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File 1981-0059

Project Name: First Security Savings and Loan Assoc. - Rezone RMF 32 to P Amendment to Final Plan

Date 2/4/02

P r e s e n t	S c a n n e d	<p>A few items are denoted with an asterisk (*), which means they are to be scanned for permanent record on the in some instances, not all entries designated to be scanned by the department are present in the file. There are also documents specific to certain files, not found on the standard list. For this reason, a checklist has been provided.</p> <p>Remaining items, (not selected for scanning), will be marked present on the checklist. This index can serve as a quick guide for the contents of each file.</p> <p>Files denoted with (**) are to be located using the ISYS Query System. Planning Clearance will need to be typed in full, as well as other entries such as Ordinances, Resolutions, Board of Appeals, and etc.</p>			
X		*Summary Sheet - Table of Contents			
X		Review Sheet Summary			
X		Application form			
X		Review Sheets			
		Receipts for fees paid for anything			
		*Submittal checklist			
		*General project report			
		Reduced copy of final plans or drawings			
		Reduction of assessor's map			
		Evidence of title, deeds			
X	X	*Mailing list to adjacent property owners			
		Public notice cards			
		Record of certified mail			
X		Legal description			
		Appraisal of raw land			
		Reduction of any maps - final copy			
		*Final reports for drainage and soils (geotechnical reports)			
		Other bound or nonbound reports			
		Traffic studies			
		Individual review comments from agencies			
		*Consolidated review comments list			
X	X	*Petitioner's response to comments			
		*Staff Reports			
		*Planning Commission staff report and exhibits			
		*City Council staff report and exhibits			
		*Summary sheet of final conditions			
		*Letters and correspondence dated after the date of final approval (pertaining to change in conditions or expiration date)			
DOCUMENTS SPECIFIC TO THIS DEVELOPMENT FILE:					
X	X	Action Sheet	X	Gamma Radiation Survey Form	
X	X	Review Sheet Summary	X	Letter from Ken Rabideau to Ctivy Dev. Dept. re: project scope guarantee - 4/30/81	
X		Review Sheets	X	Floor Plans of first and second floor	
X		Zoning Violation Report	X	Site Plan	
X	X	Letter from Dennis Edson to Bob Golden, Planning Dept. re: increasing the size of building - 2/14/84	X	X	Handwritten Notes to file - 5/12/83
X	X	Ordinance No. 2036, 2044 - **	X		Note to Bob from Leila (all of names available) re: final approval- 1/18/82
X		Public Notice Posting - 5/15/81 and 1/13/82	X	X	Memo from Alex to Bob re: approved plan - 1/27/82
X	X	Planning Commission Minutes - ** - 5/26/81, 1/26/82			
X	X	Letter from Ken Hunt to Bob Golden re: provision of sidewalk - 2/8/82			
X	X	Impact Statement - 4/30/81 and 1/4/82			
X		Deed			
X	X	Development Plan			
X		Request for Treasurer's Certificate of Taxes Due			

2945-114-00-034 #59-81
Leo L. & M. Colinne Dawson
410 Hall
Grand Junction, CO 81501

2945-114-00-043 #59-81
Leland A. Schmidt & Carolyn
Ann Hjelt
536 Bookcliff Drive
Grand Junction, CO 81501

2945-114-16-009 #59-81
2945-114-16-011 A.R. 5/23/81
Janell J. & Terrence I. Boggs
453½ Sandia
Grand Junction, CO 81501

2945-114-18-006 #59-81
Chris Joufflas & George
c/o Rente III Dustys
1000 N. 9th Street
Grand Junction, CO 81501

2945-113-12-942 #59-81
School District #51
Administrative Service Center
2115 Grand Avenue
Grand Junction, CO 81501

Ken Rabideau
Dennis Edson
3360 Star Ct #59-81
Grand Jct CO 81501

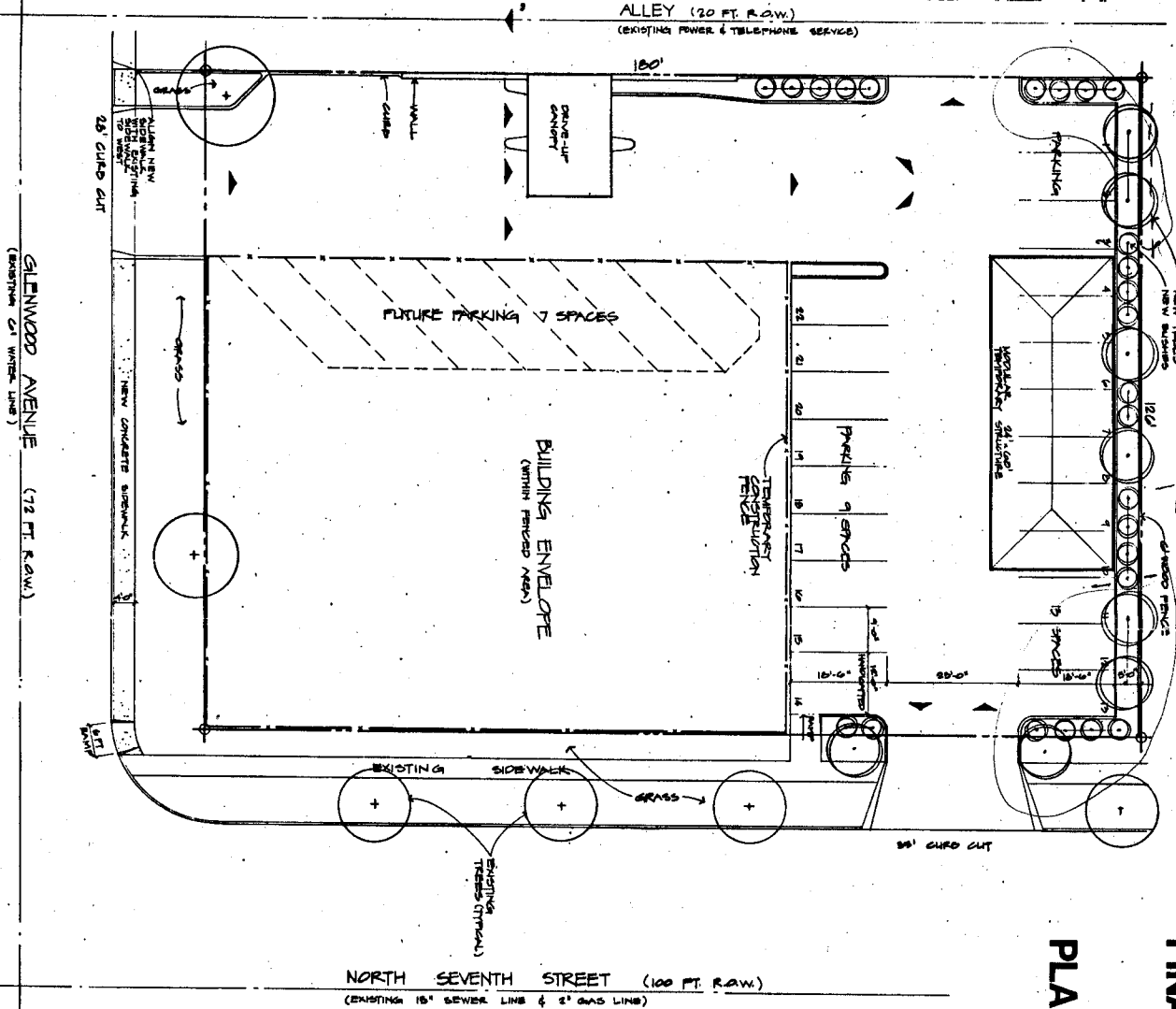
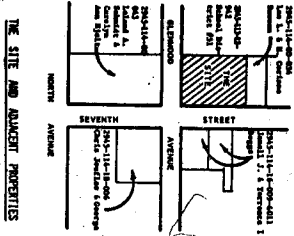
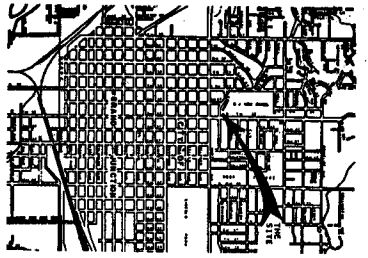
Chambles Assoc.
Conni Mc Donough
Box 2104 #59-81
Grand Jct CO 81502

Richard Dewey #59-81
2236 Tiffany Ct.
Grand Junction, CO 81501

FSSLA
P.O. Box 2270 #59-81
Grand Jct. CO 81501

Dillion-Hunt
804 Grand Ave #59-81
Grand Jct. CO 81501

FINAL DEVELOPMENT PLAN AMENDMENT IN A PLANNED BUSINESS ZONE

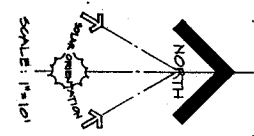


NORTH SEVENTH STREET (100 FT. R.O.W.)
(EXISTING 15" SEWER LINE & 2" GAS LINE)

