofs, porches, roof dormers and other architectural details.
- b. Break up the mass of larger buildings into forms that are similar in scale to the single family residential character.
- c. Facades must be composed of smaller sections, similar in scale and material finish to single family residential structures.



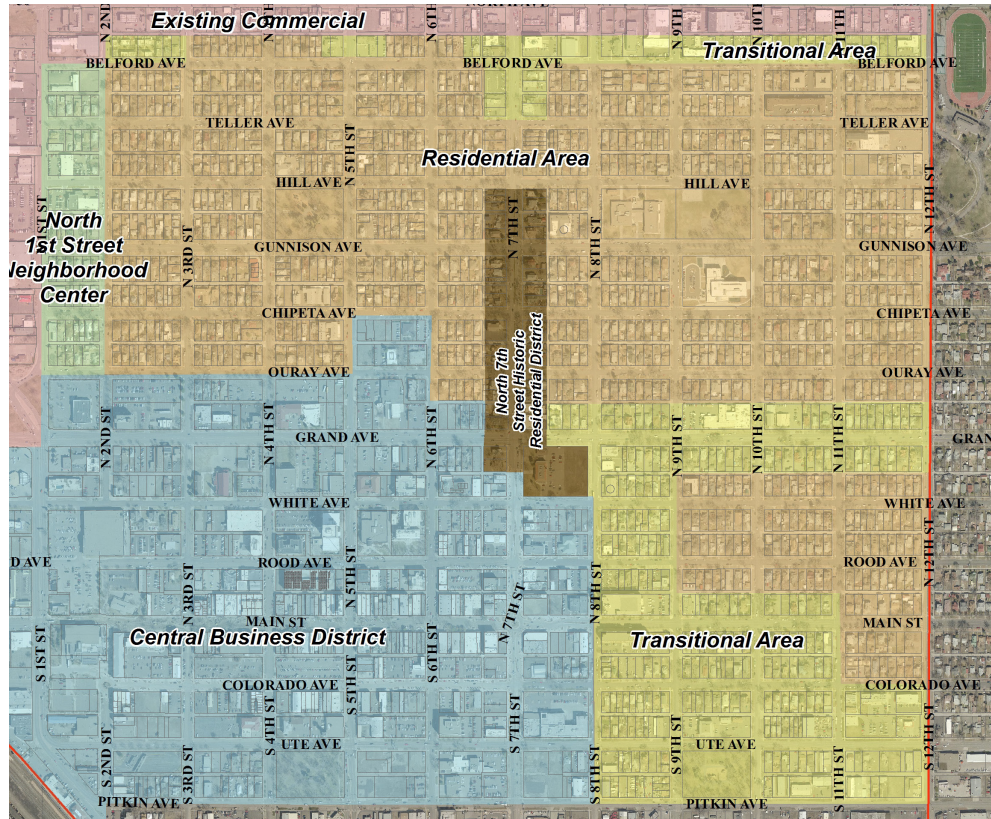
- d. Off-street parking for multifamily development shall not be located in the front yard setback. Parking shall be in the rear or side yards.
- e. Develop pedestrian links between the front sidewalk and building entrances and between parking and rear or side entrances.

### **C. Guidelines**

1. Demolition of existing historic homes in order to construct new residential structures is strongly discouraged.
2. Front yard setbacks along side streets may be reduced to ten (10) feet on properties within the Downtown District Residential subareas.
3. Maintain and enhance the pattern of landscaped front yards that gives the downtown residential core neighborhood a distinctive, friendly appearance.
4. Upon review and approval of the Director, new construction of accessory structures may be allowed to be built at historic setbacks (e.g. there could be a zero foot setback from the alley and 3 feet from neighboring property line).
5. Where available, some required parking may be on the street but only in front of the actual use, not in front of other properties/uses.

## 5. TRANSITIONAL AREAS STANDARDS AND GUIDELINES

The following standards and guidelines apply to the Transitional areas shown on the map below (yellow areas). The standards and guidelines are intended to apply to new development or substantial redevelopment within the area.



**A. Policy:** The peripheral areas of the Central Business District (CBD) provide a mix of established residential uses and low intensity, nonretail, neighborhood service and office uses that are compatible with adjacent residential uses and neighborhoods. New development or reuse of existing structures will maintain compatibility with residential building scale and appearance.

### B. Standards

#### 1. Land Use and Development Intensity

- a. Uses within these areas shall be as allowed by the *Zoning and Development Code* for the respective zone district(s).
- b. Any mix of residential and nonresidential uses on the same lot shall be located in the same structure.
- c. No uses within the downtown transitional areas shall open earlier than 7:30 am and shall close no later than 8:00 pm.
- d. Maximum building size shall not exceed 10,000 square feet unless a Conditional Use Permit is issued.



- e. Outdoor storage and display areas associated with uses in the downtown Transitional areas are prohibited.

## 2. Architectural Considerations

New residential or non-residential construction, including additions and rehabilitations, in the downtown Transitional areas shall be designed to have a single family residential character consistent with existing buildings in the area. “Consistent” means the operational, site design and layout, and architectural considerations described below.



