



PLANNING COMMISSION AGENDA
CITY HALL AUDITORIUM, 250 NORTH 5TH STREET
TUESDAY, MARCH 12, 2013, 6:00 PM

Call to Order

Welcome. Items listed on this agenda will be given consideration by the City of Grand Junction Planning Commission. Please turn off all cell phones during the meeting.

Copies of the agenda and staff reports are located at the back of the auditorium.

Announcements, Presentations and/or Prescheduled Visitors

Consent Agenda

Items on the consent agenda are items perceived to be non-controversial in nature and meet all requirements of the Codes and regulations and/or the applicant has acknowledged complete agreement with the recommended conditions.

The consent agenda will be acted upon in one motion, unless the applicant, a member of the public, a Planning Commissioner or staff requests that the item be removed from the consent agenda. Items removed from the consent agenda will be reviewed as a part of the regular agenda. Consent agenda items must be removed from the consent agenda for a full hearing to be eligible for appeal or rehearing.

1. Minutes of Previous Meetings

[Attach 1](#)

Approve the minutes of the January 22, 2013 regular meeting.

2. Premier Tire Warehouse Special Permit - Special Permit

[Attach 2](#)

Forward a recommendation to City Council for a Special Permit on 0.504 acres in a C-2 (General Commercial) zone district.

FILE #: SPT-2013-66

APPLICANT: John Perschbacher - Premier Tire

LOCATION: 461 Glenwood Avenue

STAFF: Senta Costello

*** * * END OF CONSENT CALENDAR * * ***

*** * * ITEMS NEEDING INDIVIDUAL CONSIDERATION * * ***

Public Hearing Items

On the following item(s) the Grand Junction Planning Commission will make the final decision or a recommendation to City Council. If you have an interest in one of these items or wish to appeal an action taken by the Planning Commission, please call the Planning Division (244-1430) after this hearing to inquire about City Council scheduling.

3. Greater Downtown Plan and Zone District Overlay – Comprehensive Plan Future Land Use Map Amendments, Comprehensive Plan Text Amendment, Rezoning Properties within the Greater Downtown Plan area, Zoning Overlays for Corridors and the Downtown District [Attach 3](#)

a. Comprehensive Plan and Future Land Use Map Amendments

Forward a recommendation to City Council for the Greater Downtown Plan as an amendment to the Comprehensive Plan which includes Future Land Use Map amendments.

FILE #: CPA-2011-1067
APPLICANT: City of Grand Junction
LOCATION: Citywide
STAFF: Kristen Ashbeck

b. Comprehensive Plan Text Amendment

Forward a recommendation to City Council to amend the text of the Comprehensive Plan to allow the R-O (Residential Office) zone district in the Downtown Mixed Use land use designation.

FILE #: CPA-2012-216
APPLICANT: City of Grand Junction
LOCATION: Citywide
STAFF: Kristen Ashbeck

c. Rezone

Forward a recommendation to City Council for City-initiated rezoning of properties within the Greater Downtown Plan area.

FILE #: RZN-2012-217
APPLICANT: City of Grand Junction
LOCATION: Citywide
STAFF: Kristen Ashbeck

d. Text amendments to Section 21.07 and Section 21.03.020(d) – Zoning Code Amendments

Forward a recommendation to City Council to amend Section 21.07 of the Zoning and Development Code to add the Greater Downtown Plan; and to amend Section 21.03.020(d) to include the R-O (Residential Office) zone district as a district that implements the Downtown MU (Mixed Use) land use classification.

FILE #: ZCA-2012-363
APPLICANT: City of Grand Junction
LOCATION: Citywide
STAFF: Kristen Ashbeck

General Discussion/Other Business

Nonscheduled Citizens and/or Visitors

Adjournment

Attach 1 Minutes

GRAND JUNCTION PLANNING COMMISSION JANUARY 22, 2013 MINUTES 6:00 p.m. to 7:18 p.m.

The regularly scheduled Planning Commission hearing was called to order at 6:00 p.m. by Chairman Wall. The public hearing was held in the City Hall Auditorium.

In attendance, representing the City Planning Commission, were Reginald Wall (Chairman), Ebe Eslami (Vice Chairman), Gregory Williams, Keith Leonard, Jon Buschhorn, Christian Reece and Steve Tolle (1st Alternate). Commissioner Loren Couch was absent.

In attendance, representing the City's Public Works and Planning Department – Lisa Cox (Planning Manager), Scott Peterson (Senior Planner) and Dave Thornton (Principal Planner).

Also present was Jamie Beard (Assistant City Attorney).

Lynn Singer was present to record the minutes.

There were 17 interested citizens present during the course of the hearing.