GLENWOOD AVENUE (72 FT. R.O.W.)
(EXISTING 24" WATER MAIN)

- NOTES**
1. Existing 1-story office building with 1,000 to 10,000 square feet of space within and near the building envelope area. Structure height shall be approximately 25 feet.
 2. The lot shall be a minimum 20' wide temporary structure which covers all front and side setbacks of the property.
 3. The property shall be developed for commercial use, the use and structure shall be approved by the City.
 4. Existing wall collapse located on the site will be removed from the building prior to construction.
 5. Utility services will be from the alley to the west for power and telephone, and from the alley to the east for water, sewer and gas services.
 6. Site grading and drainage plans and arrangements will be completed under the approval of the City Engineer and available prior to a permit for a building.
 7. The final development will provide adequate circulation, parking spaces and facilities in accordance with City policies and regulations. All materials and construction shall be approved by the City.
 8. All landscaping will be retained and placed in accordance with the City of the property. Screening will be provided along the north alley.
 9. Development Schedule:
 - a. Completion of the work and of the property.
 - b. Beginning of the permanent structure.
 - c. Completion of the permanent structure.
 - d. All work of contract for construction.
 10. Additional parking and storage and also facilities including:
 - a. Additional parking and storage.
 - b. Additional facilities including:
 1. Completion of the work and of the property.
 2. Beginning of the permanent structure.
 3. Completion of the permanent structure.
 4. All work of contract for construction.
 11. Legal description:

Legal description: Lot 10 of Block 1011, Subdivision, Section 11, T2N, 20E, Range 10E, North of and West of the 1st 1/4 of Sec. 14, T2N, 20E, Section 11, T2N, 20E.
 12. A final implementation order with appropriate signatures and recording spaces shall be submitted to the City Engineer for the permit for the work within the building envelope.



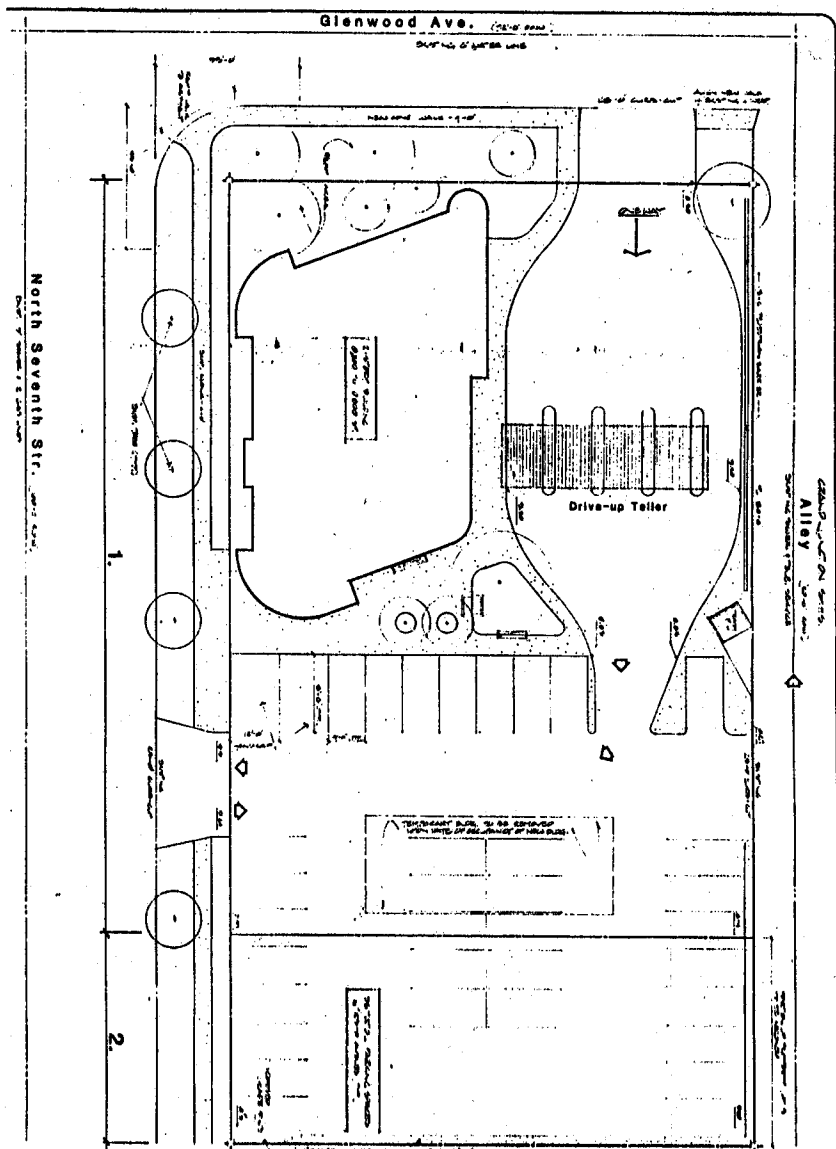
OFFICE COPY
#59-81

DATE	BY	CHK
7/20/81	CM	
7/20/81	CM	
7/20/81	CM	

1

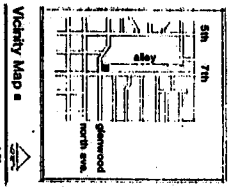
**FIRST SECURITY
SAVINGS & LOAN**



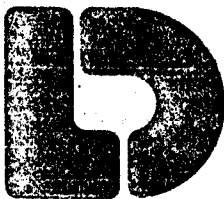


SITE PLAN

- NOTES:**
1. All dimensions are in feet.
 2. All areas are to be landscaped.
 3. All areas are to be paved.
 4. All areas are to be finished.
 5. All areas are to be finished.
 6. All areas are to be finished.
 7. All areas are to be finished.
 8. All areas are to be finished.
 9. All areas are to be finished.
 10. All areas are to be finished.



DILLON HUNT PC ARCHITECTURE AND PROJECT MANAGEMENT
 1524 LINDEN AVENUE • CHANDLER, ARIZONA 85024 • (480) 949-7200
FIRST SECURITY SAVINGS & LOAN



Lincoln DeVore

1000 West Fillmore St.
Colorado Springs, Colorado 80907
(303) 632-3593
Home Office

April 18, 1979

Laura Hall
P.O. Box 400
Grand Junction, Colorado 81501

Re: SUBSURFACE SOILS INVESTIGATION
COMMERCIAL BUILDING
GRAND JUNCTION, COLORADO

Gentlemen:

Transmitted herewith is the report giving the results of a subsurface soils investigation for the proposed Commercial Building located on Seventh and Glenwood Avenue in Grand Junction, Colorado.

Respectfully submitted,

LINCOLN DEVORE TESTING LABORATORY, INC.

By: Edward M. Morris
Edward M. Morris
Geologist

Reviewed by George D. Morris, P. E.

EMM/sjc
LDTL Job No. J-435

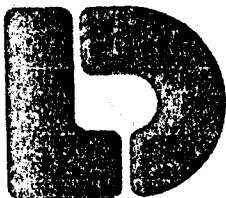
2700 Highway 50 West
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(303) 546-1150

P.O. Box 1427
Glenwood Springs, Colo 81601
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1000 West Fillmore St.
Colorado Springs, Colorado 80907
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Home Office

, April 18, 1979

Laura Hall
P.O. Box 400
Grand Junction, Colorado 81501

Re: Subsurface Soils Investigation
7th and Glenwood Avenue, Grand Junction

Dear Ms. Hall;

At your request, personnel of this laboratory have completed a subsurface soils investigation of a site located at the intersection of 7th & Glenwood Avenue, in Grand Junction, Colorado. The purpose of this investigation was to determine the general suitability for construction of a medium weight, three story commercial building. A full basement is planned under the majority of the structure. The construction of the structure is to be a steel framework, metal joist and concrete floor slab. The exterior walls are to be of wood sheathing and siding.

The site is located on the northeast corner of 7th and Glenwood Avenue and the building is approximately 40 feet by 144 feet in dimension and will face 7th Street.

The surface soils on this site are colluvial in origin, having been deposited by the action of sheet-wash originating in the Bookcliffs, to the north of the site. The deposit is normally a stratified deposit of clays and silts with some fine sand seams being noted. Colluvial soils of this nature are commonly highly stratified and differences may be encountered across the site.

The soils are normally found in a low density, saturated condition similar to that noted in the lower portion of the drill holes performed by Lincoln DeVore. If ground water conditions permit, the surface soils dry out somewhat and may densify to some degree. The amount of drying and densification is dependent on many factors and can vary widely over a particular site. Minor changes in clay content, while not enough to be readily apparent to the eye and usually only minor variances during laboratory analysis, can greatly influence the amount of drying and densification of the upper soil "crust". This particular circumstance is apparent on this site. Test Hole #1, in the south portion of the building

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area, has a dry surface and does not become saturated and soft until about 12 feet below the surface. A series of thin sandy strata were found in the 12 to 15 foot interval and this zone is apparently acting as a capillary break, preventing the ground water from saturating the upper soils.

