

**GRAND JUNCTION CITY COUNCIL  
READINESS SESSION**

**MONDAY, NOVEMBER 5, 2012, 5:00 P.M.  
CITY HALL AUDITORIUM  
250 N. 5<sup>TH</sup> STREET**

*To become the most livable community west of the Rockies by 2025*

**1. Draft Greater Downtown Plan**

**[Attach R-1](#)**

In addition, a hard copy of the plan and overlay will be placed in the City Council office and a hard copy of the proposed Zoning Overlay will be placed in City Council boxes



Date: October 24, 2012

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Title/ Phone Ext: Senior Planner  
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Proposed Meeting Date:

November 5, 2012

## **CITY COUNCIL STAFF REPORT WORKSHOP**

**Topic:** Greater Downtown Plan Update

**Staff:** Tim Moore, Deputy City Manager  
Kathy Portner, Neighborhood Services Manager  
Harry Weiss, Downtown Development Authority Director

**Summary:** City Council gave staff direction at its September 19, 2011 workshop to move forward with a planning process for a Greater Downtown Plan. Since the last update to City Council on April 30, 2012, staff has continued to engage property owners and other community groups to develop a detailed plan.

Staff will present the draft plan and proposed zoning overlay, with specific detail for the Central Business District, to the City Council and DDA Board on November 5, 2012.

### **Greater Downtown Plan Background:**

The Greater Downtown plan document consists of two major components; the Plan and a Zoning Overlay. The Plan is a component of the Comprehensive Plan and establishes and distinguishes the Downtown District, Rail District and River District. The draft document outlines goals and policies for each of the districts and proposes Future Land Use Designations and zoning for each parcel. Adoption of the Plan and Zoning would correct zoning conflicts that currently exist, primarily in the rail district.

The Zoning Overlay further implements many of the goals and policies found in the Plan with area specific design standards and guidelines. Design standards and guidelines are proposed for specific corridors in the Rail District and for subdistricts of the Downtown area, including the Central Business District (CBD).

Options: Consideration of adoption of the documents could be in whole or parts;

- ✓ Adoption of the Greater Downtown Plan would include the rezones necessary to eliminate the future land use/zoning conflicts that currently exist.
- ✓ The proposed design standards and guidelines could be considered individually for each district or subdistrict and adopted in whole or in part.

There is some opposition to the Plan with two specific issues:

Part of the proposal is to not require parking for new development in the CBD, acknowledging the preference for shared parking. Such a standard would not preclude parking on site, but would just not mandate it. Until a payment-in-lieu of parking program is established to help pay for shared parking we are recommending deleting this provision from the document.

The second objection is a proposed standard of a minimum building height of two (2) stories for new construction to maximize density and intensity in the CBD. The proposed standard lists a number of exemptions from the height requirement and outlines a variance process. The standard would not apply to existing structures except for expansions that exceed 100% of the existing square footage.

**Greater Downtown Plan Recent Actions and Next Steps:**

1. Sent letter to all property owners affected by a potential zoning change.
2. Ongoing meetings with community groups and property owners
  - Chamber of Commerce
  - Downtown Development Authority
  - Rail District Property Owners
3. Draft documents initially made available to the public July, 2012.
4. Revised draft of Central Business District overlay provided Chamber of Commerce September, 2012.
5. Tentative adoption schedule:
  - Planning Commission hearing in November 2012.
  - City Council hearings in December/January 2012/2013.

**Previously presented or discussed:** The last update to City Council was at a Workshop on April 30, 2012.

**Attachments:**

Draft Greater Downtown Plan and Zoning Overlay

In addition, a hard copy of the plan and overlay will be placed in the City Council office and a hard copy of the proposed Zoning Overlay will be placed in City Council boxes

# Greater Downtown Plan

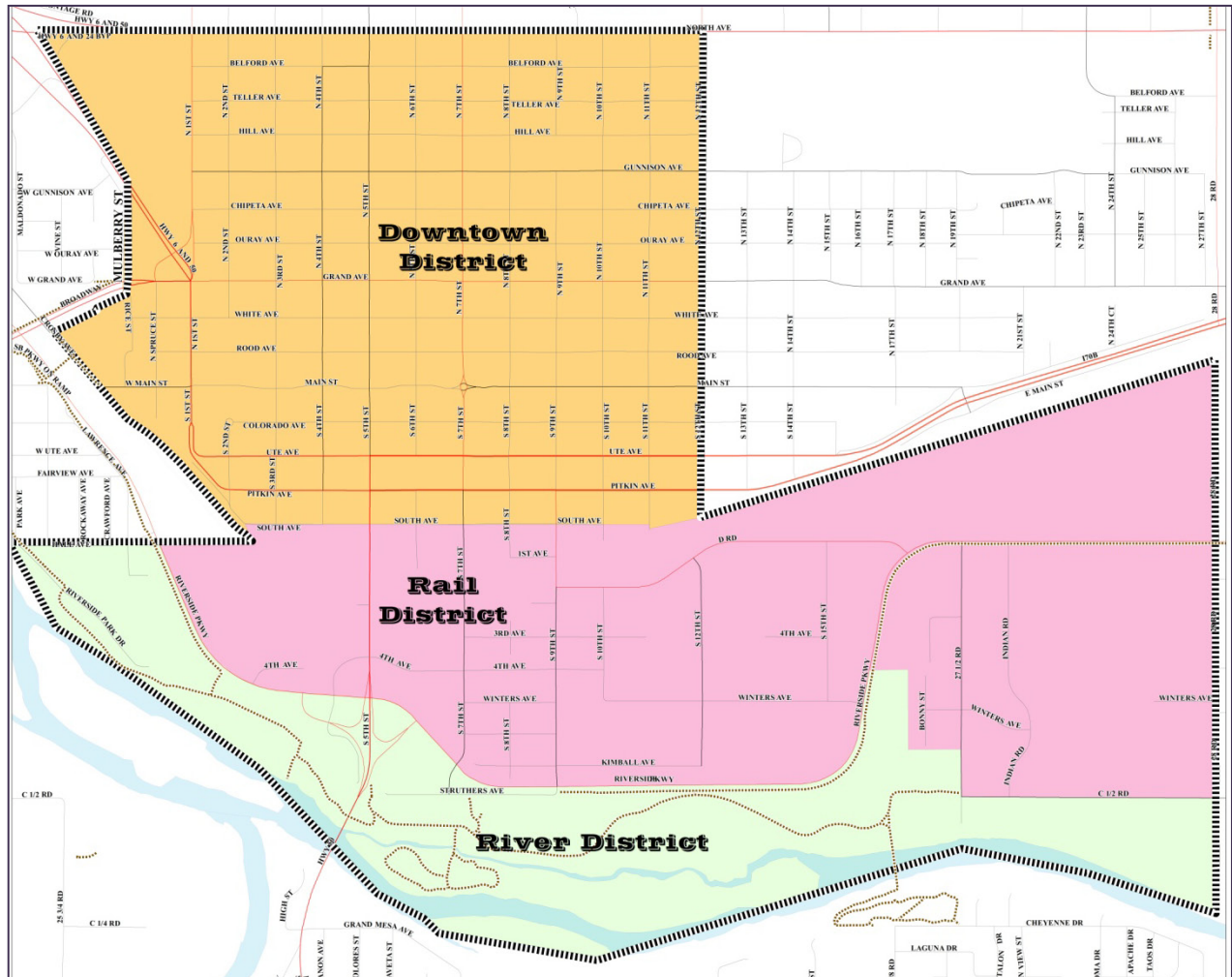
# DRAFT



<b>1. STUDY AREA CONTEXT</b>	4
<b>2. PLANNING BACKGROUND</b>	4
<b>3. COMPREHENSIVE PLAN</b>	5
<b>4. SITE ANALYSIS</b>	6
A. Downtown District	6
B. Rail District	10
C. River District	12
<b>5. GREATER DOWNTOWN GOALS AND POLICIES</b>	14
A. Area-Wide Goals and Policies	15
B. Downtown District Goals and Policies	16
C. Rail District Goals and Policies	17
D. River District Goals and Policies	18
<b>6. IMPLEMENTATION STRATEGIES</b>	18
A. Downtown District Future Land Use and Zoning	19
B. Rail District Future Land Use and Zoning	22
C. River District Future Land Use and Zoning	24
D. Development Standards	27
E. Greater Downtown Circulation Plan	27
F. Entryways and Signage	34
G. Economic Redevelopment	36
H. Greater Downtown Area Parks	38
I. City-owned Jarvis Property	40
<b>APPENDICES</b>	43
A. Future Land Use Map	44
B. Zoning Map	45
C. Circulation Plan	46
D. Development Concepts	48
E. Public Participation Process	54

## 1. STUDY AREA CONTEXT

The Greater Downtown planning area is comprised of three sub-districts: the Downtown, Rail and River Districts. The areas are distinctly different due to their location, influences and historic development but complement each other to form a developed area that is central to the community with uses that range from single family residential to rail-oriented heavy industrial.



## 2. PLANNING BACKGROUND

A *Strategic Downtown Master Plan* that encompassed the original square mile was developed through the Downtown Development Authority (DDA) and the guidance of a steering committee of interested downtown merchants, property owners and policymakers during 2007-2008. The Plan defined an overall vision and goals for downtown and included implementation strategies such as a zoning overlay.

The vision of the Plan addressed: the transportation network and other infrastructure, introduction of green treatments, creation of design standards and strategies for community marketing and promotion efforts. The goals of the plan were to:

- Maintain and enhance the economic, cultural and social vitality of Downtown
- Promote downtown living by providing a wide range of housing opportunities
- Enhance the transportation system to accommodate automobiles, bikes and pedestrians, and provide adequate, convenient parking
- Stabilize and enhance the historic residential neighborhoods
- Establish and promote a unique identity
- Jump-start the revitalization and reinvestment in the downtown area with strategic catalyst projects

The *Strategic Downtown Master Plan* was considered by City Council on September 14, 2009 but, due to pending adoption of the *Comprehensive Plan*, Council voted to continue the Plan to an unspecified future date.

A *South Downtown Neighborhood Plan* encompassed the area between the railroad tracks and the Colorado River and the Riverside neighborhood on the west to 28 Road on the east. A plan for the area was developed from 2006-2008 with 15 community focus group meetings, 3 public open houses with 80-100 people in attendance at each open house. The Plan included an existing conditions analysis, goals and implementation including a circulation and trails plan, economic development strategies, rezoning some properties and zoning overlay. The goals of the Plan were to:

- Create, maintain and enhance a green waterfront
- Recognize existing heavy industry and rail service that supports it
- Recognize the distinction between “industrial” streets and “public” streets
- Promote higher quality, “cleaner” uses in the area generally between 7<sup>th</sup> and 9<sup>th</sup> Streets
- Improve entry points and connections to downtown
- Increase light industrial opportunities
- Create areas of mixed uses to screen and transition to the heavy industry
- Create and enhance redevelopment opportunities and partnerships

The *South Downtown Neighborhood Plan* was considered by City Council on June 16, 2008 but was not adopted.

The planning process for the *Greater Downtown Plan* reanalyzed the two previous planning efforts and made revisions as conditions have changed, included areas that had not been covered by either of those plan, and integrated them into a single plan for the downtown area. In addition, the *Greater Downtown Plan* incorporates elements of the Downtown Development Authority’s potential projects in order to support the DDA’s Downtown Plan of Development, as well as incorporating elements of the *Comprehensive Plan* discussed below.

### **3. COMPREHENSIVE PLAN**

The Grand Junction Comprehensive Plan is based on extensive public input that identifies what kind of community we want to have and identifies ways to achieve our vision. It charts the course to help us become the most livable community west of the Rockies. It establishes a vision that focuses the community on what it should do to sustain the quality of life that all residents desire and expect. The Comprehensive Plan establishes the following guiding principles that will shape growth, all of which apply to development of the Greater Downtown area.

**A. Concentrated Centers** – The Plan calls for three types of centers; the City Center, Village Centers and Neighborhood Centers.

**B. Sustainable Growth Patterns** – Fiscal sustainability where we grow efficiently and cost-effectively. Encourage infill and redevelopment.

**C. Housing Variety** – Allow, encourage more variety in housing types that will better meet the needs of our diverse population.

**D. A Grand Green System of Connected Recreational Opportunities** – Take advantage of and tie together the exceptional open space assets of Grand Junction, including the Colorado River, our excellent park system, trails and our surrounding open spaces.

**E. Balanced Transportation** – Accommodate all modes of transportation including air, transit, freight, auto, bike and pedestrian.

**F. A Regional Center** – Preserve Grand Junction as a provider of diverse goods and services and residential neighborhoods.

Specific policies within the Comprehensive Plan further support the concepts of the Greater Downtown Plan as outlined below.

**Goal 4.** Support the continued development of the downtown area of the City Center into a vibrant and growing area with jobs, housing and tourist attractions.

**Goal 5.** To provide a broader mix of housing types in the community to meet the needs of a variety of incomes, family types and life stages.

**Goal 6.** Land use decision will encourage preservation of existing buildings and their appropriate reuse.

**Goal 8.** Create attractive public spaces and enhance the visual appeal of the community through quality development.

**Goal 9.** Develop a well-balanced transportation system that supports automobile, local transit, pedestrian, bicycle, air and freight movement while protecting air, water and natural resources.

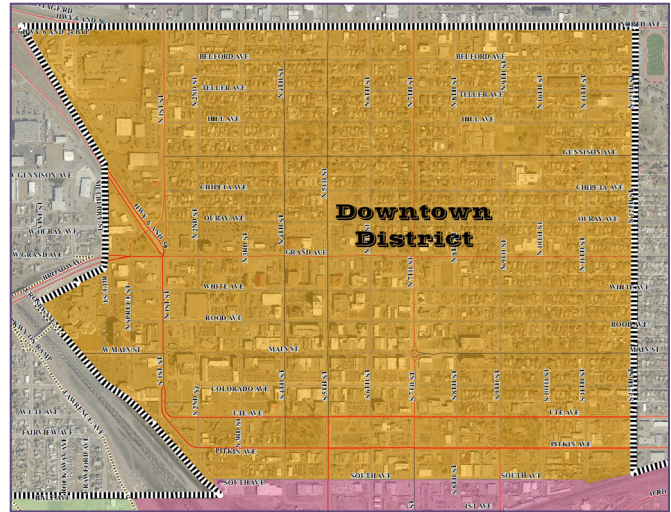
#### **4. SITE ANALYSIS**

##### **A. Downtown District**

The Downtown District encompasses the original square mile, the area south of North Avenue between 1<sup>st</sup> Street and Highway 6 & 50, and the area south of Grand Avenue between 1<sup>st</sup> Street and the railroad tracks (see map).



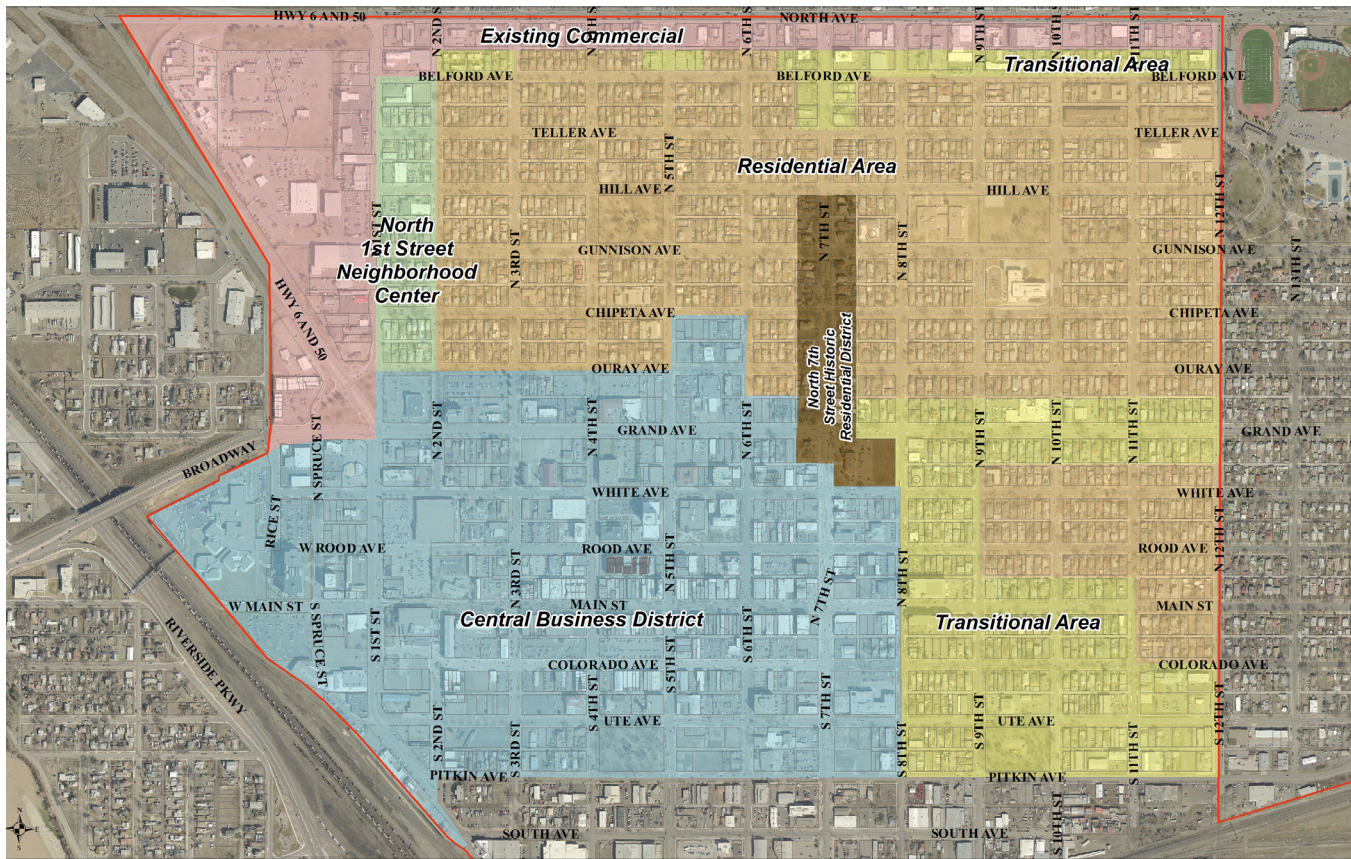
The Downtown District should retain its role as the City’s center and a regional destination. With a diverse mix of land uses, civic and public amenities, art and cultural facilities, the Downtown District offers a hub of activity that supports and reflects the regional demographics. Every effort should be made to keep and maintain existing public amenities and services in the Downtown District. These uses attract large numbers of residents and visitors to downtown which contributes to its economic vitality.



The District benefits from a number of characteristics that make it appropriate for development of retail, office, residential, institutional and community uses as described below.

- On average, urban residents spend a greater percentage of household income on retail expenditures, particularly on items such as apparel and food away from home. This indicates an opportunity for additional specialty retail and entertainment space in the Downtown District.
- Downtown housing has been and is expected to continue to increase in density with smaller households comprised of young and old, and moderate and lower-income residents. However, with a growing concentration of middle-aged, moderate- to high-income households in the City as a whole, there is an opportunity for Downtown to attract more diverse, higher-income resident base. The entirety of Grand Junction (urban and fringe) faces a growing shortage of quality affordable housing for its very low- and moderate-income residents, as well as working-wage families. This planning process emphasized the need for a set of strategies tailored specifically to the housing challenges present within the Downtown District.
- Downtown commercial vacancy and rental rates are approaching levels required to support new development and/or redevelopment. However, “seed” money will likely be necessary to leverage private investment in projects that will catalyze reinvestment activity throughout the Downtown District.
- Among the higher growth employment sectors in Mesa County are service industries often consisting of small businesses. This represents an opportunity for Downtown to develop not only additional live/work units, but also to promote the adaptive re-use of historically significant buildings and less traditional spaces including former church facilities.
- Forecasts indicate that more than one million square feet of employment space (office), more than 1.6 million square feet of retail space and nearly 1,100 residential units could be absorbed in the market over the next ten years, from which the Downtown District could benefit. The degree to which downtown is able to capture new demand within the trade area and beyond will be a function of the redevelopment process itself. Redeveloping key catalyst areas as residential, retail, employment and community destinations will necessarily increase its ability to capture not only a greater share of trade area demand, but also to reach beyond those boundaries.

- The Downtown District is comprised of many uses that result in a number of diverse neighborhoods. While some areas within downtown are comprised of one single use, such as residential or business/commercial, there are many areas that include a range of uses, such as along Grand Avenue. In order to more fully understand specific issues facing the many, diverse neighborhoods within downtown, the area was divided into four primary sub-areas: Central Business District (CBD), Transitional, Residential and North 1<sup>st</sup> Street Neighborhood Center.



**Downtown Subareas**    Central Business District    North 1st Street Neighborhood    Existing Commercial    Residential Area    Transitional Area    7th Street Historic

**Central Business District (CBD).** The CBD contains primarily commercial uses and is located south of Grand Avenue. The CBD also contains a significant number of public and civic facilities. While much of the CBD is zoned to permit a wide range of uses, there are several new projects that have integrated uses into one cohesive project. Pedestrian accessibility and amenities, public facilities, traffic patterns, building heights and commercial development densities combine to create an urban character that attracts local businesses, employees, residents and visitors to the CBD.



*Main Street Character*

**Transitional Subarea.** The Transitional areas of downtown contain a mix of residential, commercial and institutional uses. Development densities and building heights are not as extreme as in the CBD and

residential uses, which includes single family homes adapted to multifamily uses, are prevalent throughout the Transitional areas. With the exception of the main arterial Grand Avenue, traffic is less congested and parking is available, both on-street and in publicly- and privately-owned surface parking lots.

Existing homeowners in the Transitional areas, especially along Grand Avenue, expressed concern over the rapidly expanding commercial character of the area. Traffic congestion, parking issues, vehicle accessibility, lighting and signage were each cited as an adverse impact that existing single family residential homeowners were grappling with as more homes are converted to business uses.



*New Infill Development Building on Right*



*Residential Subarea Character*

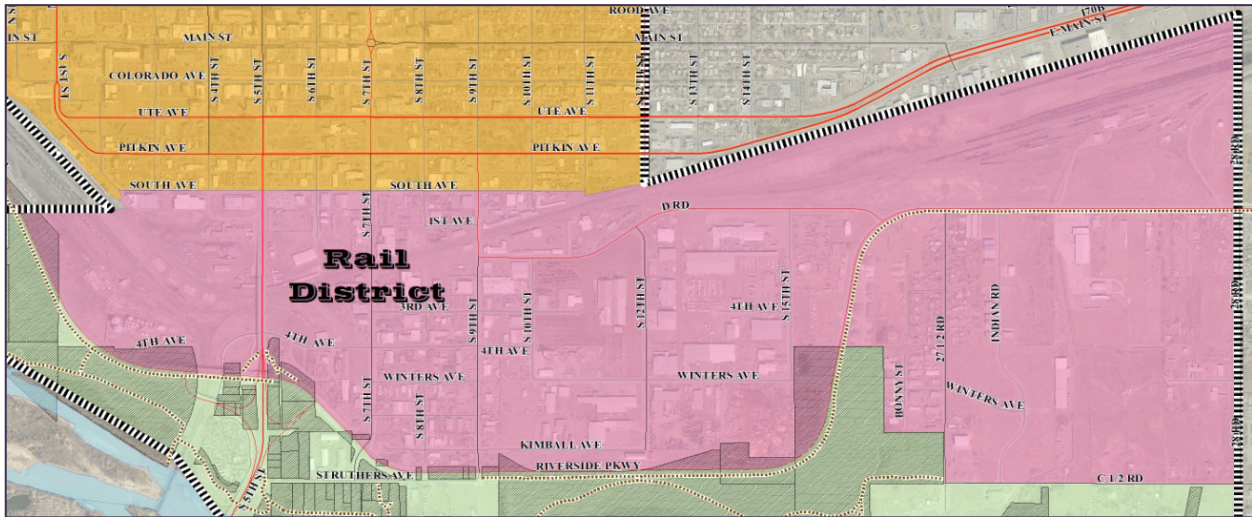
**Residential Subarea.** A significant amount of the Downtown Core contains single family residential uses. The large number of older, historic structures results in a definitive architectural character that local residents want preserved. The North Seventh Street Historic Residential District contains 34 historic homes with a high concentration of early Twentieth Century architectural styles and construction methods. The district covers five blocks of North Seventh Street with the most intact historical residential area in Grand Junction with a variety of Queen Anne, Colonial Revival and Mission style homes.

Along the edges of the downtown residential areas, commercial activity is encroaching on established single-family residential neighborhoods. Although existing residents expressed their satisfaction with existing development and uses along the North 1<sup>st</sup> Street and North Avenue, many participants expressed concern over future expansion and potential rezoning along these edges and wanted to see the Plan respect existing zone districts and development patterns and prohibit any further rezoning or variances.

**North 1<sup>st</sup> Street Neighborhood Center.** This subarea of the Downtown District is an extension of the Neighborhood Center defined along the North Avenue corridor. The intent of the Neighborhood Center is to provide for limited employment, residential, open space and limited retail uses that primarily provide convenience to the immediate neighborhood. While much of the area is already developed with commercial area, there is a need to create a better transition between the commercial uses facing North 1<sup>st</sup> Street and the side streets to the residential areas on the east side of North 2<sup>nd</sup> Street.

## **B. Rail District**

The Rail District is the area bounded by South Avenue and the railroad tracks to the north, Riverside Parkway and C-1/2 Road along the west and south and 28 Road on the east. The man-made framework of the area consists of the buildings and infrastructure that already exist, some of which is very old, and some of which was recently constructed. The railroad tracks along the northern portion of the Rail District as well as the sidings that come into the area are very important considerations. Several spurs continue to



be heavily used by the industry in the area, while others have been abandoned. The former Grand Junction Steel site, located in the center of the Rail District historically received three to four rail cars per week of raw material and finished products were then trucked from the site. Castings, Inc. handles approximately 480,000 pounds of materials and products in containers by rail each week. Thus, as in the past and as long as it continues to be used as a means of transport, the railroad is a valuable asset to this area and to the community as a whole.



*Existing Sidings in the Rail District*

The existing street network in the Rail District is incomplete. The plan for the area must address how the existing streets should be used and how new streets should be planned in the eastern and western ends of the District to continue to provide and enhance access for many modes of transportation to, from and through the area. Presently, 9<sup>th</sup>, 12<sup>th</sup> and 15<sup>th</sup> Streets are the primary north-south streets utilized by the heavy commercial and industrial uses in the area. 7<sup>th</sup> Street is generally perceived by the community as the “public” access to and from the Rail District and points south. Certainly, the completion of the Riverside Parkway through and along the southern edge of the Rail District has had a major impact on the area with a positive influence.

Part of the existing conditions of the built environment is the pattern of land ownership and use. In the central part of the Rail District there are numerous small parcels. Some have been aggregated into large holdings such as for the larger industries in the area including the former Grand Junction Steel site, Whitewater Building Materials Castings, Inc. and Munroe Pump. The railroad has large landholdings in the area as do various public entities. City-owned properties primarily include remnant parcels that were acquired for construction of the Parkway. Mesa County recently disposed of several small parcels and the State of Colorado has the Department of Transportation complex on the east side of 9<sup>th</sup> Street and D Road. To the east, the parcels are larger but some are not configured very conducive to development. There are also still remnants of the early days of this area. There are some remaining pockets of occupied single family residential homes.

There are also a few isolated commercial structures with historic significance, most notably the one pictured below which is a remnant of a sugar beet factory complex. The building was previously most visible from the riverfront trail and Orchard Mesa but it is now very visible from the Riverside Parkway. As this area becomes more familiar to people passing through on the trail and on the Parkway, perhaps some of the buildings like this one can become a more integral part of redevelopment.



*Existing Single Family Residence*

It is estimated that there is a daytime population of over 1,000 employees in the Rail District that have very little available in the way of goods, services, restaurants and other commercial uses in close proximity. There appears to be a need and a desire to promote and develop uses that could not only service the daytime working population but also support the recreational and park users in evenings and on weekends.



*Remnant Building from Sugar Beet Factory*



*Existing Business on South 7<sup>th</sup> Street*

There are surrounding influences that impact the Rail District such as the proximity of the area to the Downtown District. This plan should consider the main entrances and connections to the other Districts within the Greater Downtown area and how they can be improved as well as determine if or where there are barriers such as the railroad crossing and how those can be mitigated.

An inventory of existing land uses within the Rail District was completed as part of the planning process, identifying patterns of development and architectural character. Existing land use in the more developed central area of the District is and has historically been primarily heavy commercial and industrial with remnant pockets of residential. The area between 5<sup>th</sup> Street and 9<sup>th</sup> Street is characterized by smaller parcels with older structures. A few businesses have aggregated parcels into larger parcels which are more conducive to future improvement and/or redevelopment. This area presents the best opportunities within South Downtown for redevelopment that could be accomplished through creative incentives and partnerships.

The heavy industries are primarily located between 9<sup>th</sup> and 15<sup>th</sup> Streets, clustered around the existing railroad spurs on larger parcels. Since the rail and its users are valuable assets to the area and the community as a whole, the core of this area is unlikely to change. However, there appears to be some opportunity and community support to create transitional areas of varied land uses in a tier surrounding the core industrial area. These transitional areas can be used to create compatibility between adjacent uses such as the park and the heavy industrial as well as help visually screen the industrial areas.



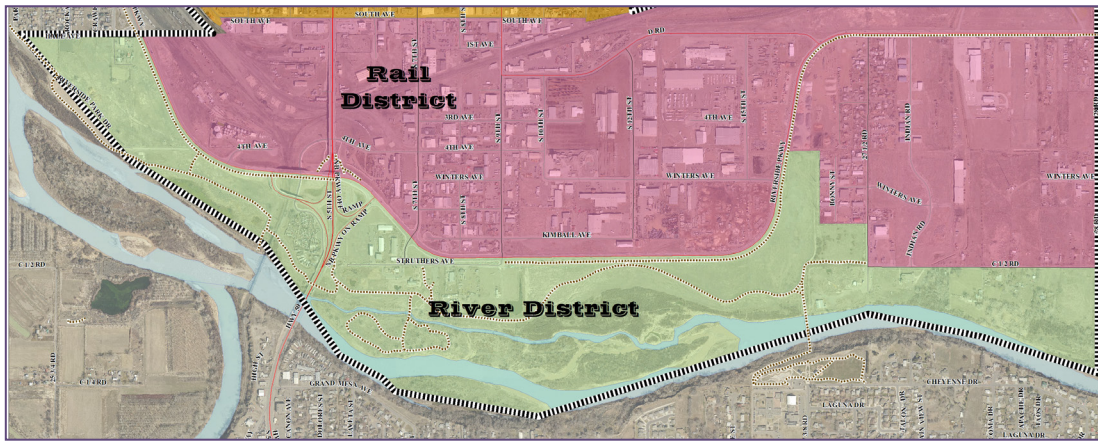
*Existing Heavy Industrial Uses*

On the eastern side of the Rail District, much of the property is held in larger ownerships and is vacant or underutilized. There is also a small pocket of existing residential use along 27-1/2 Road just south of the Riverside Parkway but which have been zoned for future commercial/industrial uses for many years. Much of the eastern area presents the greatest opportunity for increasing heavy commercial and industrial use within the Rail District and the Greater Downtown area as a whole.

### **C. River District**

The River District is located on either side of the confluence of the Gunnison and Colorado Rivers, roughly between the Riverside neighborhood to the northwest to 28 Road on the east and the Riverside Parkway to the Colorado River. At this “grand junction”, the area transformed from an agricultural based community into the commercial and industrial area it is today. Over time, the area has been used to store mill tailings along the river, process sugar beets in the historic beet packing complex, and more recently the community has rediscovered its natural value for green space, trails and public uses such as the Botanic Gardens. The majority of the property in the River District is publicly owned including the Las Colonias Park site, the Botanic Gardens property, the Jarvis property, some remnants of land that were acquired for construction of the Parkway and State Parks properties on the east end of the District.

There is an existing trail through the area that must be recognized as the area develops and redevelops. The plan must consider that the trail will eventually be extended to the east along the river and that improved public access to the trail system throughout the area is a necessity.



Located within walking distance from the other downtown areas, the River District offers both easy access to recreational amenities along the river, as well as convenient access to shopping and businesses in the adjacent downtown core. It also functions as a gateway into downtown from Highway 50. Its location and context establishes it as one of the most important places in the City.

The River District is a critical area of the community. It virtually is THE Grand Junction. The Colorado River has a big influence on the area which presents both constraints and opportunities. The floodplain associated with the River has been altered by construction of a levee that protects a large part of the Railroad-River District from flood inundation but there are some areas that are still impacted by potential flooding of the River.



*Botanical Gardens*

On the other hand, the River does present excellent opportunities to maintain and enhance amenities that have already been placed along the River including the Botanic Gardens, the Riverfront trail system, the Old Mill pedestrian bridge and the community investment of the Riverside Parkway

The proximity of the area to Orchard Mesa influences the plan both physically and visually. There are also recreation and open space uses within and nearby – Eagle Rim Park on Orchard Mesa and the Botanic Gardens along the River. The Greater Downtown Plan should address how these spaces should be connected, enhanced and integrated into redevelopment of the area.

The topography of the site is also an important consideration. While the Railroad-River District area itself is flat, it is significantly lower than Orchard Mesa to the south. This makes it a very visible area as well as presents some unique opportunities for views and vistas.

In addition, the plan must consider planning efforts that have been completed for areas within the River District including the Botanical Gardens, Las Colonias Park and the City-owned Jarvis property. The Greater Downtown Plan must also integrate with the adjacent uses to the east that were included in the Pear Park Neighborhood Plan.



*Views of the River District from Eagle Rim Park*

Several design concepts have been developed for the Las Colonias Park site which lies south of the Riverside Parkway from 9<sup>th</sup> Street to 27 Road. A trail has been constructed through the property and now that the Parkway has been in use, there is renewed interest in developing a more specific plan. Some amenities being considered are parking, restrooms, shelters, play area, open space, expanded walk/bike trails, open space for festivals, music, and educational presentations, various wetlands and arbor education opportunities, a kayak park, tree walk and a disc golf course.

The Jarvis property was historically used as a salvage yard until purchased and cleaned up by the City in the late 1980s. Since then, the property has been vacant with the exception of the recent trail construction through the site. Other uses on the west end of the South Downtown Neighborhood are industrial along the south side of the railroad tracks. A mixed use conceptual plan has been developed for the Jarvis property which was considered through the development of the Greater Downtown Plan.



*Views of the Rail and River District from Eagle Rim Park*

There are areas of low density residential uses along the south side of C-1/2 Road just west of 28 Road. These uses are intended to remain, along with future development of the riverfront trail system from 27-1/2 Road to 28 Road. The Colorado State Parks has acquired several properties in this area in anticipation of continued trail development.