ANNOUNCEMENTS, PRESENTATIONS AND/OR VISITORS

Lisa Cox, Planning Manager, requested to amend the staff report for Agenda Item No. 2, the Feuerborn Annexation, to state that criterion number 1 of section 21.02.140(a) was not applicable and, therefore, had not been met as stated in the staff report. Staff would like to amend the report to reflect that criteria 2 through 5 had been met under the Findings and Conclusions.

Consent Agenda

1. Minutes of Previous Meetings

Approve the minutes of the November 13 and December 11, 2012 regular meetings.

2. Feuerborn Annexation – Zone of Annexation

Request a recommendation of approval to City Council to zone 2.68 +/- acres from County RSF-R (Residential Single Family – Rural) to a City C-1 (Light Commercial) zone district.

FILE #: ANX-2012-518
APPLICANT: Don Lilyquist – Maverik Inc
LOCATION: 2902 & 2906 D Road
STAFF: Scott Peterson

3. Colorado Mesa University Rezone – Planned Development – Extension Request

***** Continued from the January 8, 2013 regular meeting *****

Request a recommendation of approval to City Council for an extension of five (5) additional years until December 2017 for the previously approved Outline Development Plan to allow a mixture of residential, commercial and light industrial uses on 154.08 +/- acres in a PD (Planned Development) zone district.

FILE #: ODP-2008-154
APPLICANT: Colorado Mesa University Real Estate Foundation
LOCATION: 2899 D-1/2 Road
STAFF: Scott Peterson

Chairman Wall briefly explained the Consent Agenda and invited the public, planning commissioners, and staff to speak if they wanted any item pulled for additional discussion. After discussion, there were no objections or revisions received from the audience or Planning Commissioners on the Consent Agenda items.

MOTION:(Commissioner Reece) “Mr. Chairman, I move that we approve the Consent Agenda with that one correction.”

Commissioner Eslami seconded the motion. A vote was called and the motion passed unanimously by a vote of 7 - 0.

Public Hearing Items

4. Rock Shop Enclave Annexation – Zone of Annexation – PULLED 1/11/2013

Request a recommendation of approval to City Council to zone 49.82 acres from County RSF-R (Residential Single Family Rural) and County I-2 (General Industrial) to a City I-1 (Light Industrial) zone district.

FILE #: ANX-2012-574
APPLICANT: City of Grand Junction
LOCATION: South of D Road, East of South 15th Street and South of the Riverside Parkway on both sides of 27-1/2 Road north of Las Colonias Park
STAFF: Brian Rusche

5. North Avenue Overlay Zone District – Zoning Code Amendment

Request a recommendation of approval to City Council of a text amendment to the Zoning and Development Code (Title 21, Grand Junction Municipal Code) to add the North Avenue Overlay Zone District.

FILE #: ZCA-2012-572
APPLICANT: City of Grand Junction
LOCATION: North Avenue between 1st Street to east I-70 Business Loop
STAFF: Dave Thornton

Staff's Presentation

Dave Thornton, Principal Planner, Public Works, Utilities and Planning Department, said that they had been working on the North Avenue Overlay for close to a year. He said that the overlay would cover the area from 1st Street to I-70 Business Loop on the east and all properties that touched North Avenue would be within the overlay district

boundary. The City has adopted two different corridor plans that encompassed that area. In 2007 the first plan was adopted by City Council which comprised the area from 12th Street east to I-70 Business Loop and in 2011 the North Avenue West Corridor Plan was adopted which planned the rest of the corridor from 12th Street west to the Business Loop.

A survey had been conducted of the community regarding commercial vacancy rates and provided a snapshot and comparison of the amount of office and retail space available within the City limits. He stated that there was currently an 11.7% vacancy rate on North Avenue versus 5.9% City-wide and stated that it was the City's desire to revitalize North Avenue and believed the overlay district helped that revitalization effort and would be beneficial for the community. An advisory committee was formed and appointed by City Council in February 2012 and was made up of various property and business owners along the corridor. The Committee met nearly monthly, took a walking/bus tour of the corridor and held an open house in September to seek feedback from other business and property owners on the corridor regarding the draft zoning overlay concepts.

Elements of the proposed overlay district include a pedestrian scale streetscape; promoting site design with more building and less landscaping; accommodating all modes of transportation and users within the right-of-way; providing incentives to encourage new development and redevelopment; removing barriers to development; and creating safer access to North Avenue businesses.

The overlay district consists of three areas of emphasis. 1. Mandatory standards that include right-of-way dedication, construction for streetscape improvements including 8 ft detached sidewalk and 8 ft wide "park strip" improvements that are required for all new development and redevelopment regardless of the underlying "base" zone. 2. "Opt in" standards that are optional, but incentivized to encourage the developer to develop under these standards by choice. The overlay standards for landscaping (50% less) and setback (zero setbacks) are reduced as incentives to opt in. A maximum setback of 10 ft. (with exceptions for pedestrian spaces like outdoor dining areas, etc.) is proposed to create the building form and vision the North Avenue Corridor Plans established. This vision includes construction of buildings that are built closer to the street, requiring a front door to face North Avenue, in addition to pedestrian amenities enhancing the visual and human scale of the corridor. 3. A "Site Upgrade Point System" is included within the North Avenue Overlay Zone district to create a vehicle that can be the structure to implement an incentive grant program in the future as money becomes available from any source.

