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e. PUR-1995-021

Date 7/12/99

P r e s e n t	S c a n n e d	<p>A few items are denoted with a (*) are to be scanned for permanent record on the ISYS retrieval system. In some instances, not all entries designated to be scanned, are present in the file. There are also documents specific to certain files, not found on the standard list. For this reason, a checklist has been included.</p> <p>Remaining items, (not selected for scanning), will be marked present on the checklist. This index can serve as a quick guide for the contents of each file.</p> <p>Files denoted with (**) are to be located using the ISYS Query System. Planning Clearance will need to be typed in full, as well as other entries such as Ordinances, Resolutions, Board of Appeals, and etc.</p>
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X	X	*Summary Sheet – Table of Contents
		Application form
		Receipts for fees paid for anything
		*Submittal checklist
		*General project report
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		Reduction of assessor's map
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		Other bound or nonbound reports
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		*Consolidated review comments list
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		*Summary sheet of final conditions
		*Letters and correspondence dated after the date of final approval (pertaining to change in conditions or expiration date)

DOCUMENTS SPECIFIC TO THIS DEVELOPMENT FILE:

X		E-mail to Michael Drollinger – 2/13/95		
X		Minor Street Plan - Draft		
X	X	Code Amendment – final and draft		
X		Letter from Tom Dixon to Builder, Developer, Realtor		
X		E-mail to Tom Dixon from Hank Masterson, re: Fire Department access roads – 11/17/94		
X		Letter from Fredrik Sultan, KLH-ITS, inc. to Tom Dixon		
X		Residential Streets – 2 nd Edition		
X		Exhibit B – Local Street Standards		
X		Exhibit A – Major Street Standards		
X		Exhibit J – Auto Court Access Standards		
X		Transport. – Boulder Brings Back the Neighborhood Street		
X		City of Boulder Emergency Vehicle Access Requirements		
X		Notes & drawing on Texas State Dept. of Highway diagram		
		Toward Community – Residential Design Guidelines for the City of San Jose		

**Minor Street Plan
Hermosa Avenue Extension
15th Street to 27 1/2 Road**

Area Description

The area covered by this Minor Street Plan lies north of Patterson Road approximately one-quarter mile, and east of 15th Street to 27 1/2 Road. There are several large, undeveloped parcels of land within this area abutting the two north-south collector streets.

Purpose

This plan identifies the need for an east-west residential collector street to serve the area by providing a transportation link between neighborhoods. This link is important to automobile traffic, bicycle traffic, pedestrian traffic and emergency vehicle access, delivery and sanitation access.

The City of Grand Junction Comprehensive Plan identifies the following goal for transportation:

Achieve a well-balanced transportation system including automobiles, bus, railroad, air, pedestrian and bicycle.

The following objectives set forth under this goal apply to this plan:

Encourage the provision of efficient circulation routes connecting all areas of Mesa County with important social, economic, and educational functions.

Encourage the orderly and economic development of transportation systems necessitated by existing and future land uses. Road and street systems should aid in the logical development of Grand Junction and Mesa County.

Achieve convenient, safe and economical highway and street systems through proper functional classification, design, improvements and maintenance.

Protect residential, commercial, industrial and public areas from undesirable and unnecessary traffic while at the same time providing proper access to these areas without hampering traffic flow and accessibility of emergency services.

Encourage a compact development pattern which will promote better use of existing routes, optimize the future demand for public transit and minimize pollution by reducing the need for auto travel.

Circulation Needs

Provide a transportation link between neighborhoods which does not force every trip to use Patterson Road. Protection of the operating capacity of Patterson Road is very important.

Allow pedestrians and bicyclists an east-west connection other than Patterson Road.

Allow emergency vehicles, delivery vehicles, and sanitation vehicles to move efficiently in the area.

Traffic Projections

Traffic counts conducted in 1992 indicated an average 1178 vehicles per day used 15th Street north of Patterson Road. Projections for the year 2015 indicate the volumes will be 4000 vehicles per day. A traffic signal is scheduled for installation at the intersection with Patterson Road in 1995.

Traffic counts conducted in 1992 indicated an average 5604 vehicles per day used 27 1/2 Road, and projections for the year 2015 indicate the volumes will approach 9000 vehicles per day. The intersection with Patterson Road is currently signalized.

Current zoning for the 15 acre parcel affected by this minor street plan is RSF-4. If developed under this zoning, 60 single family residences could be constructed, adding 600 trips per day to the street network. Interest in upzoning this parcel to construct 170 apartment units has been shown recently and this would produce an additional 1100 trips per day. The 2 acre parcel to the south is zoned RSF-8 and has the potential for producing 160 new trips per day. The 4 acre parcel to the south is zoned PR-10 and could potentially increase trips by 400 vehicles per day.

Proposed Street Section

These traffic projections indicate a residential collector street section is appropriate. The proposed street standard is designed for 1000-3000 vehicles per day. The pavement is 36' wide with 7' of vertical curb, gutter, and sidewalk on each side in a 52' right-of-way width. This design allows public access by vehicle, pedestrian and bicycle traffic.

Design Criteria

Both 15th Street and 27 1/2 Road are identified as collector streets, and the minimum intersection spacing allowed on a collector street is 300'. Hermosa Avenue must align across 15th Street. As it is extended east, the horizontal alignment must vary from a straight line to meet the minimum intersection spacing criteria on 27 1/2 Road. This means it may align with Spring Valley Circle, or be located north of the Spring Valley Circle at least 300' and must be south of Hawthorne Avenue by 300'.