## **5. GREATER DOWNTOWN GOALS AND POLICIES**

City staff held a series of meetings with property owners, including elected City officials, representatives of large industries, economic redevelopment interests and owners of small businesses and properties. The meetings were conducted as informal brainstorming sessions in order to define more specific issues,



constraints and opportunities and continue to discuss the community's vision for the Greater Downtown area. A more detailed outlined of the public planning process is included as Appendix E.

In addition to these meetings, two public open houses were held (December 2011 and February 2012) and questionnaires and information were available on the City's web site for several weeks to invite similar input from other property owners, tenants and citizens at large. Participants were asked to evaluate various design and planning concepts to determine which of these are most important to the community and should be addressed in greater detail in the Plan. The concepts addressed four major topics relative to an area plan: land use, circulation, economic (re)development and visual character. The results of the evaluations and comments gathered on the concepts were tabulated as included in Appendix E. The results, along with previous information for the CBD, show strong community support for ideas that are translated to the goals and policies listed below for the Greater Downtown Plan.

### **A. Area-Wide Goals and Policies**

**Goal 1:** Enhance the transportation system to accommodate automobiles, bikes and pedestrians, and provide adequate, convenient parking.

**Policy 1a:** Street design will accommodate travel lanes, parking, bike lanes, medians, sidewalks, and street trees, appropriate to and complementary of the adjacent land use.

**Policy 1b:** Street design will achieve a balance between travel mobility, land use access and livability and improve connections to the Greater Downtown area and the connections between sub-districts.

**Policy 1c:** Emphasize "walkability" of the Downtown area through street design that is pedestrian friendly to provide a foundation for a safe, active and livable area, including sidewalks, accessibility improvements, bicycle facilities, off-street trail connections and safe crossings, where appropriate.

**Goal 2:** Establish and improve entry points into the Greater Downtown area (refer to the Wayfinding and Signage Map on page 35).

**Policy 2a:** Street and streetscape design will include signage, landscaping and other design elements to delineate appropriate entry points into Greater Downtown.

**Goal 3:** Promote downtown living by providing a wide range of housing opportunities, primarily in the Downtown District.

**Policy 3a:** Support a regional housing strategy with an emphasis n infill, downtown housing.

**Policy 3b:** Promote development patterns and regulations that accommodate vertical mixed-use development, primarily in the Central Business District (CBD).

**Goal 4:** Redefine the land use along key corridors to provide a mix that will offer the most opportunities for redevelopment and revitalization.

**Policy 4a:** Define sub-districts and corridor areas for groupings of land uses that are complimentary to the rest of the Greater Downtown area (refer to the Subareas Map on page 7).

**Policy 4b:** Mixed uses, including residential will be encouraged in appropriate subareas and corridors.

## **B. Downtown District Goals and Policies**

**Goal 1:** Maintain and enhance the economic, cultural and social vitality of the Downtown District.

**Policy 1a:** Define sub-districts and corridor areas for groupings of land uses that are complimentary to the rest of the Greater Downtown area (refer to the Subareas Map on page 7).

**Policy 1b:** Implement infill and redevelopment policies that support downtown.

**Policy 1c:** Encourage a wide mix of uses, offering retail and commercial services at ground level and business/office/residential on upper floors in all but residential areas.

**Policy 1d:** Maintain and expand public amenities and services in the Downtown District.

**Policy 1e:** Enhance and preserve Whitman and Emerson Parks to integrate the space into the downtown fabric and encourage use by the community.

**Policy 1f:** The City and Downtown Development Authority will explore the alternative street configuration to relocate the one-way couplet of streets that are currently Ute and Pitkin Avenues to utilize Pitkin and South Avenues for this purpose.

**Policy 1g:** The study of the one-way couplet will include an analysis of alternatives for 4<sup>th</sup> and 5<sup>th</sup> Streets including returning these streets to the 2-way grid system between Ute Avenue and North Avenue.

**Policy 1h:** Minimize surface parking in the CBD and develop new means of paying for shared parking (e.g. develop a fee in lieu of required on-site parking that will be used to fund shared parking structures).

**Goal 2:** Require density/intensity in downtown as prescribed by the *Comprehensive Plan*, primarily within the Central Business District (CBD).

**Policy 2a:** Strengthen means of implementation that promote vertically mixed-use structures, primarily within the CBD.

**Policy 2b:** Require minimums in height and density/intensity for new development in the CBD.

**Policy 2c:** Require minimal or no building setbacks within the CBD to maximize site intensity/density.

**Goal 3:** Develop a pedestrian-oriented, walkable downtown.

**Policy 3a:** Prohibit uses on ground level that do not support pedestrian activity.

**Policy 3b:** Require building façade details that activate the ground floor, particularly on corner buildings to activate north-south streets.

**Policy 3c:** Within the CBD, encourage shared parking and discourage single-use, surface parking.

**Goal 4:** Stabilize and enhance the historic residential neighborhoods.

**Policy 4a:** Discourage further encroachment of non-residential uses into the established residential neighborhoods.

**Policy 4b:** Establish design standards to address conservation and enhancement of the residential development patterns and streetscape.

**Policy 4c:** Establish design standards for the transitional areas to emphasize use and development compatibility with adjacent residential areas.

**Policy 4d:** Promote the establishment of neighborhood watch and neighborhood organizations.

**Goal 5:** Recognize and promote opportunities to build sub-districts/neighborhoods, each with a unique identity.

**Policy 5a:** Develop a set of guidelines within each sub-district to address building and façade design, streetscape, landscape and other elements of site development.

**Goal 6:** Jump-start the revitalization and reinvestment in the Downtown District with strategic catalyst projects.

**Policy 6a:** Plan and budget for strategic property acquisition for future development.

**Policy 6b:** identify locations for and promote concepts of catalyst projects, including public building/housing/mixed use, live/work units, mixed-use retail/residential and mixed-use retail/office.

### **C. Rail District Goals and Policies**

**Goal 1:** Preserve the opportunity for heavy industry and rail service that supports it.

**Policy 1a:** The City will maintain industrial zoning in those areas served by rail lines and sidings.

**Goal 2:** Recognize distinction between “industrial” streets such as 9<sup>th</sup> and 12<sup>th</sup> Streets and “public” streets 7<sup>th</sup> Street and Riverside Parkway.

**Policy 2a:** Develop street sections that reflect the differences in development patterns along and the use of the street.

**Policy 2b:** In as much as possible, encourage traffic generated from the eastern area of the Rail District to travel north and east rather than east through the low density residential areas.

**Goal 3:** Promote higher quality, customer and pedestrian friendly development along 7<sup>th</sup> Street and Riverside Parkway.

**Policy 3a:** Implement design guidelines and standards along corridors that will result in site and building design improvements along the corridors.

**Goal 4:** Re-establish and improve a street grid in the Rail District.

**Policy 5a:** The City will implement a Circulation Plan in the Rail District through construction of new streets as development occurs in the Industrial and Commercial/Industrial areas.

#### **D. River District Goals and Policies**

**Goal 1:** Create/maintain/enhance a green waterfront

**Policy 1a:** Take advantage of and create opportunities and partnerships to enhance the riverfront trail system.

**Policy 1b:** Take advantage of and create opportunities and partnerships to develop Las Colonias Park and open space areas within the Jarvis property.

**Goal 2:** Create retail, general commercial and mixed use opportunities that complement the riverfront use.

**Policy 2a:** Utilize zoning, overlay districts and incentives for development and redevelopment complimentary uses.

**Goal 3:** Create/enhance redevelopment opportunities and partnerships

**Policy 3a:** The City will work with the Downtown Development Authority (DDA) to expand its boundaries.

**Policy 3b:** The City will consider implementation of incentive strategies for redevelopment.

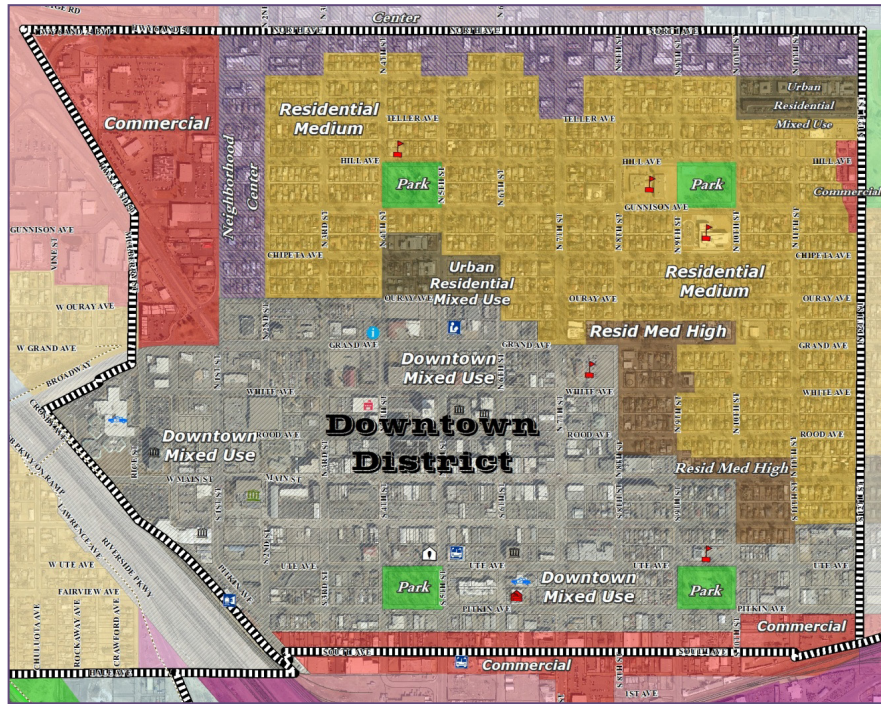
**Policy 3c:** The City will consider redevelopment opportunities for the Jarvis property including the potential for public-private partnerships.

#### **6. IMPLEMENTATION STRATEGIES**

The City has a variety of tools available through which these goals can be implemented so that the vision for Greater Downtown can materialize and eventually be realized. This Plan represents the first phase of implementation and includes the basic implementation strategies of designating Future Land Use categories, zoning properties accordingly as needed, amending development standards of the zoning districts through a zoning overlay, creating a circulation plan and establishing goals and policies for future phases of plan implementation such as economic (re)development strategies.

#### **A. Downtown District Future Land Use and Zoning**

**Future Land Use.** The Greater Downtown Plan within the Downtown District is formulated around seven general land use categories: Commercial, Downtown Mixed Use, Neighborhood Center Mixed Use, Urban Residential Mixed Use, Residential Medium High Density, Residential Medium Density and Parks and Open Space. These categories correspond with those established in the City-County *Comprehensive Plan* and are illustrated on the map below. The future Land Use Plan for the Downtown District is shown below and the general categories are more specifically described on the following page.



**Greater Downtown Future Land Use Categories**

Neighborhood Center (NCMU)	Commercial (COM)	Residential Med High (RMH)	Conservation (CON)
Downtown Mixed Use	Industrial (IND)	Residential High Mixed Use or Resid. Med High	Urban Residential Mixed Use (URMU)
Business Park Mixed Use	Commercial Industrial (CI)	Park (PK)	

*Downtown District Future Land Use Plan*

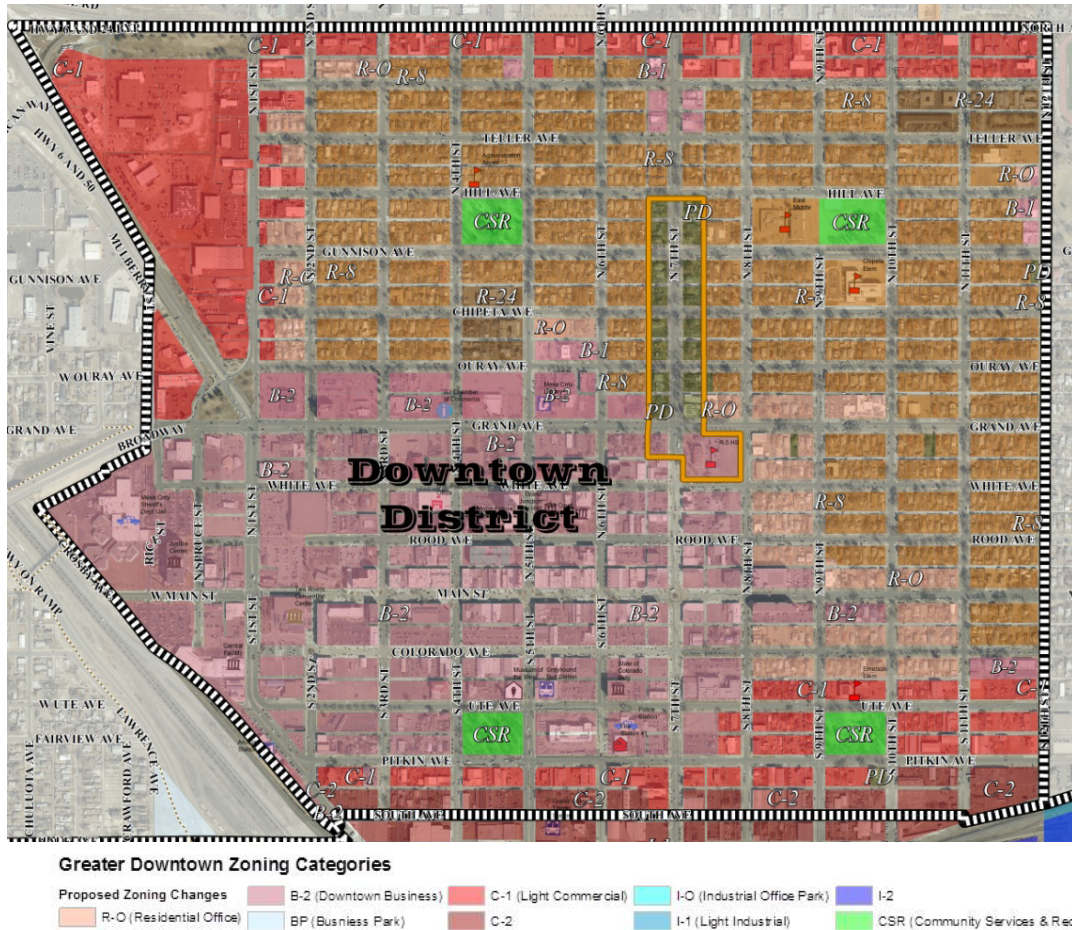
- **Commercial (C)** – Permits a wide range of commercial development including office, retail, service, lodging and entertainment with outdoor storage or operations allowed in some locations. Mixed commercial and residential developments may be encouraged in some areas. Commercial areas in the Downtown District include the areas west of 1<sup>st</sup> Street north of Grand Avenue and the north side of the South Avenue corridor.
- **Downtown Mixed Use (DMU)** – Permits employment, residential, retail and office/business park uses. A mix of uses (2 to 8 stories), either horizontal or vertical is expected. Residential densities may exceed 24 units per acre.
- **Parks and Open Space (Park)** – Public or private lands reserved for active park and recreation sites, open space, wildlife habitat, sensitive or hazardous land protection, and other

environmental conservation purposes. In the Downtown District, the four original parks are designated Park.

- **Neighborhood Center Mixed Use (NCMU)** – Mixed use development to include employment, residential open space, retail commercial may be appropriate as a secondary use, integral to other uses and structures or as small village centers. These areas are focused on the North 1<sup>st</sup> Street and North Avenue corridors.
- **Urban Residential Mixed Use (URMU)** – Permits residential development with minor amounts of service-oriented and retail commercial use. Non-residential uses are not intended for more than 10% of a development. These areas of the Downtown District are located where there is an existing cluster of higher density residential uses or where property has been aggregated that could be redeveloped for similar types of development.
- **Residential Medium High Density (RMH)** – Permits a mix of residential development types with gross densities of 8 to 16 dwelling units per acre. Duplexes, manufactured home subdivisions and low intensity attached residential development will be integrated with townhomes and low-intensity, multifamily development. Larger multifamily development and/or small neighborhood retail/service may be permitted. In the Downtown District, these areas provide a transition on the northeastern perimeter of the central business district to the single family development of the established downtown neighborhoods.
- **Residential Medium Density (RM)** – Permits a mix of residential development types with gross densities of 4 to 8 dwelling units per acre. Single family development may be integrated with other dwelling types, including duplexes and low intensity attached residential development. The established neighborhoods within the Downtown District are designated RM.

**Zoning.** Proposed zoning within the Downtown District is shown on the map on the following page. Generally, the zoning is proposed to remain the same as currently exists. The zone districts within the Downtown District are described on the following pages.

- **Residential 8 units per acre (R-8)** – Provides for medium-high density attached and detached dwellings, two-family dwelling and multifamily. R-8 is a transitional district between lower density single-family districts and higher density multifamily or business development. A mix of dwelling types is allowed in this district.



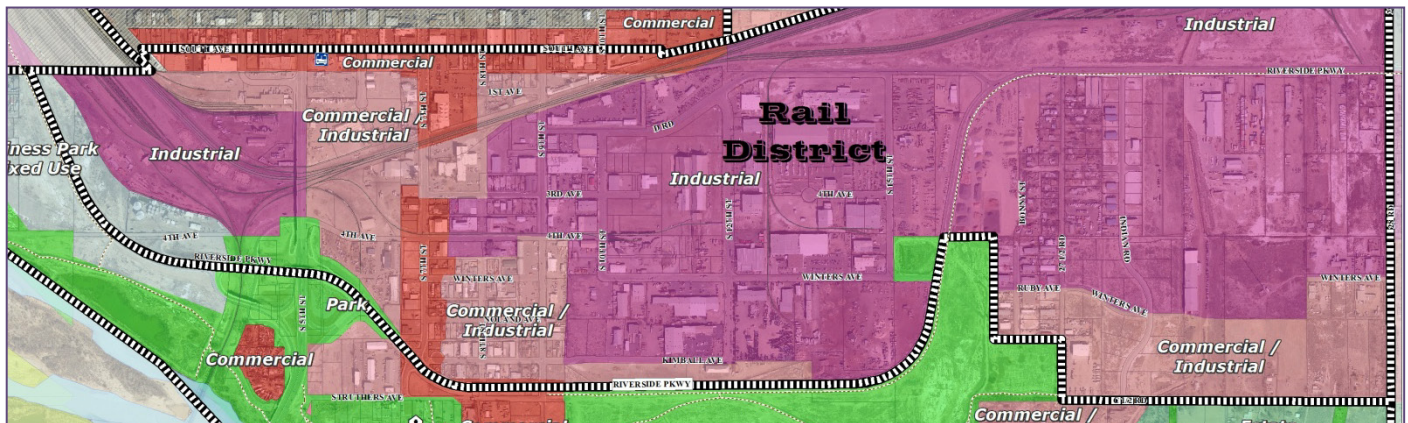
Downtown District Zoning Map

- Residential 24 units per acre (R-24)** – Provides for high density residential use. This district allows multifamily development within specified densities. R-24 may serve as a transitional district between single-family and trade zones. This district is intended to allow high density residential unit types and densities to provide a balance of housing opportunities in the community.
- Residential Office (R-O)** – Provides low intensity, nonretail, neighborhood service and office uses that are compatible with adjacent residential neighborhoods. Development regulations and performance standards are intended to make buildings compatible and complementary in scale and appearance to a residential environment.
- Neighborhood Business (B-1)** – Provides small areas for office and professional services combined with limited retail uses, designed in scale with surrounding residential uses; a balance of residential and nonresidential uses.
- Downtown Business (B-2)** – Provides concentrated downtown retail, service, office and mixed uses not including major/regional shopping centers or large outdoor sales areas. The B-2 district promotes the vitality of the downtown area as provided by the Comprehensive Plan. Pedestrian circulation is encouraged as are common parking area.

- **Light Commercial (C-1)** – Provides indoor retail, service and office uses requiring direct or indirect arterial street access, and business and commercial development along arterials. The C-1 district should accommodate well-designed development on sites that provide excellent transportation access, make the most efficient use of existing infrastructure and provide for orderly transitions and buffers between uses.
- **General Commercial (C-2)** – provides for commercial activities such as repair shops, wholesale businesses, warehousing and retail sales with limited outdoor display of goods and outdoor operations.
- **Community Services and Recreation (CSR)** – Provides public and private recreational facilities, schools, fire stations, libraries, fairgrounds, and other public-institutional uses and facilities. The district includes open space areas to prevent environmental damage to sensitive areas and to limit development in areas where police or fire protection, flood protection, or other services or utilities are not readily available.

## B. Rail District Future Land Use and Zoning

**Future Land Use.** The Greater Downtown Plan within the Rail District is formulated around five general land use categories: Business Park Mixed Use, Commercial, Commercial industrial, Industrial and Parks and Open Space. These categories correspond with those established in the City-County *Comprehensive Plan*. The future Land Use Plan for the Downtown District is shown below and the general categories are more specifically described on the following page.



### Greater Downtown Future Land Use Categories

Neighborhood Center (NCMU)	Commercial (COM)	Residential Med High (RMH)	Conservation (CON)
Downtown Mixed Use	Industrial (IND)	Residential High Mixed Use or Resid. Med High	Urban Residential Mixed Use (URMU)
Business Park Mixed Use	Commercial Industrial (CI)	Park (PK)	

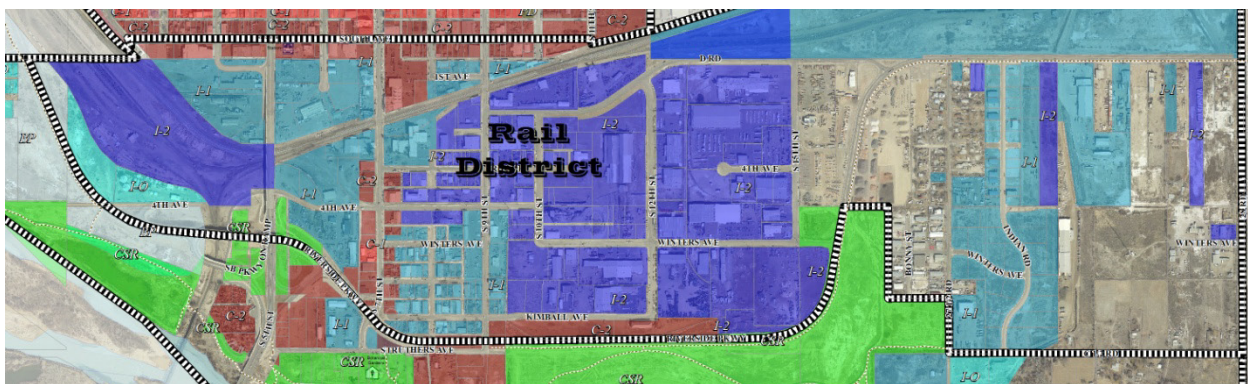
*Rail District Future Land Use Plan*

- **Business Park Mixed Use (BPMU)** – Permits business, light industrial, employment-oriented areas with the allowance of multifamily development. Within the Rail District, this land use category applies to the City-owned property along Riverside Parkway west of 5<sup>th</sup> Street.



- **Commercial (C)** – Permits a wide range of commercial development including office, retail, service, lodging and entertainment with outdoor storage or operations allowed in some locations. Mixed commercial and residential developments may be encouraged in some areas. Commercial areas in the Rail District are focused on the primary corridors of 5<sup>th</sup> Street, 7<sup>th</sup> Street, South Avenue and the north side of the Riverside Parkway between 7<sup>th</sup> and 12<sup>th</sup> Streets.
- **Commercial Industrial (CI)** – Permits heavy commercial, offices and light industrial uses with outdoor storage, with some outdoor operations. Yard operations may be permitted where adequate screening and buffering can be provided to ensure compatibility with existing and planned development in the vicinity of the proposed use. Residential uses are limited. Within the Rail District, Commercial Industrial land use provides transitional areas between Commercial and Industrial areas.
- **Industrial (I)** – Predominant uses are heavy commercial and industrial operations. Residential uses are not appropriate. Industrial areas in the Rail District are focused on existing infrastructure including rail and roadway corridors.
- **Parks and Open Space (Park)** – Public or private lands reserved for active park and recreation sites, open space, wildlife habitat, sensitive or hazardous land protection, and other environmental conservation purposes. In the Rail District, the Park areas correspond with City-owned property along 5<sup>th</sup> Street and the Riverside Parkway.

**Zoning.** Proposed zoning within the Rail District is shown on the map below. The majority of the zoning is proposed to remain the same as currently exists. The zone districts within the Rail District are described on the following pages.



**Greater Downtown Zoning Categories**

Proposed Zoning Changes	B-2 (Downtown Business)	C-1 (Light Commercial)	I-O (Industrial Office Park)	I-2
R-O (Residential Office)	BP (Business Park)	C-2	I-1 (Light Industrial)	CSR (Community Services & Rec)

*Rail District Zoning Map*

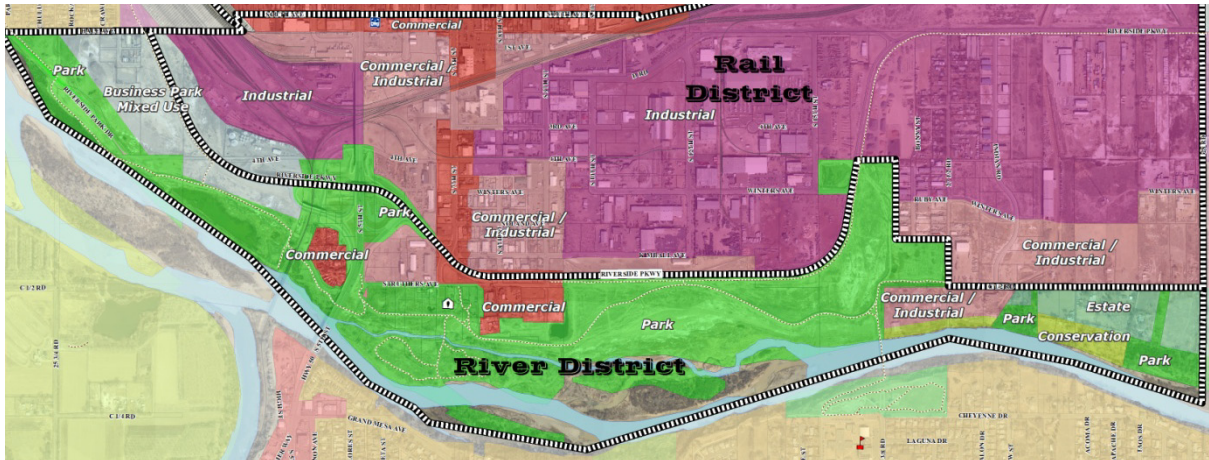
- **Light Commercial (C-1)** – Provides indoor retail, service and office uses requiring direct or indirect arterial street access, and business and commercial development along arterials. The C-1 district should accommodate well-designed development on sites that provide excellent transportation access, make the most efficient use of existing infrastructure and provide for orderly transitions and buffers between uses.

- **General Commercial (C-2)** – provides for commercial activities such as repair shops, wholesale businesses, warehousing and retail sales with limited outdoor display of goods and outdoor operations.
- **Community Services and Recreation (CSR)** – Provides public and private recreational facilities, schools, fire stations, libraries, fairgrounds, and other public-institutional uses and facilities. The district includes open space areas to prevent environmental damage to sensitive areas and to limit development in areas where police or fire protection, flood protection, or other services or utilities are not readily available.
- **Business Park Mixed Use (BP)** – Provides for a mix of light manufacturing and employment centers, limited commercial services and multifamily residential uses in a business park setting with proper screening and buffering, all compatible with adjoining uses.
- **Industrial/Office Park (I-O)** – Provides for a mix of light manufacturing uses, office park, limited retail and service uses in a business park setting with proper screening and buffering, all compatible with adjoining uses.
- **Light Industrial (I-1)** – Provides for areas of light fabrication, manufacturing and industrial uses which are compatible with existing adjacent uses, access to transportation and the availability of public services and facilities.
- **General Industrial (I-2)** – Provides areas of heavy and concentrated fabrication, manufacturing and industrial uses which are compatible with adjacent uses, easy semi-tractor trailer access to the State highway system and/or railroads and the availability of public services and facilities.

### C. River District Future Land Use and Zoning

**Future Land Use.** The Greater Downtown Plan within the River District is formulated around six general land use categories: Commercial, Commercial Industrial, Parks and Open Space, Conservation, Estate and Business Park Mixed Use. These categories correspond with those established in the City-County *Comprehensive Plan*. The following page shows the Future Land Use Plan for the River District is and includes more detailed descriptions of the general categories.

- **Business Park Mixed Use (BPMU)** – Permits business, light industrial, employment-oriented areas with the allowance of multifamily development. Within the River District, this land use category applies to the City-owned property along the river and the Riverside Parkway west of 5<sup>th</sup> Street.
- **Commercial (C)** – Permits a wide range of commercial development including office, retail, service, lodging and entertainment with outdoor storage or operations. Mixed commercial and residential developments may be encouraged in some areas. Commercial areas in the River District focus on private property along 5<sup>th</sup> Street and the south side of the Riverside Parkway east of the Botanical Gardens.



**Greater Downtown Future Land Use Categories**

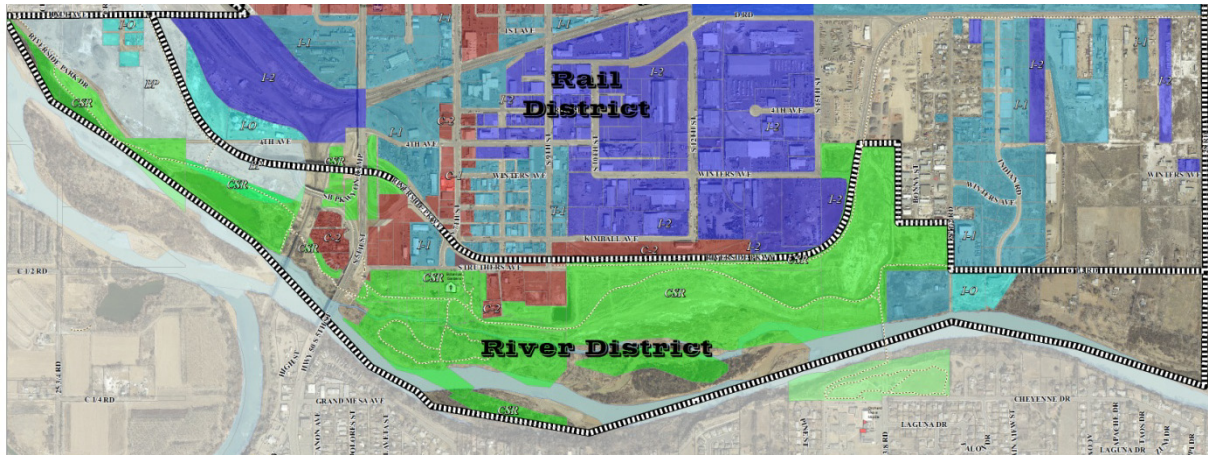
Neighborhood Center (NCMU)	Commercial (COM)	Residential Med High (RMH)	Conservation (CON)
Downtown Mixed Use	Industrial (IND)	Residential High Mixed Use or Resid. Med High	Urban Residential Mixed Use (URMU)
Business Park Mixed Use	Commercial Industrial (CI)	Park (PK)	

*River District Future Land Use Plan*

- **Commercial (C)** – Permits a wide range of commercial development including office, retail, service, lodging and entertainment with outdoor storage or operations. Mixed commercial and residential developments may be encouraged in some areas. Commercial areas in the River District focus on private property along 5<sup>th</sup> Street and the south side of the Riverside Parkway east of the Botanical Gardens.
- **Commercial Industrial (CI)** – Permits heavy commercial, offices and light industrial uses with outdoor storage, with some outdoor operations. Yard operations may be permitted where adequate screening and buffering can be provided to ensure compatibility with existing and planned development in the vicinity of the proposed use. Residential uses are limited. Within the River District, Commercial Industrial land use corresponds with areas of existing heavy commercial/light industrial zoning.
- **Parks and Open Space (Park)** – Public or private lands reserved for active park and recreation sites, open space, wildlife habitat, sensitive or hazardous land protection, and other environmental conservation purposes. In the River District, the Park areas correspond with City-owned property along 5<sup>th</sup> Street, the Riverside Parkway and the Colorado River.
- **Estate** – Typical “estate” style single family homes on large lots of 1 to 3 acres. Centralized services might be needed depending on site conditions and proximity to existing services. Uses are residential. The Estate areas in the River District are limited to the large lot single family development along the eastern end of the Colorado River.
- **Conservation** – Public or private lands reserved for open space, wildlife habitat, sensitive or hazardous land protection and other environmental or conservation purposes. The areas shown

as Conservation in the River District represent the community’s desire to continue the riverfront trail system east of 27-1/2 Road.

**Zoning.** Proposed zoning within the River District is shown on the map on the below. The majority of the zoning is proposed to remain the same as currently exists. The zone districts for properties currently within the City limits in the River District are described below and on the following pages.



**Greater Downtown Zoning Categories**

Proposed Zoning Changes	B-2 (Downtown Business)	C-1 (Light Commercial)	I-O (Industrial Office Park)	I-2
R-O (Residential Office)	BP (Business Park)	C-2	I-1 (Light Industrial)	CSR (Community Services & Rec)

*River District Zoning Map*

- **Light Commercial (C-1)** – Provides indoor retail, service and office uses requiring direct or indirect arterial street access, and business and commercial development along arterials. The C-1 district should accommodate well-designed development on sites that provide excellent transportation access, make the most efficient use of existing infrastructure and provide for orderly transitions and buffers between uses.
- **General Commercial (C-2)** – Provides for commercial activities such as repair shops, wholesale businesses, warehousing and retail sales with limited outdoor display of goods and outdoor operations.
- **Community Services and Recreation (CSR)** – Provides public and private recreational facilities, schools, fire stations, libraries, fairgrounds, and other public-institutional uses and facilities. The district includes open space areas to prevent environmental damage to sensitive areas and to limit development in areas where police or fire protection, flood protection, or other services or utilities are not readily available.
- **Business Park Mixed Use (BP)** – Provides for a mix of light manufacturing and employment centers, limited commercial services and multifamily residential uses in a business park setting with proper screening and buffering, all compatible with adjoining uses.

- **Industrial/Office Park (I-O)** – Provides for a mix of light manufacturing uses, office park, limited retail and service uses in a business park setting with proper screening and buffering, all compatible with adjoining uses.
- **Light Industrial (I-1)** – Provides for areas of light fabrication, manufacturing and industrial uses which are compatible with existing adjacent uses, access to transportation and the availability of public services and facilities.

#### **D. Development Standards**

The community desires to improve the visual character of the Greater Downtown areas that are most visible along major public corridors or from the major public spaces. Consequently, the Plan promotes a higher quality built environment through improved architectural character, reduced visual clutter and enhanced streetscape. These elements are addressed through the Greater Downtown Zoning Overlay detailed in a companion document to this Plan. The elements of the overlay are intended to augment the zoning district standards in the Zoning and Development Code.

#### **E. Greater Downtown Circulation Plan**

The Circulation Plan for Greater Downtown is shown in Appendix C. The plan identifies a street network that includes both existing and proposed streets and both major and minor streets. The Circulation Plan also identifies potential enhancements within Greater Downtown that could be implemented over a number of years. The desired cross-sections and level of streetscape development along the streets within Greater Downtown support the overall Plan goals, land uses and circulation to and from the area for pedestrian, bicycle and vehicles.