- a. Every new building and addition shall be located so that it aligns with existing neighborhood buildings. “Aligns” means elevation (*e.g.*, horizontal lines of peaks of roofs, cornices, window sills) and plan (*e.g.*, setbacks from the street and rear property lines and spacing between structures/setbacks from side property lines).
- b. Main entrances shall open onto a street and shall align with those of adjacent residential buildings. For example, in areas adjacent to the downtown Transitional areas, raised foundations and steps that define the main entrance are prevailing residential characteristics. Door styles shall be similar to those found on residential buildings.
- c. Each new building, its mass in relation to open spaces and its windows, doors, and openings shall be visually compatible. Visually compatible means compatible with adjacent and neighboring buildings including mass, shape, window, doors, openings, roof shape, roof pitch and orientation. For example, a large building shall be compatible with surrounding smaller dwellings by dividing its mass into smaller components to create a building elevation that is more like the size and proportion of the nearby single family homes.
- d. New buildings shall have the same number of stories and a height which is compatible with those of nearby single family residential buildings. Two and one-half (2½) stories shall be the maximum subject to maximum height of thirty-five feet (35').
- e. The roofs of new buildings shall be visually compatible with nearby dwellings. Roof pitch shall be at least 4:12.
- f. Structures shall be visually compatible with surround residential structures. Visually compatible includes the relationship of width to height, and the spacing of windows and doors. For example, tall evenly-spaced rectangular windows are typical of certain residential styles near the downtown Transitional areas.
- g. The exterior of all new buildings, additions and alterations shall be similar in size and appearance to nearby dwellings. Sign materials should be visually compatible with materials used on the building façade.



### 3. Signs

Development of non-single family uses in the downtown Transitional areas may directly abut existing single family residential areas. Thus, in order to maintain compatibility, more restrictive sign regulations shall apply.

- a. Flush wall signs and monument signs shall be the only sign type allowed. One real estate sign advertising the property for sale or lease, shall not exceed 10 square feet.
- b. Signs shall be located at least 10 feet behind the front property line. Total sign area, excluding real estate signs advertising the property for sale of lease, shall not exceed 25 square feet per street frontage. The sign allowance for one street frontage may be transferred to a side of a building that has no street frontage, but cannot be transferred to another street frontage. Monument signs shall not exceed 8 feet in height.



- c. Illumination shall comply with provisions of the *Zoning and Development Code* pertaining to Nighttime Light Pollution. Signs may only be illuminated between 7:30 a.m. and 8:00 p.m.
- d. The area of flush wall signs and monument signs shall be calculated according to the *Zoning and Development Code*. Sign enhancement features such as bases, pillars, and other decorative elements as part of monument signs shall not be counted as part of the maximum square footage of the sign, provided such features do not exceed the size of the sign face.

### 4. Parking and Site Development

- a. Non-single family uses in the downtown Transitional areas shall be designed and operated not to increase on-street parking in front of single family dwellings in the neighborhood.
  - On-site parking shall be provided pursuant to the *Zoning and Development Code*.
  - On-site parking spaces shall only be located in the side and rear yards; and screened from nearby single family residential uses by a solid wall, fence or vegetation having a height of not less than 4 feet nor more than 6 feet (vegetation may exceed 6 feet in height).
  - Where available, some required parking may be on the street but only in front of the actual use, not in front of other properties/uses.