A residential street connection to the south will be required for development of the adjacent property.

Affected Parcels

The following parcels would be required to access the Hermosa Avenue extension and share in the cost of construction for their proportionate share based on frontage.

2945-013-00-016
2945-013-00-018
2945-013-00-020
2945-013-00-037

Issues and Concerns

The following issues and concerns were expressed to staff by residents of the area and by potential developers. The issue or concern is highlighted with the staff response following.

Cut through traffic will be increased if a street connection is made by extending Hermosa Avenue, resulting in a majority of the traffic using 15th Street.

In 1995 the intersection of 15th Street and Patterson Road will be signalized, making both the 15th Street and 27 1/2 Road intersections with Patterson Road equally attractive. The attraction of the mall and downtown to draw traffic west and south will be offset by the attraction to the Horizon Drive area and the tendency by drivers to seek the path of least resistance. This path is usually the one with the fewest stops, delays and left turns.

How much additional traffic will use Hermosa Avenue to 12th Street to turn left and can the street handle it? Similarly, what effects will the increased traffic have on F 1/2 Road and Ridge Drive with a future connection from 12th to 15th Street?

The focus of this plan is primarily on the parcels directly affected by the extension of Hermosa Avenue. However, it is important to consider the effects of development on adjacent neighborhoods. Development of the approximately 21 acres affected

DRAFT

by this plan is estimated to generate between 1100 and 1600 trips per day.

24 hour counts were taken in December, 1994 to establish a benchmark of existing traffic conditions. A summary of those counts is presented.

F 1/2 Road east of 12th St.
Hermosa Avenue between 12th and 15th Street
Ridge Drive east of 15th Street

High development costs will be a result of requiring the extension of Hermosa Avenue.

DRAFT

Code Amendment

Please review and comment on this initial approach to the "auto court" concept. It is hoped that this can go before the Planning Commission in January.

FILE: #1-94(X)

DATE: November 17, 1994

STAFF: Tom Dixon

REQUEST: Text Amendment
Text Addition

*FOCUS SHOULD BE A
"PRIVATE STREET STANDARD".*

APPLICANT: City of Grand Junction

OVERVIEW: In many communities, both in the United States and in countries with similar approaches to residential design such as Canada and Australia, dead-end street standards serving a limited number of residential units are allowed in certain instances. Two cities, Portland, Oregon and Boulder, Colorado, have gone so far as to develop "skinny street" standards that reduce street widths according to function and traffic usage.

Consideration of these alternative street designs reflect changes in how streets serving a limited number of residences are viewed today. Short, dead-end streets serving a limited number of lots are often referred to as "auto courts". The proposed use of "auto courts" as an alternative to standard street requirements has been proposed in several recent subdivision proposals within the City. Presently, the ZDC is silent on this issue and there is no provision for allowing them or set standard as to when they should or should not apply.

The proposed code amendment is intended to allow "auto courts" as a development alternative for residential subdivisions. This provision will support an infill development strategy and is capable of promoting high-quality development patterns in situations when it is not feasible to require or develop full width streets which serve a limited area and number of residences. *DO WE WANT TO LIMIT TO CERTAIN ZONES?*

The general code language should follow something like this:

1) PURPOSE: The intent of the auto court design is to allow a short and narrow private street standard that serves up to six separate residential lots or units and extends for no more than 150 feet from a public street. These types of streets are intended to create alternatives to the current public street standards which are designed to allow two-way traffic flows at moderate speeds and parking on both sides of the street. The "auto courts" can: 1) permit greater flexibility in residential street access, 2) encourage more creative design and clustering in residential subdivisions, 3) decrease development infrastructure costs, 4) provide a public benefit by reducing public street maintenance costs, 5) provide

*OR A LONGER SYSTEM AND LONGER THAN 300 FT? (12 UNITS?)
SHOULD WE SPECIFY "COMMON-WALL" UNITS?
(I.E. DUPLEX, TOWNHOMES, ETC.)*

Code Amendment

FILE: #1-94(X)

DATE: January 16, 1995

STAFF: Tom Dixon, AICP

OVERVIEW: In many communities, both in the United States and in countries with similar approaches to residential design such as Canada and Australia, private street standards serving a limited number of residential units are allowed in certain instances. Two cities, Portland, Oregon and Boulder, Colorado, have gone so far as to develop "skinny street" standards that reduce street widths according to function and traffic usage.

The City of Grand Junction is evaluating the feasibility of creating alternative street standards in certain instances. This reflects changes in how streets serving a limited number of residences are viewed. Private streets could be allowed in certain circumstances where the scale and character of a project might be enhanced. Short, dead-end streets serving a limited number of lots without a cul-de-sac turnaround are sometimes referred to as "auto courts". The proposed use of "auto courts" as an alternative to standard cul-de-sac requirements has been proposed in several recent subdivision proposals within the City. A variation to the "auto court" concept is a looped private street, essentially a pair of "auto courts" that link together with each end connecting to a public right-of-way. Looped private streets have also been proposed for some recent development projects.

Presently, the Zoning and Development Code is silent on this issue and there is no provision for allowing private streets. The adoption of minimum standard for "auto courts" looped private streets, or similar variations is needed.

The proposed code amendment is intended to allow "auto courts" and looped private streets as a development alternative for residential development. This provision will support an infill development strategy and is capable of promoting high-quality development patterns in situations when it is not feasible to require or develop full widths streets which serve a limited area and/or number of dwelling units.