Mr. Thornton noted that there were areas where an existing building was within the required right-of-way, closer than 16 feet from the curb and the Committee was not suggesting that those portions of the buildings within that area be demolished in order to meet the standards. He mentioned that a higher priority was to keep the sidewalk as wide as possible and lessen the amount of distance between the sidewalk and the curb. He said the multi-purpose easement required in all areas of Grand Junction wasn't needed along North Avenue as utilities were either in an alley or already installed in the roadway. He added that ultimately North Avenue would have 11' travel lanes with an optional bike path, but the bike path wouldn't occur until it was deemed that traffic

conditions along the corridor would be safe enough to accommodate them, occurring at a future time when access and other safety improvements have been completed. The streetscape improvements placed in the area between the curb and the sidewalk could include things such as bike racks, shelters for the buses and trash cans. Long-term the entire corridor would see detached sidewalks and pedestrian lighting. By example, the sidewalk in front of Colorado Mesa University fell within inches of the proposed standard for sidewalk and park strip widths and is an example of what one could see throughout the corridor when developed to the standards.

Mr. Thornton identified another area of interest where the overlay standards try to emphasize a more urban streetscape and meet the goals and visions of the corridor plans to bring new development up closer to the street. Because some areas of the corridor were built in a suburban fashion, there is an opportunity to increase intensity and density of development by using the overlay standards. He clarified the opt-in standards were set up that if a developer wanted to opt-in, they would have to abide by the vision of the corridor plans – to bring the buildings closer to the street – the setbacks had been reduced and because the buildings would be closer to the street, there would not be a need for a landscaping strip required under the traditional suburban development of a commercial zone district. The design of the overlay was to reduce landscaping, screening and buffering so the overlay would allow for the ability to create a decorative wall instead of a landscaping strip similar to what was allowed in downtown Grand Junction for parking lots abutting North Avenue. This would allow a property owner to better utilize more of their property. He said that under the vision of the corridor, parking would be either to the rear or the side of the building.

Mr. Thornton next gave a comparison of setbacks and gave various examples in various zone districts. The corridor plan encourages a pedestrian emphasis especially areas around CMU and the high school that had a tremendous amount of pedestrian traffic. He identified other standards of the overlay which included the location of drive-thru lanes, no parking lots in front of buildings, awning/portico-type treatments on buildings and improving access.

Next, in order to emphasize the corridor streetscape feel that tries to create interest in the buildings, the overlay provides guidelines to try to create visual interest along the corridor that would enhance the building's architecture. If a building was built within the maximum setback up to the property line, a front door facing North Avenue would also be required creating a visual impression that welcomes passing traffic and identifies the business with the building. If one opted-in, setbacks and landscaping would be reduced. Mr. Thornton provided some examples of existing Grand Junction businesses on minor arterial streets (24 ½ Road and Horizon Drive) where the front door to the street was part of their image and which he felt was advantageous. He also discussed minimization of access points that were not needed, creating safer access. He reminded the Commission that landscaping would be reduced by approximately fifty percent under the opt-in standards.

Another area of emphasis was the sign upgrade point system and Mr. Thornton clarified that this was just setting the stage for a future funding source to create a hierarchy of priorities and a point system whereas if money was available, the point system would determine how much of a project would be funded.

It is hoped that people would take advantage of the opt-in standards where they could save money on landscaping and use more of their site. In cases where new development or redevelopment of a site was not proposed the North Avenue Advisory Committee wanted to try to encourage North Avenue property owners to take steps to bring their existing structures and sites more into the vision of the corridor. The committee wanted a system that would provide incentives and they talked about various ways of accomplishing that vision. The "Site Upgrade Point System" Improvement Table is a priority list that provides a point system based on today's costs and was established so that when the incentive program was funded, the amount of a grant would be based on the points accrued. The committee continues to work on how to administer the point system, but that is not a part of this proposed Overlay Zone.

Staff found that the proposed Zoning Code amendment to include the North Avenue Overlay Zone, met the vision, goals and policies of the Comprehensive Plan and is consistent with the applicable approval criteria.

Questions

Commissioner Williams raised a question regarding mandatory standards and whether fences and walls would be included if a building was in the right-of-way. Mr. Thornton said that it was strictly buildings, most of them existing along the west end of the corridor, where it would be necessary to adjust the improvements along the front of the building. Although any fence or wall as part of the reconstruction would be expected to be removed and brought into conformance.