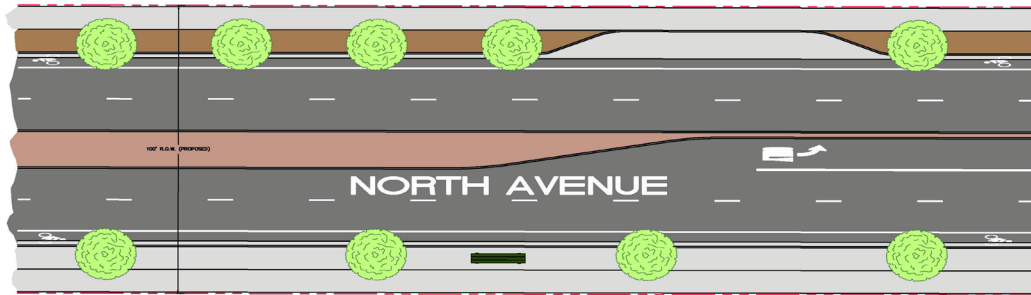
**Policy:** Conduct a more detailed traffic analysis of the area to determine needed intersection control and street cross-sections.

**Major Street Corridors.** Major streets in the Grand Junction urbanized area are classified according to their function in the transportation network. The two components of function are to provide access to properties and to carry traffic from point to point. In order to preserve safety and capacity and enhance the quality of living, the relation of these two components should be inversely proportionate, with the busier streets having limited access and the quieter streets providing access to properties. The components of the major street system have been identified on a functional classification map, known as the Grand Valley Circulation Plan that has been adopted by the City of Grand Junction and accepted by Mesa County. The Greater Downtown Circulation Plan is intended to augment the Grand Valley Circulation Plan and provide more specific guidance on multimodal circulation improvements as development and redevelopment occurs in the Greater Downtown area.

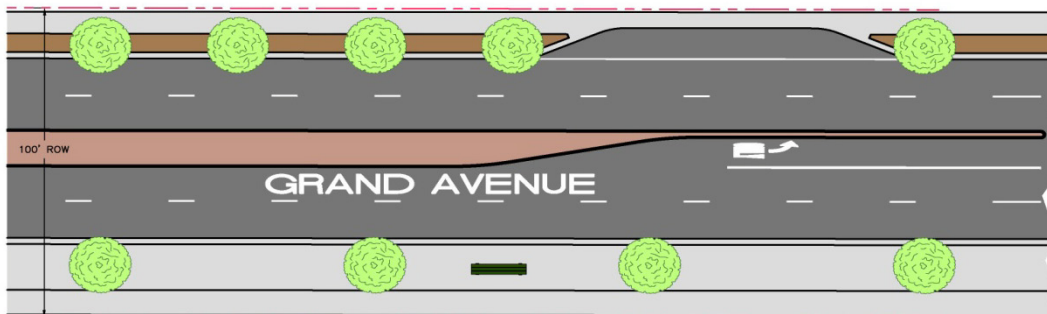
As properties develop and redevelop within Greater Downtown, the Circulation Plan will be implemented through construction or improvement of streets when warranted by the proposed development. The specific design of each street is generally based on the land use and zoning of the properties along it. For example, in areas that will be zoned Commercial, the Commercial Street cross-section will apply, unless modified by suggestions in the Greater Downtown Plan. Similarly, streets within Industrial areas are to be developed/improved according to the City's adopted Industrial street cross-section, unless modified by suggestions in this Plan. The street classifications and proposed street sections for the major corridors in

Greater Downtown are described below. The concept drawings included on the following pages further illustrate elements of the Circulation Plan.

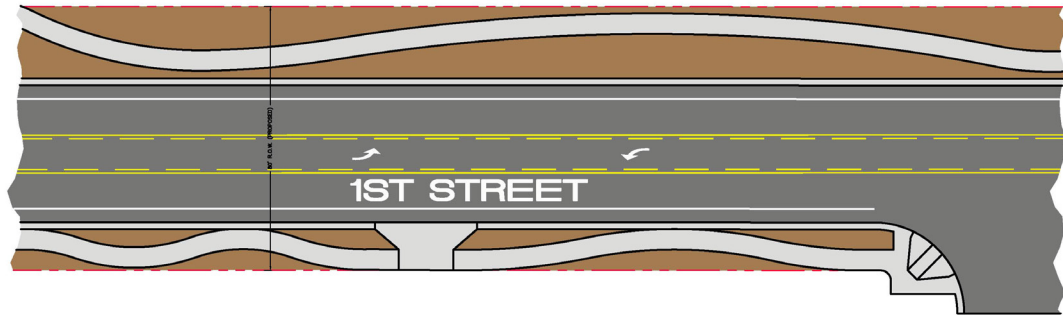
- **North Avenue** – Arterial Street. A potential layout for North Avenue between 1<sup>st</sup> and 12<sup>th</sup> Streets was adopted with the *North Avenue West Corridor Plan*. The layout is incorporated into the Greater Downtown Plan as depicted below.



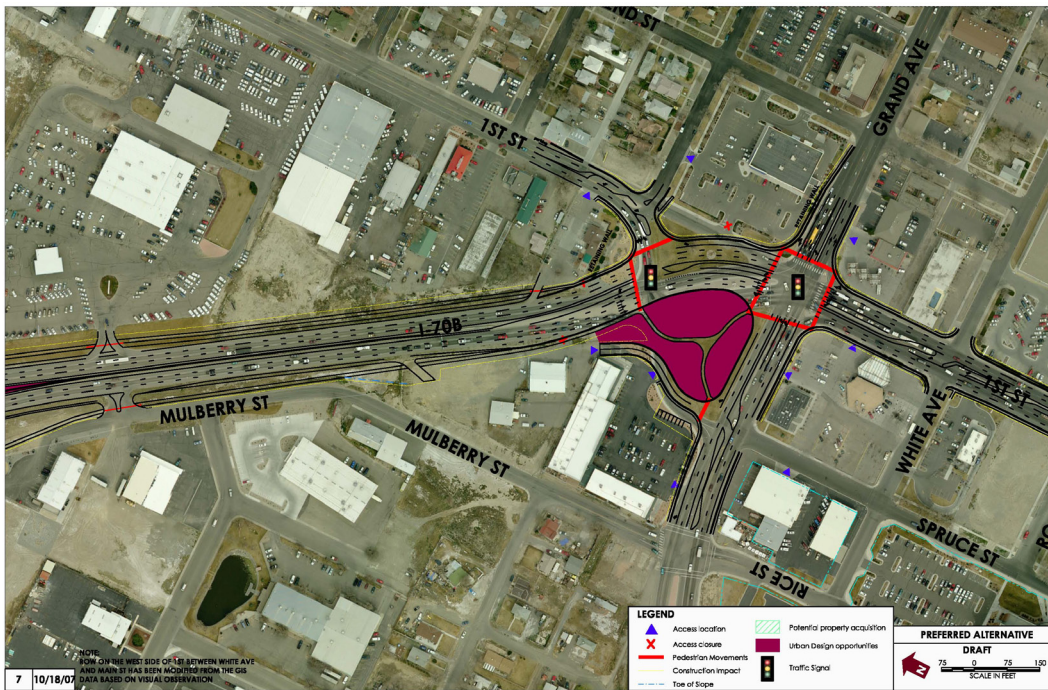
- **Grand Avenue** – Arterial Street. While Grand Avenue is an established street with improved streetscape elements the length of the segment within Greater Downtown (1<sup>st</sup> to 12<sup>th</sup> Streets), there could be improvements made in some blocks that would enhance traffic flow, pedestrian safety and visual aesthetics. A typical, potential median improvement to Grand Avenue is depicted below.



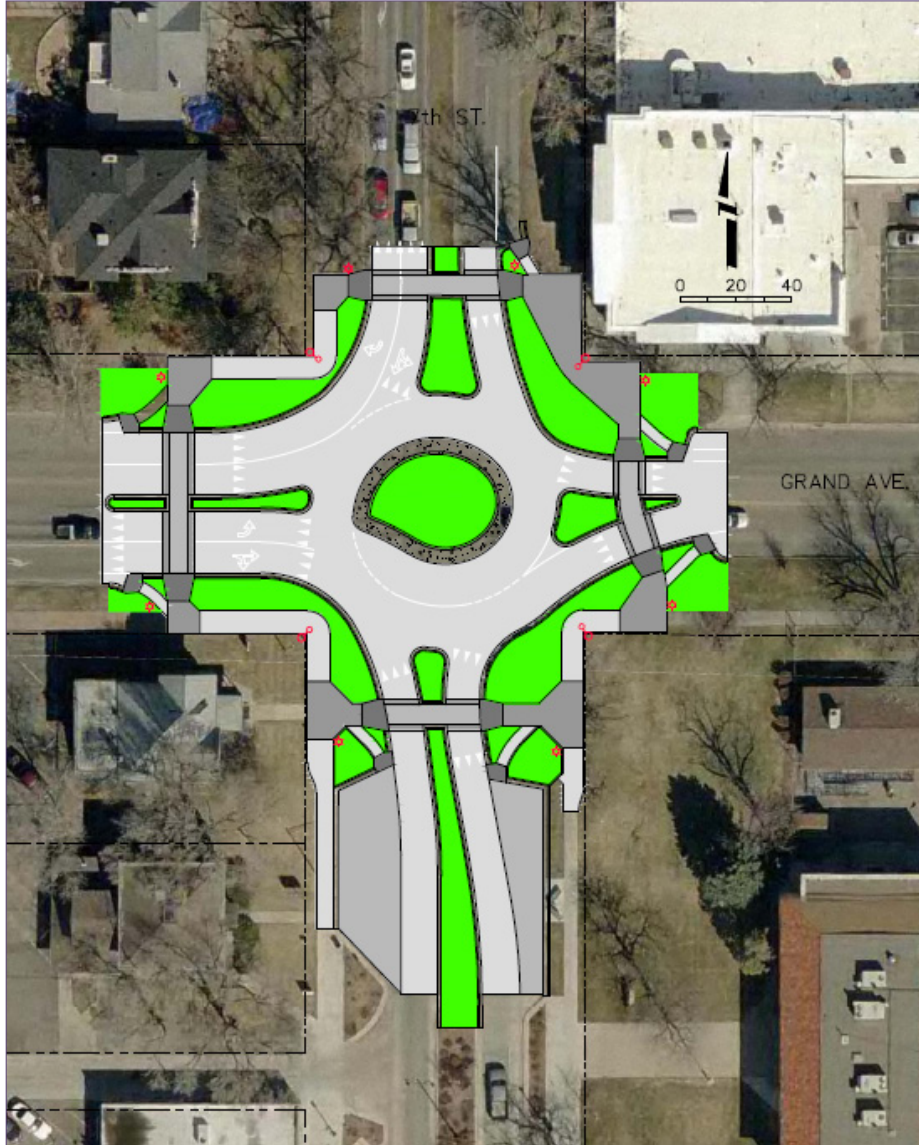
- **North 1<sup>st</sup> Street** – Arterial Street. The segment of North 1<sup>st</sup> Street from Grand Avenue to North Avenue is deficient in street design and pedestrian amenities. The Greater Downtown Plan envisions eventual redevelopment of this street to improve safety, drainage, pedestrian circulation, access and improve on-street parking. A potential layout is depicted below.



- 1<sup>st</sup> Street and Grand Avenue Intersection** – The Colorado Department of Transportation (CDOT) completed an Environmental Assessment for the I-70B/6 & 50 corridor which recognized circulation needs at the 1<sup>st</sup> and Grand intersection. A potential design is shown below. The Greater Downtown Plan incorporates these ideas as well as potential improvements on 1<sup>st</sup> Street from Grand Avenue to Pitkin Avenue.

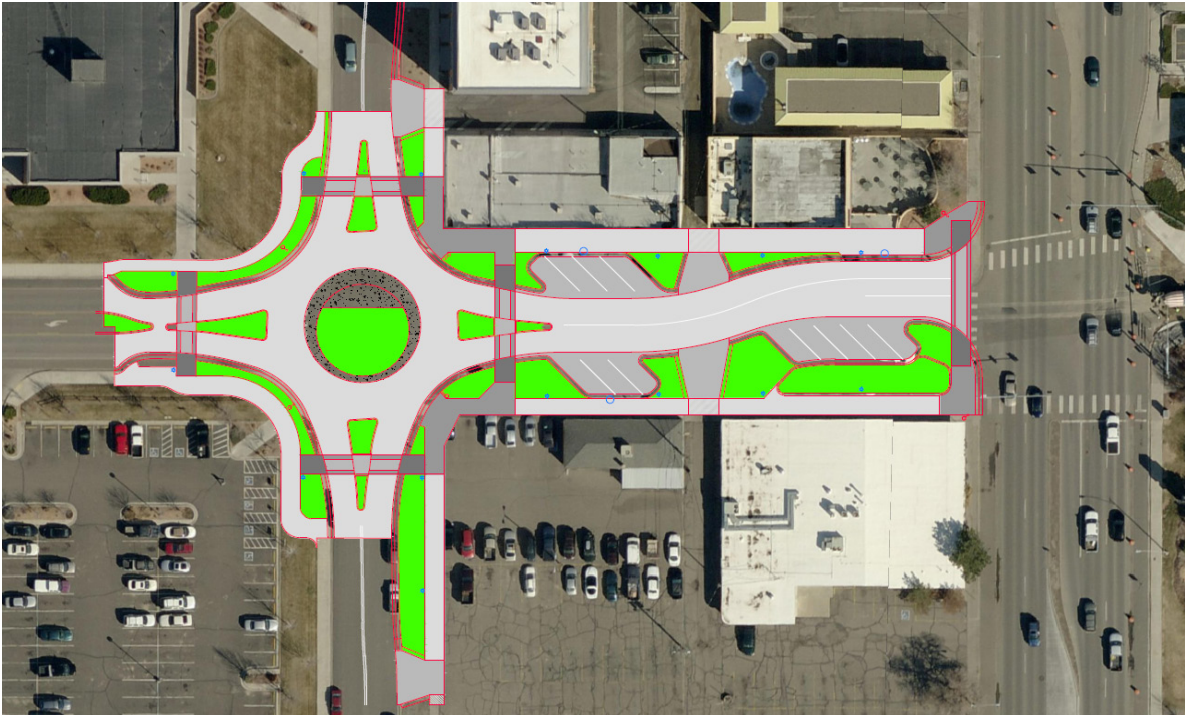


- **7<sup>th</sup> Street and Grand Avenue Intersection** – A design for a roundabout was included with the plans for 7<sup>th</sup> Street improvements that were completed in 2007-2008. The roundabout was not constructed at that time but the intersection still warrants vehicular and pedestrian improvements. The plan as originally proposed is shown below.





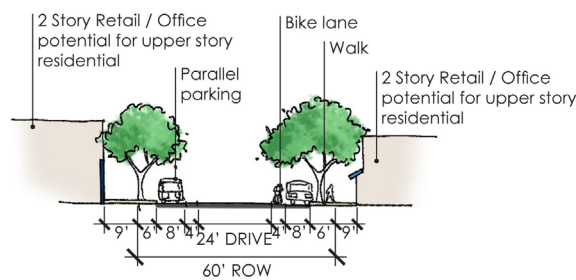
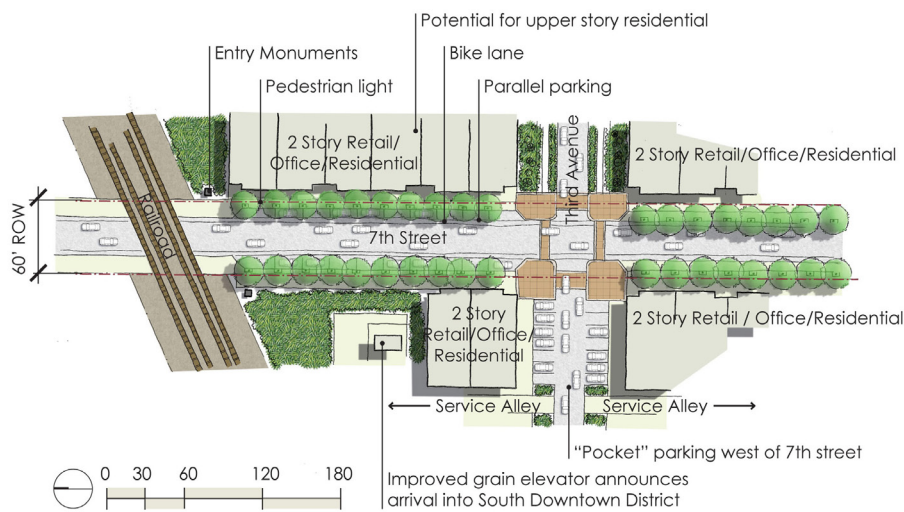
- **West Main Street and Spruce Street Intersection** – Vehicular and pedestrian traffic has increased on the West Main Street corridor due to activity at the County Justice Center and the Mesa County Central Services building. To ease traffic flow through the area and improve pedestrian safety, this potential project for a roundabout at Spruce and West Main Streets was originally conceived during the visioning of Main Street in 2008. The roundabout would serve as a visual anchor to the west end of Main Street just as the one at 7<sup>th</sup> Street serves the east end. The roundabout would also allow for a circulator bus to serve the Main Street corridor from Spruce Street to 7<sup>th</sup> Street.



- **Riverside Parkway** – Arterial Street. The right-of-way width varies; multi-lane; bike lanes; detached walk on the south side; no on-street parking. Completed in 2007.



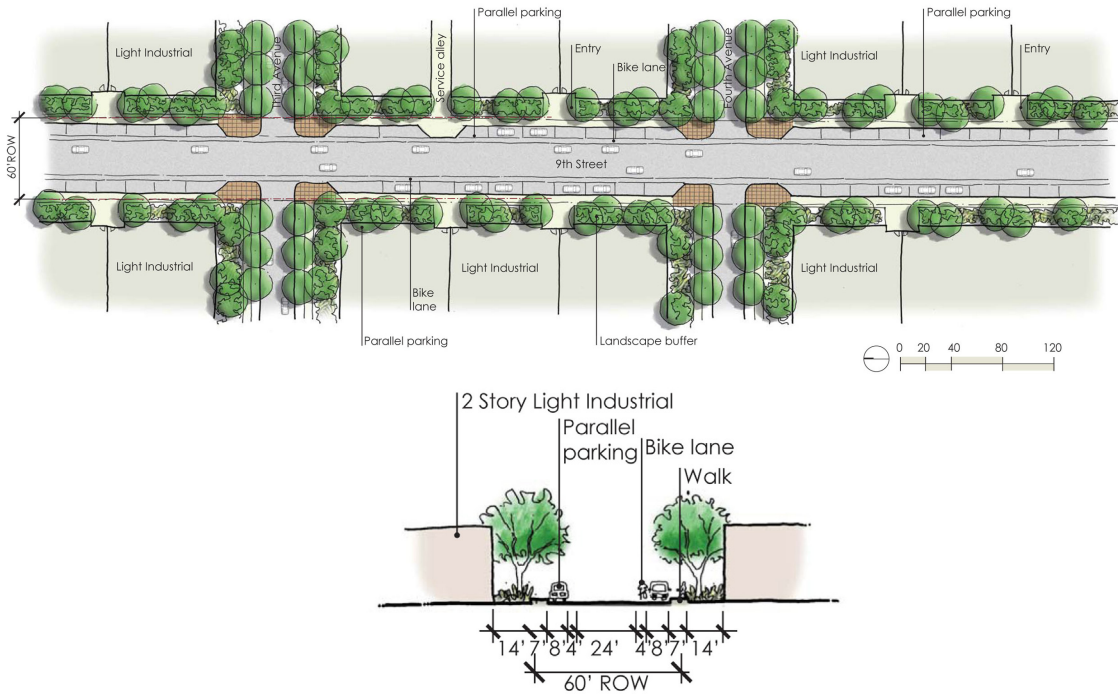
- South 7<sup>th</sup> Street** – Collector Street. 60-foot right-of-way width; 2 lanes; bike lanes; on-street parking both sides; detached walks with landscaping. The streetscape plan for South 7<sup>th</sup> Street should be enhanced with a similar design as what is currently under construction on 7<sup>th</sup> Street south of Grand Avenue. Generally, the design would continue the 7<sup>th</sup> Street boulevard treatments from downtown, Ute and Pitkin to the Botanical Gardens and riverfront area with additional street trees, historic street lights, street furniture and public art. This design would create a more consistent visual character to connect the South Downtown Neighborhood with the traditional downtown area and improves the visual design of the corridor and emphasizes its use as the primary public north-south corridor through the neighborhood. The design features enhanced pedestrian facilities with colored concrete, pedestrian safe zones at the “bulb-outs” for easier crossing and additional landscaping.



**7th Street Section**

- South 9<sup>th</sup> Street and 27-1/2 and C-1/2 Roads** – Collector Street. 60-foot right-of-way width; 2 lanes; bike lanes; on-street parking both sides; detached sidewalk preferred where possible. The streetscape plan for South 9<sup>th</sup> Street is to develop similar to what presently exists along South 7<sup>th</sup> Street with a more defined hardscape of curb and gutter, enhanced pedestrian facilities and street trees. This design improves the visual quality of the corridor without requiring improvements on private property or compromising adjacent uses. The design also allows the street to function for

the commercial/light industrial traffic that it carries as well as provides for a more comfortable pedestrian or bicycle environment since South 9<sup>th</sup> Street may be used by the public to access business/commercial areas and Las Colonias Park.



### 9th Street Section

- **Kimball Avenue** – Collector Street. 60-foot right-of-way width; 2 lanes; on-street parking on one side; detached walks with landscaping.



- **D Road (from 9<sup>th</sup> Street east to the Riverside Parkway)** – Arterial Street. Section yet to be determined.

**Local Streets.** The Local Street network provides access to individual parcels and serves short length trips to and from collector and higher order streets. Trip lengths on local streets should be short with a lower volume of traffic along with slower speeds. Design of local streets occurs through the development process and will be in accordance with the City’s adopted Transportation Engineering Design Standards (TEDS). It is important in the design process to provide connections to adjacent parcels and subdivisions for efficient vehicle travel and a safe network for pedestrians and bicycles.

**Trails.** The Grand Valley Trails Plan is a planning document that shows the location of future bicycle facilities, trails and pedestrian paths. Implicit in the plan is the construction of sidewalks in accordance with the adopted street cross-sections as previously illustrated. One of the major purposes of the City’s Urban Trails Committee is facilitating linkages from the riverfront trail system to the urban area. As development or redevelopment occurs, construction of trails, paths, bike lanes and pedestrian facilities in accordance with the adopted plan either occurs with the development or the City constructs the same with the collection of the Transportation Capacity Payment (TCP) as part of a more comprehensive capital improvement project.

The Grand Valley Trails Plan shows the following proposals within Greater Downtown.

- Extension of Riverfront Trail from Las Colonias Park east to 28 Road (and beyond).
- Bike Routes (signs but no separate lane) on Grand Avenue from 7<sup>th</sup> Street east and 28 Road between the Riverside Parkway and C-1/2 Road.
- Sharrow Route (bikes share lane with cars) on Grand Avenue between 1<sup>st</sup> and 7<sup>th</sup> Streets.

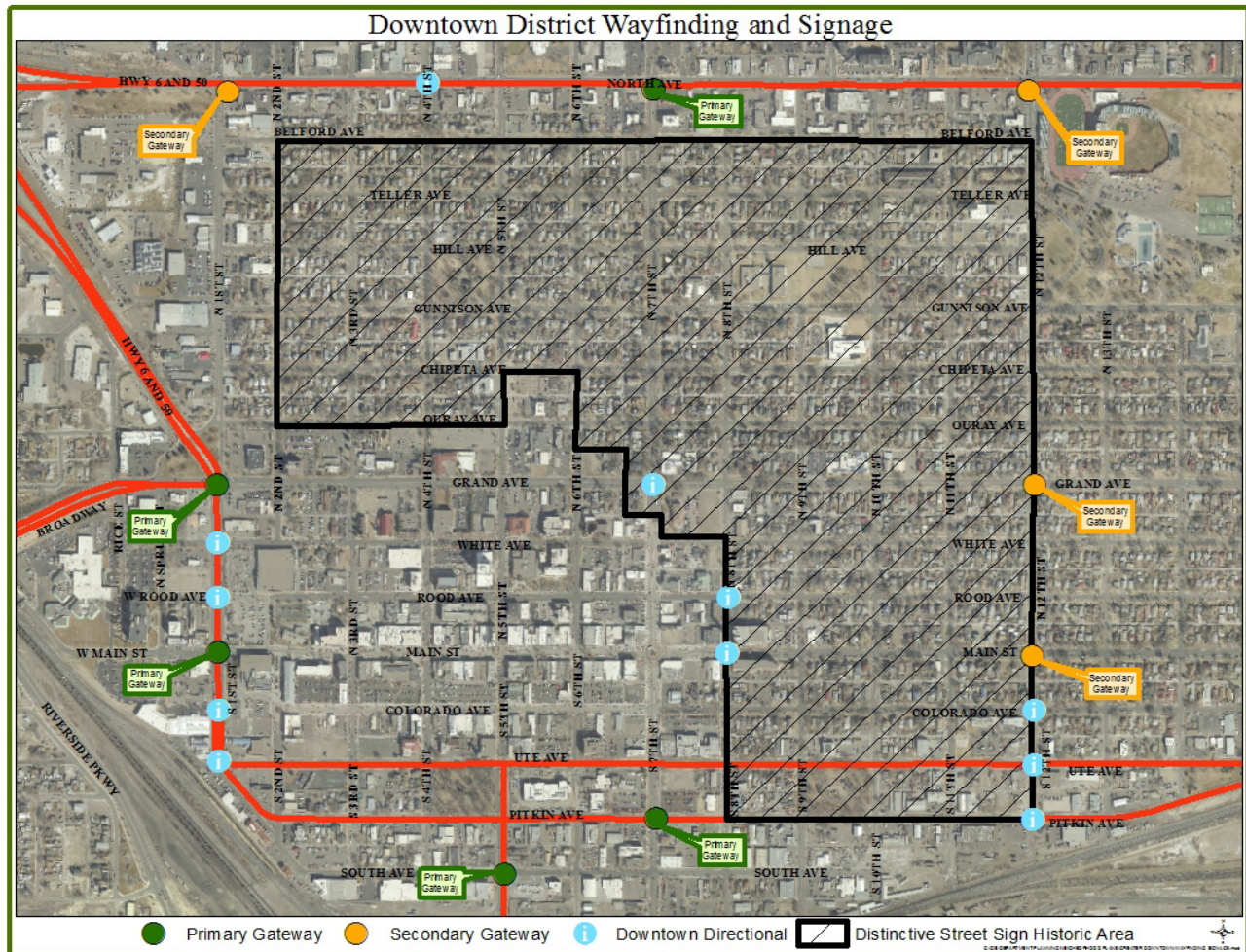
**Riverside Parkway Pedestrian Overpass.** It is envisioned that eventually there may be need for one or more pedestrian overpasses from the commercial areas of the Rail District to the riverfront areas and Las Colonias Park. Development, activities and uses in the future park and types of development along the north side of the Parkway will dictate where these may be needed based on the level of pedestrian traffic. An overpass on the western end of the area in the vicinity of 7<sup>th</sup> or 9<sup>th</sup> Street could also serve as an entrance feature to the neighborhood as further discussed in section F. below.

**Public Transportation (GVT).** Grand Valley Transit (GVT) presently serves the Downtown District but does not provide service in the Rail and River Districts. Future transit needs within the Rail and River Districts will need to be monitored as more areas are developed or redeveloped and as Las Colonias Park becomes more developed and active.

## **F. Entryways and Signage**

Important intersections in the street network offer opportunities to develop a unique theme and identity for the Greater Downtown area. The primary intersections vary in scale and include entries to the Downtown, Rail and River Districts. Each of these should be developed according to general concepts and criteria that are appropriate for their scale, function and importance.

## Downtown District Entryways and Signage



While the Downtown District has a strong base of local and regional users, approaches to the District offer no distinguishing features or directional signage that orient first time visitors. When approaching or traversing perimeter streets, visitors have no sense that they are within eyesight of the historic town site or even that they are headed in the right direction toward Downtown. Creating gateways at key entry points, as well as simple perimeter identifiers, will orient visitors, as well as strengthen and celebrate the heritage of the Downtown Districts. Because of the diversity of uses and neighborhoods within the Downtown District, the design of entryways should include a range of sizes, poles and ornamentation of signage and varied landscape elements, lighting and other features that are integrated with the signage. The primary gateways to the Downtown District and between Downtown and the Rail District include the following intersections.

- 7<sup>th</sup> Street and North Avenue
- 1<sup>st</sup> Street and Grand Avenue
- 1<sup>st</sup> Street and Main Street
- 5<sup>th</sup> Street and South Avenue
- 7<sup>th</sup> Street and Pitkin Avenue

In addition, the distinctive public sign palette already started in the Downtown District should be continued and expanded to include street signs and directional signs that have recognizable poles, ornamentation, colors, fonts and logos. The DDA will further develop sign standards and guidelines for private signage placed on buildings or as freestanding signs.

### **Rail and River District Entryways**

**5<sup>th</sup> Street/Riverside Parkway.** There are opportunities to celebrate the entry into Grand Junction and the Rail and River Districts at the 5<sup>th</sup> Street bridge and Struthers Avenue area in conjunction with the Western Colorado Botanical Gardens with attractive low scale signage and sculpture. In addition, there are smaller monuments at various points along the Riverside Parkway that indicate to motorists that they are approaching or traveling on the Riverside Parkway (shown below). Due to the scale of the 5<sup>th</sup> Street/Riverside Parkway intersection and the publicly-owned area around it, this intersection affords the opportunity to create a monument/sculpture of a much larger scale to mark the entrance and give identity to the Rail and River Districts and/or to this “Grand Junction”.

Another possibility in this vicinity is if a pedestrian overpass is desired/needed near the 7<sup>th</sup> Street/Riverside Parkway intersection it could serve several purposes: pedestrian access across the Parkway, include design elements that give a distinct character to the Rail and River Districts and integrate with surrounding open space, pocket parks and/or water features at the landings on each side of the Parkway.

**South 7<sup>th</sup> Street/Railroad Tracks.** There is an existing silo on the Mesa Feed property that identifies the Rail District as you travel south on 7th Street. It is of a scale that is visible from the southern perimeter of downtown and represents the historical agriculture and industrial base upon which the Rail and River Districts have developed. Such a structure could be enhanced and/or replicated to become an even stronger element at this major entrance to the area.

**28 Road/Riverside Parkway.** This intersection is a smaller scale than the others but a neighborhood entry could be created, particularly along the north side. The sense of arrival at this location could be created through a water feature, public art, an architectural feature with signage paving patterns and/or landscaping. The design of the entry feature should be of the same character of those that might be created at the other major entry points.

## **G. Economic Redevelopment**

### **Downtown District Economic Redevelopment**

While the Downtown District is the heart of the community, it is but one subset of a larger market and has strengths which can be capitalized on and limitations which should be overcome. Downtown has a tremendous influence on the economic well-being of the entire region. Therefore, it is widely accepted that early projects in any revitalization effort should be publicly assisted until market conditions reach levels where new construction can support itself.

The *Grand Junction Strategic Downtown Master Plan* presented guiding principles which; while general in nature, were considered responsive to prevailing conditions, market opportunities, framework elements and stakeholder input. These guiding principles are still relevant to the Greater Downtown Plan and are listed and described below.

- **Downtown is one submarket that competes with other submarkets in Grand Junction.** The downtown environment, while presenting tremendous opportunity for investment in a setting uniquely positioned to offer both heart and history, carries with it certain limitations, particularly for land-intensive non-destination-oriented land uses. Several market sectors, however, not only survive, but also thrive in a downtown setting. Recognize the obstacles associated with downtown development and encourage regulatory and financial solutions including public subsidies and creative financing mechanisms.
- **Downtown must be market-responsive** to changing conditions, with implementation tools and mechanisms in place to both offset competitive disadvantages and capitalize on competitive assets. Market conditions should be continually monitored and information distributed to a broad audience including developers, business and property owners, lenders, city staff and elected and appointed officials.
- **Downtown infrastructure must be protected and retained** including physical features, service organizations, a mix of employers, historic residential neighborhoods and community attitudes toward Downtown. Unlike many communities across the country, Downtown Grand Junction's existing infrastructure is more than sufficient to promote itself as a downtown neighborhood. These assets, which provide the impetus for investment, need to be protected and promoted.
- **Downtown's "tool bag" must contain a variety of strategies and mechanisms to attract investment.** These tools can be financial, physical, market, or organizational in nature and can be used independently or in various combinations. Given the obstacles associated with downtown development, it is imperative that whatever mix of tools is put in place it be comprehensive, flexible and creative.
- **Public investment must leverage private investment.** Historically, the planning, financing and implementation of projects in the downtown market were the primary responsibility of public sector entities such as the City and the DDA. However, while the public sector continues to play a significant role in most downtown efforts, a critical component to the success of any revitalization strategy today is participation by both the public and private sectors. Leveraging of resources is key, as no one entity, public or private, has sufficient resources alone to sustain a long-term downtown improvement effort.
- **Public policy must support downtown development.** Experience has proven that main street or downtown development will best succeed if regional growth management programs reward efficient development patterns. If growth is allowed to occur in a land extensive, inefficient way that effectively subsidizes lower densities, main street development will operate at a competitive disadvantage. Given Grand Junction's existing land use patterns, downtown is susceptible to continued dilution of its role as the community's central business and shopping district.
- **Public-private partnerships are essential.** Under any investment strategy, local government needs to have strong involvement, a visible presence, perhaps be the entity that provides continuing leadership, and always provide regulatory incentives and seed capital for early projects. Not only does government have the legal responsibility to address many of the implementation components, but it is also the logical conduit to local, regional, state and federal

funding sources.

### **Rail and River District Economic Redevelopment**

The changes that have occurred in portions of Greater Downtown such as completion of the Riverside Parkway and planning for the future development of Las Colonias Park have already had a positive influence on the River and Rail Districts. Many properties have been renovated or redeveloped, new uses are relocating to the area and property values are generally on the rise. The Greater Downtown Plan envisions this trend continuing and being enhanced by the following redevelopment concepts:

- Allow existing heavy industry to remain, taking advantage of rail spurs within the area.
- Intensified commercial edge along the north side of the Riverside Parkway with opportunities for mixed use development.
- New general commercial, retail and residential uses will provide activity at the edge of the park after business hours to create a safe park environment that gives “ownership” of the park to the adjacent local business owners and residents.
- New retail and commercial uses such as restaurants, shops and services along South 7<sup>th</sup> Street to serve the employees, recreational users and residents of the neighborhood.
- Commercial Industrial uses bridge the existing industrial and the commercial corridors.