The proposed code language is as follows:

1) PURPOSE: The intent of the private street standard is to allow two types of alternative street designs which serve a limited purpose. One is an auto court, a short and narrow private street that serves up to six residential dwelling units, extends for no more than 150 feet from a public street, and terminates in a dead-end. The second type is a looped private street which connects on both ends to public streets, serves no more than 12 residential units, and has a total length no greater than 300 feet from the edge of pavement with a public street.

The two types of private streets are intended to create alternatives to the current public street standards which are designed to allow two-way traffic flows at moderate speeds and parking on both sides of the street. These private streets can: 1) permit greater flexibility in residential street access, 2) encourage more creative design and clustering in residential development, 3) decrease development infrastructure costs, 4) provide a public benefit by reducing public street maintenance costs, 5) provide safe residential environments, and 6) promote attractive streetscapes that give neighborhoods character and identity by allowing alternative streets surfaces, finishes and designs.

2) STANDARDS: The following standards shall apply to auto courts.

a) An auto court shall serve no more than six (6) residential dwellings units if it deadends and no more than twelve (12) dwelling units if it is a looped drive. For private looped streets, additional off-street parking shall be required at the rate of one space per two units and shall be located in one or more parking pods within 200 feet of any unit the street serves. These parking areas are reserved for guest parking and shall not be used for the parking of residents vehicles and/or recreation vehicles for more than a 24-hour period.

b) The maximum length of a private street shall not exceed 150 feet if it deadends and shall not exceed 300 feet if it is looped. These lengths are measured along property lines from the public street.

c) A community mail box and a common trash/garbage collection area shall be located on or near the public right-of-way. Such installations shall be sited so that mail delivery and recycling/garbage pick-up can be accomplished without entering onto or through the private street.

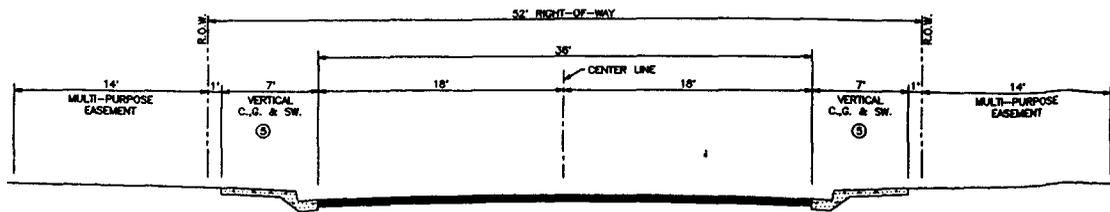
d) Fire hydrants to serve the dwelling units shall be located as required by the Uniform Fire Code.

e) An auto court or private street shall be constructed to the "private street section" standard within the City's adopted street standards. Finished surface may be composed of other variable hard surfaces such as brick, interlocking pavers, cobblestones, or similar finishes, designed by a professional Engineer and as approved by the City Engineer. Asphalt pavement will not be allowed.

f) The width of the auto court shall not be less than 20 feet. The pavement width shall be widened through horizontal curves as required to accommodate the movement of a WB-40 truck. Parking of automobiles shall be limited to the front of garages or in designated parking pods and shall not be permitted along the sides of the auto courts unless adequate width provision is made, as approved by the City Engineer.

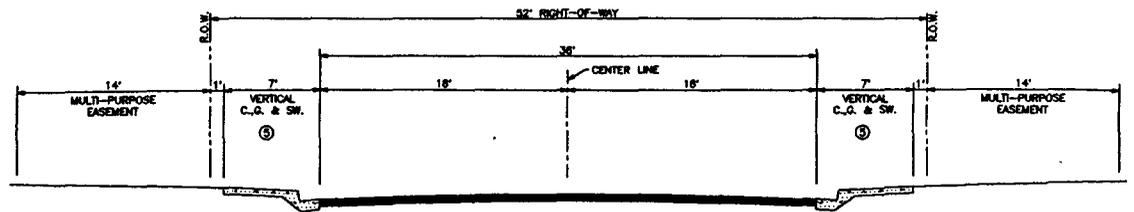
g) All entrances to garages shall have at least a 20-foot depth separated from the common maneuvering area of the auto court in order to accommodate additional parking.

- h) Each driveway shall have a minimum 15-foot turning radius into the maneuvering portion of the auto court unless otherwise approved by the City Engineer.
- i) A utility easement shall be required for a portion of or the full width of the private street when necessary for service delivery.
- j) The use of auto courts or private streets shall be confined to residential development in Planned Residential zones as approved through a Minor or Major Subdivision.
- k) A common maintenance agreement shall be required for all dwelling units that access the auto court or private street segment. Such agreements shall be recorded with a final plat and shall go with each dwelling unit. This agreement shall not be terminated except by written consent by the Director of Public Works of the City of Grand Junction.
- l) Each residential structure accessed from the private street shall have a landscaped area of at least five (5) feet deep between the street and the structure except for the driveway to the garage.