Commissioner Eslami asked if there was any plan (design) in place for this project. Mr. Thornton said there wasn't a design for the corridor at this time, but one was anticipated this year as part of a federal grant the City was awarded by the Federal Highways Administration which would design three miles, on both sides, from 1st Street to 29 Road.

Commissioner Reece asked if the remaining areas outside of the three miles identified would have the same consistency in design. Mr. Thornton said that they hoped it would but it was too soon to determine whether they would have the funds to follow through to the I-70 Business Loop with this design project.

Chairman Wall asked as far as remodeling, in order to trigger the mandatory standards would that only be an external remodel. Mr. Thornton confirmed that it would be for new development or redevelopment that already requires such mandatory requirements of right-of-way dedication and landscaping the right-of-way areas which is required in all nonresidential zoning districts in the City as per city policies and would not include minor interior remodels.

Chairman Wall raised a question where businesses may share an entrance or an exit and the front door was on North Avenue. He gave the example if one had to walk from the back parking lot to the front so that one had to walk through the entrance or exit way. He asked if there was a standard for a sidewalk or something else so that people did not have to walk through the entrance and/or exit. Mr. Thornton said that the opt-in standards really deal with frontage and the parking lot standards would still have to be

adhered to. The standards are already in the Code to minimize conflicts which would force people to have to walk through a parking lot.

Commissioner Leonard raised a question regarding building design standards and saw the building orientation toward the street but didn't see any guide to what the façade should look like. Mr. Thornton said that the opt-in did not require a certain architectural standard.

Commissioner Leonard talked about a longer range view of development and believed that people remembered buildings, but not necessarily landscaping. He would like to see architectural standards implemented city-wide. Mr. Thornton said that they encouraged people to do the right thing by reducing some of the requirements such as landscaping and setbacks which would increase the amount of buildable area for more of an urban standard versus a suburban standard. He added that if it was found that this over time did not work, staff could go back to the community to see where the support was and modify the overlay to include some architectural features.

Commissioner Eslami asked if there was an architectural committee for the new buildings. Mr. Thornton said there was not a review committee. He confirmed that the 24 Road corridor guidelines provided some articulation and various requirements for development and, furthermore, Horizon Drive had a business district but no architectural requirements.

Commissioner Reece asked if the incentive program improvement table was amendable. Mr. Thornton said that it was amendable. If there was any additional emphasis that the Planning Commission wanted to add, that could be amended. Also, if the business community didn't respond in a way that made sense architecturally, staff and/or the Advisory Committee could go back to the community. The Planning Commission could request that be looked at for inclusion in the overlay.

Commissioner Eslami asked why not include some restrictions now. Chairman Wall interjected that he believed there was a lot more time for some of these items to be discussed more fully, but felt the overlay as it was now, with the understanding that there would be opportunity to amend or add to it at a later date, was adequate as presented.

Public Comment

Kevin Bray, appeared as a member of the North Avenue Advisory Committee, and gave a little history. He said that the committee was created almost a year ago with 15 to 20 members of primarily business owners, City staff have attended as needed, two commercial real estate experts and the committee had benefited from Councilman Susuras (City Council) and Commissioner Eslami (Planning Commission). He reiterated that the mandatory standards were a result of the marriage between the 2007 East Plan and the 2011 West Plan and the right-of-way requirement was a previous requirement and pointed out that there was really no new regulation being requested. He said that the opt-in standards provided flexibility in design and would provide property and business owners a reason to reinvest in their properties. The site upgrade point system really gave people a way to upgrade their site without going through a full redevelopment and gave them options to upgrade the corridor. As the

priorities were developed, the main focus of the steering committee was having a safe, cohesive look along the corridor and the public infrastructure. The focus was the ability to redevelop infrastructure and provide incentives for property and business owners to invest in the public side. Mr. Bray encouraged the Committee to approve this document. He also pointed out that this was a joint process between the business community, the property owners and the City.

Discussion

Commissioner Eslami thanked all of the people involved and the City staff for their time they had invested in this project. In order to successfully do this giant and complicated project, two things were needed – lots of money and a miracle. He felt that with the cooperation of the citizens who can envision the future, this miracle could be accomplished. He stressed that those people could not give up and hoped that the City would not give up on them.

Commissioner Williams agreed with the plan and believed it was a good plan for North Avenue's development and redevelopment. He liked the options it provided to the business owners and thought the direction that this plan took offered less traffic conflicts. He said that he was fully in favor of this plan.

Commissioner Leonard said that he thought it was a good start and reiterated that he would like to see more consideration by the City for the architectural design. He too thanked the people for their work and time put into this.

Chairman Wall stated that he liked the plan and thought a lot of work had been put into it. He added that the overlay projects were more challenging as there were a lot more things to consider. He went back to a statement made previously by Mr. Bray that this was a public infrastructure project that dealt with safety.