In addition, discussions with the Grand Junction Economic Partnership, Business Incubator, Manufacturers’ Council and Chamber of Commerce during development of the Greater Downtown Plan brought to light many opportunities for the area, the majority of which is within the established Mesa County Enterprise Zone. The Plan outlines goals, policies and strategies that can be used to further the economic (re)development of the Rail and River Districts in Greater Downtown.

- Need for flex space for different types of small business – new to area or graduating from the Business Incubator. Opportunity for these businesses to serve employee base, residents and recreational users in the area.
- Opportunity to develop additional incentives for redevelopment that has taken advantage of partnerships and/or assembled parcels of land totaling a minimum of ½ acre or more
- Allow for live-work opportunities
- Opportunity to develop partnerships

### **H. Greater Downtown Area Parks**

The four downtown parks were included on the original town plat created by the Grand Junction Town Company in 1882. All four parks still exist as downtown open space. The City acquired Las Colonias Park from the State Parks Department in 1997. It encompasses the mill tailings clean-up site.





**1. Hawthorne Park – 400 Gunnison Avenue**

- New restroom/shelter constructed 2010
- South picnic shelter requires new roof; scheduled 2013
- Playground surfacing replaced in 2012
- Playground equipment on a 5 to 7 year replacement schedule
- Irrigation is good, 10-15 year replacement

**2. Washington Park – 10<sup>th</sup> Street and Gunnison Avenue**

- No restrooms or shelter at this park
- Playground equipment scheduled for replacement in 2013
- Playground surface scheduled for replacement every 2 years
- Irrigation will be replaced every 5 to 7 years

**3. Whitman Park – 5<sup>th</sup> Street and Pitkin Avenue**

- Restroom scheduled for replacement in 2013
- Development of a new master plan for the park recommended (see possible concepts below)

#### 4. Emerson Park – 9<sup>th</sup> Street and Pitkin Avenue

- Restroom scheduled for replacement in 2013
- Playground area for ages 2-5 is good
- Playground for ages 5-15 scheduled for replacement in 5 to 7 years
- Playground surfacing will be replaced in 2012
- Development of a new master plan for the park recommended (see possible concepts below)

#### Potential Concepts for Whitman and Emerson Parks

- Provide for a diversity of uses to create a higher level of utilization.
- Minimize the impact of adjacent streets and automobile traffic.
- Activate the edges of the parks with mixed use.
- Contemplate programmed, active use.
- Redevelop as more active, flexible urban open space rather than as passive, green parks.

#### 5. Las Colonias Park – Struthers Avenue and 7<sup>th</sup> Street

- A master planning process for the park will occur in 2012-2013
- Some amenities being considered are parking, restrooms, shelters, play area, open space, expanded walk/bike trails, open space for festivals, music, and educational presentations, various wetlands and arbor education opportunities, kayak park, tree walk and a disc golf course.



#### City-Owned Jarvis Property

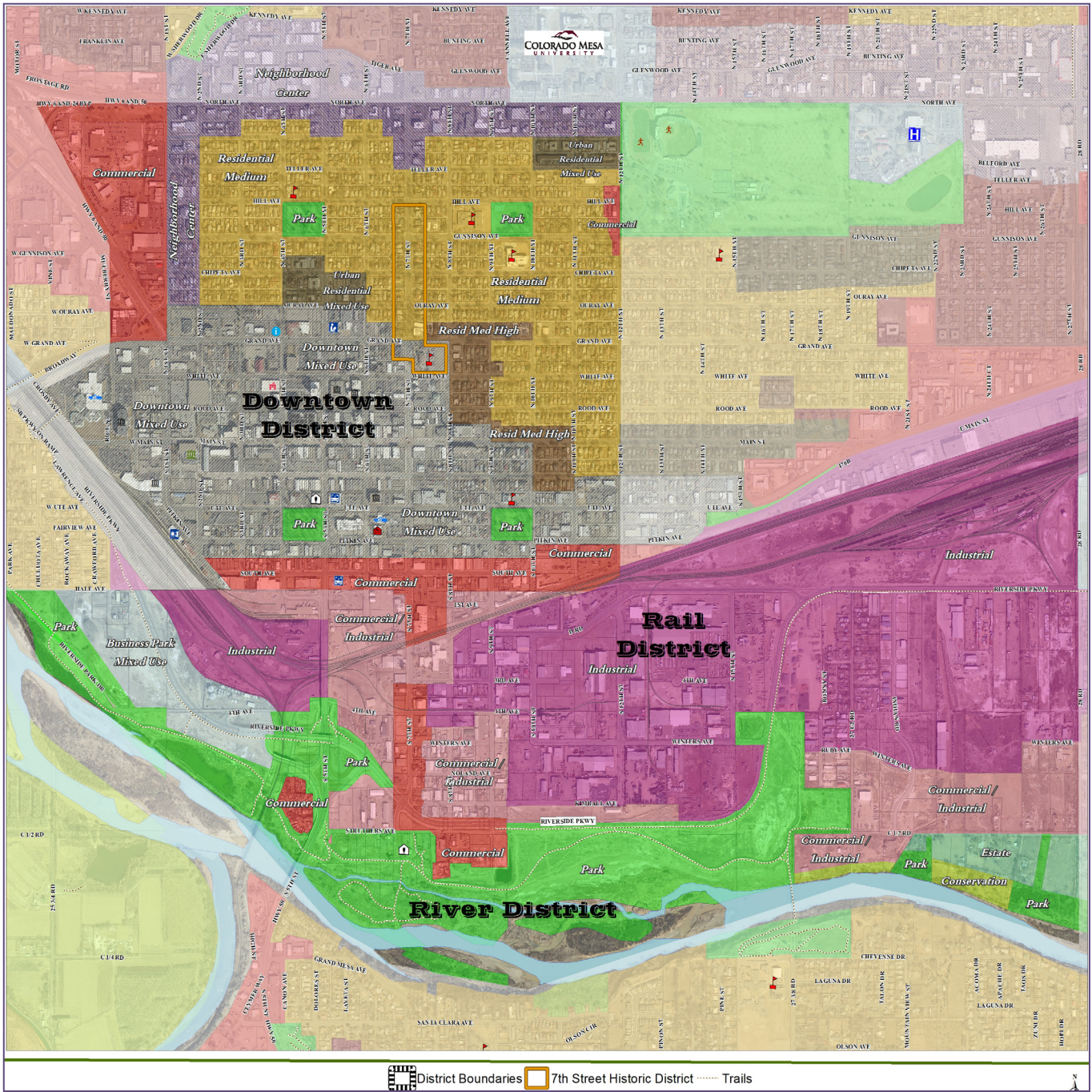
The City completed an initial planning analysis for the City-owned property on the west side of 5<sup>th</sup> Street between the Colorado River and the Riverside Parkway known as the Jarvis Property. The property is constrained by natural features and the encroachment of the Parkway, but does have approximately 43 acres of developable land.

The initial study was to chart a direction for revitalization of the property. It summarized the key assets, identified some important issues and potential impediments to development, analyzed current market conditions and outlined a concept for organizing potential development of the property. The property and potential project are viewed as a unique opportunity for the City to chart the future of a rare property type, a place where it may be possible to provide a mix of uses, including residences, along the bank of the Colorado River. The major opportunities and constraints identified for potential development of the Jarvis Property were to: continue the riverfront greenway through the property, restore habitat, relocate the high voltage power facilities that run through the site, and provide flood protection for the property.

## **APPENDICES**

- A. Future Land Use Plan
- B. Zoning Map
- C. Circulation and Trails Plans
- D. Development Concepts
- E. Summary of Public Process and Questionnaires/Comments

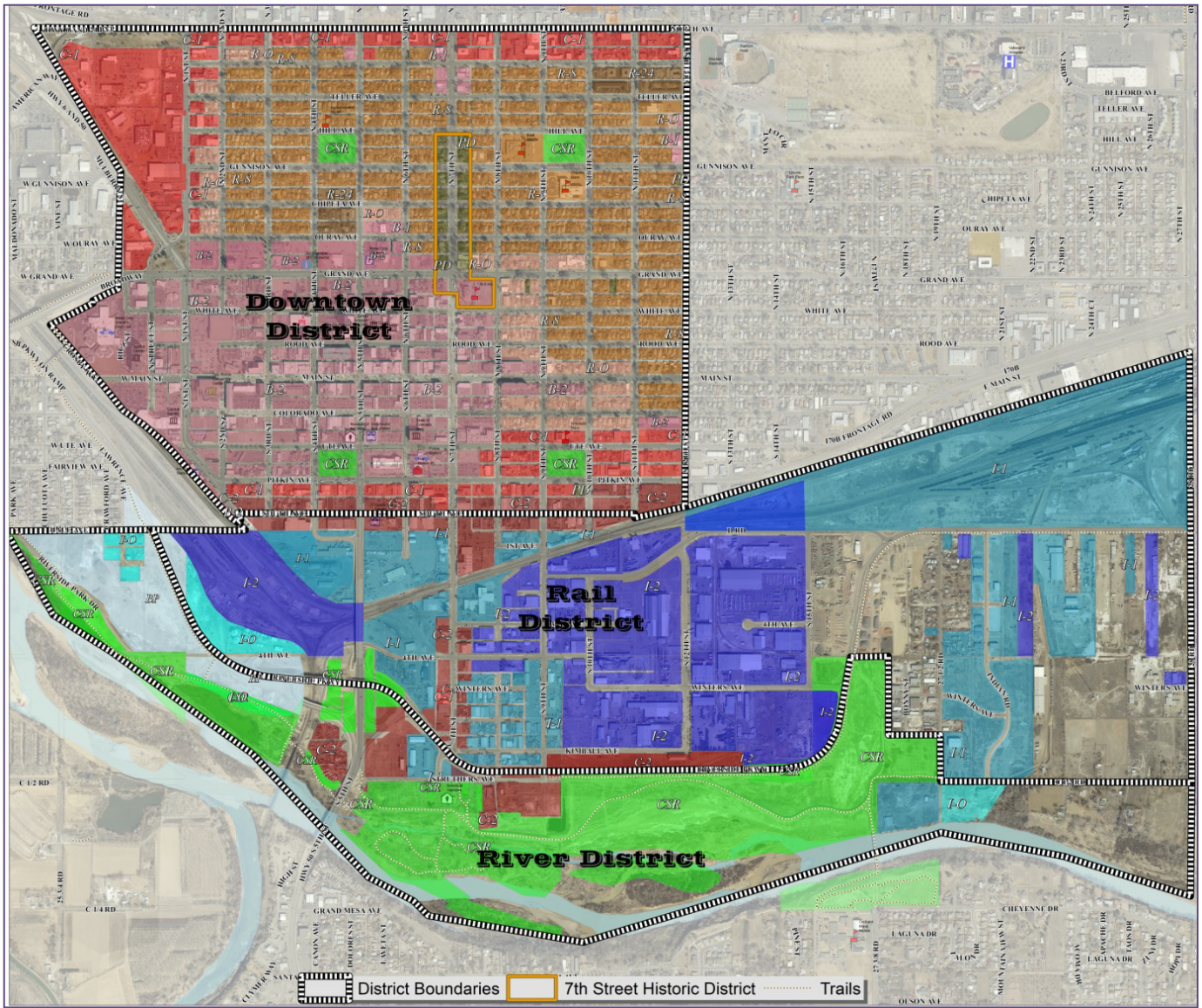
# Appendix A – Future Land Use Map



### Greater Downtown Future Land Use Categories

- |                            |                            |   |                                    |
|----------------------------|----------------------------|---|------------------------------------|
| Neighborhood Center (NCMU) | Commercial (COM)           | Residential Med High (RMH)                    | Conservation (CON)                 |
| Downtown Mixed Use         | Industrial (IND)           | Residential High Mixed Use or Resid. Med High | Urban Residential Mixed Use (URMU) |
| Business Park Mixed Use    | Commercial Industrial (CI) | Park (PK)                                     |                                    |

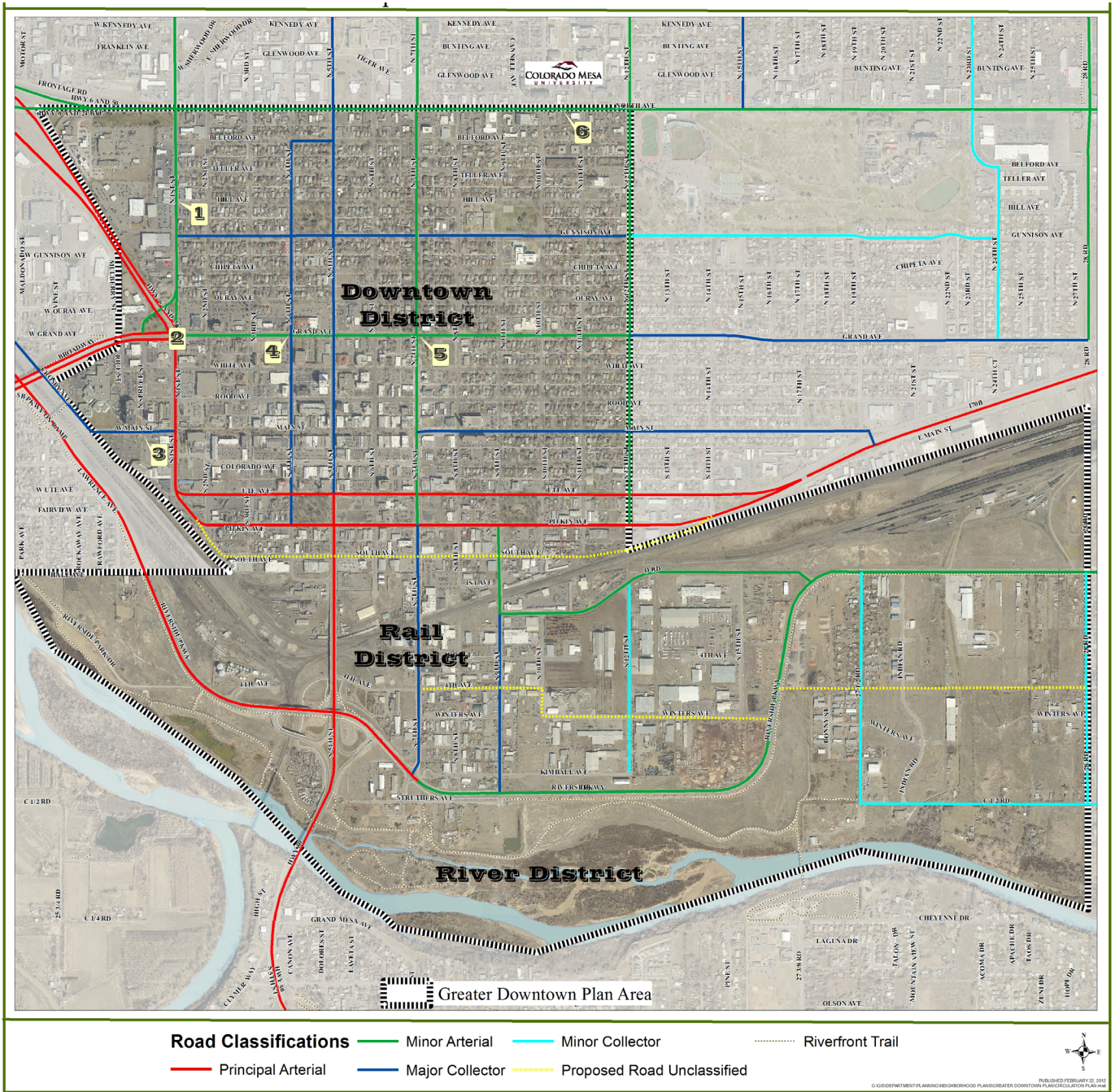
# Appendix B – Zoning Map



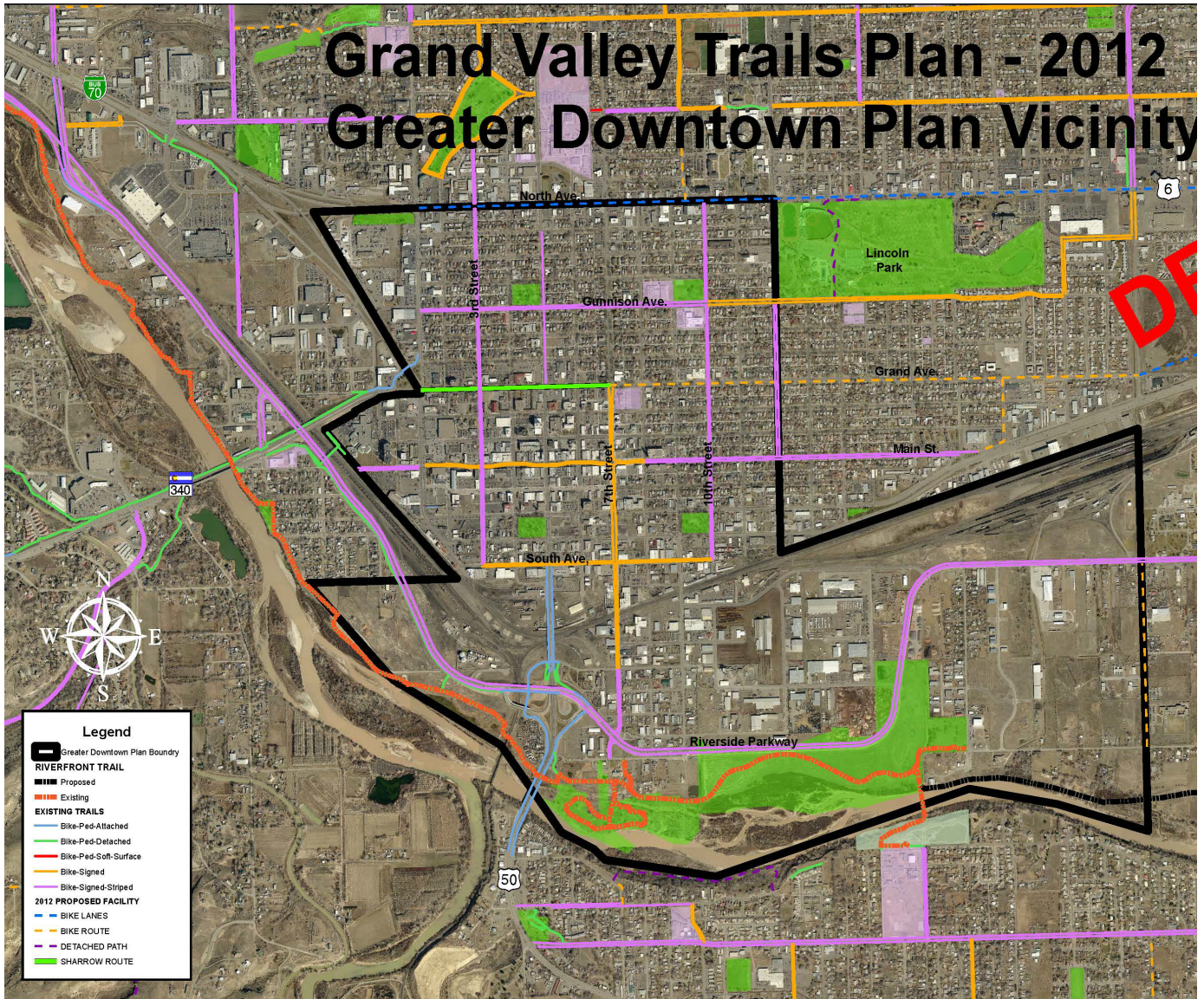
## Greater Downtown Zoning Categories

Proposed Zoning Changes	B-2 (Downtown Business)	C-1 (Light Commercial)	I-O (Industrial Office Park)	I-2
	R-O (Residential Office)	BP (Business Park)	C-2	I-1 (Light Industrial)
				CSR (Community Services & Rec.)

# Appendix C – Circulation and Trails Plans



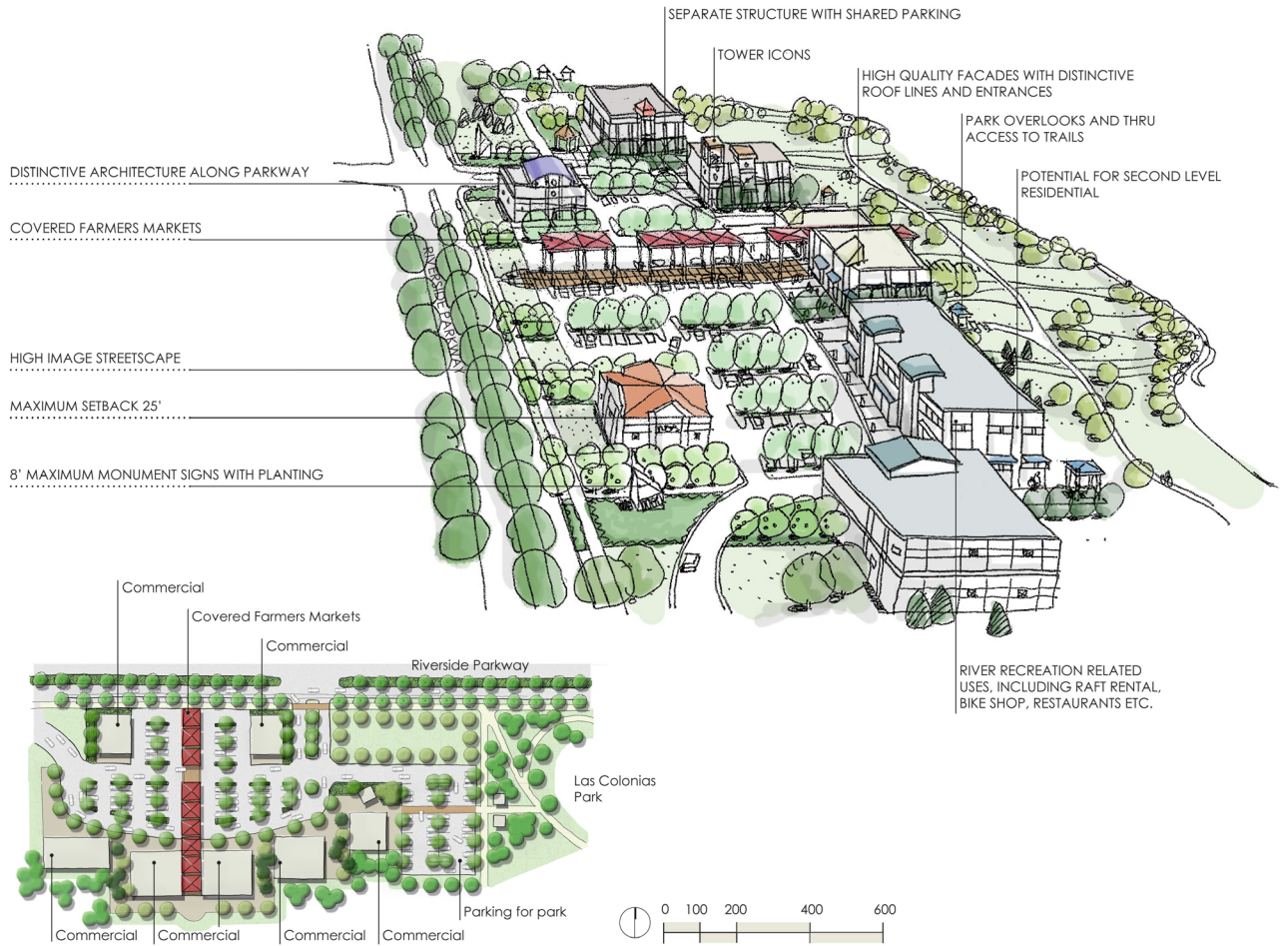
# Grand Valley Trails Plan - 2012 Greater Downtown Plan Vicinity





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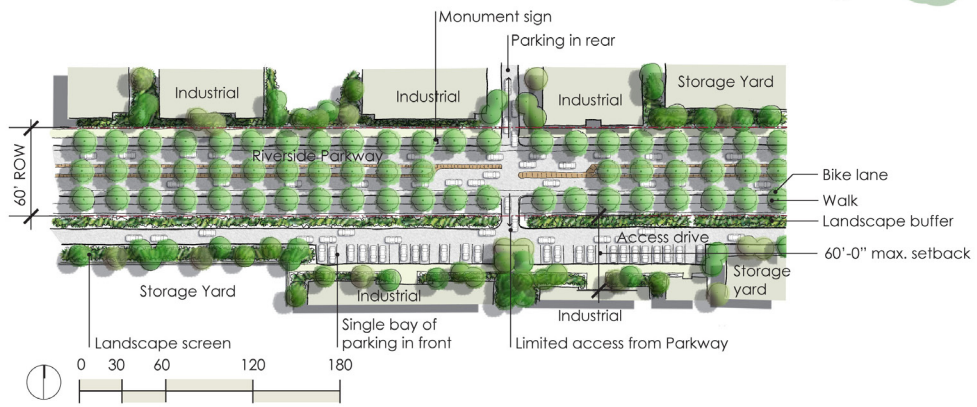
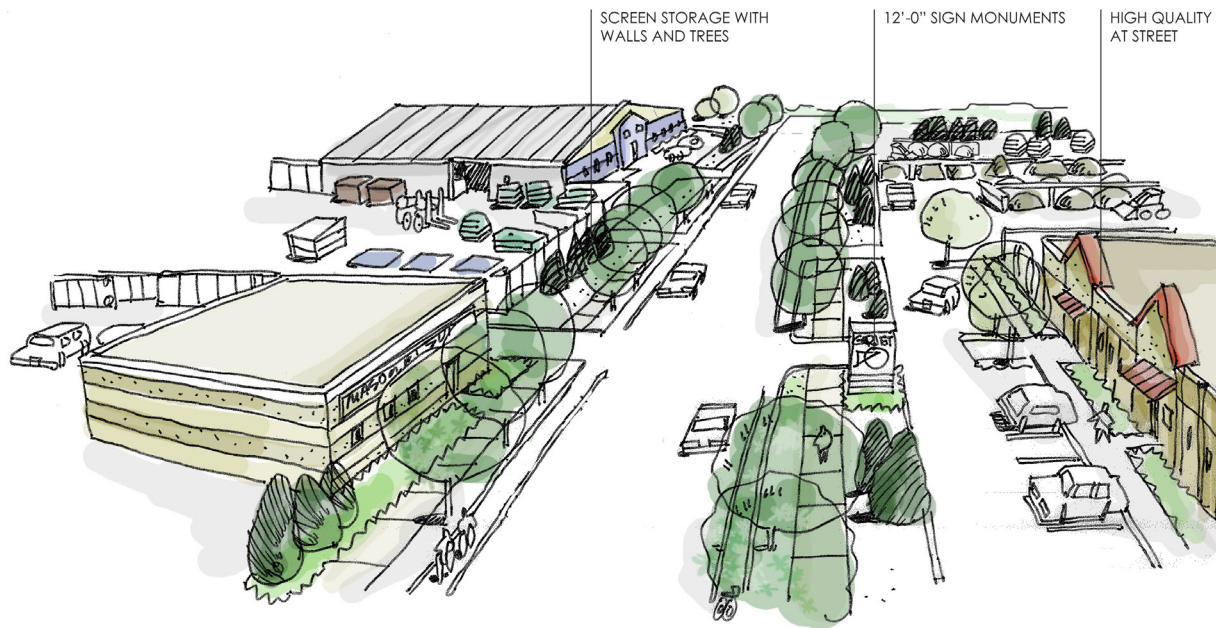
# Appendix D – Development Concepts



## 7th and Struthers Redevelopment

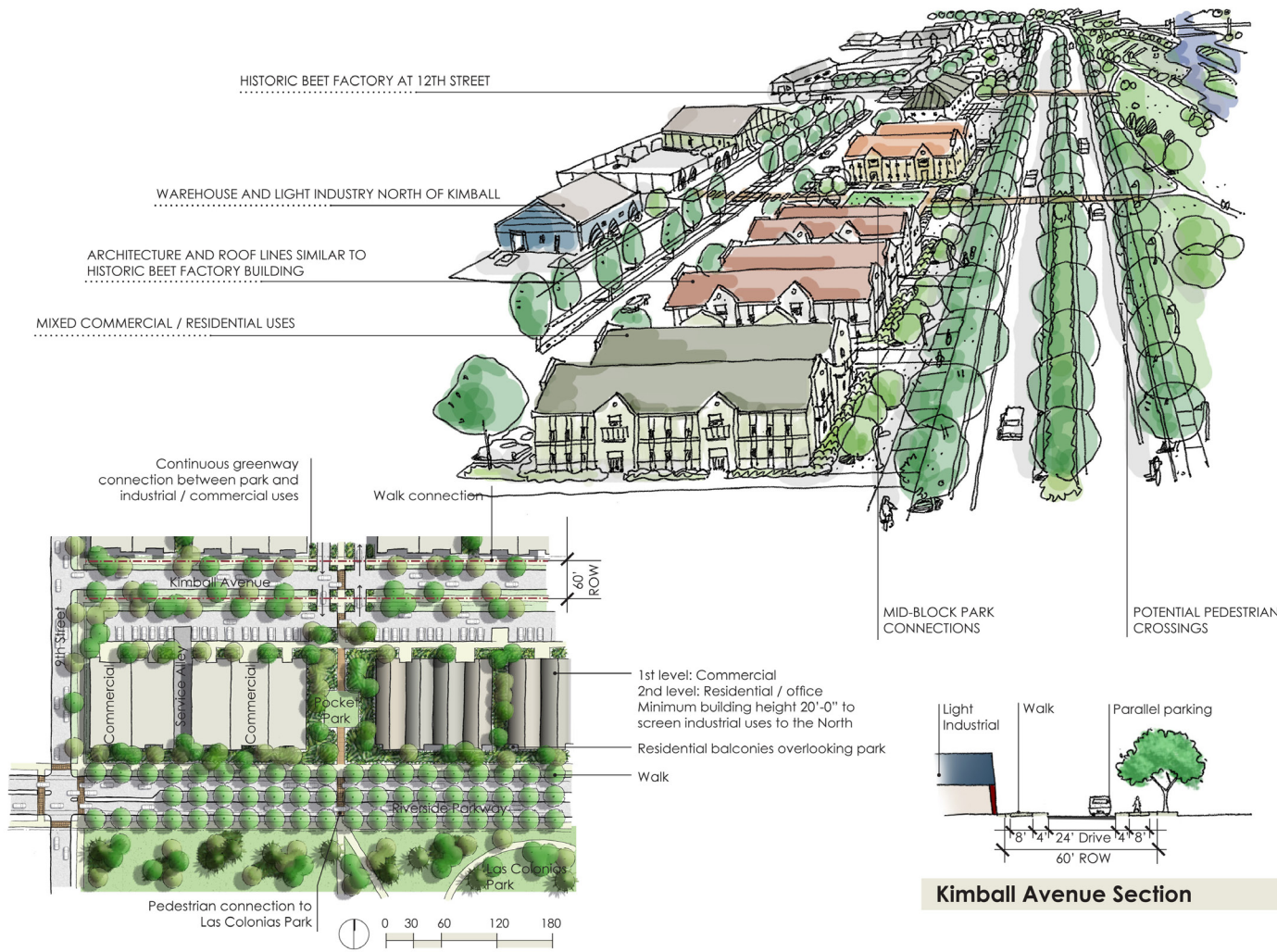
### SOUTH DOWNTOWN DESIGN GUIDELINES

EDAW | AECOM



**Riverside Parkway Between 27 1/2 and 28 Road**

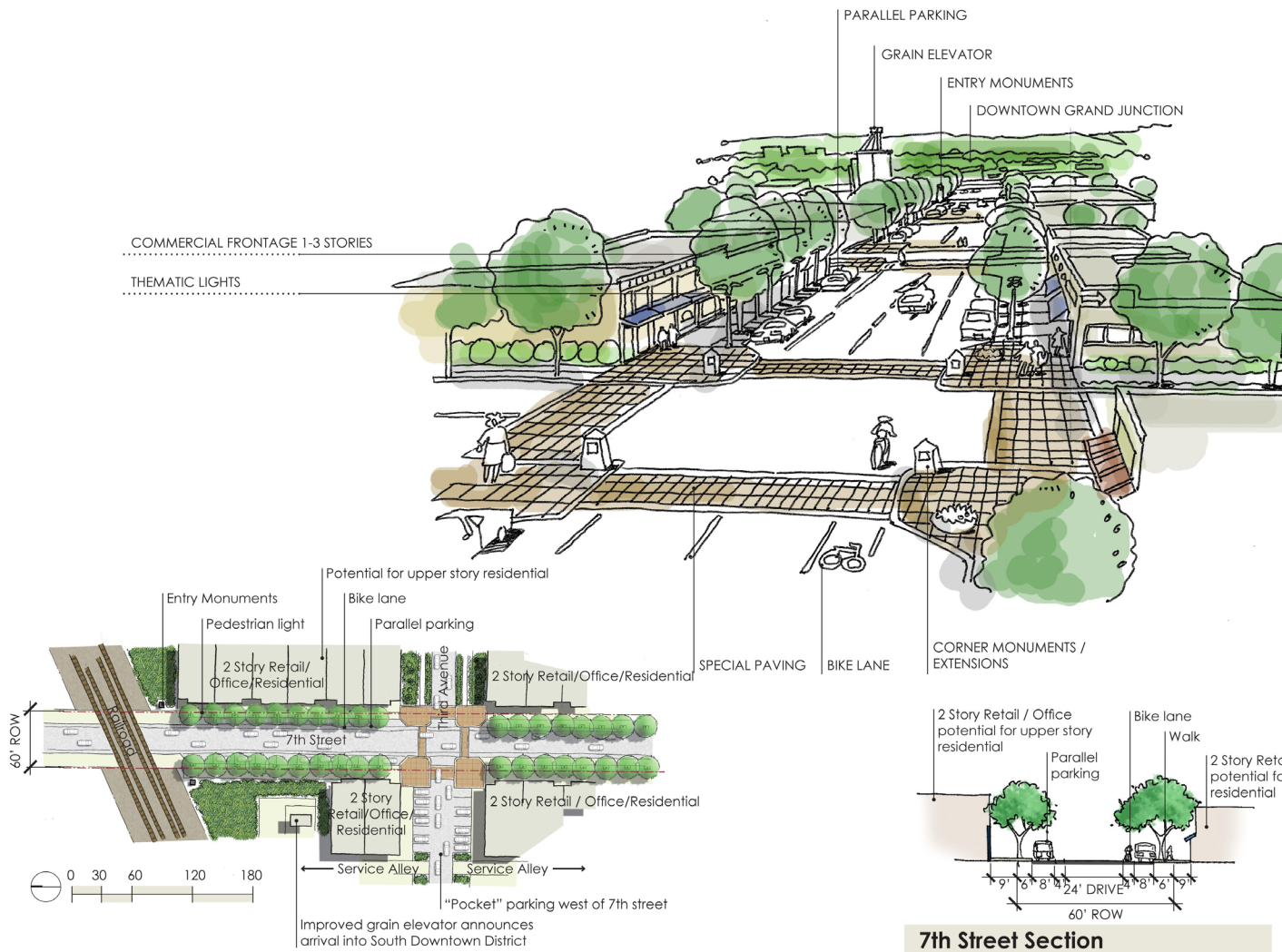
**SOUTH DOWNTOWN DESIGN GUIDELINES**



**Riverside Parkway Between 5th Street East and 12th Street**

**SOUTH DOWNTOWN DESIGN GUIDELINES**

EDAW AECOM

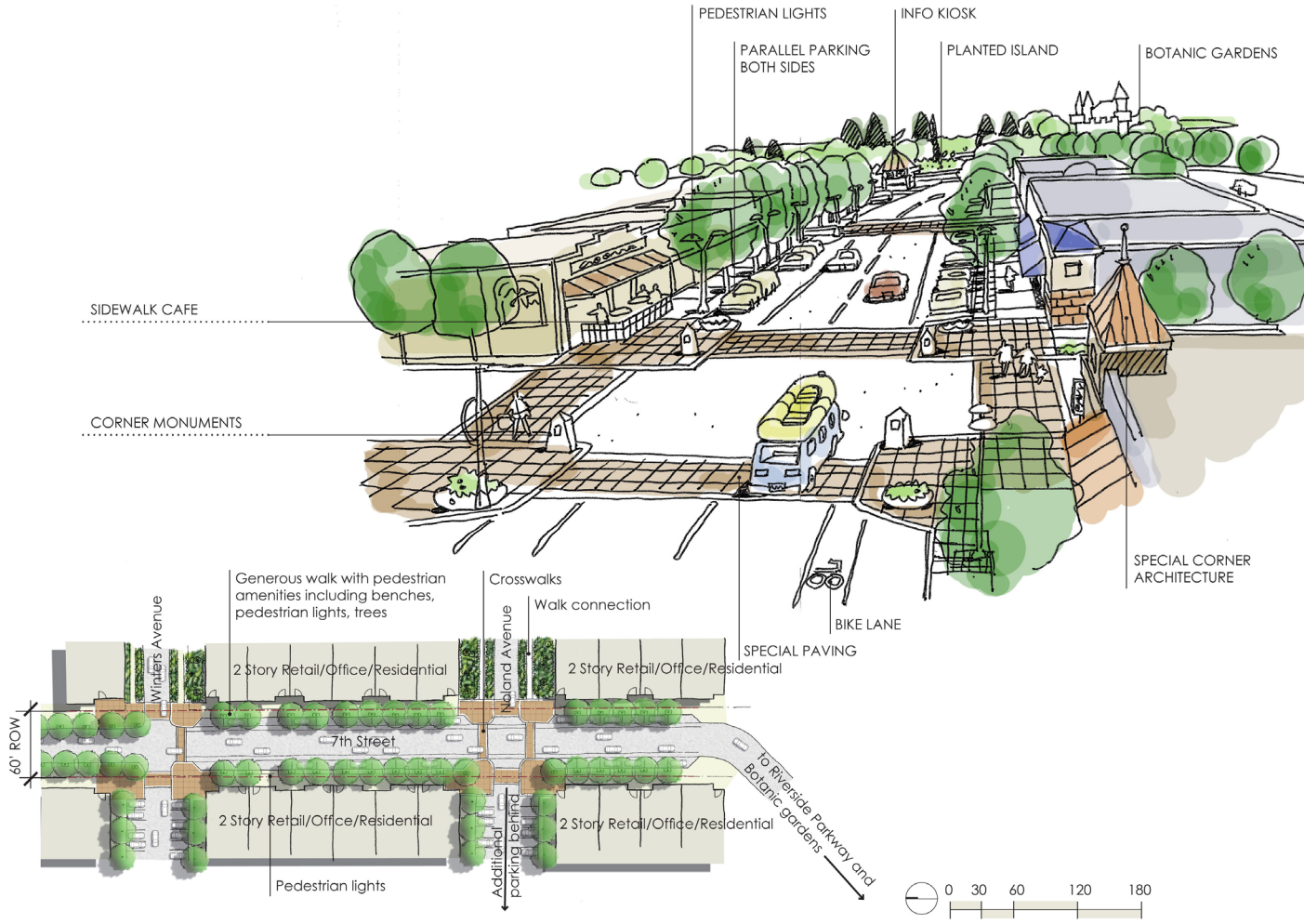


**7th Street Looking North**

**7th Street Section**

**SOUTH DOWNTOWN DESIGN GUIDELINES**

EDAW



**7th Street Looking South**

**SOUTH DOWNTOWN DESIGN GUIDELINES**



**Existing Conditions**

FRONT YARD PATIO  
 TREE LAWN  
 NO PARKING ON SOUTH SIDE

GARDEN GATES

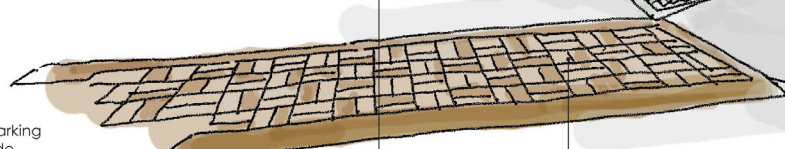
NEW BUILDING OF COMPATIBLE STYLE AND SCALE