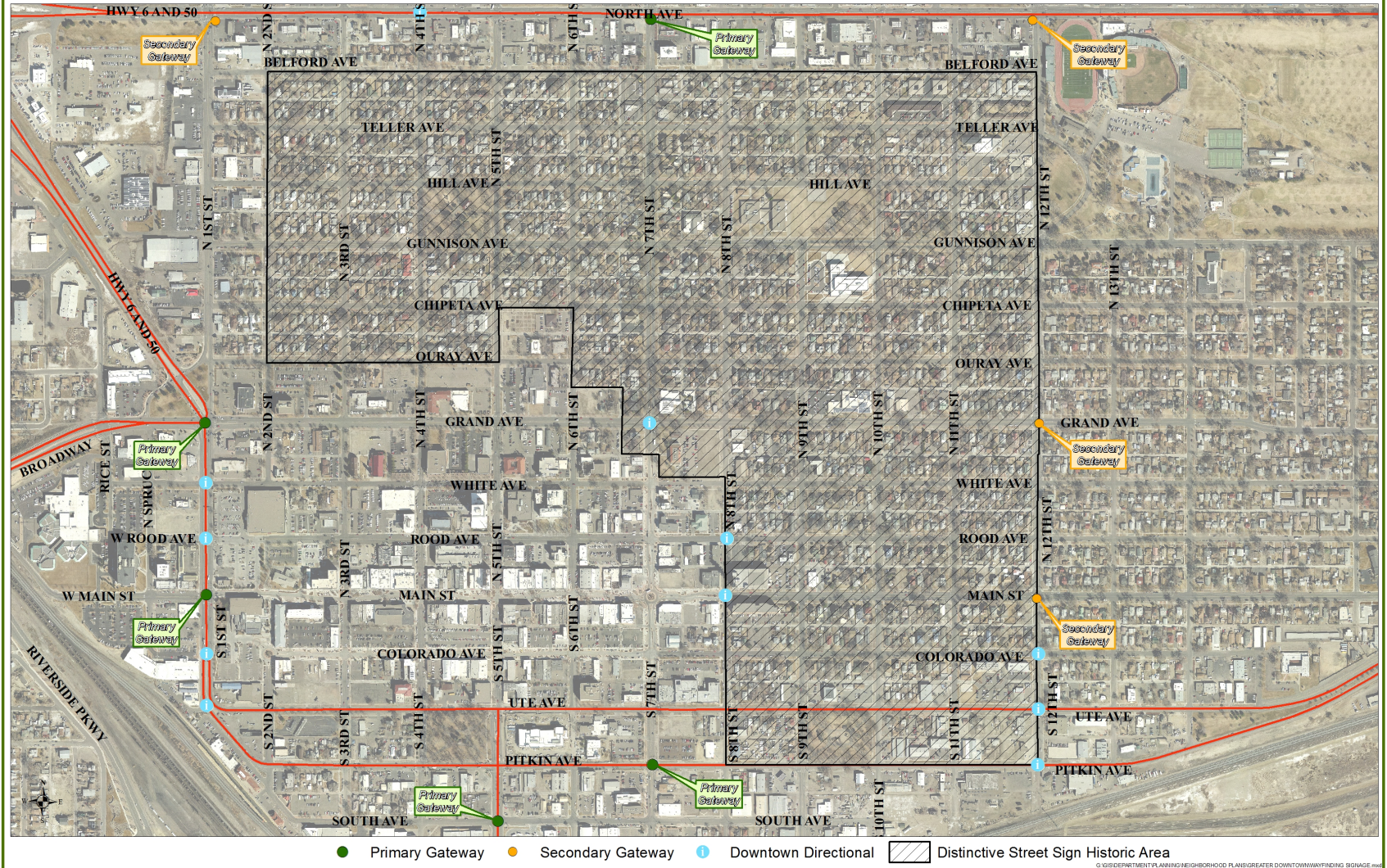
- b. Service entrances, loading areas and dumpster areas shall be located only in the rear or side yard. Each loading area shall be screened from each adjacent residential use or zone.
- c. Front yards shall be reserved for landscaping, sidewalks, driveway access to parking areas and signage.
- d. Outdoor lighting shall comply with the lighting provisions of the *Zoning and Development Code*.

## **APPENDICES**

### **A. Downtown District Wayfinding and Signage Map**



# Downtown District Wayfinding and Signage





### **ATTACHMENT 3**

#### **CITY OF GRAND JUNCTION, COLORADO ORDINANCE NO. \_\_\_\_\_**

#### **AN ORDINANCE ADOPTED A NEW ZONING MAP FOR PROPERTIES WITHIN THE GREATER DOWNTOWN PLAN AND ZONING OVERLAY GENERALLY INCLUDING THE ORIGINAL SQUARE MILE, THE AREA BETWEEN SOUTH AVENUE AND THE COLORADO RIVER AND THE RIVERSIDE NEIGHBORHOOD TO 28 ROAD**

#### **RECITALS.**

The City has adopted the Greater Downtown Plan as a part of the Comprehensive Plan. The Greater Downtown Plan includes a Future Land Use Map identifying uses for parcels within the Greater Downtown area. As part of the implementation of the Greater Downtown Plan, a Zoning Map has been created that is consistent with the Future Land Use Map and the goals set forth in the Greater Downtown Plan.

The Grand Junction Planning Commission is charged with the legal duty to prepare and consider and recommend action to City Council regarding amendments to the Zoning and Development Code for the City.

The Greater Downtown Plan Zoning Overlay was heard in a public hearing by the Grand Junction Planning Commission on July 24, 2012 where the Planning Commission recommended that the City Council adopt the Zoning Overlay.

City Council finds that the proposed Greater Downtown Plan Zoning Map are in conformance with the zoning criteria stated in section 21.02.140 of the Municipal Code.

#### **NOW THEREFORE, BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF GRAND JUNCTION:**

That the Greater Downtown Plan Zoning Map, City of Grand Junction, Colorado, in the form of the document attached hereto (Exhibit A), and as recommended for adoption by the Grand Junction Planning Commission is hereby adopted.

The existing maps depicting and describing the zone and districts of lands within the Greater Downtown Plan area, which are a part of the City's Zoning and Development Code are hereby repealed and reenacted with the attached map (Exhibit A).

The Greater Downtown Zoning Map, including the text of the Greater Downtown Plan Zoning Overlay, in accordance with paragraph 51 of the Charter of the City of Grand Junction, shall be published in pamphlet form with notice published in accordance with the Charter.

**INTRODUCED** on first reading the \_\_\_\_\_ day of \_\_\_\_\_, 2012 and ordered published pamphlet form.

**PASSED** and **ADOPTED** on second reading the \_\_\_\_\_ day of \_\_\_\_\_, 2012 and ordered published in pamphlet form.

\_\_\_\_\_  
President of City Council

ATTEST:

\_\_\_\_\_  
City Clerk

# EXHIBIT A – ZONING MAP