MOTION:(Commissioner Williams) “Mr. Chairman, on File ZCA-2012-572, an amendment to the Zoning and Development Code (Title 21 of the Grand Junction Municipal Code), to add an overlay zone district for North Avenue, I move that the Planning Commission forward a recommendation of approval of the proposed amendment with findings, facts and conclusions listed in the staff report.”

Commissioner Reece seconded the motion. A vote was called and the motion passed unanimously by a vote of 7 - 0.

General Discussion/Other Business

None.

Nonscheduled Citizens and/or Visitors

None.

Adjournment

With no objection and no further business, the Planning Commission meeting was adjourned at 7:18 p.m.

Attach 2
Premier Tire Warehouse

CITY OF GRAND JUNCTION
PLANNING COMMISSION

MEETING DATE: March 12, 2013
PRESENTER: Senta Costello

AGENDA TOPIC: Premier Tire – Special Permit – SPT-2012-66

ACTION REQUESTED: Forward a recommendation to City Council for a Special Permit for Premier Tire.

BACKGROUND INFORMATION				
Location:		461 Glenwood Avenue		
Applicants:		ATD Investments, LLC dba Premier Tire – John Perschbacher		
Existing Land Use:		Vacant warehouse		
Proposed Land Use:		Warehouse		
Surrounding Land Use:	North	Mesa County School Dist 51 vocational training		
	South	Beauty School		
	East	Martin Mortuary		
	West	Office		
Existing Zoning:		C-2 (General Commercial)		
Proposed Zoning:		C-2 (General Commercial)		
Surrounding Zoning:	North	B-1 (Neighborhood Business)		
	South	C-2 (General Commercial)		
	East	C-1 (Light Commercial)		
	West	C-2 (General Commercial)		
Future Land Use Designation:		Neighborhood Center		
Zoning implements the Comprehensive Plan?		<input type="checkbox"/>	Yes	<input checked="" type="checkbox"/> No

PROJECT DESCRIPTION: Application for a special permit to allow interim use of the property for an indoor storage and operations warehouse in a C-2 (General Commercial) zone district with a contradicting Comprehensive Plan Future Land Use designation of Neighborhood Center, in accordance with Section 21.02.120 of the Grand Junction Municipal Code.

RECOMMENDATION: Recommend approval of the special permit

ANALYSIS:**1. Background**

The subject property consists of one parcel, known as 461 Glenwood Avenue. Historically, the property has been used as a warehouse.

The applicant, Premier Tire, is proposing to use the warehouse as a storage facility for its tire distribution business, creating a distribution hub for its western slope and eastern Utah customers.

The business hours are 7:30 am – 5:30 pm and use three delivery trucks consisting of two pickup trucks and one 18' box truck, which leave the site between 8:00 am and 8:30 am Monday – Friday and return at approximately 5:00 pm the same day. The trucks will be loaded from the alley and parked in the parking spaces south of the building after returning in the evening. The business will use the alley access for ingress/egress from the property. The delivery vehicles will be traveling south from the site to access the main thoroughfare – North Avenue. The warehouse will also receive inventory deliveries Monday – Thursday at approximately 7:30 am that arrive on a 16'-24' box truck. The use of the alley for ingress/egress and the vehicles traveling south rather than toward the high school minimize any conflicts and/or interaction with the pedestrian traffic, which primarily consists of students crossing North 5th Street from the main high school campus to the classroom building located on the northwest corner of Glenwood Avenue and North 5th Street (see map below).



The site is currently zoned C-2 (General Commercial) with the Comprehensive Plan Future Land Use Map identifying this area as Neighborhood Center, which are in discrepancy with each other.

The Zoning and Development Code allows a special permit for interim uses. Staff determined that an indoor operations/storage with outside loading from the south door warehouse, as described on the site plan attached to this staff report is an appropriate interim use for the property. A special permit would be appropriate for this project under the conditions described in this report, allowing the building to be used for the time being, while still preserving the future vision for the area as a neighborhood center according to the Comprehensive Plan.

Special Permit:

The special permit (GJMC Section 21.02.120) is a City Council discretionary review process that was added to the 2010 Zoning and Development Code to add flexibility when considering a land use that may be less than permanent or temporary in nature. A special permit may be permitted under circumstances particular to the proposed location and subject to conditions that provide protection to adjacent land uses. A special permit is a possibility when more flexibility is required beyond that afforded to the Director of Public Works and Planning through the administrative adjustment process. A special permit is allowed in all zone districts for a development that is proposed as an interim use established with a minimal investment and with a development design that can be easily redeveloped as envisioned by the Comprehensive Plan. (Section 21.02.120(b)(2)(ii).)