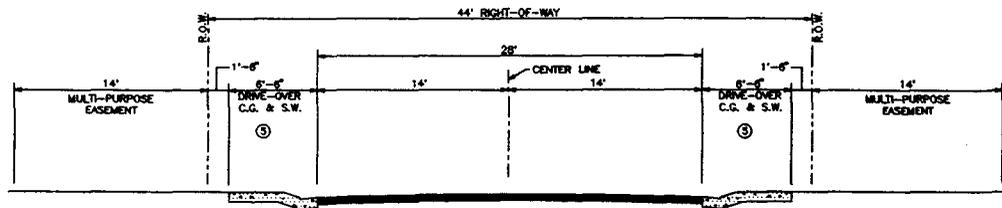
URBAN RESIDENTIAL COLLECTOR

1000 TO 3,000 A.D.T.
ON STREET PARKING ALLOWED
ONLY WHERE LEFT TURN LANE IS NOT REQUIRED



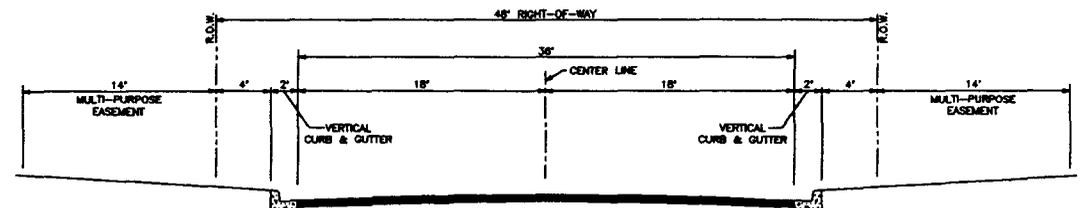
COMMERCIAL STREET SECTION

0 TO 3000 ADT
NO ON-STREET PARKING



URBAN RESIDENTIAL STREET

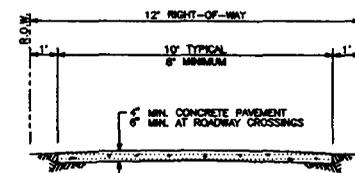
0 TO 1000 A.D.T.



INDUSTRIAL STREET SECTION

0 TO 3000 ADT
NO ON-STREET PARKING

NOTE:
STREETS IN INDUSTRIAL AREAS MAY BE CONSTRUCTED WITH 5' WIDE LONGITUDINAL 'V' PANS IN LIEU OF CURBS & GUTTERS WHERE APPROVED BY THE CITY ENGINEER.



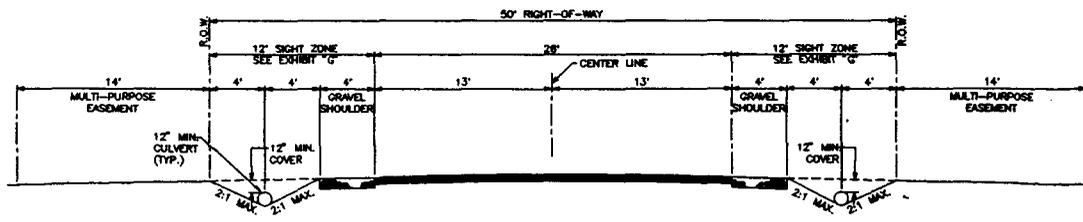
TWO-WAY OFF STREET BICYCLE PATH

RESIDENTIAL TRIP GENERATION RATES

DEVELOPMENT TYPE	VEHICLE TRIPS PER DWELLING UNIT	
	WEEKDAY	PEAK HOUR
SINGLE-FAMILY DETACHED	9.55	1.02
APARTMENT, GENERAL	6.47	0.89
CONDOMINIUMS/TOWNHOUSES	5.86	0.54
MOBILE HOME PARK	4.81	0.56
RETIREMENT COMMUNITY	3.30	0.34
PLANNED UNIT DEVELOPMENT	7.44	0.72
CHURCH - TRIPS PER 1,000 SQ. FT. FLOOR AREA	9.32	1.42

REFERENCE FOR ABOVE AND OTHER DEVELOPMENTS:
LATEST EDITIONS OF THE "TRIP GENERATION MANUAL" BY THE INSTITUTE OF TRANSPORTATION ENGINEERS (I.T.E.)

A.D.T.-AVERAGE DAILY TRAFFIC



RURAL ROADWAY

LESS THAN 500 A.D.T. AND
WITH RESIDENTIAL PARCELS OF
NO LESS THAN 2 ACRES.

NOTES:

- DRIVE OVER CURBS SHALL BE INSTALLED ONLY ON RESIDENTIAL STREETS WITH LESS THAN 1000 A.D.T.
- ALL STREETS AND ROADWAYS SHALL BE SURFACED WITH HOT BITUMINOUS PAVEMENT (HBP) OR PORTLAND CEMENT CONCRETE (PCC). ALL PAVEMENT STRUCTURES SHALL BE DESIGNED IN ACCORDANCE WITH THE COLORADO DIVISION OF HIGHWAYS, ROADWAY DESIGN MANUAL (LATEST EDITION), OR OTHER APPROVED METHOD. MINIMUM ASPHALT PAVEMENT THICKNESS SHALL BE 3" ON RESIDENTIAL STREETS AND 4" ON COMMERCIAL, INDUSTRIAL OR MIXED USE STREETS.
- SEE EXHIBIT "C" FOR DETAILS OF MULTI-PURPOSE EASEMENTS ADJACENT TO ROAD RIGHT-OF-WAY.
- DRIVEWAY CULVERTS ON RURAL ROADS SHALL BE INSTALLED AND MAINTAINED BY PROPERTY OWNERS.