The north portion of the site, vicinity of Test Hole #2, is very moist on the surface and becomes saturated about 7 feet below the ground surface. The higher moisture at the 2½ foot deep level was the result of the wet weather and poor drainage in the hole vicinity, which allowed a good deal of water to seep into the soils.

The soils on this site have been grouped into one soil type, as indicated on the attached drilling logs and soil summary sheets. The predominant soil encountered on this site is the stratified silty clay, with minor areas of sandier material noted, primarily in Test Hole #2. All of the soils were encountered in a fairly low density state and the saturated soils will be susceptible to long term consolidation. The upper surface soils, particularly in the vicinity of Test Hole #1 are of sufficient density that some minor expansion may be detected. This is very minor and will not affect foundations, but may move floor slabs on grade slightly. These soils contain sulfates in detrimental quantities.

The nearly saturated to fully saturated soils beneath this site is felt to be a permanent feature of the site and must be taken into consideration when designing these structures. Deep basements are not recommended on this site since the depth to free water could decrease during times of high rainfall, which could be detrimental to the structure. In addition, the potential for a high capillary rise exists on this site, which could also be detrimental to the soil bearing capabilities. If a basement is used, the design and construction processes must take the presence of the saturated zone into account.

A footing and stemwall foundation can be used on this site, however, the maximum bearing capacity of the soils is variable across the site and with depth. Several test holes, correlated with projected foundation elevations would be necessary to accurately define the maximum allowable bearing capacities of the soils. As an overall site average, a maximum bearing capacity of 1200 psf can be used, based on the two test holes drilled by this office. The south portion of the site has apparently a good deal more bearing capacity and maximum values on the order of 2100 to 3000 psf can be found, depending on the foundation depth and the actual extent of these stiffer soils.

An alternate type of shallow foundation system which could be used on this site would be a rigid reinforced mat or raft foundation. If this mat foundation was placed at sufficient depth that the weight of the soil removed from the foundation was equal to the weight of the building, the soil would experience no net change in stress and the foundation would be a fully compensating raft.

If the raft is not fully compensating or if a footing and stemwall system is used, long term consolidation of the lower soils should be expected. The actual amount of consolidation will depend on the type and loading conditions of the foundation type which is used.

The soils on this site are sensitive to the addition of moisture, as the bearing capacities of the soils may be reduced if the soils become saturated. Adequate drainage must be provided at all times and water should never be allowed to pond above the building foundation materials.

It should be mentioned that if the potential for consolidation cannot be tolerated, a deep foundation, consisting of drilled piers or driven piles founded on the terrace gravels or the Mancos Shale Formation can be used. In this portion of Grand Junction, the firm, high bearing capacity deposits are located from 50 to 65 feet below the existing ground surface. Generally speaking, for a building of this size and configuration, this office would normally recommend either drilled piers or driven caissons as the most preferable foundation type. The shallow foundation type entails a greater degree of risk which may or may not be acceptable. The rather changable density and moisture conditions encountered across the site will make it rather difficult to design a properly balanced, shallow foundation which will experience a minimum of differential settlement.

Criteria for design of any specific foundation type can be presented almost immediately, if it is so desired. Following are general recommendations for the shallow, footing and stemwall foundation type.

For the typical shallow foundation case, the foundation system should be balanced to within ± 300 psf around the entire structure. The criteria for balancing will depend upon the nature of the structure itself.

Stem walls for continuous footings should be designed as grade beams capable of spanning at least 12 feet. Horizontal reinforcement should be placed continuously around the structure with no gaps or breaks in the reinforcing steel unless they are specially designed. Beams should be reinforced at both the top and the bottom, with the majority of the reinforcement being placed at the bottom of the beam. When stem walls will retain soil in excess of 4 feet in height, vertical reinforcement may be necessary and should be designed. To design such vertical reinforcing, the equivalent fluid pressure of the soils should be taken as approximately 38 pcf, in the active state.

Where building floor slabs are to be used, they may be placed directly on grade or over a compacted gravel blanket of 4 to 6 inches in thickness. If the gravel bed is chosen, it must be provided with a free drainage outlet to the surface and must not be allowed to act as a water trap beneath the floor slab.

The floor slab should be constructed in such a manner that it acts independently of columns and bearing walls. Additionally, floor slabs on grade should be placed in sections no greater than 30 feet on a side. Deep construction or contraction joints could be placed at these lines to facilitate even breakage. This will help reduce unsightly cracking caused by differential movement.

Adequate drainage must be provided in the foundation area both during and after construction to prevent the ponding of water. The ground surface around the building should be graded in such a manner that water will be carried quickly away from the structure. Minimum gradient within 10 feet of the structure will depend upon surface landscaping. Bare or paved areas should have a minimum gradient of 2%, while landscaped areas should have a minimum gradient of 5%. Roof drains should be carried across all backfilled areas and discharged well away from the structure.

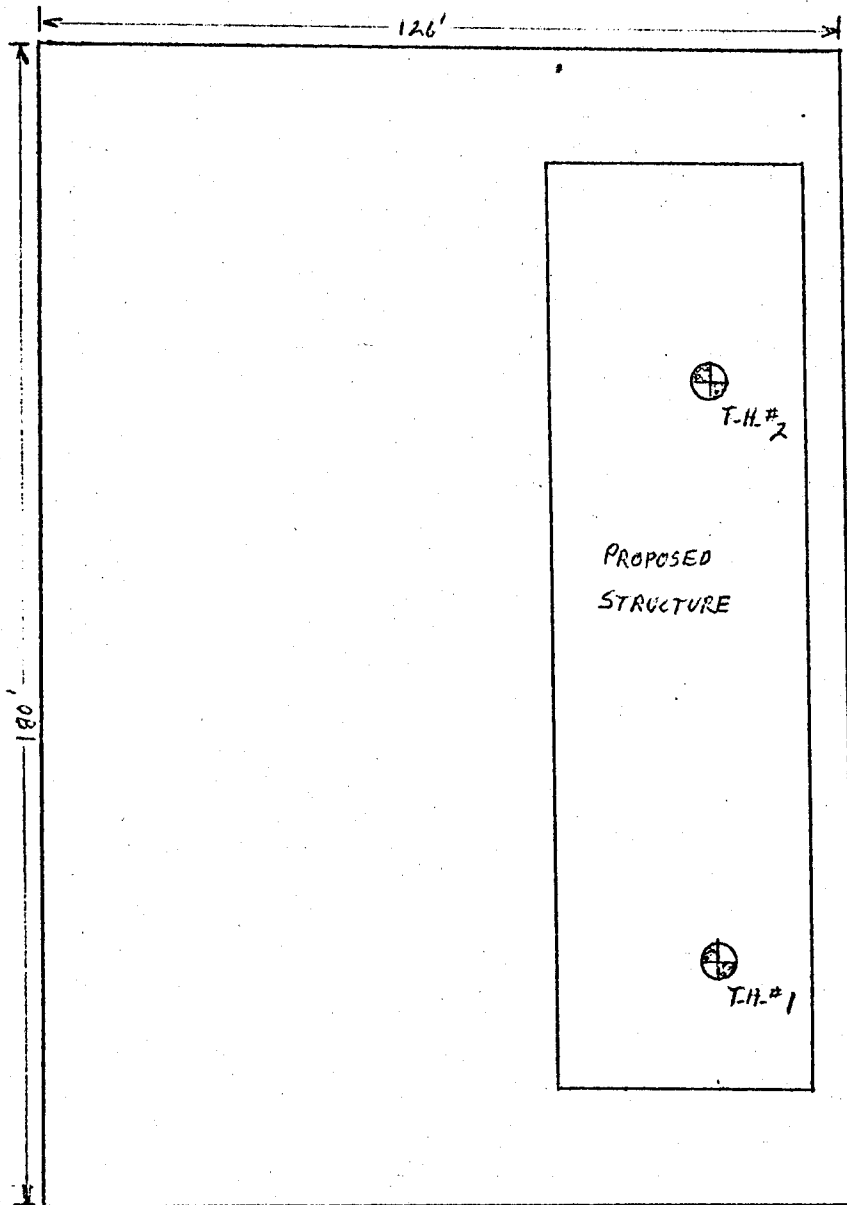
Any topsoil or debris should be removed from the construction area prior to beginning construction of the foundation. Additionally, should any pockets of debris, organic material, or unusually loose material be encountered during excavation for the footings, this material should be removed and replaced with backfill compacted to 95% of the maximum Proctor dry density, ASTM D-698.