Staff considers the proposed use of the property as an appropriate interim use for the following reasons:

- The site has been vacant for some time, and to encourage use of the building and the site, the use of the property for an indoor operations warehouse is an appropriate interim use so long as the off-site impacts of a warehouse use can be controlled. The proposed warehouse use includes minimal daily delivery trips (7 round trips per day) and utilizes pickup trucks and small commercial trucks (no semitrailers) for its business. The minimal daily trips, use of the alley for ingress/egress, truck routes to and from the site and small commercial type vehicles being used will minimize potential negative impacts to the neighborhood north of the site.
- At some point we anticipate that the market will increase the value of the property such that it will be “ripe” for redevelopment into a lighter commercial use that will be in conformance with the Comprehensive Plan and vision of the North Avenue Corridor Plan. In this case, because the building is constructed as a warehouse, with no windows and large overhead doors, such “ripeness” would most likely include demolition of the building and/or consolidation of parcels. Because either of these would require a significant investment, the building could remain vacant for some time unless an interim use is authorized.

- If the type of items stored is changed from tires to another type of inventory, the permittee must submit the proposed change to the Director, who shall determine if the permit terms are still met by the proposal. If the Director determines there is no substantial change to the intensity of the warehouse use, the permit shall remain valid (until otherwise extinguished). Intensity of use shall be determined with reference to the following, without limitation: number of trips per day, size and number of trucks, change to traffic circulation pattern, scope of services offered. In the event of a non-substantial change, the Director may impose additional permit conditions to ensure compliance with applicable fire, building code, Persigo/waste treatment, health department and/or environmental regulations. If the Director determines that the proposed change is substantial due to the increase in intensity of use, a new appropriate land use approval will be required. The special permit shall terminate upon approval of the new appropriate land use.

The proposed special permit is valid only for a warehouse with indoor operations/storage; outside loading from the southern door is allowed as long as the alley is not blocked. No outdoor storage is authorized. Onsite parking is located on the south side of the building. The special permit will terminate if the warehouse use is abandoned (by non-use) for six months or longer or if the property is redeveloped into any other use.

2. Consistency with the Comprehensive Plan:

The site is currently zoned C-2 (General Commercial) with the Comprehensive Plan Future Land Use Map identifying this area as Neighborhood Center, which are in discrepancy with each other. The special permit review provides an opportunity for additional flexibility when considering a land use that may be less than permanent, yet still furthers the following goals of the Comprehensive Plan. With approval of a special permit, the proposed use meets the following goals of the Comprehensive Plan.

Goal 6: Land use decisions will encourage preservation and appropriate reuse.

The existing building was originally constructed as a warehouse, making the use of the structure for any other purpose infeasible. The building has been vacant for several years. City of Grand Junction Police Department and neighboring property owners report it has become a “hang-out” location for the students in the area, creating issues for the neighboring properties ranging from trash to vandalism. A special permit allowing the building to continue being used as a warehouse until the market supports redevelopment allows the building to become a more viable contribution to the community and a more positive part of the neighborhood.

Goal 12: Being a regional provider of goods and services the City and County will sustain, develop and enhance a healthy, diverse economy.

The site is centrally located in the community and located near a major circulation route (North Avenue and the Business Loop) for the urban area of the valley as well as roads connecting to the larger region of the western slope and eastern Utah. An indoor operations/storage with outside loading from the south door warehouse in this central location would allow for city wide and regional distribution with minimal impacts to the neighborhood.

3. Section 21.02.120 of the Grand Junction Zoning and Development Code – Special Permit:

To obtain a special permit, the Applicant must demonstrate compliance with the following criteria:

(1) Comprehensive Plan. The Special Permit shall further the goals and policies of the Comprehensive Plan. The Special Permit shall serve to determine the location and character of site(s) in a Neighborhood Center, Village Center, City Center or Mixed Use Opportunity Corridors on the Future Land Use Map of the Comprehensive Plan;

The proposed special permit furthers Goals 6 and 12 of the Comprehensive Plan by allowing the interim use of the property for an indoor operations/storage with outside loading from the south door warehouse without substantial site improvements which leaves the land available to be redeveloped with full site upgrades when the market is ripe.

The Neighborhood Center in which the site is located extends from North 1st Street to North 12th Street]. The site is located 5 blocks east of the western end of the Neighborhood Center. The special permit authorizes indoor operations and indoor storage only, so the only effects of the use that are not consistent with the Neighborhood Center have to do with the pick up and delivery activities. The impacts from truck deliveries and pick up activities are mitigated by terms of the special permit that limit business hours, how and where the trucks move to and from the site, the types of trucks that can be used, and where they can park. No permanent changes to the building or site are proposed or authorized by the special permit, in order to preserve the long-term potential of the property to be used in a manner more consistent with a Neighborhood Center.

(2) Site Plan Review Standards. All applicable site plan review criteria in GJMC 21.02.070 (g) and Submittal Standards for Improvements and Development (GJMC Title 22), Transportation Engineering Design Standards (GJMC Title 24), and Stormwater Management Manuals(s) (GJMC Title 26);

If a conflict between the C-2 (General Commercial) zoning and the Neighborhood Center Future Land Use designation did not exist the proposed

use would have been processed administratively with no required site improvements (Section 21.08.040(b)).