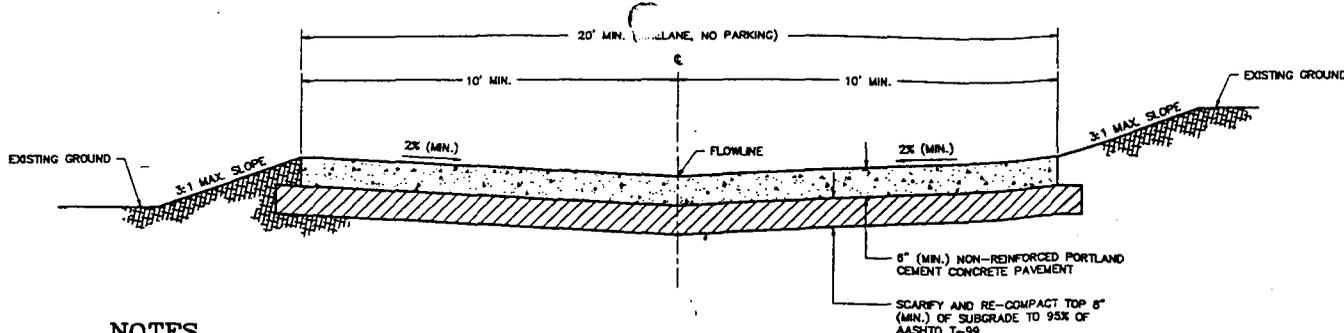
- UPON THE APPROVAL OF THE COMMUNITY DEVELOPMENT DIRECTOR, A PEDESTRIAN TRAIL SYSTEM MAY BE SUBSTITUTED FOR AN ATTACHED SIDEWALK IF IN THE OPINION OF THE DIRECTOR, PROPERTIES ADJACENT TO THE STREET SECTION COULD EASILY ACCESS THE TRAIL AND THE TRAIL SYSTEM LINKS TRANSPORTATION OR RECREATION NODES OUTSIDE AND WITHIN THE DEVELOPMENT. TRAIL WIDTH SHALL BE EQUAL TO THE STANDARD FOR A TWO-WAY OFF STREET BICYCLE PATH. THE TRAIL SHALL BE LOCATED ON DEDICATED PUBLIC RIGHTS-OF-WAY.

DESCRIPTION	DATE	DRAWN BY	N.O.P.	DATE	SCALE
ADOPTED BY CITY COUNCIL RESOLUTION	7/1/92	T.A.B.	T.A.B.	9/29/92	BLANK PROFILE
REVISION		J.D.M.	J.D.M.	9/29/92	HORIZ. N.T.S. HORIZ.
REVISION					VERT.

DEPARTMENT OF PUBLIC WORKS AND UTILITIES
ENGINEERING DIVISION 244-1554
CITY OF GRAND JUNCTION, COLORADO

EXHIBIT "B"
LOCAL STREET STANDARDS

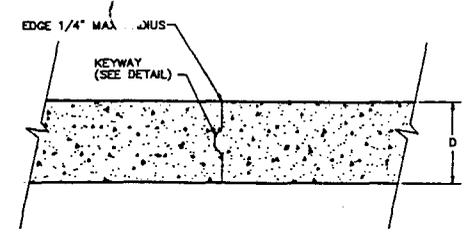
SHEET NO. _____
OF _____
FILE NO. _____
EX-9-94.DWG



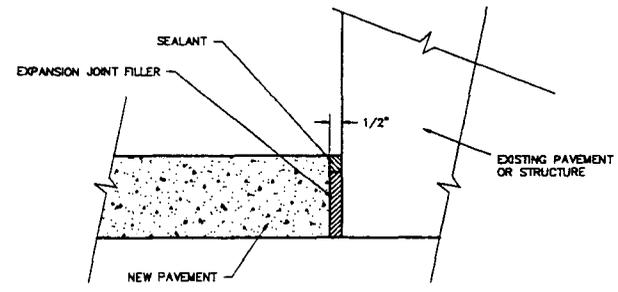
NOTES

1. FLAT, SUPERELEVATED OR CROWN PAVEMENT SECTIONS MAY BE USED WHEN ADEQUATE DRAINAGE IS PROVIDED AND APPROVED.
2. MINIMUM LONGITUDINAL GRADE SHALL BE 1%.

AUTO COURT SECTION

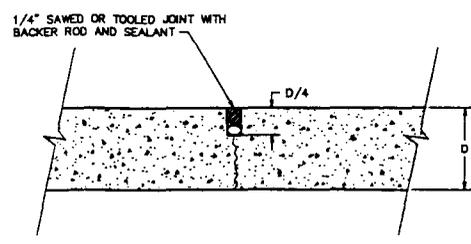


CONSTRUCTION JOINT

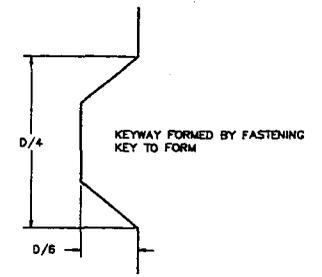


ISOLATION JOINT

REQUIRED WHERE NEW PAVEMENT JOINS EXISTING PAVEMENT, CASTINGS OR STRUCTURES.