Backfill around the structure and in utility trenches leading to the structure should be compacted to at least 90% of maximum Proctor dry density. The native soils may be used for this purpose. Material should be placed in lifts not to exceed 6 inches compacted thickness and at a moisture content approximately equal to the Proctor optimum moisture content, plus or minus 2%. Backfill should be compacted to the required density by mechanical means. No water flooding techniques of any type should be used in the placement of fill on this site.

The open foundation excavation should be inspected prior to the construction of forms or placing of concrete to establish that proper design bearing material has been reached and that no debris, soft spots, or areas of organic material are located within the foundation area.

It should be noted that if a typical shallow foundation system is used on this site, considerable settlement beneath the foundation system can be anticipated, due to the low density condition of the subsurface soils.

It is believed that all pertinent points concerning the subsurface soils on this site have been covered in this report. If soil types and conditions other than those outlined herein are noted during construction on the site, these should be reported to the Laboratory so that changes and recommendations may be made if necessary. Should questions arise, or further information be required please feel free to contact our office at anytime.



7TH STREET R.O.W.

GLENWOOD STREET R.O.W.


T.H.#2

PROPOSED
STRUCTURE

T.H.#1



SCALE
1"=30'

 TEST BORING LOCATION

THE LINCOLN-DEVORE TESTING LABORATORY
COLORADO: Colorado Springs, Pueblo, Glenwood WYOMING: Rock Springs
Springs, Montrose, Gunnison.

SOILS DESCRIPTIONS:

SYMBOL	USCS	DESCRIPTION
		Topsoil
		Man-made Fill
	GW	Well-graded Gravel
	GP	Poorly-graded Gravel
	GM	Silty Gravel
	GC	Clayey Gravel
	SW	Well-graded Sand
	SP	Poorly-graded Sand
	SM	Silty Sand
	SC	Clayey Sand
	ML	Low-plasticity Silt
	CL	Low-plasticity Clay
	OL	Low-plasticity Organic Silt and Clay
	MH	High-plasticity Silt
	CH	High-plasticity Clay
	OH	High-plasticity Organic Clay
	Pt	Peat
	GW/GM	Well-graded Gravel, Silty
	GW/GC	Well-graded Gravel, Clayey
	GP/GM	Poorly-graded Gravel, Silty
	GP/GC	Poorly-graded Gravel, Clayey
	GM/GC	Silty Gravel, Clayey
	GC/GM	Clayey Gravel, Silty
	SW/SM	Well-graded Sand, Silty
	SW/SC	Well-graded Sand, Clayey
	SP/SM	Poorly-graded Sand, Silty
	SP/SC	Poorly-graded Sand, Clayey
	SM/SC	Silty Sand, Clayey
	SC/SM	Clayey Sand, Silty
	CL/ML	Silty Clay

ROCK DESCRIPTIONS:

SYMBOL	DESCRIPTION
<u>SEDIMENTARY ROCKS</u>	
	CONGLOMERATE
	SANDSTONE
	SILTSTONE
	SHALE
	CLAYSTONE
	COAL
	LIMESTONE
	DOLOMITE
	MARLSTONE
	GYPSUM
	Other Sedimentary Rocks
<u>IGNEOUS ROCKS</u>	
	GRANITIC ROCKS
	DIORITIC ROCKS
	GABBRO
	RHYOLITE
	ANDESITE
	BASALT
	TUFF & ASH FLOWS
	BRECCIA & Other Volcanics
	Other Igneous Rocks
<u>METAMORPHIC ROCKS</u>	
	GNEISS
	SCHIST
	PHYLLITE
	SLATE
	METAQUARTZITE
	MARBLE
	HORNFELS
	SERPENTINE
	Other Metamorphic Rocks

SYMBOLS & NOTES:

SYMBOL	DESCRIPTION
	9/12 Standard penetration drive Numbers indicate 9 blows to drive the spoon 12" into ground.
	ST 2-1/2" Shelby thin wall sample
	W ₀ Natural Moisture Content
	W _x Weathered Material
	Free water table
	γ _D Natural dry density
	T.B. - Disturbed Bulk Sample
	② Soil type related to samples in report
	15' W _x Form. Top of formation
	Test Boring Location
	Test Pit Location
	Seismic or Resistivity Station. Lination indicates approx. length & orientation of spread (S = Seismic, R = Resistivity)

Standard Penetration Drives are made by driving a standard 1.4" split spoon sampler into the ground by dropping a 140 lb. weight 30". ASTM test des. D-1586.

Samples may be bulk, standard split spoon (both disturbed) or 2-1/2" I.D. thin wall ("undisturbed") Shelby tube samples. See log for type.

The boring logs show subsurface conditions at the dates and locations shown, and it is not warranted that they are representative of subsurface conditions at other locations and times.

L LINCOLN
DeVORE
TESTING
LABORATORY

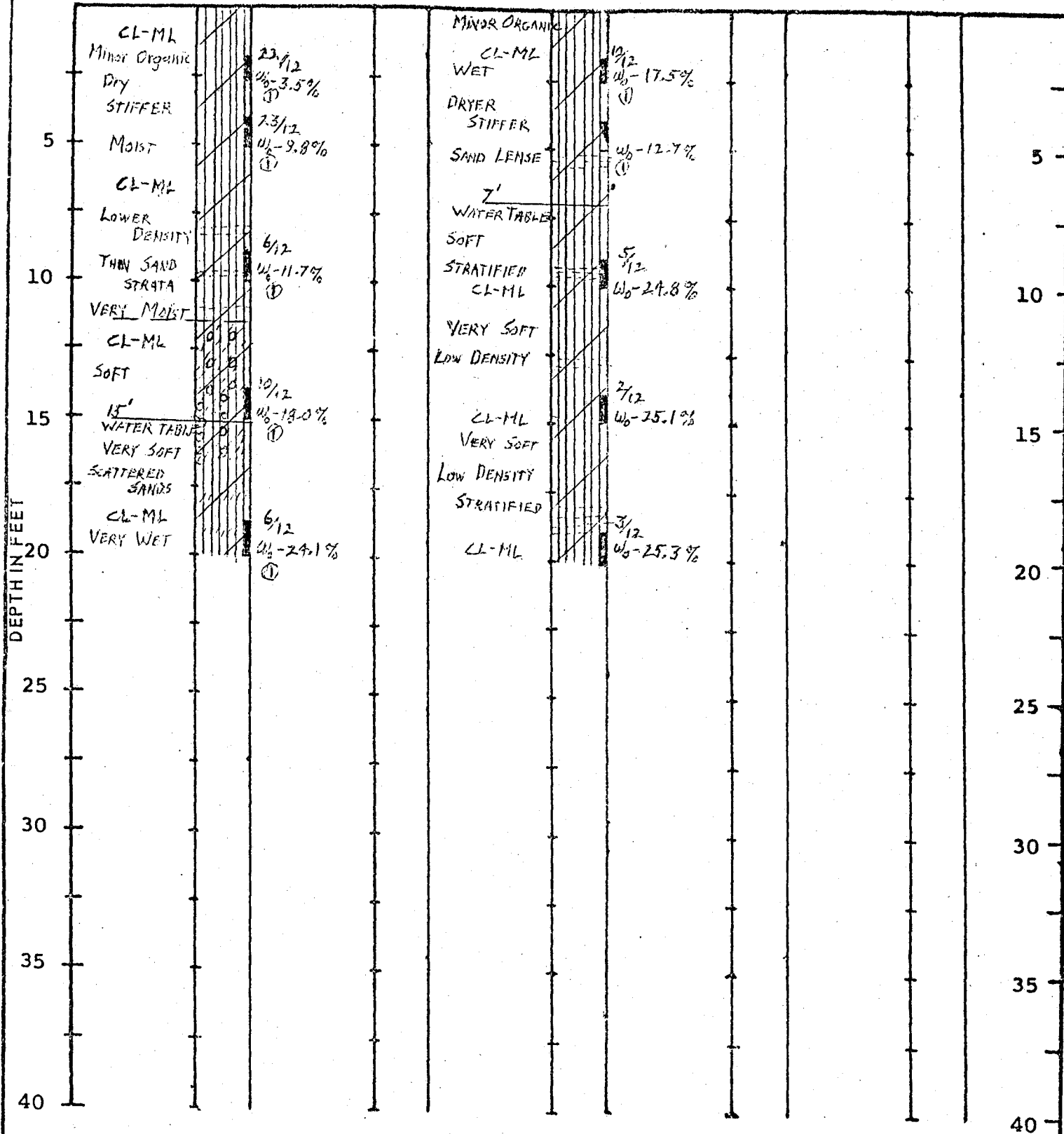
COLORADO: Colorado Springs, Pueblo,
Glenwood Springs, Montrose, Gunnison,
Grand Junction. - WYO. - Rock Springs