(3) District Standards. The underlying zoning district standards established in Chapter 21.03 GJMC, except as expressly modified by the proposed Special Permit;

The proposed use as a warehouse is an allowed land use in the C-2 (General Commercial) Zoning District with a site plan approval.

(4) Specific Standards. The use-specific standards established in Chapter 21.04 GJMC.

There are no use-specific standards established for a warehouse with indoor operations and storage.

FINDINGS OF FACT/CONCLUSIONS AND CONDITIONS:

After reviewing the Premier Tire Special Permit application, SPT-2013-66 for a special permit, I, as Project Manager make the following findings of fact and conclusions:

1. The requested indoor operations/storage with outside loading from the south door warehouse use as proposed on the attached site plan is consistent with the Comprehensive Plan as an interim use with the approval of the attached special permit with the conditions stated therein.
2. The review criteria in Section 21.02.120 of the Zoning and Development Code for a special permit have all been met.

STAFF RECOMMENDATION:

I recommend that the Planning Commission forward a recommendation of approval to City Council of the requested special permit, SPT-2013-66 with the findings, conclusions and conditions of approval as defined in the Staff Report and in the Special Permit.

RECOMMENDED PLANNING COMMISSION MOTION:

Mr. Chairman, on the request for a Special Permit for Premier Tire, application number SPT-2013-66, located at 461 Glenwood Avenue, I move that the Planning Commission approve the special permit with the facts and conclusions stated in the Staff Report and Proposed special permit.

Attachments:

Site Location Map / Aerial Photo Map

Comprehensive Plan Map / Existing Zoning Map
General Project Report
Site Plan
Pictometry Pictures
Proposed Special Permit

Site Location Map



Aerial Photo Map



Comprehensive Plan Future Land Use Map



Existing Zoning Map



**GENERAL PROJECT REPORT
641 GLENWOOD AVENUE
Special Use Permit Application**

A. Project Description.

1. Location. The project is located at the southwest corner of Glenwood Avenue and North 5th Street. The property lies immediately north of the former Valley Lumber property, for which the subject parcel was formerly used as warehouse storage. The easterly boundary of the property adjoins the west side of North 5th Street. The northerly boundary of the property adjoins the south side of Glenwood Avenue. The street address of the property is 641 Glenwood Avenue. The property is legally described as Lot 5, Block 11, Sherwood Addition and is identified as Mesa County Tax Parcel No. 2945-113-15-013.

2. Acreage. The property consists of approximately .655 acres according to the records of the Mesa County Assessor.

3. Proposed Use. The site is zoned C-2. This special use application seeks to authorize the use of the property as a warehouse and distribution facility, to be utilized by the applicant for a wholesale tire distribution center, with the possibility of some retail sales in the future, but with the possibility for use in the future for other warehouse and limited retail uses, and with no expiration date applicable to the special use permit.

The proposed improvements to the existing building in order to utilize it for the proposed purposes are very limited. Existing parking spaces, which are sufficient for the proposed use and are located on the property immediately south of the building, will be restriped. The existing office space in the building will be refreshed. Racks not exceeding ten 10 feet in height will be installed with aisles of not less than 8 feet in width separating them as shown by the floor plan accompanying this application. A new unisex bathroom will be constructed as shown on the floor plan with new domestic water and sewer connections. A sprinkler system approved by the Fire Department will be installed. A split glycol system is anticipated at this time.

B. Public Benefit.

The public will benefit from will be to repurpose and reuse a long vacant warehouse facility with minimal changes, providing jobs and sales tax revenue for the City and enhanced availability of tires for area retailers.

C. Neighborhood Meeting.

No neighborhood meeting is required for this application.

D. Project Compliance, Compatibility and Impact.

1. Adopted Plans and Policies. The future land use of the subject parcel is designated C-1, so a special use permit is required for the proposed use of the subject property.

2. Land Use and Surrounding Area. The vicinity of the subject property contains a variety of uses. The property to the north across Glenwood Avenue is zoned B-1. The property immediately to the east across 5th Street is zoned B-1 and is occupied by Martin's Mortuary. The property to the south and west is zoned C-2. The former Valley Lumber building, presently vacant, is located to the south.

3. Site Access and Traffic Patterns. The subject property has direct access to Glenwood Avenue to the north and to 5th Street by an east-west alley adjacent to and immediately south of the property. Existing doors in the existing building open on to both accesses.

4. Availability of Utilities. All utilities are available to the property. A water line is located in Glenwood Avenue and a fire hydrant is located at the northeast corner of the property. Domestic water and sewer taps do not currently

serve the property, both of which will be required for the bathroom to be added as part of the proposed improvement of the property.

5.Special or Unusual Demands on Utilities. A sprinkler system will be required for the proposed use of the property. Applicant is working with the Fire Department to confirm availability of sufficient pressure and an acceptable system. A split glycol system is anticipated at this time.