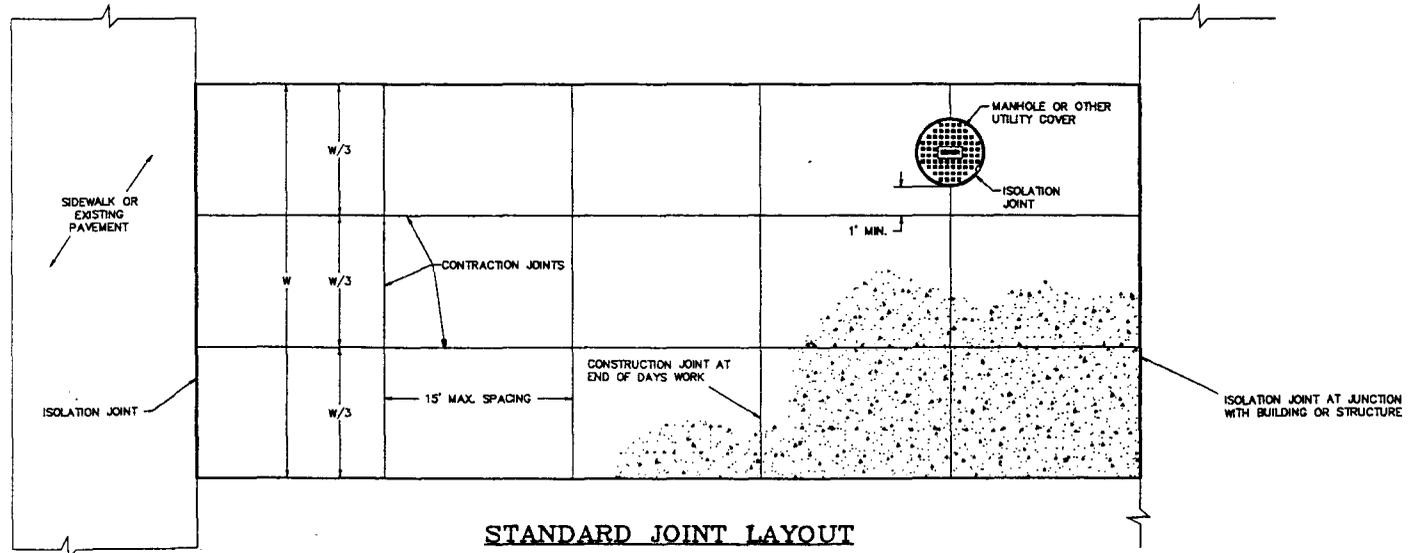
CONTRACTION JOINT



KEYWAY DETAIL

GENERAL NOTES

1. PORTLAND CEMENT CONCRETE SHALL MEET THE REQUIREMENTS OF C.D.O.T. CLASS B. ADMIXTURES SHALL NOT BE USED WITHOUT WRITTEN APPROVAL BY THE CITY ENGINEER.
2. CONCRETE SHALL BE PLACED AND CONSOLIDATED BY HAND OR WITH APPROVED MECHANICAL METHODS. CONCRETE PLACED BY HAND SHALL BE STRUCK OFF WITH A STRAIGHT EDGE AND THE SURFACE SEALED WITH A BULL FLOAT. THE USE OF MECHANICAL OR HAND TROWELS WILL NOT BE PERMITTED. THE FINAL SURFACE TEXTURE SHALL BE MADE WITH A COURSE BROOM TRANSVERSE TO THE DIRECTION OF VEHICULAR TRAVEL.
3. A CONSTRUCTION JOINT SHALL BE PLACED AT THE END OF A DAYS WORK OR WHERE CONCRETE PLACEMENT IS DISCONTINUED FOR MORE THAN 30 MINUTES.
4. AN ISOLATION JOINT SHALL BE INSTALLED WHERE NEW CONCRETE PAVEMENT IS PLACED AGAINST AN EXISTING PAVEMENT, CASTING OR STRUCTURE.
5. TRANSVERSE AND LONGITUDINAL CONTRACTION JOINTS SHALL BE CONSTRUCTED TO THE DIMENSIONS AND AT THE SPACING SHOWN ON THE JOINT LAYOUT DETAIL. TRANSVERSE JOINTS SHALL EXTEND THE ENTIRE WIDTH OF THE PAVEMENT. JOINTS MAY BE FORMED IN THE PLASTIC CONCRETE OR SAWED AFTER THE CONCRETE HAS HARDENED. FORMED JOINTS MAY BE CONSTRUCTED BY INSTALLING A PARTING STRIP TO BE LEFT IN PLACE OR BY PRESSING STRAIGHT EDGE INTO THE PLASTIC CONCRETE. SAWING OF JOINTS SHALL BEGIN AS SOON AS THE CONCRETE HAS HARDENED SUFFICIENTLY TO PERMIT SAWING WITHOUT EXCESSIVE RAVELING AND BEFORE UNCONTROLLED CRACKING OCCURS.
6. ALL ISOLATION AND CONTRACTION JOINTS SHALL BE SEALED BEFORE THE PAVEMENT IS EXPOSED TO TRAFFIC. PRIOR TO SEALING, ALL FOREIGN MATERIAL SHALL BE REMOVED FROM THE JOINTS AND THE JOINTS SHALL BE THOROUGHLY DRY. JOINT FILLING SHALL BE DONE WITHOUT SPILLING MATERIAL ON THE EXPOSED SURFACE OF THE CONCRETE. ALL SEALANT MATERIAL PLACED ON THE SURFACE SHALL BE IMMEDIATELY REMOVED. JOINT SEALING COMPOUND SHALL MEET THE REQUIREMENTS OF A.S.T.M. D1190 OR D3405. ALTERNATELY, LOW MODULUS MATERIAL SUCH AS SILICONE, HOT OR COLD-POURED RUBBERIZED ASPHALT OR NITRATE RUBBER MAY BE USED.
7. WHEN CONCRETE HAS BEEN PLACED IN COLD WEATHER AND THE TEMPERATURE MAY DROP BELOW 35 DEGREES F. STRAW OR INSULATING BLANKETS SHALL BE PROVIDED TO PROTECT THE CONCRETE. THE METHODS AND MATERIAL USED SHALL BE SUCH THAT A MINIMUM TEMPERATURE OF 40 DEGREES F. WILL BE MAINTAINED AT THE SURFACE OF THE CONCRETE FOR 5 DAYS. ANY CONCRETE DAMAGED BY FROST ACTION SHALL BE REMOVED AND REPLACED AT THE OWNERS EXPENSE. NO CONCRETE SHALL BE PLACED ON FROZEN SUBGRADE.
8. AFTER FINISHING OPERATIONS HAVE BEEN COMPLETED AND IMMEDIATELY AFTER THE FREE WATER HAS LEFT THE SURFACE, THE SURFACE AND THE SIDES OF THE SLAB SHALL BE COATED WITH A UNIFORM LAYER OF MEMBRANE CURING COMPOUND APPLIED AT THE RATE OF NOT LESS THAN ONE GALLON PER 200 SQUARE FEET OF SURFACE. IF USING FIXED FORM CONSTRUCTION, CURING COMPOUND SHALL BE APPLIED TO THE SIDES OF THE SLAB AT THE TIME OF FORM REMOVAL. AREAS IN WHICH THE CURING MEMBRANE IS DAMAGED WITHIN A PERIOD OF 5 DAYS SHALL BE RESPRAYED WITH CURING COMPOUND. CURING COMPOUND MAY BE OMITTED WHEN THE PAVEMENT IS OTHERWISE PROTECTED FROM SUNLIGHT AND INCLEMENT WEATHER FOR 5 DAYS. MEMBRANE CURING COMPOUNDS SHALL CONFORM TO THE REQUIREMENTS OF AASHTO-M-148 AND/OR ASTM-C-1183.
9. THE PAVEMENT SHALL BE CLOSED TO TRAFFIC FOR 7 DAYS AFTER THE CONCRETE IS PLACED OR UNTIL IT REACHES A COMPRESSIVE STRENGTH OF 2500 PSI.