**EXPLANATION OF BOREHOLE LOGS
AND LOCATION DIAGRAMS**

Test Hole No.
Top Elevation

1

2



DRILLING LOGS

LINCOLN-DeVORE TESTING LABORATORY
COLORADO SPRINGS-PUEBLO, COLORADO

SUMMARY SHEET

Soil Sample CL - ML
 Location 7th & Glenwood Avenue
 Boring No. one Depth 9'
 Sample No. one

Test No. J-435
 Date 4-13-79
 Test by ZDT

Natural Water Content (w) 11.7 %
 Specific Gravity (Gs) 2.71

In Place Density (ρ_o) 105.8 pcf

SIEVE ANALYSIS:

Sieve No.	% Passing
1 1/2"	
1"	
3/4"	
1/2" 100	
4 97.8	
10 94.8	
20 93.2	
40 92.5	
100 90.6	
200 86.2	

Plastic Limit P.L. 19.3 %
 Liquid Limit L.L. 25.8 %
 Plasticity Index P.I. 6.5 %
 Shrinkage Limit 16.2 %
 Flow Index _____
 Shrinkage Ratio _____ %
 Volumetric Change _____ %
 Lineal Shrinkage _____ %

MOISTURE DENSITY: ASTM METHOD

Optimum Moisture Content w_o _____ %
 Maximum Dry Density ρ_d _____ pcf
 California Bearing Ratio (cbr) _____ %
 Swell 1 Days 4.0 %
 Swell against 984 psf w_o gain 9.5 %
 Remolded Swell Sample _____

HYDROMETER ANALYSIS:

Grain size (mm)	%
<u>.021</u>	<u>49.9</u>
<u>.005</u>	<u>29.6</u>
_____	_____
_____	_____
_____	_____
_____	_____
_____	_____
_____	_____

BEARING:

Housel Penetrometer (av) 1200 psf
 Unconfined Compression (qu) 1230 psf
 Plate Bearing: _____ psf
 Inches Settlement _____
 Consolidation % unde. _____ psf

PERMEABILITY:

K (at 20°C) _____
 Void Ratio _____
 Sulfates _____ ppm.
 +2000

SOIL ANALYSIS

LINCOLN-DeVORE TESTING LABORATORY
 COLORADO SPRINGS, COLORADO

REVIEW SHEET SUMMARY

FILE NO. #59-81 DUE DATE 5-18-81

ACTIVITY PB - First Security Savings & Loan

PHASE Revised Final (Williamsen Bldg.)

LOCATION NW Corner of 7th & Glenwood

PETITIONER Leland Schmidt/Joseph Lacy

PETITIONER ADDRESS 536 Bookcliff Drive

ENGINEER Chambliss Associates

<u>DATE REC.</u>	<u>AGENCY</u>	<u>COMMENTS</u>
5/12/81	City Fire Dept.	We cannot accept this as a final. We need a fire flow done on this building. To do this we need square footage type of construction and distance to exposures. Also we need existing hydrant locations and proposed hydrant locations with line sizes. With this information we will be able to determine if you have adequate fire protection.
5/18/81	City Utilities	None.
5/18/81	Mountain Bell	Existing 20' utility easement in alley on the west side of property will be sufficient for our use.
5/18/81	Transportation Engineer	The overall traffic circulation and parking concept is O.K. while access to alley's by businesses is not generally desirable, the character and existing use of this alley would not be affected by the bank. With regard to the temporary structure, it's location is not compatible with parking layout, as shown. The structure encroaches into the aisle and reduces needed maneuvering space.
5/18/81	City Engineer	Final Development Plan looks good from standpoint of driveway locations and sizes, public sidewalks and the proposed planter and curbing to define and somewhat control that alley at Glenwood. All driveway and sidewalk work in the streets rights-of-way must be according to City Standard Drawing ST-1 and will require a permit issued to a licensed concrete contractor by the City Engineer's office.
5/18/81	Staff Comments:	<p>Check if mobile unit has drive up windows? (drive-up policy).</p> <p>North portion permanently landscaped in phase I. Is drive-up canopy part of temp. or perm. bldg.</p> <p>Alley access in question - (what impact on 7th & Glenwood).</p> <p>Bushies at entry off of 7th - no blocked views.</p> <p>Need dimensions/ht./scale of perm. bldg.</p> <p>Future parking area unclear.</p> <p>Ht./Dimensions of canopy bldg.</p> <p>Fire access thru drive-up.</p> <p>Trash p/u needs to be indicated.</p> <p>Need phasing.</p> <p>Employee parking.</p> <p>Internal circulation on final plan in question (should enter on 7th, exit on Glenwood).</p> <p>Left turn on to 7th needs impact statement.</p> <p>Fits 7th St. policy.</p> <p>Lighting needs to be shown.</p> <p>Previously approved for 12,000 sq. ft. - now 8 - 10,000 sq. ft.</p> <p>Need bike racks</p>

Signage needs to be shown.
Improvements guarantee for sidewalks.
Commitment letter with time of removal of
temp. bldg.

Project must obtain bldg. permit within 1 year
of approval or schedule rehearing.

5/19/81 City Parks/Rec. No' comment.

5/26/81 KAMICAR/RINKER PASSED 6-0 A MOTION TO RECOMMEND TO THE CITY COUNCIL APPROVAL OF #59-81, REVISED FINAL PLAN, FIRST SECURITY SAVINGS AND LOAN ASSOCIATION, FOR USE AS A FINANCIAL INSTITUTION; THAT THE REVISED FINAL PLAN APPROVAL OF THE BUILDING LOCATION AND THE NORTHERN ACCESS BE GRANTED; THAT THE ADDITIONAL ACCESS MOVEMENTS BE READDRESSSED AT THE TIME THAT THE PLANS FOR THE BUILDING ARE PROCESSED, AND THAT THE TEMPORARY DRIVE-UP BUILDING FACILITY BE ALLOWED FOR A PERIOD OF NO MORE THAN TWO YEARS FROM MAY 22, 1981.

REVIEW SHEET SUMMARY

FILE NO. 59-81 DUE DATE 1/15/82
 ACTIVITY Rezone RMF32 to P and Amendment to final plan
 PHASE Final Plan ACRES _____
 LOCATION For rezone location is: 1221 N. 7th - for amendment to final plan NW corner of
7th and Glenwood
 PETITIONER First Security Savings and Loan Assoc. - Leland Schmidt
 PETITIONER ADDRESS 1211 N. 7th Street, Grand Junction, CO 81501
 ENGINEER Dillon-Hunt P.C.

OVERALL CONSIDERATIONS

- OVERALL COMPATABILITY** 7th Street Corridor Policies
- CONSISTENCY**
- ADJACENT PROPERTY**
- CHANGE IN THE AREA**
- TRAFFIC IMPACT**

HAS THIS BEEN ADDRESSED
 YES NO

<u>DATE REC.</u>	<u>AGENCY</u>	<u>COMMENTS</u>
	Staff Comments	1) How will landscaping be maintained? Any way to preserve existing vegetation? 2) Will the screen fence to the north be an adequate buffer? 3) What is a pedestrian barrier - (need detail)? 4) Any bike racks provided? 5) Need trash pick-up coordinated with Sanitation Dept. 6) Handicap parking space designated as such. 7) Parking stalls next to curb cut off 7th in question - too close to entry. 8) 1 space next to alley in question - backing into ingress/egress. 9) Employee parking to be designated. 10) Note with 10,800 square feet building - need 36 plus employee parking with some stalls in question may require smaller building. 11) Will need dimensions of proposed building - since this is a final, need exact height, elevation and dimensions. 12) Any neighborhood input? 13) Drive-up lanes should be designated. 14) Need detailed signage. 15) Has fire flow and fire access and fire hydrants been approved for drive-up and building? (see previous review comments). 16) Lighting detail needed. 17) How will the alleyway be impacted as a result of this proposal?