IRON FENCE

6'-0" GARDEN STRIP

NEW SIDEWALK, CURB AND GUTTER

REH-STRU

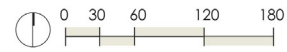


ON STREET PARKING NORTH SIDE

RAISED BRICK CROSSWALK WITH CORNER PLANTING



60' ROW



**Noland Avenue**

**SOUTH DOWNTOWN DESIGN GUIDELINES**

## **APPENDIX E: PUBLIC PARTICIPATION PROCESS**

### **TECHNICAL COMMITTEE**

The Greater Downtown Plan technical committee was comprised of staff members from various public agencies including City Public Works and Planning, City Parks and Recreation, City Geographic Information Systems, Mesa County Planning, the Regional Transportation Planning Office, Mesa County Facilities and Parks, the Downtown Development Authority and the Mesa County Public Library District. The Committee met three times during the course of developing the Greater Downtown Plan and members attended public open houses to discuss concerns and proposals with participants.

### **PUBLIC OPEN HOUSES**

Two public open houses were held in December 2011 and February 2012 to present concepts and solicit input from property owners and interested citizens. Notifications/ invitations to both public open houses were mailed to all property owners within the Greater Downtown Plan area. Approximately 60 people attended the first open house and 40 attend the second open house.

### **QUESTIONNAIRES AND COMMENTS**

A series of questionnaires were available at the December 2011 open house and on the City's web site that were used to solicit public comment and weigh community opinions on design concepts that might be proposed with the Plan. 130 questionnaires were returned. In addition, citizens could provide other written comments at both open houses. The results of the questionnaires and the written comments are included on following pages.

### **LETTERS/MEETINGS WITH INDIVIDUAL PROPERTY OWNERS**

City Public Works and Planning staff coordinated meetings with key individual property owners, businesses or others that contacted the City regarding the Greater Downtown Plan. In addition, individual letters were mailed to property owners along the corridors that may be impacted by the land use and zoning proposals of the Greater Downtown Plan. Follow up meetings or conversations were held with property owners that responded to the letter. The meetings/conversations including the following individuals or businesses: Marie Ramstetter, Jim Golden, Woodstove Warehouse, 4NR Properties, Struth LLC, Peggy Cox, Kathy Ziola, John Crouch, the Redstone Group (Sugar Beet building), Butch Jarvis, VanGundy's, Castings, Inc., KelMac Industries (GJ Steel site), Sem Materials and Whitewater Sand and Gravel.

### **COMMUNITY PRESENTATIONS/DISCUSSIONS**

Public Works and Planning staff conducted several presentations and discussions regarding the Greater Downtown Plan with community groups and businesses including the Chamber of Commerce, Bray and Company Realty, the Downtown Development Authority and Rail and River District corridor property owners.



**CITY COUNCIL, CITY PLANNING COMMISSION AND MESA COUNTY PLANNING COMMISSION WORKSHOPS**

City Public Works and Planning staff attended several workshops with elected and appointed City and County officials to inform and solicit input on the Greater Downtown Plan during its development.

## Central Business District – Questionnaire Results

1. Establish a cohesive character/theme that harmonizes new structures with the existing buildings through common materials, scale and architectural details

#	Answer	%
1	Strongly Disagree	7%
2	Disagree	7%
3	Neither Agree nor Disagree	15%
4	Agree	37%
5	Strongly Agree	33%
	Total	100%

2. Promote high density, vertically mixed use structures (e.g. retail at street level and residential or office above)

#	Answer	%
1	Strongly Disagree	0%
2	Disagree	4%
3	Neither Agree nor Disagree	4%
4	Agree	37%
5	Strongly Agree	56%
	Total	100%

3. Building height restrictions in the CBD should be based on guidelines that address compatibility with surrounding uses.

#	Answer	%
1	Strongly Disagree	7%
2	Disagree	26%
3	Neither Agree nor Disagree	19%
4	Agree	30%
5	Strongly Agree	19%
	Total	100%





4. Taller buildings should be located in the center of the CBD, with lower buildings on the edges of the CBD.

#	Answer	%
1	Strongly Disagree	4%
2	Disagree	12%
3	Neither Agree nor Disagree	31%
4	Agree	31%
5	Strongly Agree	23%
	Total	100%





5. Preserve and restore significant historic structures

#	Answer	%
1	Strongly Disagree	0%
2	Disagree	0%
3	Neither Agree nor Disagree	13%
4	Agree	30%
5	Strongly Agree	57%
	Total	100%




6. Promote infill development that is compatible with the existing downtown character.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		9%
3	Neither Agree nor Disagree		13%
4	Agree		43%
5	Strongly Agree		35%
	Total		100%




7. The streetscape will be dominated by buildings rather than surface parking lots.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		9%
3	Neither Agree nor Disagree		13%
4	Agree		35%
5	Strongly Agree		43%
	Total		100%




8. Shared parking and parking structures should be encouraged in the CBD to reduce the amount of surface parking lots.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		0%
3	Neither Agree nor Disagree		17%
4	Agree		39%
5	Strongly Agree		43%
	Total		100%

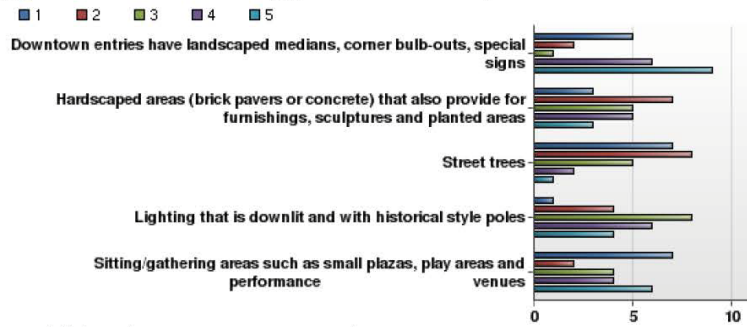
9. Explore new ways to pay for public parking.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		0%
3	Neither Agree nor Disagree		27%
4	Agree		36%
5	Strongly Agree		36%
	Total		100%

10. Provide streetscape details that complement the architectural character of downtown Grand Junction.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		0%
3	Neither Agree nor Disagree		9%
4	Agree		52%
5	Strongly Agree		39%
	Total		100%

11. The most important streetscape characteristics to me are (Please rank the options below in order of your most preferred with 1 being your favorite and 6 being your least favorite):



12. Please provide any additional comments you may have.

**Text Response**

#2 if you consider parking #4 Keep very tall buildings - like the Alpine Bank Building OUT of downtown.

#11 Xeroscape, dark sky!!, sitting/gathering along rail district as well

The focus within the CBD should be less on specific architectural styles and/or materials, but on those necessary elements to create a streetscape (location of entrances, location of parking, uses of the sidewalk, etc). The collection and range of architectural styles is what illustrates the evolution of a place and brings unique character to that place.

I think there should be a legitimate concrete Skatepark in the downtown park area as an anchor for destination based year round usage... feel free to contact me for any info or questions... (info@bin707.com)

The CBD has a core of historical structures on Main Street and Colorado, but the modern buildings on 7th and what the new Marriott adds is a varied and interesting streetscape with both modern and historic structures. This diversity of design allows for creativity, originality of builders and not a cookie cutter approach to the CBD dictated by code. That said, a 20 story medical center next to the transitional housing areas makes no sense so some type of restrictions should apply. The removal of trees on Main was necessary given the 50 year life cycle, but better planning needs to be done to assure we maintain a natural canopy of vegetation and the City also needs to do a better job of maintaining the fountains assuring they are functioning and serviced as needed. The one located across from the Avalon has malfunctioned much of the time since installation. Also winterization and maintenance of these wonderful outdoor water features must be planned operationally. Parking will always be an issue and the City needs to be more in touch with the reality of what they are approving as evidenced by the latest "VET" housing development done by Catholic Outreach which was totally lacking substantive evidence for the staff justification of parking. The result will be loss of parking for downtown businesses and there is no law that says the facility has to be used for VA occupants forever.

**Downtown Area-Wide Concepts – Questionnaire Results**

1. Examine the possibility of making 4th and 5th Streets both 2-way streets between Grand and North Avenues

#	Answer	%
1	Strongly Disagree	32%
2	Disagree	18%
3	Neither Agree nor Disagree	14%
4	Agree	18%
5	Strongly Agree	18%
	Total	100%

2. Establish a distinctive public sign palette for the original square mile to include street signs and directional signs that have recognizable poles, ornamentation, colors, fonts and logos.

#	Answer	%
1	Strongly Disagree	9%
2	Disagree	5%
3	Neither Agree nor Disagree	32%
4	Agree	27%
5	Strongly Agree	27%
	Total	100%

3. Emphasize “walkability” of Downtown through ongoing improvements to pedestrian crossings, bicycle facilities (e.g. bike lanes on streets, bike racks at strategic locations).

#	Answer	%
1	Strongly Disagree	4%
2	Disagree	0%
3	Neither Agree nor Disagree	4%
4	Agree	30%
5	Strongly Agree	61%
	Total	100%

4. Establish and improve gateways to the Downtown Area (please select your top 4 choices).

#	Answer	%
1	7th Street and North Avenue	37%
2	1st Street and Grand Avenue	84%
3	12th Street and Grand Avenue	16%
4	1st and Main Streets	84%
5	12th and Main Streets	21%
6	5th Street and South Avenue	68%
7	7th Street and Pitkin Avenue	58%
8	Other – please describe	32%

Other – please describe	
	River front trail
	1st - NORTH
	4th & North
	14th & I-70B (Desert Vista Park)
	7th and Main

2	Disagree	10%
3	Neither Agree nor Disagree	25%
4	Agree	40%
5	Strongly Agree	20%
	Total	100%

5. Please provide any additional comments you may have.

**Text Response**

Make the accessibility to the river and riverfront trails from downtown area more inviting.

#1 Why?

Make user-friendly ways to get to river trails

#4 improve how? They are all fine now Making 4th & 5th only partly 2-way would be a disaster. There would be auto accidents - people driving in the wrong lanes No more roundabouts!

In time pedestrian traffic will increase yet travel today in the next 20 years most travel will be via cars

#1 but make transition from 1-way to 2-way very clear with islands & other street features Main & Gunnison are ideal 7th from downtown to River is already bike-friendly ok

The directional signage beginning on Horizon Drive and leading to the Downtown along 7th Street already has a distinctive color scheme. However, this scheme was compromised at the intersection of 7th Street and Grand Avenue when the sign pole was painted black. If a color scheme is created, it should build on this work and must not be compromised.

**North Central Business District – Questionnaire Results**

1. Establish a cohesive character/theme that harmonizes new structures with the existing buildings through common materials, scale and architectural details

#	Answer		%
1	Strongly Disagree		10%
2	Disagree		10%
3	Neither Agree nor Disagree		25%
4	Agree		45%
5	Strongly Agree		10%
	Total		100%

2. Promote vertically mixed use structures (e.g. retail or office at street level and residential or office above)

#	Answer		%
1	Strongly Disagree		5%
2	Disagree		5%
3	Neither Agree nor Disagree		10%
4	Agree		50%
5	Strongly Agree		30%
	Total		100%

3. The maximum building height of 65 feet shall only be allowed on parcels that front Grand Avenue. The buildings should “step down” so that the front of buildings that are directly across the street from residential buildings or uses are only 35 feet in height.

#	Answer		%
1	Strongly Disagree		5%
2	Disagree		10%
3	Neither Agree nor Disagree		25%
4	Agree		40%
5	Strongly Agree		20%
	Total		100%

8. Where available, some parking for non-residential uses may be on the street but only in front of the actual use, not in front of other adjacent uses.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		16%
3	Neither Agree nor Disagree		37%
4	Agree		32%
5	Strongly Agree		16%
	Total		100%

9. Should similar guidelines and standards be applied to the 1st Street Corridor from Grand Avenue to Belford Avenue?

#	Answer		%
1	Yes		78%
2	No		22%
	Total		100%

10. Please provide any additional comments you may have.

**Text Response**

limit mixed-use intrusion into residential areas

#1 but not exactly alike #8 there's just no space on 4th St

#1 existing bldgs are crap #6 I don't notice anything about Grand west of 7th worth harmonizing with.

Keep it simple. Trees are such a gift. Keep it "big western town" rather than a city. Really, we have a choice western town here that offers relief from cities. May we manage to preserve & maintain that western "wide open spaces" air.

The one-block depth of this subarea creates a challenge. I agree that a maximum setback is necessary, both along Grand and 1st Street. However, this will result in the relocation of parking, loading, etc to the rear of the block, assuming the properties within the block are assembled. I believe that it would be a mistake to severely limit the height and/or building locations along Ouray and 2nd Street. The width of these streets and the streetscape itself would be better used as the transition. This would allow greater utilization of the properties and provide a better defined "edge" between the business and the residential area.

**Transitional Areas – Questionnaire Results**

1. Front yards of transitional uses shall be reserved for landscaping, sidewalks and driveway access to parking areas and signage to maintain the residential character.

#	Answer		%
1	Strongly Disagree		5%
2	Disagree		11%
3	Neither Agree nor Disagree		16%
4	Agree		42%
5	Strongly Agree		26%
	Total		100%

2. Keep signs for the non-residential uses subordinate to the residential character.

#	Answer	%
1	Strongly Disagree	0%
2	Disagree	5%
3	Neither Agree nor Disagree	16%
4	Agree	58%
5	Strongly Agree	21%
	Total	100%

3. Regulate maximum building size in transitional corridors.

#	Answer	%
1	Strongly Disagree	11%
2	Disagree	21%
3	Neither Agree nor Disagree	16%
4	Agree	42%
5	Strongly Agree	11%
	Total	100%

4. Regulate hours of operation for transitional uses.

#	Answer	%
1	Strongly Disagree	11%
2	Disagree	33%
3	Neither Agree nor Disagree	28%
4	Agree	17%
5	Strongly Agree	11%
	Total	100%

5. Regulate building, site and signage lighting for transitional uses to minimize impact on adjacent residential core.






#	Answer	%
1	Strongly Disagree	5%
2	Disagree	0%
3	Neither Agree nor Disagree	5%
4	Agree	58%
5	Strongly Agree	32%
	Total	100%

6. Regulate parking and screening on non-residential sites to minimize impact on adjacent residential core.





#	Answer	%
1	Strongly Disagree	6%
2	Disagree	6%
3	Neither Agree nor Disagree	11%
4	Agree	39%
5	Strongly Agree	39%
	Total	100%








7. Transitional uses should not be allowed to have outdoor storage areas.

#	Answer		%
1	Strongly Disagree		5%
2	Disagree		16%
3	Neither Agree nor Disagree		37%
4	Agree		16%
5	Strongly Agree		26%
	Total		100%





8. Where available, some parking for non-residential uses may be on the street but only in front of the actual use, not in front of other adjacent uses.

#	Answer		%
1	Strongly Disagree		6%
2	Disagree		0%
3	Neither Agree nor Disagree		35%
4	Agree		41%
5	Strongly Agree		18%
	Total		100%

9. Reuse of residential structures and new construction in the transitional corridors shall retain residential character.

#	Answer		%
1	Strongly Disagree		6%
2	Disagree		12%
3	Neither Agree nor Disagree		6%
4	Agree		47%
5	Strongly Agree		29%
	Total		100%

10. Regulate the spacing of non-traditional residential uses (e.g., service organizations, group homes) so as to equitably disburse them throughout the downtown area.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		20%
3	Neither Agree nor Disagree		10%
4	Agree		50%
5	Strongly Agree		20%
	Total		100%

11. The most important residential characteristics to me are (check all that apply):

#	Answer		%
1	Maintain landscaped front yards (no parking in the front yard)		68%
2	Setback of building from street		32%
3	Small, low signage		53%
4	Maintain or construct building forms that are typical of residential architecture (e.g. 1-1/2 to 2 stories, sloping roofs, window pattern, porches)		53%
5	Use materials that are similar in color and texture as those in the residential neighborhood (e.g. roofing, siding)		37%
6	Minimize the visual impact of parking provided for the transitional uses		58%
7	Other – please describe		16%

Other – please describe

Cleanliness and general appearance. In general, the current businesses in this area look nicer than the residences.

blend old & new (modern) architecture thru committee approval/aesthetic guidelines ie: Bozeman, MT

Feel free to tear it up and start over

12. Please provide any additional comments you may have.

**Text Response**

I am a part owner of a business at 9th and Rood. My main concern is traffic control. There are regularly accidents at 9th and Main, we hear each one and watch them out our windows. In my opinion, having watched this intersection for over a decade, you could drastically reduce the number of these accidents by reducing or eliminating the on-street parking on Main street in the vicinity of this intersection. I'm convinced most of these accidents are caused by visual error. People travelling either way on 9th will stop, look each way, see traffic on Main, and PULL OUT ANYWAY because they think, in a quick glance, the vehicle they saw was parked on Main. In a quick glance, drivers sometimes cannot differentiate between a parked and a travelling vehicle. The City put blinking lights around the stop signs for a while, I see they're gone now, I suspect they statistically did not help reduce the number of accidents. Please try my idea. I'm sure the property owners will complain. But it could be you or your family that is in the next accident at 9th and Main.

Bozeman, MT!!!

#9 the transitional area has no redeeming qualities.





The Transistional Area needs to be redefined. It encompasses existing commercial uses between 7th and 12th Street, many with frontage on North Avenue, which are identified as part of a student commercial/entertainment district. It also encompasses the Ratekin and Qwest towers, both zoned Downtown Business, which should be included in the CBD.

**Residential Areas – Questionnaire Results**




1. No large-scale redevelopment projects should be allowed within the downtown residential core.

#	Answer		%
1	Strongly Disagree		10%
2	Disagree		20%
3	Neither Agree nor Disagree		20%
4	Agree		20%
5	Strongly Agree		30%
	Total		100%




2. The downtown residential core should be preserved for residential uses only with no further encroachment of non-residential uses.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		24%
3	Neither Agree nor Disagree		29%
4	Agree		10%
5	Strongly Agree		38%
	Total		100%





3. Maintain the existing character of the house styles within the downtown residential core neighborhood – new construction or alteration must be compatible with key architectural characteristics and site elements of the neighborhood.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		15%
3	Neither Agree nor Disagree		0%
4	Agree		45%
5	Strongly Agree		40%
	Total		100%

4. Maintain and enhance the pattern of landscaped front yards that gives the downtown residential core neighborhood a distinctive, friendly appearance.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		0%
3	Neither Agree nor Disagree		14%
4	Agree		48%
5	Strongly Agree		38%
	Total		100%

5. Regulate the scale of accessory structures to maintain their character as subordinate to the primary residence.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		14%
3	Neither Agree nor Disagree		10%
4	Agree		43%
5	Strongly Agree		33%
	Total		100%

6. New construction of accessory structures may be allowed to be built at historic setbacks (e.g. there could be a zero foot setback from the alley and only 3 feet from neighboring property line).

#	Answer		%
1	Strongly Disagree		10%
2	Disagree		20%
3	Neither Agree nor Disagree		25%
4	Agree		35%
5	Strongly Agree		10%
	Total		100%

7. Where existing residential zoning allows, provide a diversity of housing types through development of multi-family housing that is in keeping with the character of the neighborhood.

#	Answer		%
1	Strongly Disagree		5%
2	Disagree		19%
3	Neither Agree nor Disagree		14%
4	Agree		29%
5	Strongly Agree		33%
	Total		100%

8. Discourage tearing down existing historic homes in order to construct new residential structures.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		10%
3	Neither Agree nor Disagree		19%
4	Agree		38%
5	Strongly Agree		33%
	Total		100%

9. Regulate the spacing of non-traditional residential uses (e.g. service organizations, group homes) so as to equitably disburse them throughout the downtown residential area.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		6%
3	Neither Agree nor Disagree		24%
4	Agree		59%
5	Strongly Agree		12%
	Total		100%

10. Enhance access to and improvements within existing public open spaces (e.g. parks and school grounds) within the downtown residential core.

#	Answer	%
1	Strongly Disagree	0%
2	Disagree	0%
3	Neither Agree nor Disagree	18%
4	Agree	47%
5	Strongly Agree	35%
Total		100%

11. Maintain and enhance the historic character of the streetscape.

#	Answer	%
1	Strongly Disagree	0%
2	Disagree	0%
3	Neither Agree nor Disagree	6%
4	Agree	63%
5	Strongly Agree	31%
Total		100%

12. The most important streetscape characteristics to me are (check all that apply):

#	Answer	%
1	Street Trees	94%
2	Landscaping rather than parking or other uses in park strips (area between sidewalk and curb)	71%
3	Distinctive street signs for downtown residential core	24%
4	Distinctive street lighting for downtown residential core	41%
5	Minimally maintained landscaping in front yards	6%
6	Detached sidewalks (space between the sidewalk and the curb)	47%
7	Other – please describe	0%
Other – please describe		

13. Please provide any additional comments you may have.

Text Response

Allow for Bed & Breakfast establishments to operate within original square mile, provided that signage, parking areas and lighting can blend in with neighboring properties.

#8 depending on the existing quality of the building #11 evergreens!

#2 small-scale mixed use would be ok #8 "historic" does not necessarily mean "good" The trees and lawns are pleasant, but GJ could afford to mlpout a significant effort toward xeriscape concepts. Honestly it seems criminal how folks in this town gush the Colorado River al over the place with little or no thought toward making a more rational truce with the desert.

I'd love to see softer lighting, going upward. For good ideas, check out [www.darksky.org](http://www.darksky.org). Xcel says people need only a 60 watt bulb (or equivalent on houses). I recommend (& long for) banishment of motion-sensor lighting... My plea is keep it simple. Preserve the "big western town" feel of this town. No more narrowing of streets. Please avoid turning into a city (like Boulder). THANKS!

Prohibiting "non-residential" uses can create unintended outcomes. For example, should a church be allowed to expand and offer a coffee shop or other gathering place open to the public? This sort of amenity within the residential area is what adds value to the area. It is important to permit multi-family (re)development within the original square mile for two reasons. One, it has excellent infrastructure to service the population and proximity to services. Two, it is the only area within Grand Junction that has lost population in the 2010 Census (Tract 2 and 3), suggesting a loss of families from the area and/or an aging population of smaller households.

## Corridor Overlays – Questionnaire Results

1. Create a mix of higher quality, new uses along South Avenue.

#	Answer	%
1	Strongly Disagree	9%
2	Disagree	0%
3	Neither Agree nor Disagree	18%
4	Agree	9%
5	Strongly Agree	64%
	Total	100%

2. Create a more pedestrian-friendly commercial and service corridor along South Avenue/South 7th Street that complements the street improvements that have already been completed.

#	Answer	%
1	Strongly Disagree	9%
2	Disagree	0%
3	Neither Agree nor Disagree	18%
4	Agree	18%
5	Strongly Agree	55%
	Total	100%

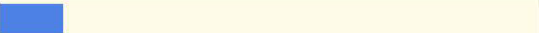



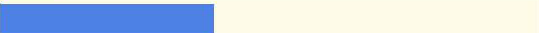
3. Do not allow billboards on the South Avenue/South 7th Street corridor.

#	Answer	%
1	Strongly Disagree	5%
2	Disagree	5%
3	Neither Agree nor Disagree	27%
4	Agree	9%
5	Strongly Agree	55%
	Total	100%





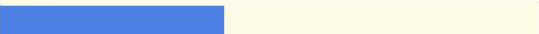
4. Promote higher quality architecture and screening of outdoor uses for sites that face South Avenue and South 7th Street.

#	Answer	%
1	Strongly Disagree	10%
2	Disagree	5%
3	Neither Agree nor Disagree	14%
4	Agree	14%
5	Strongly Agree	57%
	Total	100%





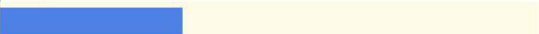
5. Should similar guidelines and standards be applied to the 1st Street corridor from South Avenue to Grand Avenue?

#	Answer		%
1	Strongly Disagree		14%
2	Disagree		0%
3	Neither Agree nor Disagree		24%
4	Agree		14%
5	Strongly Agree		48%
	Total		100%



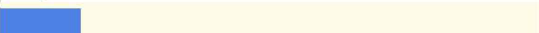


6. Limit expansion and construction of any new outdoor uses and operations (such as outdoor operation of rail yards, salvage yards, etc.) on properties that abut the South 5th Street right-of-way.

#	Answer		%
1	Strongly Disagree		14%
2	Disagree		14%
3	Neither Agree nor Disagree		14%
4	Agree		9%
5	Strongly Agree		50%
	Total		100%

7. Provide screening on the South 5th Street bridge that limits views to adjacent uses but still provides longer vistas to the east (Grand Mesa) and west (Colorado National Monument and Uncompahgre Plateau).

#	Answer		%
1	Strongly Disagree		23%
2	Disagree		5%
3	Neither Agree nor Disagree		23%
4	Agree		9%
5	Strongly Agree		41%
	Total		100%

8. Promote higher quality architectural features on building facades that directly face or can be seen from the 5th Street bridge on the east and west sides of the bridge.

#	Answer		%
1	Strongly Disagree		9%
2	Disagree		9%
3	Neither Agree nor Disagree		18%
4	Agree		18%
5	Strongly Agree		45%
	Total		100%

9. Do not allow billboards on the South 5th Street corridor.

#	Answer		%
1	Strongly Disagree		9%
2	Disagree		18%
3	Neither Agree nor Disagree		23%
4	Agree		5%
5	Strongly Agree		45%
	Total		100%

10. Create a mix of higher quality, new uses along South 7th Street and Riverside Parkway.

#	Answer		%
1	Strongly Disagree		9%
2	Disagree		5%
3	Neither Agree nor Disagree		27%
4	Agree		18%
5	Strongly Agree		41%
	Total		100%

11. Create a more pedestrian-friendly commercial core for area employees, residents and visitors.

#	Answer		%
1	Strongly Disagree		9%
2	Disagree		0%
3	Neither Agree nor Disagree		23%
4	Agree		14%
5	Strongly Agree		55%
	Total		100%

12. Integrate new commercial and mixed use with park, trail, recreation and open space uses.

#	Answer		%
1	Strongly Disagree		10%
2	Disagree		5%
3	Neither Agree nor Disagree		14%
4	Agree		19%
5	Strongly Agree		52%
	Total		100%

13. Do not allow billboards along the South 7th Street/Riverside Parkway business park mixed use corridor.

#	Answer		%
1	Strongly Disagree		10%
2	Disagree		0%
3	Neither Agree nor Disagree		19%
4	Agree		14%
5	Strongly Agree		57%
	Total		100%



14. Promote commercial and mixed uses along the north side of the Riverside Parkway that would transition to and screen industrial areas behind to the north.

#	Answer		%
1	Strongly Disagree		5%
2	Disagree		15%
3	Neither Agree nor Disagree		20%
4	Agree		30%
5	Strongly Agree		30%
	Total		100%

15. Use the remnant building of the historic sugar beet factory as a guide for scale, height and architectural elements for potential new development along Riverside Parkway.

#	Answer		%
1	Strongly Disagree		10%
2	Disagree		5%
3	Neither Agree nor Disagree		19%
4	Agree		19%
5	Strongly Agree		48%
	Total		100%






16. Promote higher quality architectural treatments for building facades that face the Riverside Parkway.

#	Answer		%
1	Strongly Disagree		14%
2	Disagree		5%
3	Neither Agree nor Disagree		5%
4	Agree		36%
5	Strongly Agree		41%
	Total		100%






17. Regulate screening of outdoor storage in the front yard area to minimize visual impact of these uses along the Riverside Parkway.

#	Answer		%
1	Strongly Disagree		14%
2	Disagree		9%
3	Neither Agree nor Disagree		5%
4	Agree		27%
5	Strongly Agree		45%
	Total		100%

18. Promote higher quality streetscape with minimal building setbacks, use of front yard space, signage, fencing and landscaping regulations.