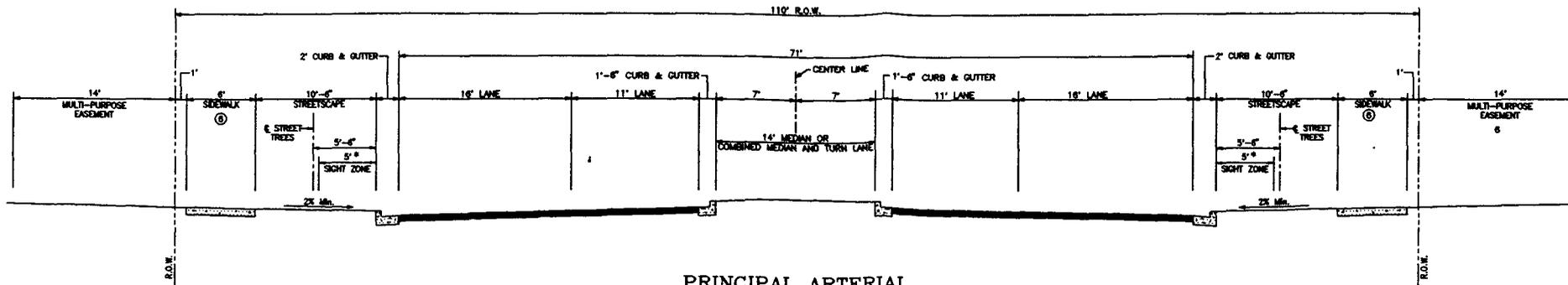


STANDARD JOINT LAYOUT

DESCRIPTION	DATE	DRAWN BY	DATE	SCALE
		DPW		
		CHECKED BY		
		APPROVED BY		
		FILED BOOK NO.		

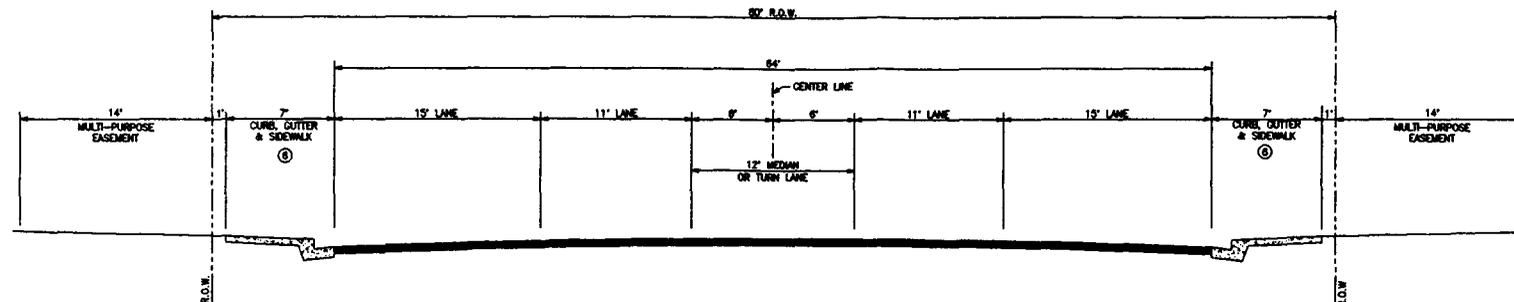
DEPARTMENT OF PUBLIC WORKS AND UTILITIES
ENGINEERING DIVISION 244-1554
CITY OF GRAND JUNCTION, COLORADO