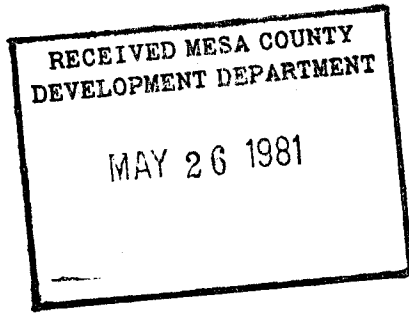
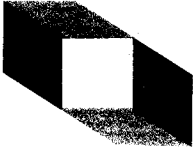
Continued on page 2

<u>DATE REC.</u>	<u>AGENCY</u>	<u>COMMENTS</u>
	Staff Comments Continued	Project must obtain building permit within 1 year of final approval or be scheduled for a rehearing.
1/12/82	City Fire	The fire dept. will require a fire hydrant to be installed on the northwest corner of Glenwood Ave. and 7th St.. The reason for this is that to utilize hydrants other than the one on 6th & Glenwood, the fire dept. would have to lay hose across 7th St., which is a main arterial street. The Fire Dept. will have no objections to this rezone and final, if the above hydrant is installed. Hydrant agreement must be signed. We estimate a fire flow of 1000 GPM.
1/12/82	Public Service	Gas & Electric: No objections to rezone. Customer to contact P.S.Co. for service of utilities. CB 1-6-82 THI 1-7-82.
1/14/82	Mountain Bell	No requests.
1/15/82	City Engineer	Sidewalk on Glenwood should be detached at least 4 feet from driveway to 7th Street. Access points look OK. Certain parking stalls next to driveway accesses should be deleted because they will conflict with ingress/egress. Existing utilities should be shown on the plan. All curb, gutter and sidewalk work in the right-of-way will require a permit to the contractor from City Engineers office. The alley is a traffic problem but I do not feel that should mean the City denies them reasonable access to the alley. Their drive-up will function better with that access rather than forcing the egressing vehicles to exit onto 7th Street. The alley is posted as one-way (Southbound).
1/18/82	Staff Comments	1) This is a final plan and we need more details. 2) Parking lot broken up with more landscaping. 3) Better design and utilization for parking lot could be shown. 4) No existing utilities shown - need detailed utilities composit - this is a final. 5) Outdoor section lighting shown.

LATE REVIEW SHEETS

1/18/82	Transportation Engineer	
1/18/82	City Utilities	
1/19/82	Mailed copy of Summary to Petitioner and Engineer.	
2/9/82	GJPC Minutes 1/26/82	MOTION: (COMMISSIONER TRANSMEIER) "ON ITEM #59-81, REZONE FROM RMF-32 TO P FOR THE FIRST SECURITY SAVINGS AND LOAN, I MAKE THE RECOMMENDATION THAT WE PASS THIS TO CITY COUNCIL WITH A RECOMMENDATION FOR APPROVAL PENDING THE STAFF COMMENTS." THE MOTION WAS SECONDED BY COMMISSIONERS RINKER/DUNIVENT. CHAIRWOMAN QUIMBY READ THE MOTION AND CALLED FOR A VOTE. THE MOTION CARRIED UNANIMOUSLY, 6-0. MOTION: (COMMISSIONER TRANSMEIER) "ON ITEM #59-81, FINAL PLAN FOR THE FIRST SECURITY SAVINGS AND LOAN, I MAKE A RECOMMENDATION WE PASS THIS ON WITH A RECOMMENDATION FOR APPROVAL TO CITY COUNCIL WITH NOT LESS THAN 34 PARKING SPACES AND NOT MORE THAN 10,200 SQUARE FEET OF BUILDING ON THE SITE AND SUBJECT TO STAFF

<u>DATE REC.</u>	<u>AGENCY</u>	<u>COMMENTS</u>
	GJPC Minutes (cont.)	COMMENTS, SPECIFICALLY THE SIDEWALK BE RESOLVED BEFORE PRESENTATION TO CITY COUNCIL." THE MOTION WAS SECONDED BY COMMISSIONER O'DWYER. CHAIRWOMAN QUIMBY REITERATED THE MOTION AND SECOND AND CALLED FOR A VOTE BY THE COMMISSIONERS. THE MOTION PASSED, 6-0. CHAIRWOMAN QUIMBY ASKED MR. HUNT IF HE HAD A FEEL FOR TIME FRAME FOR THE DESIGN AND SITE REVIEW. MR. HUNT INDICATED HE HAD NOT TALKED WITH BOB GOLDIN AT THIS POINT.



May 26, 1981

First Security Savings & Loan Facility
Response to:
Review Sheet Summary

OVERALL CONSIDERATIONS

Compatability

The proposed uses and plan are generally compatible with the existing and planned uses in the area and with the consumers desire of combined uses in an area for convenience (i.e. super market/financial/beauty/barber/etc.).

Consistency

The proposed uses and plan are consistent with the previously approved use and plan for this Planned Business zone. They are also consistent with the adopted Seventh Street policy, the GENERAL LAND USE goal, objectives, and policies, the COMMERCIAL LAND USE goal, objectives and policies, the TRANSPORTATION objectives and policies, and the SPECIFIC AREA/USE POLICIES of drive-up uses and Seventh Street.

The uses and plan are also consistent with the general provisions and requirements of the City Planned Unit Development Regulation.

Adjacent Property

Adjacent uses on the east, southeast, south and southwest sides of this property are business and commercial in nature.

Grand Junction High School on the west is designed so that the area immediately adjacent to this property is used for landscaping, parking, storage, and circulation in lieu of specific educational uses.

There is an existing residential use on the north side. Screening with landscaping of a residential scale character, and design will be installed to insure an attractive separation with privacy.

Change in the Area

Development of these uses at this location will not create a significant change in the typical uses and activities of the area. It will introduce some additional traffic but only to the extent of those specific First Security customers who do not currently use the existing neighborhood facilities and streets.

Traffic Impact

The submitted Impact Statement presents the traffic projections and discusses the ability of the adjacent streets and sidewalks to accommodate that impact. The specific increase in area circulation is as addressed above in "Change in the Area".

CITY FIRE DEPARTMENT

When the architectural construction documents are completed for the structure they will include the direction of the City Fire Department and will also be submitted to the Fire Department for approval prior to final planning approval.

TRANSPORTATION ENGINEER

The aisle width will be increased by five (5) feet to comply with the minimum parking requirements.

CITY ENGINEER

A licensed concrete contractor will acquire the required permits.

STAFF COMMENTS

The temporary modular structure will not provide drive-up service.

The entire area north of the building envelope will be permanently developed and landscaped.

The drive-up service facilities and canopy will be designed and constructed as part of the permanent structure.

There will be no access from the alley, only egress.

There will be no landscaping higher than thirty (30) inches at all vehicular ingress and egress points.

See drawing "Notes" for general scale and height of permanent structure.

Future parking will meet the parking requirements.

Canopy specific design to be part of architectural documents and will meet adequate height requirements for truck, camper and fire vehicle ingress.

Trash receptacle will be located adjacent to the alley for pick-up by the city sanitation trucks.

See drawing "Notes" for phasing within the Development Schedule.

Employees parking locations will be at the direction of the First Security manager.

Internal circulation will be as shown on the presentation drawing.

Adequate lighting for security will be provided and will be directed away from the residential uses.

One bike rack will be provided.

Business sign will be designed as part of the architectural contract. A temporary sign will be located at the Seventh Street entrance and will meet sign and visibility requirements.

Improvement guarantee for the sidewalk will be submitted with the final documents.

A commitment letter will be provided for the removal of the temporary structure to reflect the desires of the City Planning Commission and Council.

Please review attached letter.

April 30, 1981

City Development Department
Reviewing Agencies
City Planning Commission
City Council

RE: First Security Savings & Loan Facility

This proposal has been prepared and submitted for your consideration prior to the commencement of architectural services for the design and construction of this facility.

We respectfully request your review, consideration and approval of this amendment to the previously approved Final Development Plan.

Having received that approval, the contract for property purchase will be executed and architectural work will commence for the permanent structure. When the design work is completed, the specific building, landscaping, parking and pedestrian area features within the building envelope will be added to the final plan and submitted to the Development Department Staff.

If these specifics are within the scope and intent of the approved proposal, the plan will be signed, dated and recorded. If the specifics exceed the project scope by more than 10% or deviate from the intent of the proposal, the project will be reviewed by you as an amendment to the approved plan. This procedure is in accordance with the City Planned Unit Development Regulations.

Thank you.

Ken Rabideau
Dennis Edson

IMPACT STATEMENT

FOR

FIRST SECURITY SAVINGS & LOAN FACILITY

April 30, 1981

I. INTRODUCTION

The First Security Savings and Loan Building and Facilities, including drive-up service, is proposed to be constructed on a .52 acre site located on the northwest corner of Seventh Street and Glenwood Avenue.

The site is currently zoned "Planned Business" and has received final approval for a 12,000 square foot office building. This application is for an amendment of the approved final plan. The site has no development on it presently.

II. AMENDMENT

This proposal is slightly smaller in scope from the previously approved office building of 12,000 square feet.

III. SITE LOCATION

This site is located one block north of a major east-west arterial highway and adjacent to a major north-south arterial street which provides access to the entire city. It is adjacent to a Safeway Supermarket, a restaurant and a small commercial center providing retail and service uses. Grand Junction High School is located to the west of the site. A residential use exists to the north of the site.

IV. FACILITY

The proposed facility will serve as the main office of the new independent First Security Savings and Loan Association. The association will provide full service including mortgage lending, savings and

checking accounts, CD's and related financial services. The facility will be designed to accommodate customer walk in, drive and park, and drive-up services.

These services will be provided on a smaller scale the first year from a temporary modular structure. The permanent facility will be constructed in the second year following approval of the amendment of the Final Development Plan.