#	Answer		%
1	Strongly Disagree		5%
2	Disagree		10%
3	Neither Agree nor Disagree		15%
4	Agree		40%
5	Strongly Agree		30%
	Total		100%

19. Do not allow billboards along the Riverside Parkway commercial/industrial corridor.

#	Answer		%
1	Strongly Disagree		5%
2	Disagree		5%
3	Neither Agree nor Disagree		33%
4	Agree		5%
5	Strongly Agree		52%
	Total		100%

20. Please provide any additional comments you may have.

Text Response
NO BILLBOARDS
I think your survey is biased - where do you say you want NO OVERLAYS
Encouraging progressive developers/landowners to make a few "1st moves" needs to be a kick-start for these standards. Once a few are on board this plan can be used as an incentive to get businesses to locate and upgrade in these districts. I envision that as the riverfront and other areas develop, GJ may be able to attract Denver businesses that cannot afford the downtown real estate they want there.
Looks good!
#7 waste of \$;#8 If you do this, no need for a screen; #9 I kinda like them; #10 Keep good reliable businesses in place; #12 if it will help our economy & Bring more businesses downtown; #14 Waste of \$. What is, is. I like seeing the rock yard. Keep it simple. Keep it real. Keep night lighting subtle, friendly, rather than glaring & blinding. Go for quality (which it sounds like you are doing). Yet please make it possible for "old-timers" to maintain their business without undue expense for dubious purpose. Keep our town Western! (that's my plea) open & friendly & not too fancy THANKS
I agree w/ all. The "strongly agree" s are of greatest importance. #15 that and lower
Mistake to not allow residential in rail district. ie: LODO in Denver, has the potential to be urban atmosphere that would attract young professionals to work & reside ie: living/working lofts. Densely populated "downtown" area can prevent urban sprawl, lessen carbon footprint and make this area a desirable place to live & work
1) This is a convoluted process. 2) Driving for answers you want 3) We need jobs, business & honor private property rights
With respect to the South Avenue corridor, is there still discussion about rerouting the I-70B corridor to Pitkin/South? If so, then that changes the type of character along that corridor, though pedestrian access should still be provided. South 5th Street (bridge) does serve as an entry point into the downtown; however, since it essentially "flies over" the existing industrial areas, it is not necessary to completely restrict those uses. The main focal point should be the end of the bridge and the forward vista into downtown, not what is underneath. The orientation of the Riverside Parkway paralleling Las Colonios Park presents something of a challenge for future commercial development, since direct access is only available at 7th St, 9th St, and Winters Avenue. These intersections should be the focus of the integration of commercial to open space concept. The areas along Kimball Avenue adjacent to the sugar beet factory are perhaps too far away from these nodes to be viable for mixed-use development

**Greater Downtown Plan Open House**  
**December 8, 2011**  
**Citizen Comments**

- I don't want changes in zoning OR overlays – leave it alone for now – that's the best way to stimulate job growth.
- # one priority should be emphasis on maintaining and enhancing the character of the downtown residential and business to provide character, charm and livability within the core area. Increased bike lanes/racks, pedestrian crossings and a strong control of architectural features will serve to ensure an attractive downtown for years to come.
- Would love to see better use of Whitman Park connected to the Museum, Renaissance Fair, Farmer's Market etc. An East/West traffic corridor south of Ute would make the area more pedestrian-friendly to downtown visitors.
- Must have strict sign codes & restrictions; NO lighted signs in residential district; Same for R-O zones.
- R-O must provide parking for residents & employees, clientele during business hours. Parking is a big problem in R-O areas where renters & employees are not allowed to park during business hours and take parking from residents. Sometimes R-O residents tie up parking for days at a time leaving homeowners without parking and must carry groceries and other items from alley or further from home. Limit the amount of families living in existing homes. Some homes have multiple families living there (with numerous vehicles).
- NO parking of vehicles on parkway.
- Support for the Ute/South shift of one-ways.
- Encourage continued support for the "Arts" and Museum in downtown.
- Support for safety for bicyclists and pedestrians. WALKABILITY!
- Support for parks and active recreation in the area.
- Agree with – no billboards on South Ave/S 7<sup>th</sup> Street/S 5<sup>th</sup> Street corridors.
- Promote higher architecture and screening for site South Ave.
- Set minimum height requirements for downtown – Build Taller.
- I like the corridor concepts connecting downtown to the river. I live & work downtown, bike the river and only need a couple connections to get there, so 5<sup>th</sup> Street & 7<sup>th</sup> Street corridors bike-friendly are important to me personally; and I think, good for the area in general.
- With proposed rezone of block between 5<sup>th</sup> & 6<sup>th</sup>, Ouray & Chipeta – would a credit union or bank be a compatible use? – Eve Tallman 683-2424.
- I agree that building sizes need to blend as they approach the residential districts from Grand Ave.
- We need better ways to notify owners of the meetings. I didn't know of the Chipeta Elementary meeting.
- Clean tamarisk, Russian Olive (tents, dogs) between S 5<sup>th</sup> Street and Railroad Bridge.
- Improve bike, pedestrian route Main to River trail.
- Put soft path along river thru Los Colonias (something easy to replace if floods).
- Put pedestrian bridge over cut in Jarvis pond from soft path loop.
- Improve pedestrian, bike route W. Main to River (by Dual Immersion Academy school, etc).
- I would like to see more development along the river. Has city planning ever taken a "field trip" to Littleton to see what they have done with their river trail? I grew up there and saw it transform from trash to a treasure.
- Since this town is named after the confluence of 2 rivers, why not have a confluence park? Buy all the trashed area down at the confluence.
- River District – Concerned with Cities priority on present & future Riverfront Trail maintenance. Does not appear to be a priority. Also concerned with City "real" concern & commitment to the River District development for the Park & Future Trail to 29 Road.
- The industrial use of the 2 blocks south of Pitkin should remain due to the already constructed infrastructure i.e. – rail yards, holding tanks, etc. to re-set this area into another location would be very cost prohibited.

Regarding the workshop at City Hall 09.19.2011 regarding  
Zoning and Future Land Conflicts

1) South downtown area;

1) South Downtown, the OPTION 3 submitted by Kathy Portner was the plan she said matched current use and that she would propose. The City Council concurred. That the “triangle on 6th Street should be I-1, historical use of rail; take change North to South side of South Ave.; West side of 7th to accommodate current users. Council agreed with one dissenting vote.

There was talk of “conditional use” and “over-lays”.  
The Mayor’s remarks were that both of these ‘conditions’ on zoning creates unpredictability. Both should be avoided whenever possible. Council concurred.

At the work shop, there was also concern that the South Downtown should be considered separate from the Downtown Plan. However, we note this has been put as one.

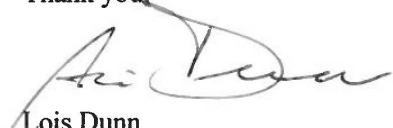
Meeting 12.08.2011 at Whitman Center

No property owners have been for a change of zoning in South Downtown. The Council and Planning Department discussed this at length at 9.19.2011 workshop and felt uses should be kept for current users and zoning should remain the same.

- 1) Why are the same meetings necessary – over and over?
- 2) Why are minutes not kept of previous meetings?

Please consider keeping the zoning the same for the South Downtown so businesses have predictability and continue operating – instead of wondering what the City is going to do to them.

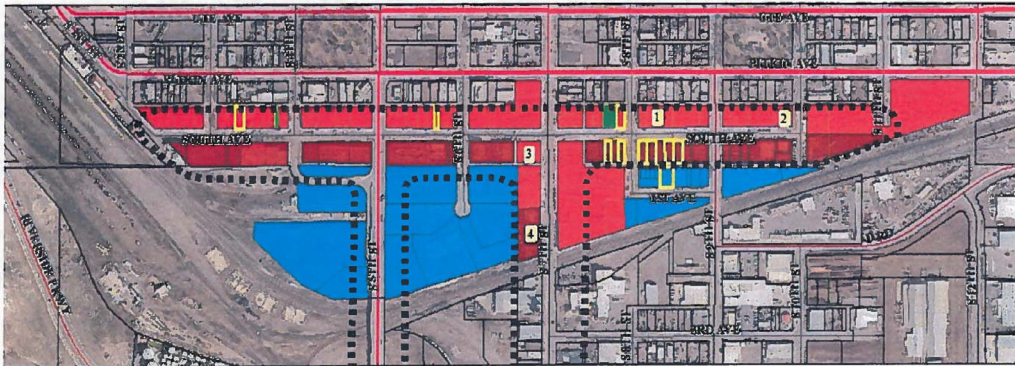
Thank you.



Lois Dunn  
PO box 1889  
Grand Junction CO 81502  
970-243-8843  
loisgdunn@gmail.com

Attached: Proposal 3 as presented at the 9.19.2011 workshop

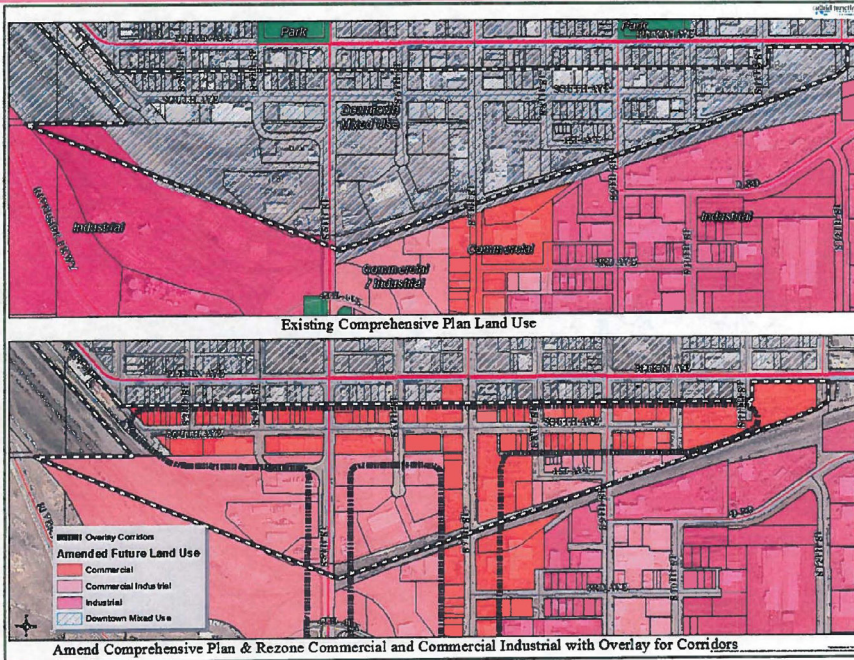
## OPTION 3: Omitted Area South Ave to RR Tracks Potential Zoning



Potential Zoning	
	C-1 (Light Commercial)
	C-2 (General Commercial)
	I-1 (Light Industrial)

Non-Conforming Use in Potential Zone	
1	Warehouse - conforms in C-2, not C-1
2	Private Gas Pumps - conforms in C-2, not C-1
3	Flea Market - conforms in C-1, no outdoor operation
4	Dible Oil - conforms in C-2, not C-1
■■■■	Overlay Corridors
■	Non-Conform Use Only with Existing Zoning
□	Non-Conforming Use with Existing & Potential Zoning

## OPTION 3: Omitted Area South Ave to RR Tracks Extend Concepts from South Downtown Plan



### **DOWNTOWN OVERLAYS**

- Transitional Areas – Need parking guidelines for business & residential uses. What about parking guidelines for rentals? Currently property owners do not have any requirements to have renters park on or in front of units. What about off street parking requirements? We need them.
- Transitional Area – Text seems to primarily be geared to business discussion. We need guidelines & standards to be inclusive of the residential aspects!! All readers need to understand we are an inclusive neighborhood comprised of residential and business interests. Current zoning states “Residential/Office”.
- 1<sup>st</sup> Street from Main St south to the depot – slow traffic/Parkway/landscaped median, shift of Ute/Pitkin to Pitkin/South.
- On street parking should be encouraged to slow traffic and act as a buffer on collector streets (e.g. Grand Ave) to buffer traffic from residential yards. Parked cars area a safety buffer for children playing in yards.
- No uses earlier than 7:30 am or after 8 pm. The Yoga Collective would immediately be in violation of this on 10<sup>th</sup> & Main.

### **PROPOSED LAND USE MAP**

- It doesn't seem to make sense putting a Commercial/Industrial area on the river, breaking up the continuity of the park areas.
- Train Depot – Why not encourage Amtrak, GVT & Greyhound Bus Co. to make a true intermodal center @ 1<sup>st</sup>/2<sup>nd</sup> and Pitkin? Then encourage higher density housing and mixed use for this area.
- Isn't the Neighborhood Center supposed to serve clients who “walk” from the immediate neighborhood? On 1<sup>st</sup> Street there are barely sidewalks to serve the NC. Isn't this commercial area being used by the “Driving” community more than the walking neighbors? Do you expect this to change?

### **PROPOSED ZONING MAP**

- The infamous Brady property will be surrounded by parks on 3 sides according to the FLU. How does the I-1 zoning make any sense? Why not get Brady to trade for some vacant land to it's north and east.
- Rail industrial zone inhibits residential uses & remodels – should be mixed use zone. This area is full of houses – not just industrial uses.
- County zoning missing from maps; any changes to County zoning considered?
- Brady Trucking need to be moved to the vacant industrial land to the east of where they are now and the land by the River needs to be part of the Park.
- Winters/Kimball – 7<sup>th</sup>/9<sup>th</sup> – There area residences there that should be retained for that use! Why can't we have residential blended use in ANY work areas? It makes not sense to segregate them and force their use change when we supposedly are trying to create a walkable city.
- Why is the Industrial use STILL specified by the Riverfront Trail? OM elevation looks right down on that area. Retain our Riverfront for a beautiful recreational feature, please!!!
- BP – doesn't allow museums; concerned that some retail sales are not allowed in the BP. Compare BP to C-2 uses.
- S 7<sup>th</sup> Street – look at potential of leaving the C-2 zoning and utilize the overlay zone for design standards.
- Would like to do mixed use, but business – residence requires owner or employee live there.
- Suggest incentives for new uses or upgrades we'd like to see – 1111 S 7<sup>th</sup> Street.
- Attached letter from John Crouch.
- Attached letter from Margaret Cox.

### **CIRCULATION/TRAILS/BUS**

- NO MORE ROUNDABOUTS. THEY DO NOT AID IN TRAFFIC FLOW! Courtesy is not practiced, yield signs are not followed. They increase traffic congestion!

- The homes that area between 7<sup>th</sup> & 9<sup>th</sup> and Winters to Noland should be preserved as residential. This area needs residential if you want such things as a brew pub & other business to make this area a beautiful thriving area – Not a home for the homeless – Thank you, Kathy Jordan.
- 1<sup>ST</sup> & Grand – Uh.... Roundabout! It’s a perfect intersection; DCOT & the City already own the land.
- Main Street east of 8<sup>th</sup> Street is too wide. Install some central medians or something to slow traffic, increase interest, create more neighborhood identity.
- 7<sup>th</sup> Street needs a sign that keeps large 53 feet long trucks off of it between Ute and Grand.
- 7<sup>th</sup> and Grand roundabout may cause vehicle/pedestrian conflict. Have walk signs/lights now. Traffic exiting roundabout does not expect or look for pedestrian traffic. These conflicts exist at 7<sup>th</sup> and Main roundabout.
- #2 – Spruce St @ Grand Ave – needs to be a right-in/right-out only – unclear if it is planned that way.
- #3 – Spruce & Main improvements look great - & should be a priority with new and increased traffic on South Spruce – Mesa County Central Services. In short term remove diagonal parking spaces closest to the intersection – poor sight distance now.
- Will a pedestrian connection from Ute Ave to Pitkin Ave be built where 6<sup>th</sup> St is now closed for the Fire & Police Facilities?
- More marked bike lanes would improve safe traffic flow.

### **CORRIDOR OVERLAYS**

- 1101 Kimball owner Bryan Wiman – We support the corridor overly that affects our property “Sugar Beet Factory”. We understand that the existing use is not affected and that is important to us. We also respect the significant beauty of the Colonias Park area and we support re-development if market demands.
- Indian Road plans contradict this?? Follow up on – 396/398 & 397/399 Indian Road
- Moving Pitkin/Ute one way traffic to Pitkin/South should not occur. The Grand Valley Transit facility would be very negatively impacted – both pedestrian users and bus ingress-egress from such a drastic increase in traffic. – John Heidernan.
- A traffic light at S 5<sup>th</sup> and South Ave would back up north bound traffic significantly worse on the overpass. The one at Pitkin already does. Moving it one block south – not a good idea. - John Heidernan.
- The proposal to move Pitkin & Ute one way traffic to Pitkin & South is of concern to us. That would essentially put our 2 businesses, Enterprise & All Pets Center in the median strip of I-70B. Our access is already a huge problem for our clients and this would make it worse.

### **PARKS**

- Brady trucking should expand to the east NOT on the riverfront.

### **GENERAL**

- Please, no more parking meters. – Rob Rubin

City of Grand Junction  
City Neighborhood Services  
Attn Kristen Ashbeck, Senior Planner  
250 North 5<sup>th</sup> Street  
Grand Junction, CO 81501

05/15/2012

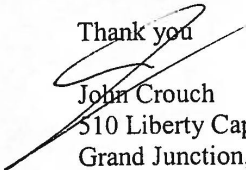
Dear Kristen:

Thank you for speaking with me regarding the proposed rezone from General Commercial (C-2) to Light Commercial (C-1).

The area of concern is commercial and best suited for the highest zoning available.. The railroad, junk yards, metal processors etc. and decades of investment in the area below Pitkin is not conducive to spotty zoning. Keep it all the highest zoning available.

To do what is suggested will force businesses that need higher zoning to move to Fruita or to the County.

Thank you

  
John Crouch  
510 Liberty Cap Court  
Grand Junction, Co 81507



Rec'd 4/25/12

Margaret Cox  
P.O. Box 2422  
Grand Junction, CO 81502

Grand Junction City Council  
City Hall  
250 N. 5<sup>th</sup> St.  
Grand Junction, CO 81501  
April 24, 2012

**RE: Re- Zone and Over-Lay Plan for South Downtown Grand Junction**

Dear City Council members,

I have recently been made aware of proposed changes to the zoning of south downtown that would negatively affect my commercial property at 805 Struthers Avenue. This 2 acre parcel is presently zoned C-2 and borders Riverside Parkway between 8<sup>th</sup> and 9<sup>th</sup> street on the south side. It is adjacent to city land that will someday become Los Colonias Park. Long range city plans include possible commercial development of privately owned land in my specific area. I am not opposed to the gradual development of the area for recreation, retail and even some residential uses. I have reviewed the proposals over the years with some interest. The only consistent conclusion I draw of these proposals is that they are projected endlessly into the future. There is never a start date.

The Planning Department has not been able to give any specific time-line on the creation of the park, saying it will take way more funding than is now in the budget. Re-zoning of that area should not take place until much of the park is completed and can demonstrate a draw for both the public and investors.

One compromise could be to change the zoning to C-1 only for individual parcels as requested by their owners. I would have no objection to that change, but would not want 805 Struthers re-zoned to C-1 at this time.

My property is a non-conforming site with non-conforming use since the zoning changed from Industrial to C-2. In years past I have sacrificed the flexibility of industrial usage. Should I decide to change my use or expand my facilities the C-2 designation already requires a major investment to meet city building codes, screening, landscaping and parking, etc. If the zoning were to change to C-1 I would no longer be allowed to use the land for storage and other outdoor uses if I changed my activities or have a building expansion over 20 percent, or if a potential buyer envisioned another use outside the C-1 zoning specifications. This limits my property's potential. It devalues my property. Quibbling about which zoning is more valuable is not productive at this point. I value my land based on the revenue it generates and its potential to provide me an income far into the future. This is based on the flexibility zoning allows.

Although the argument may someday be made that the potential use will be more valuable as a C-1 property, I see no evidence at all of this being so at this time. There is almost no retail anywhere in the area. There is no city park as a destination for the public. Efforts at upgrading the river corridor have been minimal, with the exception of the parkway itself and are not visible to the traveler on that parkway. There is no entrance to the park, no parking lot or access to the river or the pedestrian bridge to the east from the Parkway. No business has invested in a C-1 business on Struthers or anywhere near. I feel this is not the time to make changes to zoning that effect the possibilities for land owners to find tenants, have a variety of commercial functions, or ultimately to sell the real estate.

One exception, in particular, to the "grandfathering" of my outdoor usage I find unfair and would like addressed. If a property is vacant for more than one year the zoning requirements will kick in and protection as a "non-conforming" site will be lost. This does not accommodate the economic downturn that both Grand Junction and our national economy have experienced. Coming years could even be more challenging. This would represent a "taking" of sorts to force a revision of that non-conforming status. Commercial properties often times cannot be occupied consistently and, unfortunately, can remain vacant for well over twelve months. Owners already have taxes and other related expenses and should not have to lose their effective zoning status because of the misfortune of a vacancy. I would ask that this provision be removed from any Over-Lay or Re-Zoning considerations.

Please leave my property at 805 Struthers Avenue at the current zoning of C-2.  
Thank you for your consideration.

Sincerely,



Margaret Cox

970-778-9000

>>> Marie Ramstetter <ramstet@gmail.com> 3/14/2012 3:46 PM >>>  
I am still opposed to the down zone

On Wed, Mar 14, 2012 at 12:23 PM, Kristen Ashbeck <[kristena@ci.grandjct.co.us](mailto:kristena@ci.grandjct.co.us)> wrote:  
Hi Marie,

Thank you for your comment regarding the Greater Downtown Plan. We understand your concerns and would like to provide you with additional information concerning the proposed zone change of your property from C-2 to C-1. If you review the attached information, it outlines the differences between the two zones. You will see that there is not a great difference between the uses that you might consider for your property, especially since the building already exists and there is very little room on the site for expansion or for outdoor uses. The zone change certainly would not impact a current use of the building or the building itself.

If after reviewing the information (or if you are unable to open the attachment), you have further questions or comments, please do not hesitate to contact me.

Kristen Ashbeck  
Neighborhood Services / CDBG  
[970.244.1491](tel:970.244.1491)  
[kristena@gjcity.org](mailto:kristena@gjcity.org)  
[970.256.4114](tel:970.256.4114) fax  
City of Grand Junction  
250 North 5th Street  
Grand Junction CO 81501

>>> Marie Ramstetter <[ramstet@gmail.com](mailto:ramstet@gmail.com)> 3/13/2012 2:19 PM >>>

I am absolutely opposed to your attempt at down zoning my property, tax id 2945-231-00-008 located at 803 S 7th Street. Consider this a formal protest to the City.

**From:** Donna <donna@bresnan.net>  
**To:** <kristena@gjcity.org>  
**Date:** 7/29/2012 7:10 PM  
**Subject:** Greater Downtown Area Plan

Dear Kristen,

Thank you for your hard work and dedication to the development of the Greater Downtown Plan. We have reviewed the current draft and it is our hope that the Plan is approved by the City Council.

Unfortunately, a plan is only as good as the adherence, and we are not optimistic that the City will direct the appropriate departments to assure compliance. As you are aware, several ordinances currently on the books are not enforced. We are not optimistic that this will change with a new Plan.

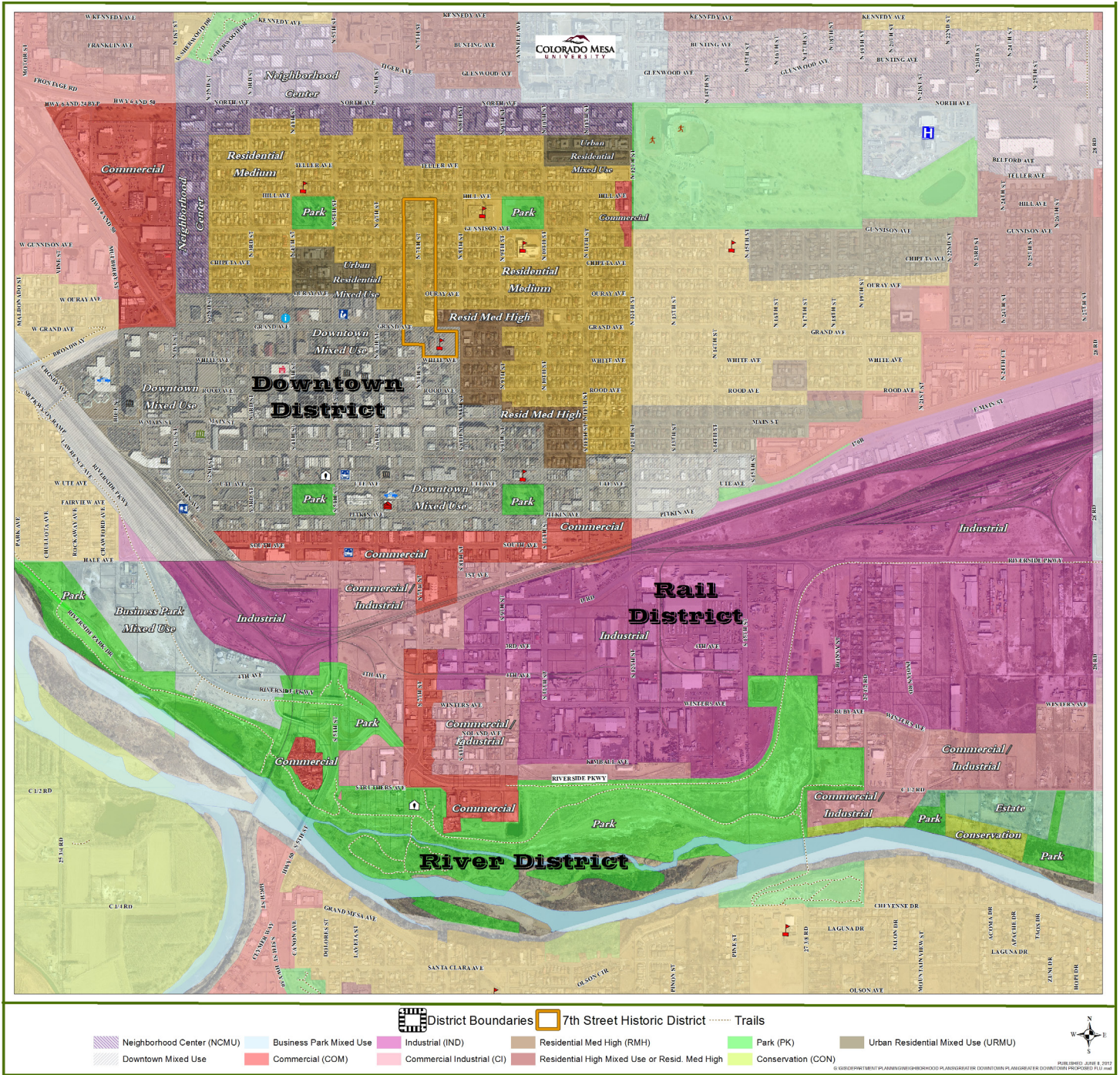
Sincerely,

James and Donna Patton  
341 Gunnison Avenue  
970-245-1678

Exhibit B - (d) Districts to Implement the Comprehensive Plan.

Zoning District	Comprehensive Plan Land Use Designation																
	RESIDENTIAL								NONRESIDENTIAL								
	Low				Medium		High										
	Rural	Estate	RL	RML	RM	RMH	RH-MU	UR RH-MU	Commercial	NC-MU	VC-MU	DT-MU	Industrial	C/I	BP-MU	P & OS	Conservation/ Mineral Extraction
RR	.	.	.	.													.
R-E	.	.	.	.													
R-1	.	.	.	.													
R-2	.	.	.	.													
R-4	.	.	.	.	.	.											
R-5	.	.	.	.	.	.											
R-8					.	.			.	.				.			
R-12					.	.			.	.				.			
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R-O					.	.	.	.	.	.	.	.		.			
B-1								.	.	.							
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C-1									.	.	.	.					
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CSR	.													.	.		.
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MXS-3																	
MXR-5																	
MXG-5										.	.						
MXS-5																	
MXR-8																	
MXG-8											.						
MXS-8																	

# EXHIBIT C – GREATER DOWNTOWN PLAN FUTURE LAND USE MAP



## **ATTACHMENT 2**

### **CITY OF GRAND JUNCTION, COLORADO ORDINANCE NO. \_\_\_\_\_**

#### **AN ORDINANCE AMENDING THE ZONING AND DEVELOPMENT CODE TO ADD SECTION \_\_\_\_\_ GREATER DOWNTOWN PLAN ZONING OVERLAY**

##### **RECITALS.**

One of the recommendations of the Greater Downtown Plan was to create standards and guidelines as a first step for implementing the Plan. The Plan recommends adoption of these standards and guidelines as a zoning overlay to apply to portions of the Greater Downtown area as specified in the Plan.

Overlay zoning is one way to create a more flexible and discretionary alternative to traditional zoning. An overlay zone is defined as “an overlay district superimposed on one or more established zoning districts which may be used to impose supplemental regulations on development in these districts, permit uses otherwise disallowed, or implement other forms of incentives”.

An overlay zone supplements the underlying zone with additional standards, guidelines and/or incentives while generally leaving the underlying zoning regulations in place. Examples might include different setbacks, increased height allowance or varied allowed uses. A parcel within the overlay zone area will thus be simultaneously subject to two sets of zoning regulations: the underlying and the overlay zoning standards and guidelines.

Overlay zone boundaries are not restricted by the underlying zoning district’s boundaries. An overlay zone may or may not encompass the entire underlying zoning district. Likewise, an overlay zone can cover more than one zoning district, or even portions of several underlying zone districts.

The Greater Downtown Plan Zoning Overlay is being proposed to cover the Central Business District (CBD), Residential and Transitional subareas of the Downtown District as well as those properties that have frontage on defined street corridors including South 5<sup>th</sup> and 7<sup>th</sup> Streets, Pitkin and South Avenues and the Riverside Parkway.

The Grand Junction Planning Commission is charged with the legal duty to prepare and consider and recommend action to City Council regarding amendments to the Zoning and Development Code for the City.

The Greater Downtown Plan Zoning Overlay was heard in a public hearing by the Grand Junction Planning Commission on July 24, 2012 where the Planning Commission recommended that the City Council adopt the Zoning Overlay.

**NOW THEREFORE, BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF GRAND JUNCTION:**

The Zoning and Development Code is hereby amended to add section \_\_\_\_ entitled "Greater Downtown Plan Zoning Overlay" to be applied to the areas described in the Greater Downtown Plan Zoning Overlay (Exhibit A) and generally described above.

That the Greater Downtown Plan Zoning Overlay, City of Grand Junction, Colorado, in the form of the document attached hereto, and as recommended for adoption by the Grand Junction Planning Commission is hereby adopted.

The full text of the Ordinance, including the text of the Greater Downtown Plan Zoning Overlay, in accordance with paragraph 51 of the Charter of the City of Grand Junction, shall be published in pamphlet form with notice published in accordance with the Charter.

**INTRODUCED** on first reading the \_\_\_\_ day of \_\_\_\_\_, 2012 and ordered published pamphlet form.

**PASSED** and **ADOPTED** on second reading the \_\_\_\_ day of \_\_\_\_\_, 2012 and ordered published in pamphlet form.

\_\_\_\_\_  
President of City Council

ATTEST:

\_\_\_\_\_  
City Clerk



# Greater Downtown Plan Zoning Overlay

# DRAFT



## TABLE OF CONTENTS

### INTRODUCTION

### CORRIDOR STANDARDS AND GUIDELINES

1. Overall Corridor Vision/Concepts
  - A. Architectural Features
  - B. Building Materials
  - C. Streetscape and Site Design
  
2. Commercial Corridor Standards and Guidelines
  - A. New Site Development or Redevelopment
  - B. Architectural Design of New Buildings/Additions or Substantial Exterior Remodel
  - C. Landscaping for New Site Development or Redevelopment
  - D. Signage
  
3. Industrial Corridor Standards and Guidelines
  - A. 5<sup>th</sup> Street Industrial Corridor Standards
  - B. Riverside Parkway Industrial Corridor Standards

### DOWNTOWN DISTRICT STANDARDS AND GUIDELINES

1. Overall Downtown District Vision/Concepts
  
2. Area-Wide Standards and Guidelines
  - A. General
  - B. Transportation
  - C. Entryways and Public Signage
  
3. Central Business District (CBD) Standards and Guidelines
  - A. Application of Standards and Guidelines
  - B. Land Use / Architecture
  - B. Site / Area Improvements
  
4. Residential Areas Standards and Guidelines
  - A. Policies
  - B. Standards
  - C. Guidelines
  
5. Transitional Areas Standards and Guidelines
  - A. Policy
  - B. Standards
  - C. Guidelines

## **INTRODUCTION**

One of the first steps in implementing the Greater Downtown Plan is a Zoning Overlay. The overlay is intended to provide guidance and criteria for the planning, design and implementation of public and private improvements in the Greater Downtown area. If properly administered and adhered to, the standards and guidelines should result in public and private development improvements (or a combination thereof) that achieve, as a minimum, a common level of quality in terms of site design, architectural design, landscaping and other site improvements.

The general purposes of the standards and guidelines are to support the overall goals of the Greater Downtown Plan.

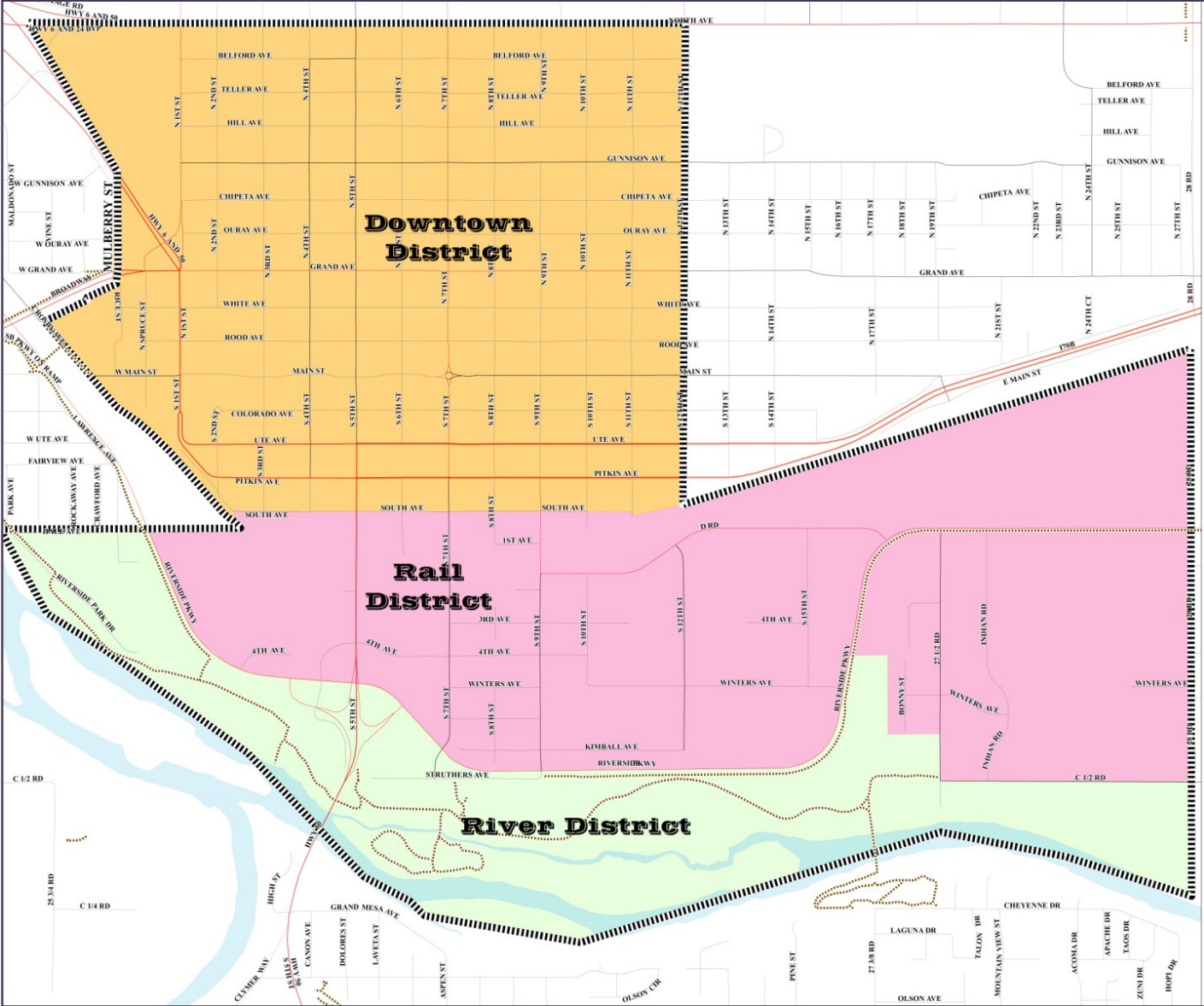
- Maintain and enhance the economic, cultural and social vitality of Greater Downtown Promote downtown living by providing a wide range of housing opportunities in appropriate areas within Greater Downtown.
- Enhance the transportation system to accommodate automobiles, bikes and pedestrians, and provide adequate, convenient parking.
- Stabilize, preserve, protect and enhance the downtown residential neighborhoods.
- Promote and protect the unique identity of Downtown.

The standards and guidelines were developed upon an analysis of the existing character of the Greater Downtown area. The area was divided into subdistricts and the Downtown District was further divided into subareas based on existing zoning, character of existing development and potential for redevelopment opportunities. In addition, primary corridors were identified for which overlay guidance was created. The subdistricts, subareas and primary corridors are shown on the maps on the following pages.