EXHIBIT "J"
AUTO COURT ACCESS STANDARDS
SHEET NO. _____
OF _____
FILE NO. _____
EX-J-94.016

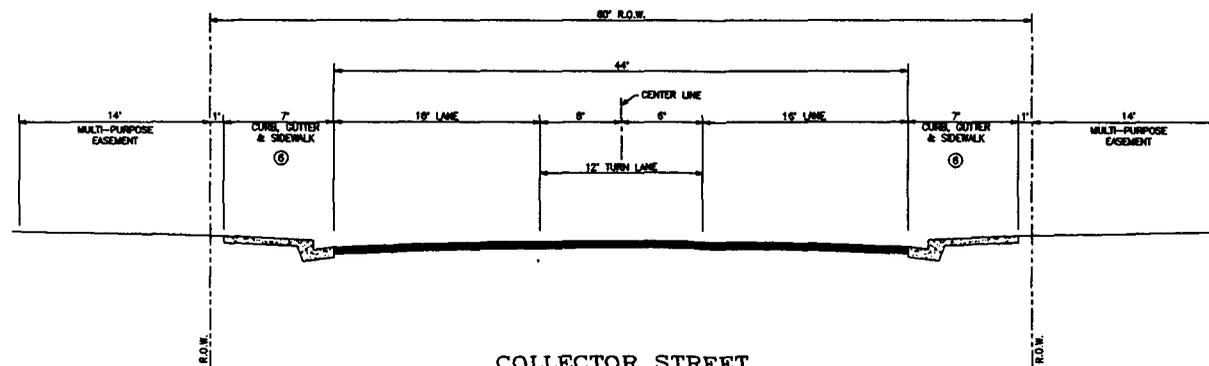


PRINCIPAL ARTERIAL
 GREATER THAN 18,000 A.D.T.
 (NO ON-STREET PARKING)

* NO TREES, SHRUBS, SIGNS, STRUCTURES OR OTHER OBSTRUCTIONS OVER 30' IN HEIGHT ALLOWED WITHIN SIGHT ZONE (EXCEPTIONS: TRAFFIC CONTROL SIGNS AND TRAFFIC SIGNAL POLES)



MINOR ARTERIAL STREET
 8,001 TO 18,000 A.D.T.
 (NO ON-STREET PARKING)



COLLECTOR STREET
 3,000 TO 8,000 A.D.T.
 (NO ON-STREET PARKING)

NOTES:

- ① MAJOR STREETS SHALL INCLUDE ALL MAJOR AND MINOR ARTERIALS AND COLLECTORS DESIGNATED ON THE GRAND JUNCTION URBANIZED AREA FUNCTIONAL CLASSIFICATION MAP.
- ② VERTICAL CURVES ARE REQUIRED ON ALL ARTERIAL AND COLLECTOR STREETS.
- ③ ALL STREETS AND ROADWAYS SHALL BE SURFACED WITH HOT BITUMINOUS PAVEMENT (HBP) OR PORTLAND CEMENT CONCRETE (PCC). ALL PAVEMENT STRUCTURES SHALL BE DESIGNED IN ACCORDANCE WITH THE COLORADO DIVISION OF HIGHWAYS, ROADWAY DESIGN MANUAL (LATEST EDITION), OR OTHER APPROVED METHOD.
- ④ ADDITIONAL RIGHT-OF-WAY WIDTH WILL BE REQUIRED FOR RIGHT TURN LANES AT INTERSECTIONS OF ARTERIAL STREETS AND WHERE SPEED CHANGE LANES ARE REQUIRED. SEE SECTION 4.8, STATE HIGHWAY ACCESS CODE.
- ⑤ SEE EXHIBIT "C" FOR DETAILS OF MULTI-PURPOSE EASEMENTS ADJACENT TO ROAD RIGHT-OF-WAY.
- ⑥ UPON THE APPROVAL OF THE COMMUNITY DEVELOPMENT DIRECTOR, A PEDESTRIAN TRAIL SYSTEM MAY BE SUBSTITUTED FOR AN ATTACHED SIDEWALK IF IN THE OPINION OF THE DIRECTOR, PROPERTIES ADJACENT TO THE STREET SECTION COULD EASILY ACCESS THE TRAIL AND THE DESTINATION OF THE TRAIL SYSTEM LINKS TRANSPORTATION OR RECREATION NODES OUTSIDE AND WITHIN THE DEVELOPMENT. TRAIL WIDTH SHALL BE EQUAL TO THE STANDARD FOR A TWO-WAY OFF-STREET BICYCLE PATH. THE TRAIL SHALL BE LOCATED ON DEDICATED PUBLIC RIGHTS-OF-WAY.

DESCRIPTION	DATE	DRAWN BY	N.O.P.	DATE	8/29/92
ADOPTED BY CITY COUNCIL RESOLUTION	7/1/92	CHECKED BY	T.A.R.	DATE	8/29/92
REVISION		APPROVED BY	J.D.N.	DATE	8/29/92
REVISION		FILED BOOK NO.		PAGE	

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DEPARTMENT OF PUBLIC WORKS AND UTILITIES
 ENGINEERING DIVISION 244-1554
 CITY OF GRAND JUNCTION, COLORADO

EXHIBIT "A"
 MAJOR STREET STANDARDS

SHEET NO.	
OF	
FILE NO.	EX-A-92.DWG