V. PROJECTED ACTIVITY

An economic feasibility study prepared for the State Savings and Loan Commission and the Federal Home Loan Bank Board presented the following projected activity.

	Operating Year (\$ Millions)		
	<u>1st</u>	<u>2nd</u>	<u>3rd</u>
Accumulated Deposits	3.0	5.4	8.7
Permanent Loans Originated	3.0	6.4	9.6
Construction Loans Originated	2.0	4.1	7.1
Estimated number of employees	6	9	12

These projections are based on anticipated branches at other locations within the community which will attract and serve a portion of the new business. Estimated daily traffic (shown below) at the proposed site is based on the above projections, with other branches absorbing 15% and 30% of the volume during the second and third years respectively.

	Operating Year (cars/month)		
	<u>1st</u>	<u>2nd</u>	<u>3rd</u>
Loan inquiries	20	37	45
New permanent loan originations	25	45	56
Construction loan originations and servicing	40	70	99
Vendors, etc.	50	75	100
Savings	<u>874</u>	<u>1180</u>	<u>1476</u>
Total	1009	1407	1776
Divided by working days per month			
Traffic daily	48	67	85

VI. IMPACT, PRESENT AND FUTURE

A. ACCESS

Seventh Street is a major north-south arterial roadway servicing a major portion of the City and the suburban area to the north.

Glenwood Avenue is a collector street which is located one block north of a major east-west arterial highway servicing a major portion of the City and the suburban area. All access streets are fully developed to accommodate the traffic and pedestrian demands of this facility. It is anticipated that existing and future public transportation services will service the site area.

B. UTILITIES

Power, gas and telephone service lines are located adjacent to this site and have available capacity to service the temporary and permanent structures.

C. CITY SERVICES

Sewer and water service lines are located adjacent to this site and have available capacity to service the temporary and permanent structures. Fire and police services have direct access to the site and the site is within one mile of the main station.

D. NEIGHBORHOOD USES

Uses to the southwest, south, southeast and east are commercial in nature and generally generate more activity and traffic movements than the proposed use. No adverse impacts are anticipated on these commercial neighbors.

Grand Junction High School is located on the west of the site. This facility will share a common alley for egress of their properties. It is anticipated that the High School traffic movements will be of greater impact on this facility than vice versa. This facility will provide a new east-west sidewalk to Glenwood Avenue which will provide for pedestrian use, especially high school students.

There is a residential use adjacent to the north of the site. Ingress, egress and parking activities in the north area of the site will contribute additional noise and increase climatic temperatures to the residential use due to the asphalt and concrete development of the parking area. Solid screen fencing and landscaping will be provided all along the north boundary of the site at the east entry to the site, and on the south boundary of the parking lot. The visual and noise barriers of the fencing and shrubs and the shade of the trees will reduce and/or minimize these impacts. Area and building lighting will be directed away from the residential use. This facility will generate less traffic movement noise than the existing uses adjacent to the residential use.

No nighttime use of the facility or parking areas is anticipated.

In keeping with community financial institutions, the facility and its grounds will be designed, developed and maintained at a quality level. Therefore, this facility will provide a positive design and maintenance example and impact to the entire neighborhood.

VII. CITY REGULATIONS AND POLICIES

This proposal conforms to and/or supports the North Avenue Corridor Policy, the Seventh Street Corridor Policy, and the Drive-Up Facility Policy.

The Facility will be designed, developed and maintained in accordance with the City Zoning, Development and Construction Codes.

May 26, 1981

City Planning Commission
City Council
City Development Department

RE: First Security Savings & Loan Facility

This proposal has been prepared and submitted for your consideration prior to the commencement of architectural services for the design and construction of this facility.

We respectfully request your review, consideration and approval of this amendment to the previously approved Final Development Plan.

Having received that approval, the architectural work will commence for the permanent structure. When the design work is completed, the specific building, landscaping, parking and pedestrian area features within the building envelope area will be added to the final plan and submitted to the Development Department Staff.

The final plan will be reviewed by the Planning Commission and Staff and if the specifics are within the scope and intent of the approved proposal, the plan will be signed, dated and recorded. If the specifics exceed the project scope by more than 10% or deviate from the intent of the proposal, the project will be scheduled for a rehearing of the submitted plan. This procedure is in accordance with the City Planned Unit Development Regulations.

Thank you.

Ken Rabideau
Dennis Edson

IMPACT STATEMENT
FOR
FIRST SECURITY SAVINGS & LOAN FACILITY
January 4, 1982

I. INTRODUCTION

The First Security Savings and Loan Building and Facilities, including drive-up service, is proposed to be constructed on a .52 acre site located on the northwest corner of Seventh Street and Glenwood Avenue.

The site is currently zoned "Planned Business" and has received final approval for the temporary structure and drive-up. This application is for an amendment of the approved final plan for the development within the "building envelope" area which was shown on that plan but not approved and requires this submittal.

II. AMENDMENT AND REZONE

This proposal is for the amendment of the final plan to incorporate the development within the "building envelope" area into the plan.

This proposal also requests the rezone of the property directly to the north of the previously approved site from Residential to Parking. This site was purchased by the petitioner for purposes of possible future expansion. The growth of their business since opening the temporary structure, however, suggests the need for a larger facility than was originally anticipated. This structure, however, will not exceed 10,800 SF, which is still smaller than the original 12,000 SF proposed for this site. The north site will only be used for parking to support the proposed building and no additional structures are planned for this site.

III. SITE LOCATION

This site is located one block north of a major east-west arterial highway and adjacent to a major north-south arterial street which provides access to the entire city. It is adjacent to a Safeway Supermarket, a restaurant and a small commercial center providing retail and service uses. Grand Junction High School is located to the west of the site. A residential use exists to the north of the site.

IV. FACILITY

The proposed facility will serve as the main office of the new independent First Security Savings and Loan Association. The association will provide full service including mortgage lending, savings and checking accounts, CD's and related financial services. The facility will be designed to accommodate customer walk in, drive and park and drive-up services. These services are presently provided on a smaller scale for a temporary modular structure.

The permanent structure will be a two story structure made of a solid material; for example, brick or concrete. The approximate height will be 27 feet with a total of 8,000 to 10,800 SF. Solar energy systems are presently being considered for energy conservation.

The drive-up facility will accommodate 8 cars stacked on the property. The overhead canopy shall be of a height that will not restrict fire protection vehicles to pass by. Two drive-ups will be provided initially with expansion to four units in the future.

V. PROJECTED ACTIVITY

An economic feasibility study prepared for the State Savings and Loan Commission and the Federal Home Loan Bank Board presented the following projected activity:

	Operating Year (\$ Millions)		
	1st	2nd	3rd
Accumulated Deposits	3.0	5.4	8.7
Permanent Loans Originated	3.0	6.4	9.6
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Estimated number of employees	6.	9	12

These projections are based on anticipated branches at other locations within the community which will attract and serve a portion of the new business. Estimated daily traffic (shown below) at the proposed site is based on the above projections, with other branches absorbing 15% and 30% of the volume during the second and third years respectively.

	Operating Year (cars/month)		
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A. ACCESS

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Glenwood Avenue is a collector street which is located one block north of a major east-west arterial highway servicing a major portion of the city and the suburban area. All access streets are fully developed to accommodate the traffic and pedestrian demands of this facility. It is anticipated that existing and future public transportation services will service the site area.

B. UTILITIES

Power, gas and telephone service lines are located adjacent to this site and have available capacity to service the permanent structure.

C. CITY SERVICES

Sewer and water service lines are located adjacent to this site and have available capacity to service the permanent structure. Fire and police services have direct access to the site and the site is within one mile of the main station. There are two locations of fire hydrants along Glenwood Avenue, 6th Street and 8th Street. Both lines are 6" lines.

D. NEIGHBORHOOD USES

Uses to the southwest, south, southeast and east are commercial in nature and generally generate more activity and traffic movements than the proposed use. No adverse impacts are anticipated on these commercial neighbors.

Grand Junction High School is located on the west of the site. This facility will share a common alley for egress of their properties. It is anticipated that the High School traffic movements will be of greater impact on this facility than vice versa. This facility will provide a new east-west sidewalk to Glenwood Avenue which will provide for pedestrian use, especially high school students.

There is a residential use adjacent to the north of the site. Ingress, egress and parking activities in the north area of the site will contribute additional noise and somewhat impact the residential use. Solid screen fencing will be provided all along the north boundary of the site. Landscaping and fencing will be provided at the east entry to the site, and on the south and west boundaries of the parking lot. The visual and noise barrier will reduce and/or minimize these impacts. Area and building lighting will be directed away from the residential use. This facility will generate less traffic movement noise than the existing uses adjacent to the residential use.

No nighttime use of the facility or parking areas is anticipated.