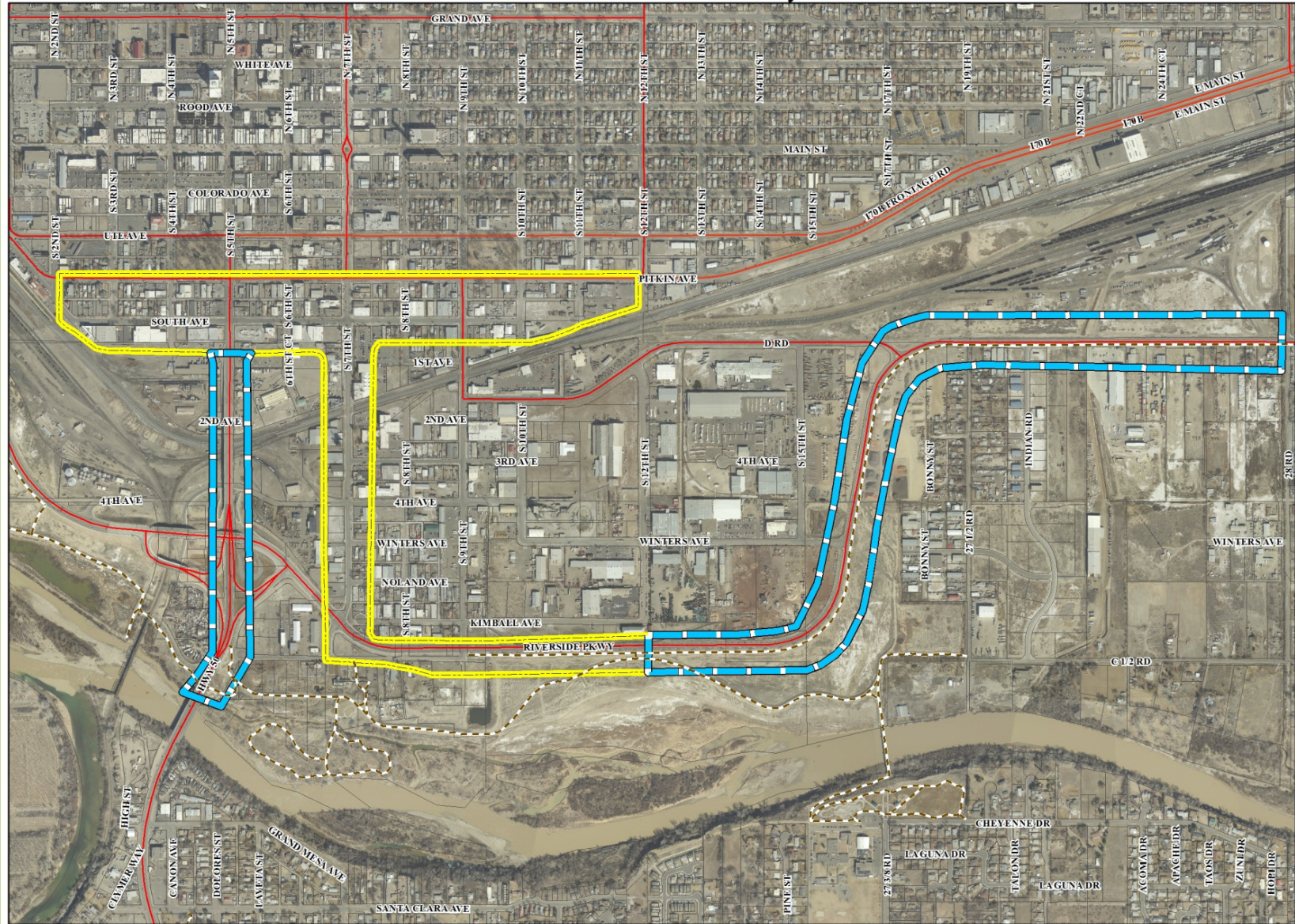
These standards supplement other development regulations such as the City of Grand Junction Zoning and Development Code, which includes detailed criteria by zone district, planned development regulations, design and improvement standards, supplemental use regulations, sign regulations and the City Transportation and Engineering Design Standards (TEDS). In the instance the following standards are silent on a development concern, the existing regulations shall apply.

The standards identify design alternatives and specific design criteria for the visual character and physical treatment of private development and public improvements within Greater Downtown. They are adopted through an overlay zoning district, which will establish the means by which the standards are administered and enforced. The Director will make all decisions and appeals and variance requests will be heard by the City of Grand Junction Planning Commission. The Downtown Development Authority (DDA) will be a review agency for all applications and will make recommendations for proposals in the Central Business District.

Greater Downtown Plan Subdistricts



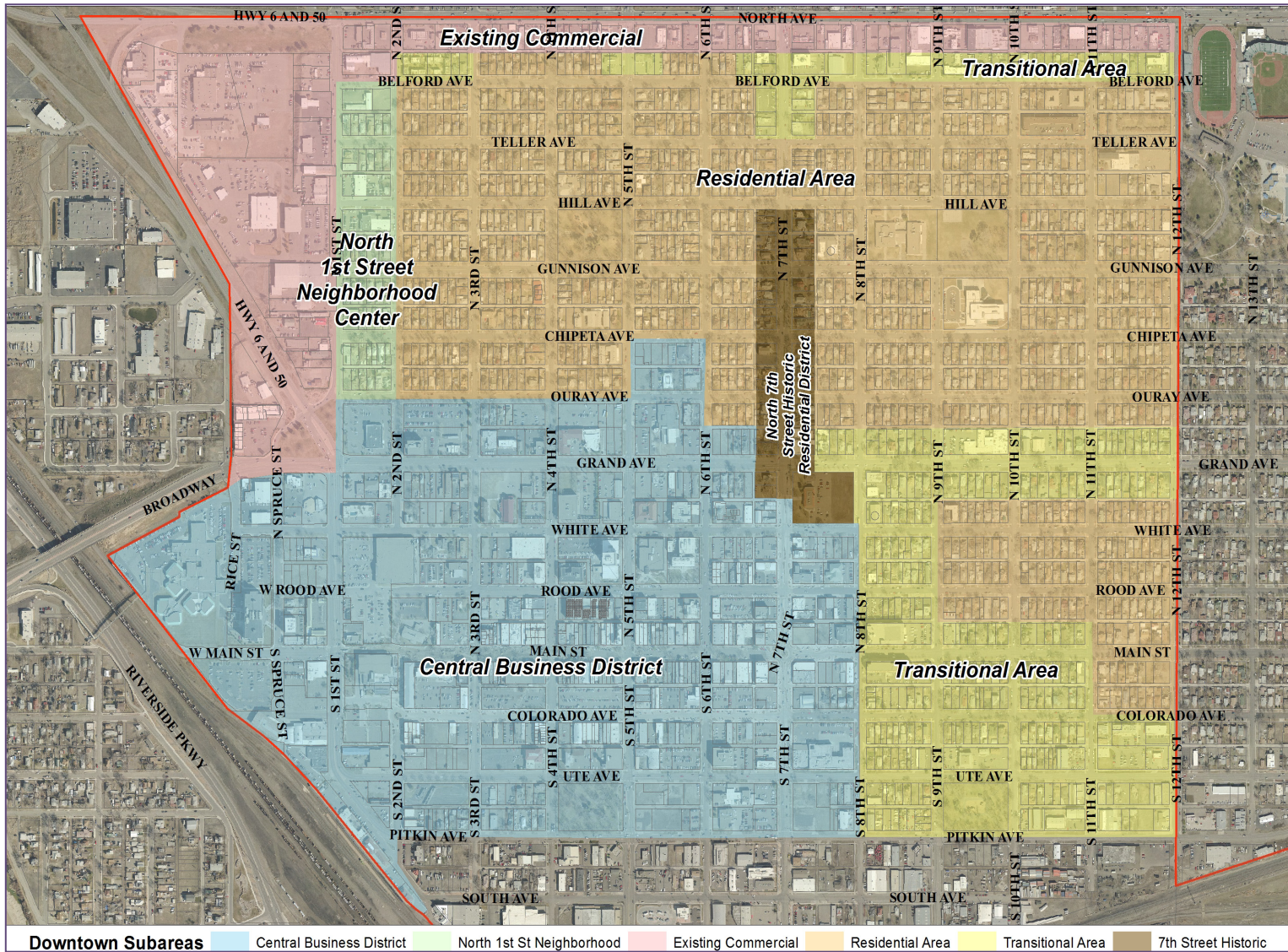
# Greater Downtown Overlay Corridors



**Greater Downtown Overlay Corridors** ▬▬ Industrial Corridor ▬▬ Overlay Corridor - - - Trails

© 2015 DEPARTMENT OF PLANNING & RECREATION. GRAND JUNCTION, COLORADO. ALL RIGHTS RESERVED.

### Downtown District Subareas



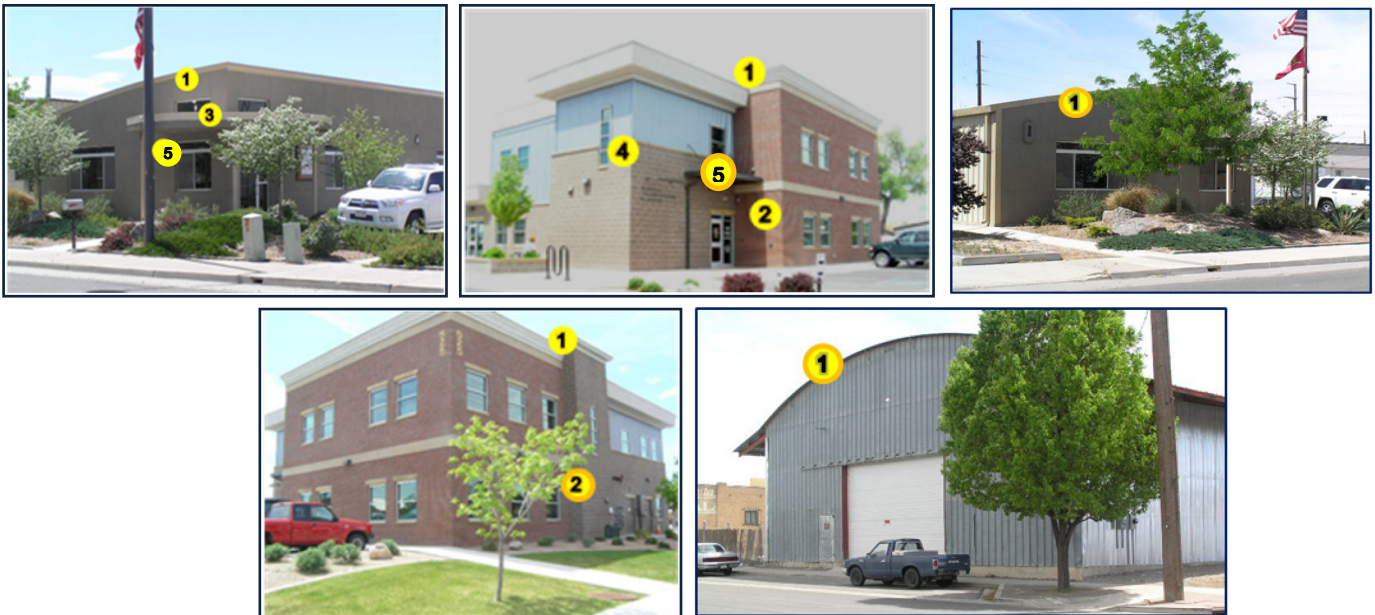
## CORRIDOR STANDARDS AND GUIDELINES

### 1. OVERALL CORRIDOR VISION/CONCEPTS

The Greater Downtown Plan Zoning Overlay standards for primary corridors in the River and Rail Districts begin to implement goals of the *Greater Downtown Plan* to 1) improve the visual impact of development along the corridors; and 2) promote higher quality architectural treatment and site design as new development and redevelopment occurs along the corridors. The visual features identified below define the concepts proposed along the corridors and, as implemented as new development and redevelopment occurs, will shape the desired character of the built environment. The features are grouped into three categories: Architectural Features; Building Materials; and Streetscape and Site Design.

#### A. Architectural Features

1. Façade modulation and roofline variations. The corridors can be improved through use of visually interesting architectural features that are designed to reduce mass and scale, including variation in the building form with recessed or projecting bays and variation in the rooflines.



2. Façade design. Design details can be used to emphasize architectural features such as the modulation and roof line changes discussed above or other features such as entryways and windows. Façade design details that break up a façade and add visual interest include:

- a. Columns and pilasters that help break up a horizontal plane of a building or other site feature.
- b. Change of material such as on an exterior wainscoting panel.
- c. Accent colors that help define and/or accentuate architectural features.

3. Defined entry. Many of the other architectural features above can be used to accentuate and define the primary entrance to a building, add architectural interest, as well as make the building more pedestrian- or customer-friendly.

4. Window sizes and shapes. The shape and sizes of windows used on a building break up the façade, provide visual variety and provide a pedestrian- and customer friendly character.

5. Awnings and porticos. Awnings and porticos are encouraged to help provide architectural interest.

## B. Building Materials

1. Mix of materials. Variations of materials used on exterior facades break up large building forms and walls.



2. Different textures, colors and tones. Use of different textures, colors and tones provide visual interest and can be used to accentuate architectural features.



## C. Streetscape and Site Design

Streetscape features along the primary corridors within the Rail and River Districts in Greater Downtown also help establish the visual character of the corridors. Examples of the desired characteristics are illustrated below.

1. Building Placement. Buildings are encouraged to be brought forward on a site to the street or sidewalk.



2. Landscaping. Many of the streets within the Rail and River Districts do not have, nor are planned to have, a sidewalk. However, streetscape interest can be created through landscaping, encouraging use of xeriscape concepts.



*Example Existing Streetscape Landscaping*

3. Streetscape Design. An urban streetscape is desired that includes hard surfaces, tree grates and street trees. This design concept exists along some of the corridors in the Rail and River District. The urban streetscape along the street can be blended with the site design of the adjacent parcel so that on-site landscaping complements and enhances the existing streetscape.



*Existing 7<sup>th</sup> Street Landscaping*

4. Detached sidewalks . Where possible, detached sidewalks are desired along primary corridors and already exist along some streets. The park strip between the curb and sidewalk can provide the landscape relief for an adjacent developed property.



*Existing Detached Sidewalk on Riverside Parkway (left) and South Avenue (right)*

5. Outdoor storage, display and operations. Minimize the visual impact of outdoor storage, display and operations areas through placement on a site behind a building, to the rear of the property or otherwise screened.



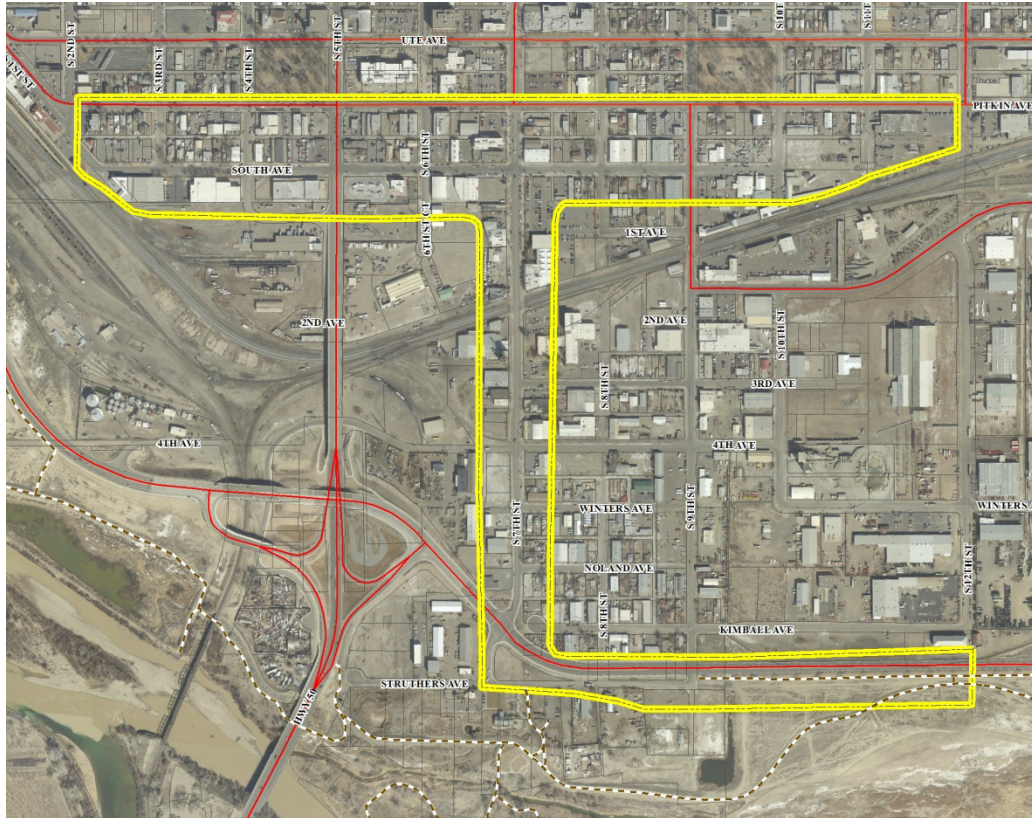
*Examples of Outdoor Storage and Operations Located Behind Buildings on 7<sup>th</sup> Street (left) and Riverside Parkway (right)*



*Screening Example on Riverside Parkway*

## 2. COMMERCIAL CORRIDOR STANDARDS AND GUIDELINES

The following Commercial Corridor Standards and Guidelines apply to those parcels on the south side of Pitkin Avenue between 2<sup>nd</sup> and 12<sup>th</sup> Street, all parcels with frontage on the east side of 2<sup>nd</sup> street between Pitkin Avenue and South Avenue, all parcels with frontage on South Avenue between 2<sup>nd</sup> Street and 12<sup>th</sup> Street, all parcels with frontage on South 7<sup>th</sup> Street between Pitkin Avenue and the south side of the Riverside Parkway and all parcels with frontage on the Struthers Avenue/Riverside Parkway area from just west of 7<sup>th</sup> Street to the alignment of 12<sup>th</sup> Street.



### A. New Site Development or Redevelopment

#### Policies

1. In order to prevent parking from dominating the visual setting of the commercial corridors, front yards shall allow only landscaping, sidewalks, driveway access to parking areas and signage. No parking shall be allowed in the front yard.

#### Standards

1. Outdoor storage and permanent display areas shall only be allowed in the rear half of the lot, beside and/or behind the principal structure. Portable display of retail merchandise may be permitted subject to the provisions of the Zoning and Development Code.

2. Parking is not to be located in the front yard – all parking shall be located behind or to the side of the building.

## **Guidelines**

1. Residential uses are allowed as allowed in the Light Commercial (C-1) zone district in the Zoning and Development Code, regardless of how the property is zoned.

## **B. Architectural Design of New Buildings/Additions or Substantial Exterior Remodel**

### **Standards**

1. For construction of new buildings and additions, or substantial exterior remodel (see definition below), the façade(s) on all buildings that face a street within the Greater Downtown Commercial Corridor shall be designed to relate directly to and reinforce the pedestrian scale and quality of the adjacent streets, civic and open spaces.

2. The façade(s) of a new building, addition or substantial exterior remodel (see definition below) that face a street within the Commercial Corridor or that face the public Riverfront Trail, shall have visually interesting architectural features and patterns that are designed to reduce mass and scale and reflect the desired vision for the corridors as described in Section 1 on pages 9 through 12. In order to do so, the façade(s) of a new building, addition or substantial remodel that face a street within the Commercial Corridor shall exhibit a minimum of 3 of the following 9 architectural design elements.

a. Variation in materials, material modules, expressed joints and details, surface relief and texture to break up building forms and wall surfaces. Such detailing may include sills, headers, belt courses, reveals, pilasters, window bays or similar features.

b. Façade articulation/variation such as recessed or projecting bays or pilaster/column projections a minimum of every 30 feet.

c. Variation in color.

d. Facade feature that emphasizes the primary building entrance through projecting or recessed forms, detail, color and/or materials.

e. Variation in roof lines/roof materials in order to add interest to and reduce the scale of buildings or expanses of blank wall. This can be accomplished through design elements such as overhangs/eaves, recesses/projections, raised cornice parapets over doors or bays and peaked roof forms.

f. Screening of mechanical equipment either located on the roof or on the ground.

g. Windows.

h. Window or entry awning.

i. Other architectural features that achieve the goals of the overall corridor vision/concept as outlined on pages 8 through 11 as determined by the Director.

3. Definition of Substantial Exterior Remodel – Exterior building alteration that is greater than or equal to 65 percent of the value of the existing site and building. This increase shall trigger 100 percent site development compliance and 100 percent architectural standards compliance. Value shall be the greater of: total actual value per the Mesa County Assessor; or a current appraisal.

**Guidelines**

1. Exterior building materials should be durable, economically maintained, and of a quality that will retain their appearance over time.

**C. Landscaping for New Site Development or Redevelopment**

**Standards**

1. On-site landscaping shall be required per the Zoning and Development Code. The total amount of required landscaping may be reduced by the Administrator if a minimum of 2 of the following five (5) conditions exist or are proposed.
  - a. A higher quality of architectural design is achieved with greater than 3 of the architectural design elements listed in B. above.
  - b. Fifty percent (50%) or more of the building façade is setback no more than 15 feet.
  - c. Street trees and other finished landscaping or hardscape exists within the public street immediately in front of the property.
  - d. All parking is located to the side and/or rear of the building.
  - e. All outdoor storage and operations are located behind the building.

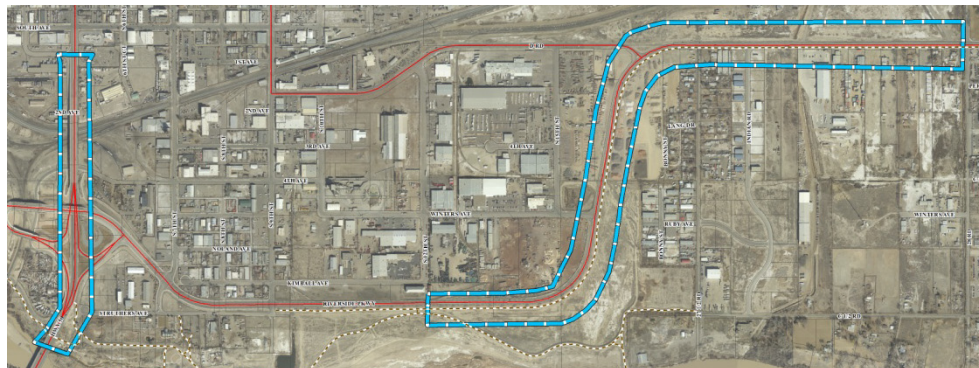
**D. Signage**

**Standards**

1. Only flush wall and monument style signs are allowed on the parcels that have frontage on 7<sup>th</sup> Street, Struthers Avenue, and/or Riverside Parkway within the Commercial Corridor. Flush wall signs are allowed per the Zoning and Development Code. Monument signs shall be a maximum of 12 feet in height with a maximum total of 100 square feet per sign face allowed per parcel. Illumination shall comply with the City of Grand Junction Zoning and Development Code.
2. New off-premise signs and billboards as defined by the Zoning and Development Code are not allowed on properties within the Greater Downtown Commercial Corridor.

**3. INDUSTRIAL CORRIDOR STANDARDS AND GUIDELINES**

The following Industrial Corridor standards and guidelines apply to all properties with frontage on 5<sup>th</sup> Street south of South Avenue to the River and on Riverside Parkway from the east edge of 12<sup>th</sup> Street east to 28 Road.



## **A. 5<sup>th</sup> Street Industrial Corridor Standards**

1. No new outdoor uses and operations shall be allowed on parcels that have frontage on 5<sup>th</sup> Street in the corridor. Maximum expansion and construction of any outdoor uses and operations (such as outdoor operation of rail yards, salvage yards, etc.) on properties that abut the 5<sup>th</sup> Street right-of-way shall be 25 percent of the total square footage as it existed on \_\_\_\_\_.
2. Screening on the 5<sup>th</sup> street bridge over the railroad tracks is required in order to limit views to adjacent uses but still provide longer vistas to the east (Grand Mesa) and west (Colorado National Monument and Uncompahgre Plateau).
3. New off-premise signs and billboards as defined by the Zoning and Development Code are not allowed on properties within the South 5<sup>th</sup> Street Industrial Corridor.

## **B. Riverside Parkway Industrial Corridor Standards**

### 1. New Site Development or Redevelopment

- a. Service entrances, service yards and loading areas shall be located only in the rear or side yard. Along the Riverside Parkway right-of-way, a six-foot (6') high solid fence or wall of stone, wood or masonry shall screen: each service yard or area from adjoining single family residential zones and uses which are not separated by a street (not counting an alley or any easement).
- b. Outdoor storage and permanent display areas shall be allowed per the zone district of the property. Any storage in the front yard adjacent to the Riverside Parkway right-of-way shall be screened with a six-foot (6') high solid architectural wall constructed of stone, wood, masonry or combination thereof with a minimum 14-foot landscape buffer provided outside of the wall unless modified per the landscaping section below.

### 2. Architectural Design of New Buildings or Substantial Exterior Remodel

- a. For construction of new buildings and additions, the façade(s) that face the Riverside Parkway on all buildings shall use exterior materials that are durable, economically maintained, and of a quality that will retain their appearance over time.
- b. The façade(s) of a new building that face the Riverside Parkway and/or Struthers Avenue or the Riverfront Trail shall exhibit a minimum of 3 of the following 8 architectural design elements:
  - (i) Variation in materials.
  - (ii) Façade modulation/articulation a minimum of every 30 feet.
  - (iii) Variation in color.
  - (iv) Façade feature that emphasizes the primary building entrance.
  - (v) Variation in roofline.
  - (vi) Windows.
  - (vii) Window or entry awning.
  - (viii) Other architectural features that achieve the goals of the overall corridor vision/concept as outlined on pages 8 through 11 as determined by the Director.

### 3. Landscaping for New Site Development or Redevelopment

a. On-site landscaping shall be required per the Zoning and Development Code. The total amount of required landscaping may be reduced by the Administrator if a minimum of 2 of the following 5 conditions exist or are proposed:

- (i) A higher quality of architectural design is achieved with greater than 3 of the architectural design elements listed in 2.
- (ii) Fifty percent (50%) or more of the building façade has no more than a 20-foot setback.
- (iii) Street trees exist within the abutting public street.
- (iv) All parking is located to the side and/or rear of the building.
- (v) All outdoor storage and operations are located behind the building.

#### 4. Signage

a. Only flush wall and monument style signs are allowed on the Greater Downtown Riverside Parkway Industrial Corridor properties. Flush wall signs are allowed per the Zoning and Development Code. Monument signs shall be a maximum of 12 feet in height with a maximum total of 100 square feet per sign face allowed per parcel.

b. New off-premise signs and billboards as defined by the Zoning and Development Code are not allowed on properties within the Riverside Parkway Industrial Corridor.

### **DOWNTOWN DISTRICT STANDARDS AND GUIDELINES**

#### **1. OVERALL DOWNTOWN DISTRICT VISION/CONCEPTS**

In order to implement the *Greater Downtown Plan*, zoning overlay standards have been developed for the subareas of the Downtown District. Application of the standards and guidelines will begin to implement goals of the plan to:

- Maintain and enhance the economic, cultural and social vitality of Downtown.
- Promote downtown living by providing a wide range of housing opportunities.
- Enhance the transportation system to accommodate automobiles, transit, bikes and pedestrians, and provide adequate, convenient parking.
- Stabilize and enhance the historic residential neighborhoods.
- Establish and promote a unique identity for each of the subareas of the Downtown District.
- Preserve and restore significant historic structures.
- Activate the edges of the downtown parks with mixed use and programmed/active use of the parks as urban open space rather than passive green parks.

#### **2. AREA WIDE POLICIES, STANDARDS AND GUIDELINES**

The following policies apply to all sub-areas of the Downtown District.

##### **A. Policies**

1. Maintain and enhance the economic, cultural and social vitality of downtown
2. Promote downtown living by providing a wide range housing opportunities
3. Enhance the transportation system to accommodate automobiles, bikes and pedestrians and provide adequate, convenient parking
4. Stabilize and enhance the historic residential neighborhoods

5. Establish and promote a unique identity
6. Preserve and restore significant historic structures
7. Activate the edges of the downtown parks with mixed use and programmed/active use of the park as urban open space rather than passive green parks.

## **B. Standards**

Due to the constraint of many downtown properties and the City's desire to promote improvement and redevelopment in the Downtown Area, the Director may make reasonable exceptions to the provisions of the *Zoning and Development Code* and the *Greater Downtown Plan Zoning Overlay* to bulk standards (except for building height), landscaping, parking or other use-specific special regulations. Appeals of the Director decision shall be decided by the Planning Commission. The following criteria shall be used to consider exceptions from the bulk standards, landscaping, parking or other use-specific special regulations.

1. Hardship Unique to Property, Not Self-Inflicted. There are exceptional conditions creating an undue hardship, applicable only to the property involved or the intended use thereof, which do not apply generally to the other land areas or uses within similar zone districts, and such exceptional conditions or undue hardship was not created by the action or inaction of the applicant or owner of the property;
2. Special Privilege. The exception shall not confer on the applicant any special privilege that is denied to other lands or structures within similar zone districts;
3. Literal Interpretation. The literal interpretation of the provisions of the regulations would deprive the applicant of rights commonly enjoyed by other properties in similar zoning districts and would work unnecessary and undue hardship on the applicant.
4. Greater Downtown Plan Goals. The proposal actually meets overall goals of the Plan better than if standards are followed.
5. Minimum Necessary. The exception is the minimum necessary to make possible the reasonable use of land or structures;
6. Conformance with the Purposes of the Zoning Overlay and the Zoning and Development Code. The granting of an exception shall not conflict with the purposes and intents expressed or implied in this Zoning Overlay or the Zoning and Development Code; and
7. Conformance with Comprehensive Plan. The granting of an exception shall not conflict with the goals and principles in the City's Comprehensive Plan.

## **C. Guidelines**

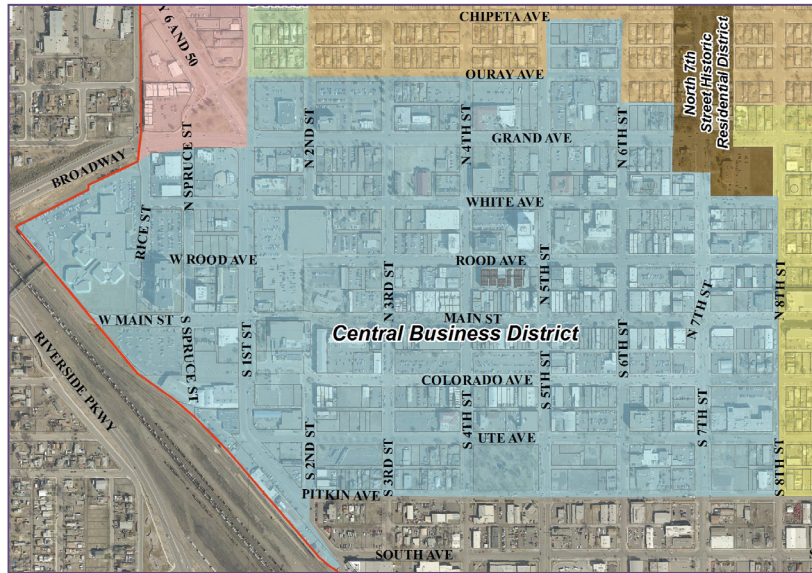
1. Projects will include good, interconnected transportation choices for better access and better health.
2. Traffic calming measures will be provided where appropriate, including pedestrian refuge areas, medians, landscaping and corner bulb-outs.



### 3. CENTRAL BUSINESS DISTRICT (CBD) STANDARDS AND GUIDELINES

The following standards and guidelines apply to the CBD area shown on the map (blue area). Further development and implementation of these concepts will be done in coordination with the DDA. The standards and guidelines are intended to apply to new development or redevelopment within the area.

#### A. Application of Standards and Guidelines



1. Unless otherwise noted below, all of the standards and guidelines shall apply under the following conditions:

a. Construction of a new building

b. Addition – Construction of an addition to an existing building if the addition increases total building square footage by 100 percent or greater (baseline is building square footage of existing building on \_\_\_\_\_, 2012). This expansion standard is cumulative after this date in 2012. Once square footage has exceeded 100 percent of the 2012 square footage, the standards and guidelines shall apply to all further expansions.

c. Substantial Exterior Remodel – Exterior building alteration that is greater than or equal to 65 percent of the value of the existing site and building. This increase shall trigger 100 percent site development compliance and 100 percent architectural standards compliance. This shall not trigger the requirement of minimum height. Value shall be the total actual value per the Mesa County Assessor or the appraised value based on an appraisal completed by a certified appraiser licensed to do business in the State of Colorado utilizing the “cost” approach.

2. The standards and guidelines do not apply to:

a. Construction of an addition to an existing building if the addition will increase the total building square footage by less than 100 percent (baseline is building square footage of existing building on \_\_\_\_\_, 2012). This expansion standard is cumulative after this date in 2012. Once square footage has exceeded 100 percent of the 2012 square footage, the standards and guidelines shall apply to all further expansions.

b. Exterior building alteration that does not exceed 65 percent of the value of the existing site and building (value determined as set forth above).

c. Interior remodel of an existing building.

**B. Land Use/ Architecture**

**1. Overall Vision/Concepts/Policies**

a. Activate the downtown core streets through emphasis on higher pedestrian traffic , businesses on the ground level that are oriented towards attracting higher pedestrian volumes, and buildings that “turn the corner” (invite activity on both the primary and cross streets). ). This includes the area between 3<sup>rd</sup> and 7<sup>th</sup> Streets and Ute and Grand Avenues, applying to the following streets: 3<sup>rd</sup> Street, 4<sup>th</sup> Street, 5<sup>th</sup> Street, 6<sup>th</sup> Street and 7<sup>th</sup> Street; Grand Avenue, White Avenue, Rood Avenue, Main Street, Colorado Avenue and Ute Avenue.



*Existing Downtown Street Activity*

b. Maintain the prominence of buildings along the streets by minimizing building setbacks.

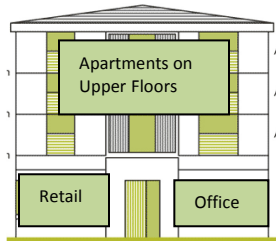
c. Encourage high quality, compatible design for all new buildings and establish a cohesive architectural character/theme that harmonizes new structures with the existing buildings through common materials, scale and basic architectural details as outlined in greater detail in the following standards and guidelines.



*Example Downtown Building Materials*

d. Typical building materials found in the CBD materials are traditional and weather well, allow a broad variety in appearance and ensure buildings are of high quality. To facilitate the creation of a cohesive architectural character/theme for new buildings, additions or exterior remodels in the CBD, the following exterior finishes are most appropriate: brick, sandstone, stucco, metal cladding, tiles, wood, glazing and decorative concrete masonry units (CMU).

e. Encourage high density, mixed-use development and structures (e.g. retail at street level and residential or office above).