In keeping with community financial institutions, the facility and its grounds will be designed, developed and maintained at a quality level. Therefore, this facility will provide a positive design and maintenance example and impact to the entire neighborhood.

VII. CITY REGULATIONS AND POLICIES

This proposal conforms to and/or supports the North Avenue Corridor Policy, the Seventh Street Corridor Policy, and the Drive-Up Facility Policy.

The Facility will be designed, developed and maintained in accordance with the City Zoning, Development and Construction Codes.

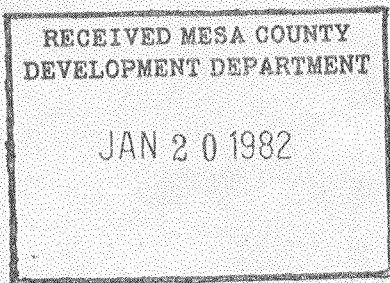
1-18-82

Bob: Sorry it took so long to get this to you, but it took a little diggin and January has really been hectic.

I find no place in the hearing where it appeared that final approval of the permanent structure was contemplated, except in some references of Robidoux and McDonnough, and then it doesn't really seem to be very clear on that point.

Should you need this in transcript form and certified, I'll do so. If you have any more questions on this, don't hesitate to call.

Leila



LEILA E. MOSHER



RECEIVED MESA COUNTY
DEVELOPMENT DEPARTMENT

JAN 26 1982

January 25, 1982

Mr. Bob Golden
Senior City Planner
City/County Development Department
559 White Avenue
Grand Junction, Colo. 81501

Re: Rezone Application - File # 59-81

Dear Bob:

The purpose of this letter is to respond to the review comments on the above referenced application.

STAFF COMMENTS:

1. An underground irrigation system will be installed for watering purposes and a staff member will be assigned the duty of grounds upkeep. We will contact the Parks and Recreation Dept. and get their suggestions on preserving the existing vegetation.
2. The fence proposed in the prior submittal was approved as adequate and this fence would be relocated to the new north property line.
3. The pedestrian barrier is proposed to control the auto/ped traffic at the drive-up.
4. A bike rack will be provided in close proximity to the building entrance.
5. We will have the trash pick-up location approved by the Sanitation Dept. and make any adjustments they require.
6. The handicap spaces shown on the plan will be marked according to city requirements.
7. The parking space next to the curb cut on Seventh will be relocated to match the previous plan.
8. The parking space next to the alley will be eliminated.

9. Employee parking will be located at the most remote part of the parking lot.

10. The 36 spaces proposed will allow for a building of 10,800 SF. By eliminating the two spaces as required above, the building size will be reduced by 600 SF. If the rezoning request of the property to the north is denied, only 29 spaces can be provided, allowing a building of 8700 SF.

11. The dimensions for the building will vary depending on the factors above and the height for the two story structure will be approximately the 27 ft. proposed in the impact statement.

12. We have contacted the neighbor to the north and he has no objections to this proposal. The renters currently in the house to be rezoned are intending to move shortly so they will not be displaced as a result of this proposal.

13. The drive-up lanes and drive-thru lane will be clearly designated.

14. The site signage proposed will be circulation and directional. The S & L sign will be integrated into the building facade and will comply with the city sign code.

15. See City Fire.

16. Security lighting of the parking area will be provided in such a manner that it will not affect the neighboring properties.

17. See City Engineer.

18. The intention of the petitioner is to be under construction in April of this year.

REVIEW AGENCY:

City Fire: The petitioner will work with the Fire Dept. in meeting their requirements for fire hydrants.

City Engineer: The petitioner will locate the sidewalk on Glenwood where the City Engineer requests but would like to suggest that an alternative would be to make the walk 8' wide in lieu of having the 4' area between the walk and curb.

All necessary permits for work in the right-of-way will be obtained from the City Engineer's office.

Existing utilities are presently shown on the plan.

Sincerely,



Kenneth Hunt

KH:kh

cc: First Security Savings

City
County
Development
Department

MEMORANDUM

Reply Requested

Date: 1/27/82

YES NO

To: Bob

From: Alex

- 1) Approved zoning of RMF-3.2 to Parking, subject to review comment.
- 2) Approved revised plan, subject to review comment, excluding employee parking issue.
 - A) Stipulation is that the # of bldg. not exceed 10,000 sq ft parking @ 34 spaces.

Resolve sidewalk issue w/ Rsh

City
County
Development
Department

MEMORANDUM

Reply Requested

Date: 5/12/83

YES NO

To: File # 59-81
xc Doug Cline

From: Bob Golden

On 5/11/83 Doug Cline from the City, the St. Maintenance Super. noted a drain cut just north of the drive-up facility at 1st Sec S/L, 7th + Glenwood. This cut is draining into the alleyway which has a low spot where the water is settling causing problems. He noted it looked more like an after thought.

5/12/83 I called Jillion/Hunt Arch & spoke w/ Ken Hunt. He verified the cut as an after thought for insurance of drainage. He will contact Doug directly on this matter.



February 8, 1982

Bob Golden
Senior City Planner
City/County Development Department
559 White Avenue
Grand Junction, Colo. 81501

Re: Rezone Application - File #59-81

Dear Bob,

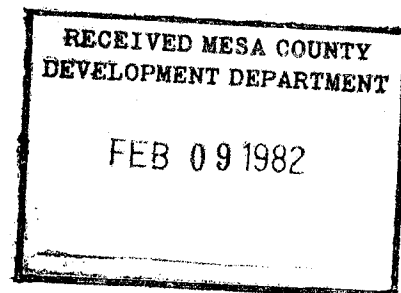
We have been directed by Ron Rish of the City Engineering Department to provide either a 4 ft. sidewalk, beginning 4 ft. from the curb, or an 8 ft. wide sidewalk beginning right at the curb on Glenwood Street, south of the proposed First Security Building.

We agree to incorporate one of these plans into our design and construction documents.

If you have any questions please do not hesitate to contact us.

Sincerely,

Kenneth Hunt



ZONING VIOLATION REPORT

NO. #59-81
DATE 7/25/83

OCCUPANT First Security Savings & Loan ADDRESS 7th & Glenwood
 OWNER Dillon & Hunt, Architects ADDRESS _____
 PHONE 245-7383 DIRECTIONS _____
 REFERRED BY Site ck. DATE _____
 VIOLATION need a bike rack + curb blocks along parking island CODE _____
 COMPLAINANT _____ ADDRESS _____

7/26 Ken Hunt - informed him of the above; he will get back with me

8/5 site ck. - no change

8/15 left message for Ken Hunt to call me

8/16 Ken Hunt - bike rack on order; First Sec. will put in curb blocks, too

9/24 Ken Rabideau - said they plan on putting in a bike rack; will consider curb blocks, too. Will call me when bike rack is in.
 → 241-3333 (bank)

10/13 Carol at 1st Security - wondered what type to put in - I said as long as it would fit 3 or 4 bikes + to call me when it's in place.

10/19 Carol - said they have ordered a bike rack

12/12 site ck. - bike rack slab is in place

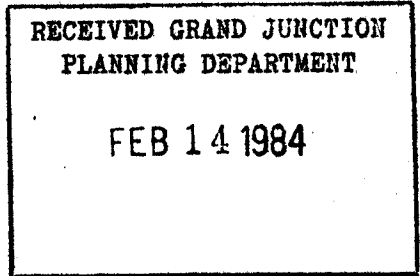
file 59-81
city



1211 North 7th Street • Grand Junction, CO 81501 • (303) 241-3333

February 14, 1984

City-County Planning Dept.
Bob Golden
559 White Avenue
Grand Junction, CO 81501



Dear Bob:

First Security Savings & Loan Association proposes to increase the size of our building by approximately 3,000 square feet by excavation of the basement. We are planning on expanding our operations to include a proof machine and microfilming machine which will need room for that we do not have right now.

The additional space will not have an effect on building dimensions, set backs, traffic circulation or building appearance. The only possible impact we anticipate is that we may be able have more employees in the building but the customer traffic will not be impacted.

This additional space will not be used for customr related services and as a result it will not have any effect on customer parking, drive-up windows demand or necessarily number of customers.

To accomodate for an increase in the number of potential employees, estimated at approximately 8-10, we have arranged to lease parking spaces from the Lutheran Church, located 1½ blocks up from us on 7th Street. They have agreed to lease us up to 20 spaces if necessary since our hours of operation do not coincide.

If we are unable to increase our basement size our alternative is to rent the house across the street and move our Accounting Department and Proofing operations offsite.

We don't have a plan available, other than we would tell you the basement would be fully excavated, and would have appropriate fire exits. It is doubtful there would be anything but open space.

We would appreciate the planning department allowing us a minor change to our PUD subject to receiving parking for at least 10 additional people.

Thank you.

Sincerely,

Dennis J. Edson
President

No parking on
Church lot
need to look
elsewhere.

DJE/rcw