*Existing Downtown Mixed Use*



f. Encourage gradual scale transitions between the CBD and adjacent neighborhoods.

g. Encourage maximizing building scale and intensity/density by offering incentives to build above the required minimum height.

## 2. Standards

### a. Building Height

- Minimum building height in the CBD shall be 2 stories.

The following uses as allowed in the zone district and as defined by the Zoning and Development Code shall be exempt from the minimum 2 story requirement:



*Existing Downtown Scale*

- (i) Schools, Colleges and Universities
- (ii) Vocational, Technical and Trade Schools
- (iii) Community Activity Building
- (iv) All Other Community Service
- (v) Museums, Art Galleries, Opera Houses, Libraries
- (vi) Day Care
- (vi) Detention Facilities
- (vii) Hospital/Clinic
- (viii) Parks and Open Space
- (ix) Religious Assembly
- (x) Funeral Homes/Mortuaries/Crematories
- (xi) Safety Services
- (xii) Utility Infrastructure and Corridors
- (xiii) Car Wash, Gasoline Service Station, Quick Lube
- (xiv) Industrial Services, Contractors and Trade Shops with Indoor Operations and Storage

Exemptions to this requirement for other uses of land or occupancies of a building not listed and that is not conducive to a vertical organization of operational space may be considered and approved by the Grand Junction Planning Commission with a formal recommendation from the Downtown Development Authority (DDA) upon a review of a conceptual level development proposal.

The following criteria shall be used to consider exceptions from the minimum height requirement.

1. Hardship Unique to Property, Not Self-Inflicted. There are exceptional conditions creating an undue hardship, applicable only to the property involved or the intended use thereof, which do not apply generally to the other land areas or uses within similar zone districts, and such exceptional conditions or undue hardship was not created by the action or inaction of the applicant or owner of the property;

2. Special Privilege. The exception shall not confer on the applicant any special privilege that is denied to other lands or structures within similar zone districts;

3. Literal Interpretation. The literal interpretation of the provisions of the regulations would deprive the applicant of rights commonly enjoyed by other properties in similar zoning districts and would work unnecessary and undue hardship on the applicant.

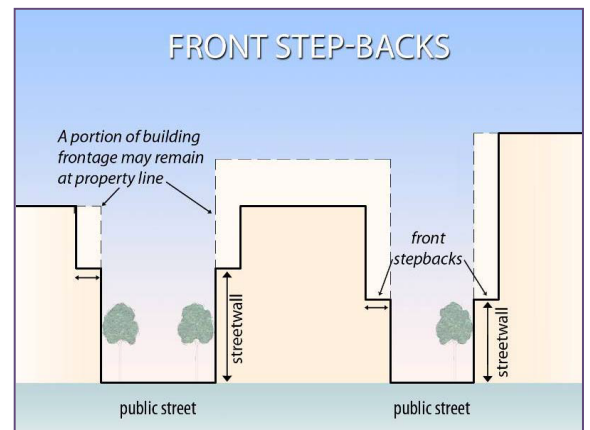
4. Greater Downtown Plan Goals. The proposal actually meets overall goals of the Plan better than if standards are followed.

5. Minimum Necessary. The exception is the minimum necessary to make possible the reasonable use of land or structures;

6. Conformance with the Purposes of the Zoning Overlay and the Zoning and Development Code. The granting of an exception shall not conflict with the purposes and intents expressed or implied in this Zoning Overlay or the Zoning and Development Code; and

7. Conformance with Comprehensive Plan. The granting of an exception shall not conflict with the goals and principles in the City's Comprehensive Plan.

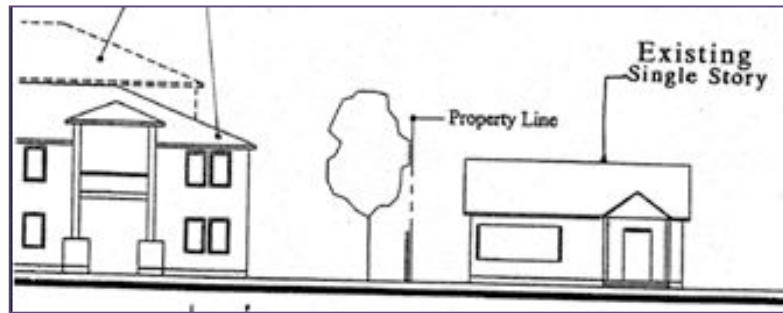
- Maximum building height in the CBD shall be 90 feet. A one-time increase of up to 25 percent per property may be considered by the Grand Junction Planning Commission.
- Upper floors of street-facing facades on buildings taller than the traditional three stories in the CBD shall step back a minimum depth of 10 feet in order to enhance the traditional scale of downtown and ensure adequate air and light at the sidewalk level.



b. Building Setbacks/Site Placement , Scale, Massing and Street Encroachment

- Maximum building setback from the abutting street shall be 2 feet or compatible with the mean setback of the immediately adjoining lots on both sides of the subject lot but in no case greater than 20 feet except on Ouray and Chipeta Avenues.
- Buildings shall be set back a minimum of 20 feet from the street on Chipeta and Ouray Avenues.
- Building setbacks of up to 10 feet from the abutting street may be allowed if there is a prescribed function for the space such as limited outdoor display, seating, outdoor dining areas or a small street park.

- The buildings will step down so that the front façade of the buildings that are directly across Ouray and Chipeta Avenues from residential buildings or uses are no taller than 40 feet. Minimum depth of the step shall be 10 feet.



*Example of Building Stepping Down to Single Family Residential Scale*

- Scale and massing of buildings or portions of buildings along Ouray and Chipeta Avenues will be compatible with residential scale.
- Architectural features on stories above street level may encroach on the public right-of-way, provided all safety considerations have been met. A Revocable Permit for such encroachments shall be reviewed and may be approved by the Director or a permanent easement for such encroachments may be reviewed and granted by City Council.
- Awnings that overhang windows or entries on street level facades are encouraged and shall be constructed of canvas or heavy cloth or metal (no plastic), utilizing primarily neutral colors.



*Existing Canopy on Main Street Building*

c. Architectural Character – The façade(s) of a new building, addition or substantial exterior remodel within the CBD that abut the streets in the high pedestrian use area listed on the following page shall have articulated architectural features and patterns that are designed to reflect the desired vision for the CBD described in Section 1 on page 19. In order to do so, the façade(s) of a new building, addition or substantial exterior remodel within the CBD shall exhibit a minimum of 5 of the following 10 architectural design elements.

- On corner parcels, façade design of ground floors “turn the corner” to induce activity and interest in the streetscape on the north-south streets within the CBD.

- Facades are articulated and have ornamentation such as varied brick patterns, change in material or color accents and window headers or columns that create shadow lines are examples of acceptable ornamentation.



*Example Ornamentation for Visual Interest*

- The street level front façade of the building is “active” with at least 50 percent of the façade in windows, with doors spaced no more than 50 feet apart. Side facades shall meet a minimum of 50 percent of this requirement.
- Façade features that emphasize the primary building entrance through projecting or recessed forms, detail, color or materials.
- Building facades are articulated on any street-facing side. Larger buildings are articulated in a hierarchy of smaller volumes and masses that better relate to other buildings and the scale of streets. This is accomplished through establishment of building bays that are distinguished by recessed or protruding elements or a variation in materials or color to break up the façade and reduce the overall scale of large buildings.



*Example Façade Articulation to Define Smaller Scale Bays*

- Higher levels of fenestration are required for buildings along both sides of the streets within the high pedestrian use area between 3<sup>rd</sup> and 7<sup>th</sup> Streets and Ute and Grand Avenues, applying to the following streets:

3<sup>rd</sup> Street, 4<sup>th</sup> Street, 5<sup>th</sup> Street, 6<sup>th</sup> Street and 7<sup>th</sup> Street, Grand Avenue, White Avenue, Rood Avenue, Main Street, Colorado Avenue and Ute Avenue

- At a minimum, fenestration be concentrated on the street level façade and diminish on upper floors (e.g. window size decreases as the floor level increases).



*Existing Facades with Diminishing Fenestration on Upper Floors*

- Buildings include a façade cap. The cap is defined by a distinct roof line or parapet. The design uses ornamentation of these features to enhance the building’s identity and support the architectural character of the CBD.

The façade cap is in three dimensions that projects and casts a shadow and relates proportionately to the overall building design.



*Example Three Dimensional Façade Cap*

- Building design minimizes the visual impact of mechanical equipment located on the roof as viewed from the adjacent streets.
- Other architectural features that achieve the goals of the overall Central Business District (CBD) vision/concept as outlined on page 20 as determined by the Director.

### 3. Guidelines

- Taller buildings will be located in the center and southern and western perimeter of the CBD, with shorter buildings on the northern and eastern edges of the CBD.
- Façade detailing should be compatible with, but not be identical to, that of a neighboring historic building. New facades should have their own, unique design. To create continuity, horizontal lines should be in alignment with neighboring buildings.

- c. Entrances are often the primary focal point of a building and, as such, should be designed to fit with the overall character of the area.
- d. Doorways may be finished with paints, stains, metal and aluminum cladding set to match the existing trim colors.
- e. Single, double, revolving and corner doorways are acceptable in new construction.

**C. Site / Area Improvements**

1. Parking

Policies

- a. Minimize single use, surface parking throughout the CBD.
- b. Maintain streetscapes dominated by buildings with parking located behind. Consider elimination of existing curb cuts as a performance benefit.
- c. Encourage shared parking.
- d. Encourage new infill development on existing, under-utilized surface parking lots.

Standards

- a. Off-street parking shall be located behind buildings and shall be accessed from the alley.
- b. Maximize opportunities for on-street parking by minimizing curb cuts along the street rights-of-way.
- c. On-street parking shall be located and designed to maintain and support a high-quality pedestrian experience on primary streets (see list of streets on page 22).

2. Streetscape and Landscape

Policies

- a. Provide streetscape details and landscaping that compliment the architectural character of downtown Grand Junction and exhibit urban character.
- b. Create downtown entries at strategic locations as shown on the Wayfinding and Signage Map in Appendix A. Enhancements may include landscaped medians, corner bulbs and special signs.

Standards

- a. All pedestrian level lighting shall be downlit and shall be in a historical style light poles.
- b. The streetscape along Grand Avenue and 4<sup>th</sup> and 5<sup>th</sup> Streets north to Ouray Avenue will continue in a design compatible with the existing improvements along Grand Avenue (e.g. decorative pavement and street trees).
- c. Landscaping is expected to comply with the Zoning and Development Code per the requirements of the zone district. The Director may consider variations for





new development or redevelopment, considering existing and proposed streetscape and/or the urban design character of the area.

d. The streetscape along 5<sup>th</sup> and 6<sup>th</sup> Streets north of Ouray Avenue to Chipeta Avenue shall transition between the urban hardscape and a more residential streetscape character (e.g. detached sidewalk, landscaping in park strip between curb and sidewalk and street trees).



*Existing Grand Avenue Streetscape*

### 3. Signage on Private Property

#### Policy

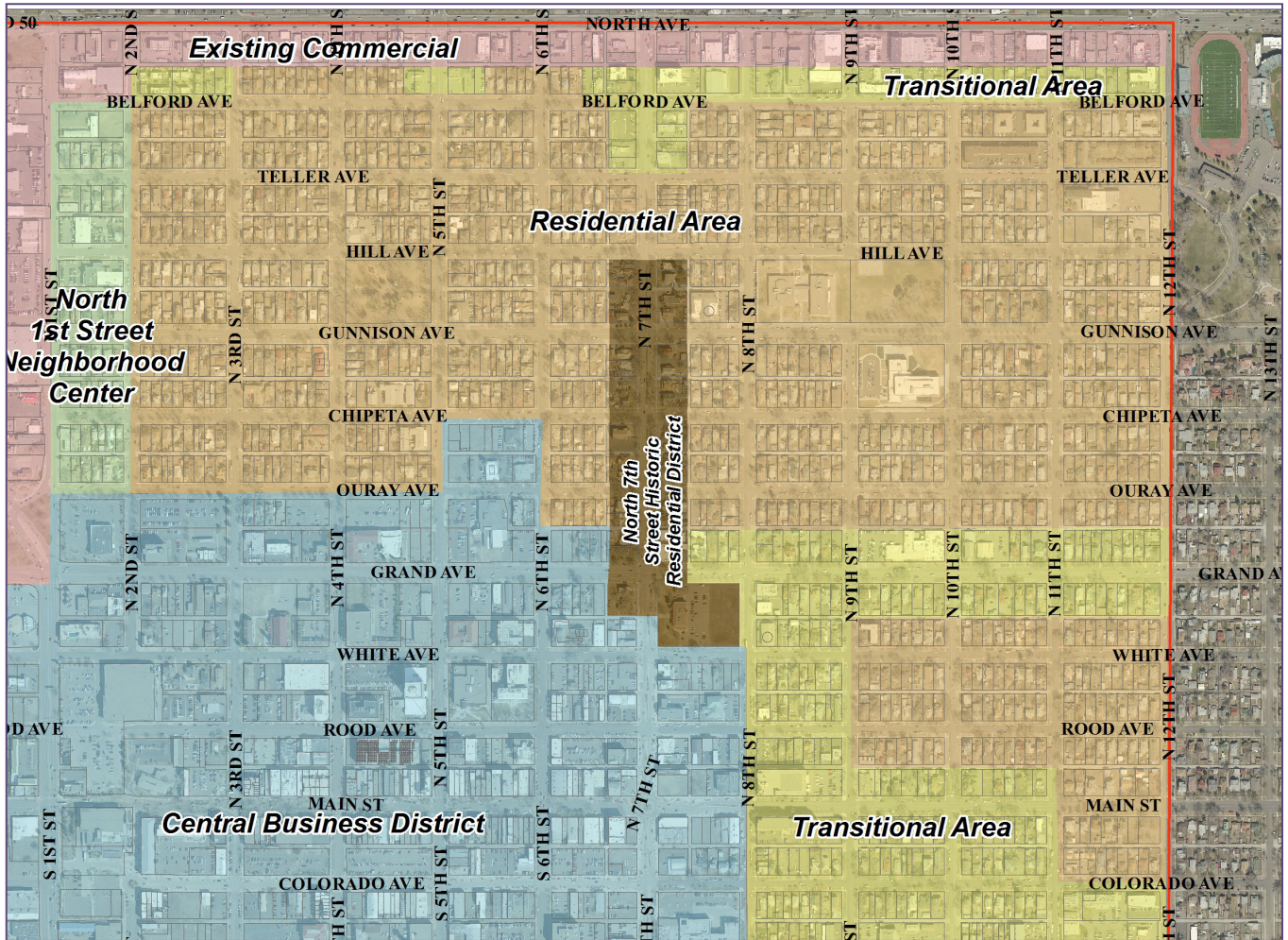
a. The DDA will assist in developing sign standards and guidelines for private signage placed on buildings or as freestanding signs for consideration and adoption by the City.

## 4. RESIDENTIAL AREAS STANDARDS AND GUIDELINES

The following standards and guidelines apply to the Residential areas shown on the map on the following page (orange areas). The standards and guidelines are intended to apply to new development or substantial redevelopment within the area.

### A. Policies

1. The existing downtown historic residential neighborhoods will be stabilized and enhanced.
2. The downtown residential core will be preserved for residential uses, with no further encroachment by non-residential uses.
3. Where existing residential zoning allows, provide a diversity of housing types through development of multifamily housing that is in keeping with the character of the neighborhood (refer to Multifamily Development section on page 28).
4. Enhance access to and improvements within existing public open spaces (e.g. parks and school grounds) within the downtown residential core such as enhanced pedestrian crossings and lighting for safety.



5. Maintain and enhance the historic character of the streetscape with emphasis on the following elements: street trees, landscaping rather than parking or other uses in the park strip between sidewalk and curb, distinctive street signs and lighting and detached sidewalks.



*Existing Residential Subarea Streetscape Character*

## B. Standards

### 1. Architectural Considerations

a. Building Style and Character. Maintain the existing character of the house styles within the downtown residential core neighborhood. New construction and alterations shall be compatible with key architectural characteristics and site elements of the neighborhood.

b. Accessory Structure Setbacks. New construction of accessory structures may be allowed to be built at a zero foot setback from the alley and 3 feet from neighboring property line(s).



*Existing Residential Building Alignment*

c. Building Mass/Scale and Proportion. New buildings or additions to existing buildings shall be visually compatible with the area. Visually compatible means compatible with adjacent and neighboring buildings including mass and scale, shape, windows, doors, openings, roof shape, roof pitch and orientation.

d. Roof Shape. The roofs of new buildings shall be visually compatible with nearby dwellings. If pitched, the roof pitch shall be at least 4:12.

e. Fenestration. Structures shall be visually compatible with surrounding residential structures. Visually compatible includes the relationship of width to height, and the spacing of windows and doors. For example, tall evenly-spaced rectangular windows are typical of many of the residential styles in the downtown area.



*Example Existing Architectural Character*

f. Materials. The exterior materials of all new buildings, additions and alterations shall be similar in size and appearance to nearby dwellings.

### 2. Accessory Structures

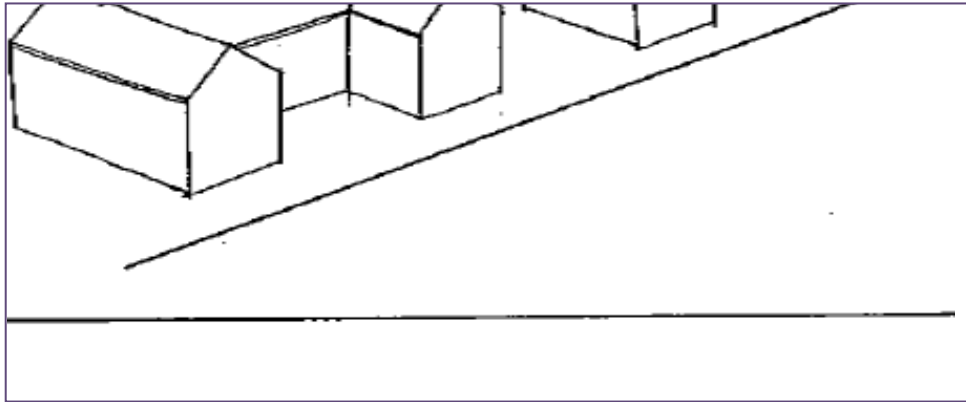
a. Accessory structures shall be no taller than the highest eave line of the principal structure.

b. The footprint size of an accessory structure shall be a maximum of 35 percent of the footprint of the principal structure.

### 3. Multifamily Development

Infill of new multifamily buildings may occur where zoning allows within the downtown residential core. However, the site design and structures for this type of development must maintain a scale and character compatible with the area. In addition to the Architectural Considerations listed in 1. above, multifamily development shall follow the standards below.

- a. Incorporate forms typical of the single family residential architecture of downtown including sloping roofs, porches, roof dormers and other architectural details.
- b. Break up the mass of larger buildings into forms that are similar in scale to the single family residential character.
- c. Facades must be composed of smaller sections, similar in scale and material finish to single family residential structures.



*Example – Break Up Façade of Larger Structure to be Compatible with Single Family Scale*

- d. Off-street parking for multifamily development shall not be located in the front yard setback. Parking shall be in the rear or side yards.
- e. Develop pedestrian links between the front sidewalk and building entrances and between parking and rear or side entrances.

#### **C. Guidelines**

1. Demolition of existing historic homes in order to construct new residential structures is strongly discouraged.
2. Maintain and enhance the pattern of landscaped front yards that gives the downtown residential core neighborhood a distinctive, friendly appearance.
3. Each new building and addition shall be located so that it aligns with existing neighborhood buildings. “Aligns” means elevation (e.g. horizontal lines of peaks of roofs, cornices and window sills) and plan (e.g. setbacks from the street and rear property lines and spacing between structures/setbacks from side property lines).

4. On a corner lot, front yard setbacks along side streets may be reduced to 10 feet on properties within the Downtown District Residential subareas.

7. Main entrances should open onto a street and shall align with those of adjacent residential buildings. For example, on many of the downtown homes, raised foundations and steps that define the main entrance are prevailing characteristics. Door styles should be similar to those found on residential buildings within the area.

8. New buildings and additions shall have the same number of stories and a height which is compatible with buildings within the same block. Two and one-half (2-1/2) stories shall be the maximum subject to the maximum height of 40 feet.

9. Parks strips will be landscaped in a traditional style, including street trees, grass, and low plantings or a combination thereof. Park strip landscaping shall include some live material – use of all non-living material such as rock is discouraged. Use of drought-tolerant plants is encouraged.



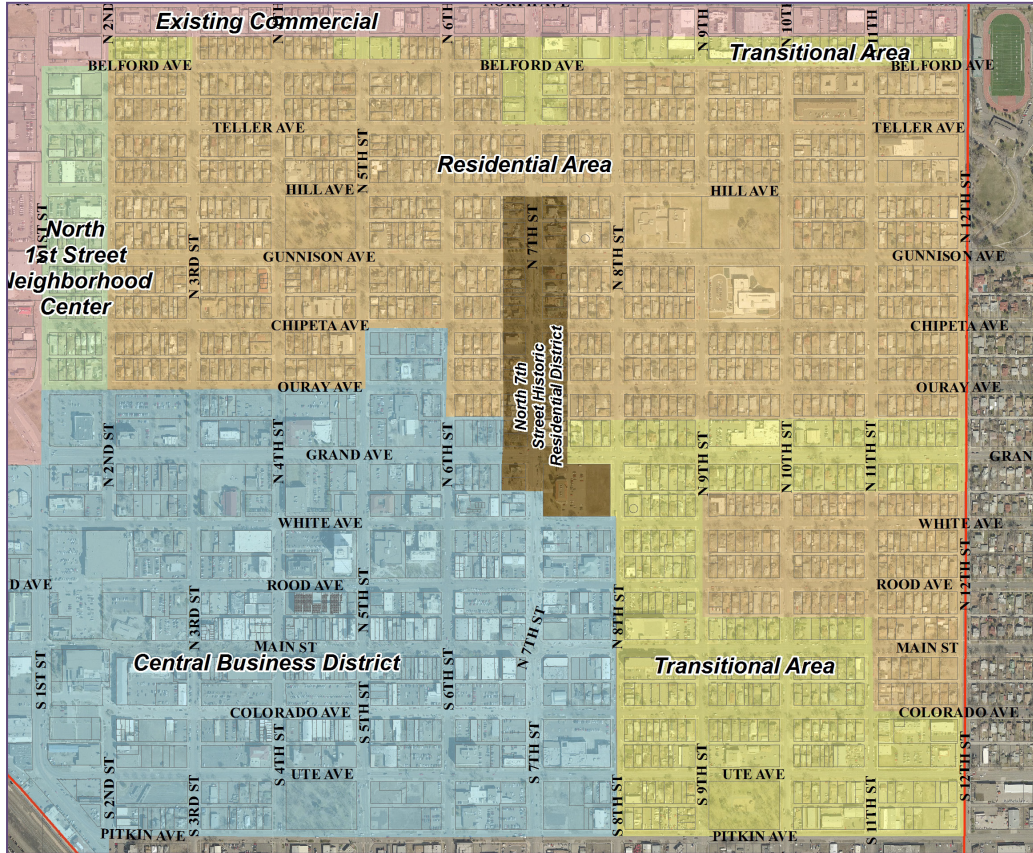
*Existing Character of Front Yards and Park Strips*

## **5. TRANSITIONAL AREAS STANDARDS AND GUIDELINES**

The following standards and guidelines apply to the Transitional areas shown on the map on the following page (yellow areas). The standards and guidelines are intended to apply to new development or substantial redevelopment within the area.

### **A. Policy**

The peripheral areas of the CBD provide a mix of established residential uses and low intensity, nonretail, neighborhood service and office uses that are compatible with adjacent residential uses and neighborhoods. New development or reuse of existing structures will maintain compatibility with residential building scale and appearance.



**B. Standards**

1. Land Use and Development Intensity

- a. Any mix of residential and nonresidential uses on the same lot shall be located in the same structure.
- b. No uses within the downtown transitional areas shall open earlier than 7:30 am and shall close no later than 8:00 pm.
- c. Maximum building size shall not exceed 10,000 square feet unless a Conditional Use Permit is issued.
- d. Outdoor storage and display areas are prohibited in the downtown Transitional areas.

2. Architectural Considerations

New residential or non-residential construction, including additions and rehabilitations, in the downtown Transitional areas shall be designed to have a single family residential character consistent with existing buildings in the area. "Consistent" means the operational, site design and layout, and architectural considerations described below.

a. Every new building and addition shall be located so that it aligns with existing buildings within the same block. "Aligns" means elevation (e.g., horizontal lines of peaks of roofs, cornices, window sills) and plan (e.g., setbacks from the street and rear property lines and spacing between structures/setbacks from side property lines).



*Example Infill Development in Transitional Area - 9<sup>th</sup> Street and Colorado Avenue*

b. Main entrances shall open onto a street and shall vertically align with those of adjacent residential buildings in the same block. For example, in areas adjacent to the downtown Transitional areas, raised foundations and steps that define the main entrance are prevailing residential characteristics. Door styles shall be similar to those found on residential buildings.

c. Each new building, its mass in relation to open spaces and its windows, doors, and openings shall be visually compatible. Visually compatible means compatible with adjacent and neighboring buildings including mass, shape, window, doors, openings, roof shape, roof pitch and orientation. For example, a large building shall be compatible with surrounding smaller dwellings by dividing its mass into smaller components to create a building elevation that is more like the size and proportion of the nearby single family homes.

d. The roofs of new buildings shall be visually compatible with buildings within the same block. When pitched, the roof pitch shall be at least 4:12.

e. Structures shall be visually compatible with surrounding residential structures. Visually compatible includes the relationship of width to height, and the spacing of windows and doors. For example, tall evenly-spaced rectangular windows are typical of certain residential styles near the downtown Transitional areas.

### 3. Signs

Development of non-single family uses in the downtown Transitional areas may directly abut existing single family residential areas. Thus, in order to maintain compatibility, more restrictive sign regulations shall apply.

a. Flush wall signs and monument signs shall be the only sign type allowed. Only one real estate sign advertising the property for sale or lease shall be allowed and shall not exceed 10 square feet.

b. Signs shall be located at least 10 feet behind the front property line. Total sign area, excluding real estate signs advertising the property for sale or lease, shall not exceed 25 square feet per street frontage. The sign allowance for one street frontage may be transferred to a side of a building that has no street frontage, but cannot be transferred to another street frontage. Monument signs shall not exceed 8 feet in height.



Example Signs within Transitional Subarea

- c. Signs may only be illuminated between 7:30 a.m. and 8:00 p.m.
- d. Sign enhancement features such as bases, pillars, and other decorative elements as part of monument signs shall not be counted as part of the maximum square footage of the sign, provided such features do not exceed the size of the sign face.

#### 4. Parking and Site Development

- a. Non-single family uses in the downtown Transitional areas shall be designed and operated not to increase on-street parking in front of single family dwellings in the neighborhood.
  - On-site parking shall be provided pursuant to the *Zoning and Development Code*.
  - On-site parking spaces shall only be located in the side and rear yards; and screened from nearby single family residential uses by a solid wall, fence or vegetation having a height of not less than 4 feet nor more than 6 feet (vegetation may exceed 6 feet in height).
- b. Service entrances, loading areas and dumpster areas shall be located only in the rear or side yard. Each loading area shall be screened from each adjacent residential use or zone.
- c. Front yards shall contain only landscaping, sidewalks, driveway access to parking areas and signage.

#### **C. Guidelines**

1. New buildings should have the same number of stories and a height which is compatible with those of nearby single family residential buildings. Two and one-half (2½) stories shall be the maximum subject to maximum height of 40 feet.
2. The exterior of all new buildings, additions and alterations shall be similar in size and appearance to nearby dwellings. Sign materials should be visually compatible with materials used on the building façade.



**ATTACHMENT 3**

**CITY OF GRAND JUNCTION, COLORADO  
ORDINANCE NO. \_\_\_\_\_**

**AN ORDINANCE ADOPTED A NEW ZONING MAP FOR PROPERTIES WITHIN  
THE GREATER DOWNTOWN PLAN AND ZONING OVERLAY**

**GENERALLY INCLUDING THE ORIGINAL SQUARE MILE, THE AREA BETWEEN  
SOUTH AVENUE AND THE COLORADO RIVER AND THE RIVERSIDE  
NEIGHBORHOOD TO 28 ROAD**

RECITALS.

The City has adopted the Greater Downtown Plan as a part of the Comprehensive Plan. The Greater Downtown Plan includes a Future Land Use Map identifying uses for parcels within the Greater Downtown area. As part of the implementation of the Greater Downtown Plan, a Zoning Map has been created that is consistent with the Future Land Use Map and the goals set forth in the Greater Downtown Plan.

The Grand Junction Planning Commission is charged with the legal duty to prepare and consider and recommend action to City Council regarding amendments to the Zoning and Development Code for the City.

The Greater Downtown Plan Zoning Overlay was heard in a public hearing by the Grand Junction Planning Commission on July 24, 2012 where the Planning Commission recommended that the City Council adopt the Zoning Overlay.

City Council finds that the proposed Greater Downtown Plan Zoning Map are in conformance with the zoning criteria stated in section 21.02.140 of the Municipal Code.

**NOW THEREFORE, BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF  
GRAND JUNCTION:**

That the Greater Downtown Plan Zoning Map, City of Grand Junction, Colorado, in the form of the document attached hereto (Exhibit A), and as recommended for adoption by the Grand Junction Planning Commission is hereby adopted.

The existing maps depicting and describing the zone and districts of lands within the Greater Downtown Plan area, which are a part of the City's Zoning and Development Code are hereby repealed and reenacted with the attached map (Exhibit A).

The Greater Downtown Zoning Map, including the text of the Greater Downtown Plan Zoning Overlay, in accordance with paragraph 51 of the Charter of the City of Grand Junction, shall be published in pamphlet form with notice published in accordance with the Charter.

**INTRODUCED** on first reading the \_\_\_\_\_ day of \_\_\_\_\_, 2012 and ordered published pamphlet form.

**PASSED** and **ADOPTED** on second reading the \_\_\_\_\_ day of \_\_\_\_\_, 2012 and ordered published in pamphlet form.

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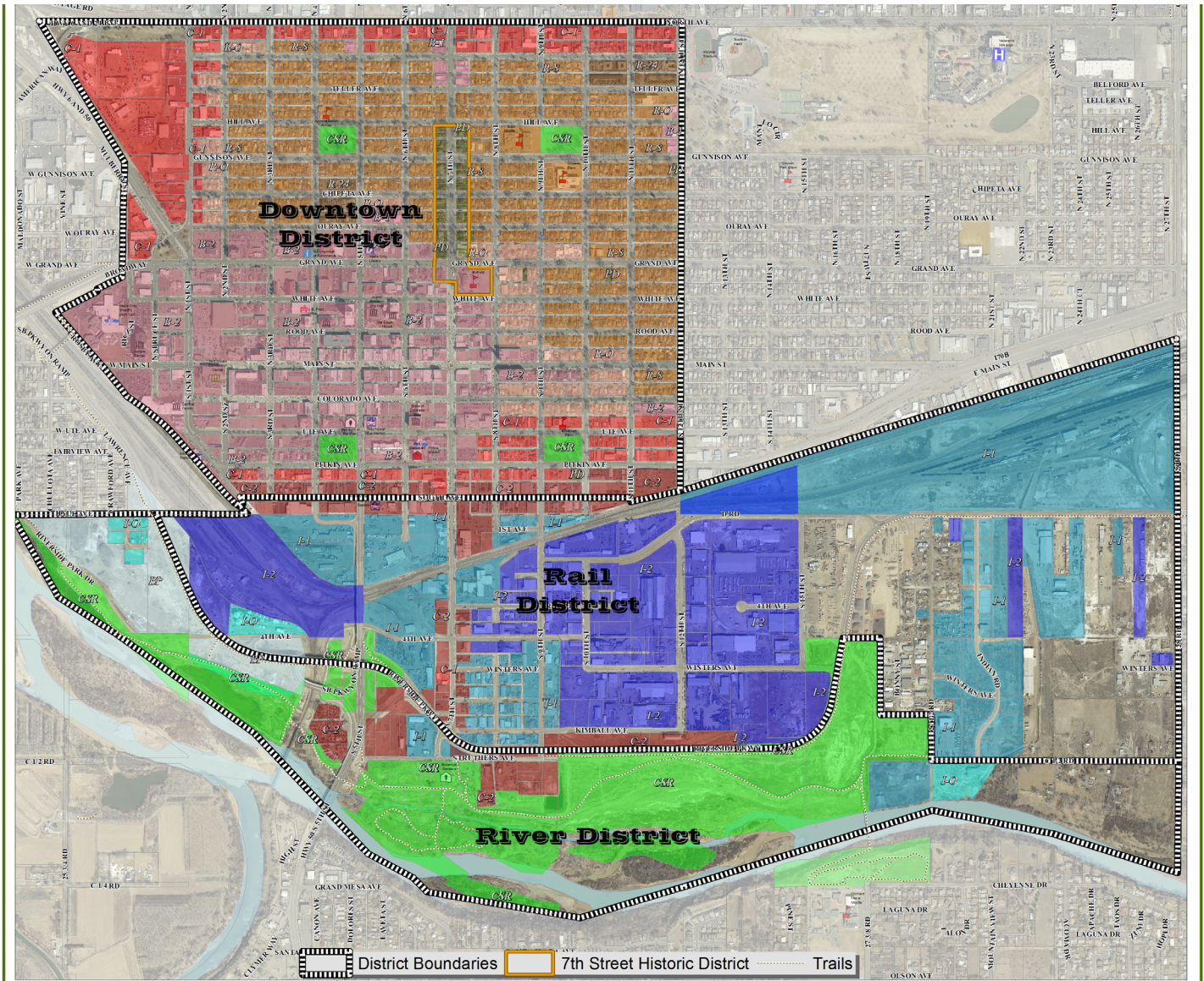
President of City Council

ATTEST:

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City Clerk

# EXHIBIT A – GREATER DOWNTOWN PLAN ZONING MAP



## Greater Downtown Zoning Categories

- |                         |                                 |                             |                         |                          |                              |                          |
|-------------------------|---------------------------------|-----------------------------|-------------------------|--------------------------|------------------------------|--------------------------|
| R-8                     | PD (Planned Development)        | R-O (Residential Office)    | B-2 (Downtown Business) | C-1 (Light Commercial)   | I-O (Industrial Office Park) | I-2 (General Industrial) |
| R-24 (16-24 units/acre) | CSR (Community Services & Rec.) | B-1 (Neighborhood Business) | BP (Business Park)      | C-2 (General Commercial) | I-1 (Light Industrial)       |                          |



PUBLISHED JUNE 8, 2012  
 © 2012 DEPARTMENT OF PLANNING AND ECONOMIC DEVELOPMENT, GREATER DOWNTOWN PROPOSED ZONING MAP