6.Effects on Public Facilities. No impacts on public facilities are anticipated.

7.Site Soils and Geology. No unusual conditions are known to or anticipated by Applicant.

8.Impact of Project on Site Geology and Geologic Hazards. None are known to or anticipated by Applicant.

9.Review Criteria.

(a)The special use permit is required because the proposed use does not comply with the Comprehensive Plan Future Land Use Designation of Neighborhood Center, although it does comply with the current C-2 zoning of the property.

(b)The project complies with the applicable provisions of the Building and Development Code.

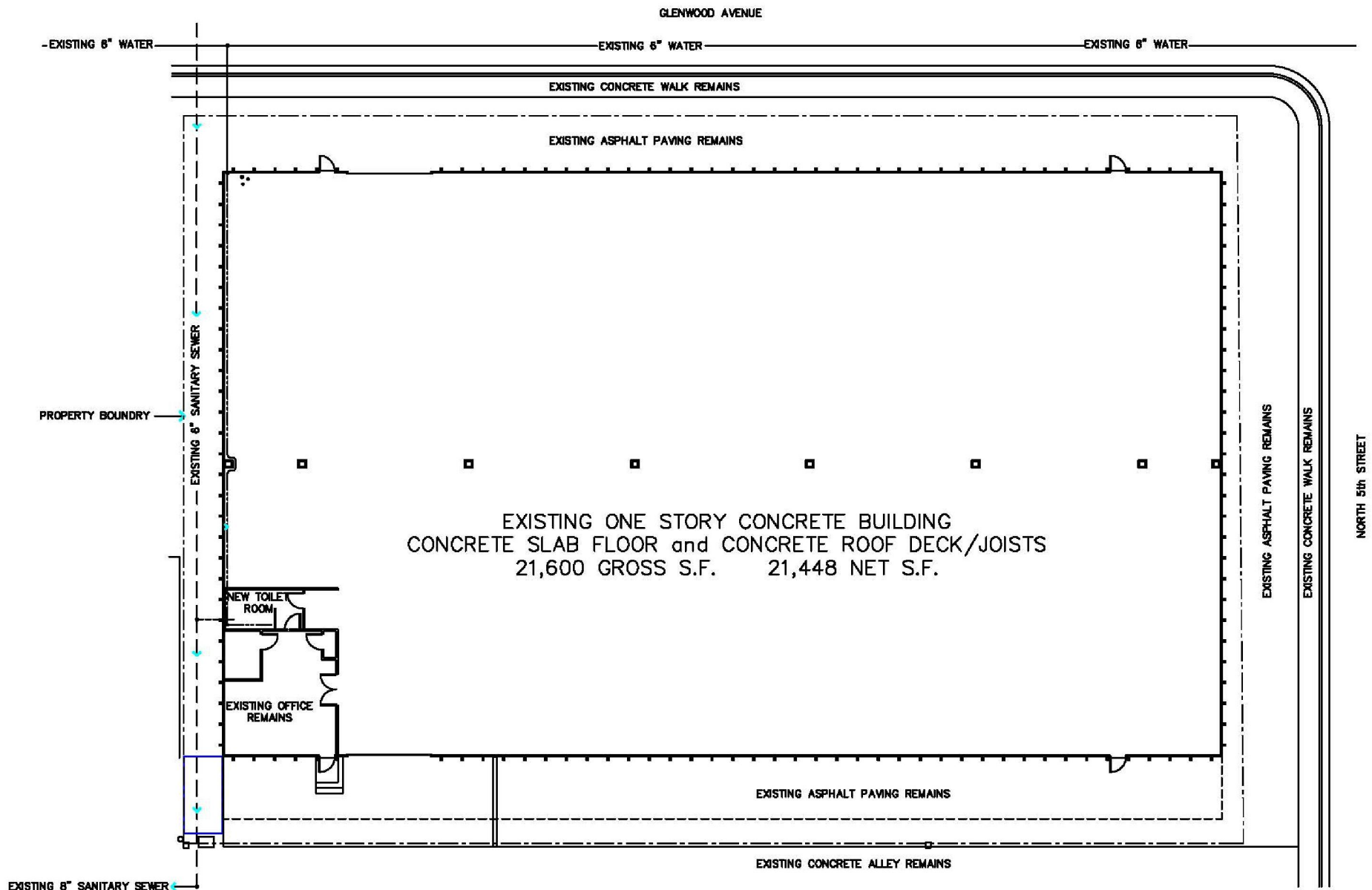
(c)The project complies with the conditions of any prior approvals because not prior approvals are applicable.

(d)Public facilities and utilities are available presently for the development of the property.

(e)Applicant has received or will obtain as part of the development of the property all applicable local, state and federal permits related to the proposed development.

E.Development Schedule and Phasing.

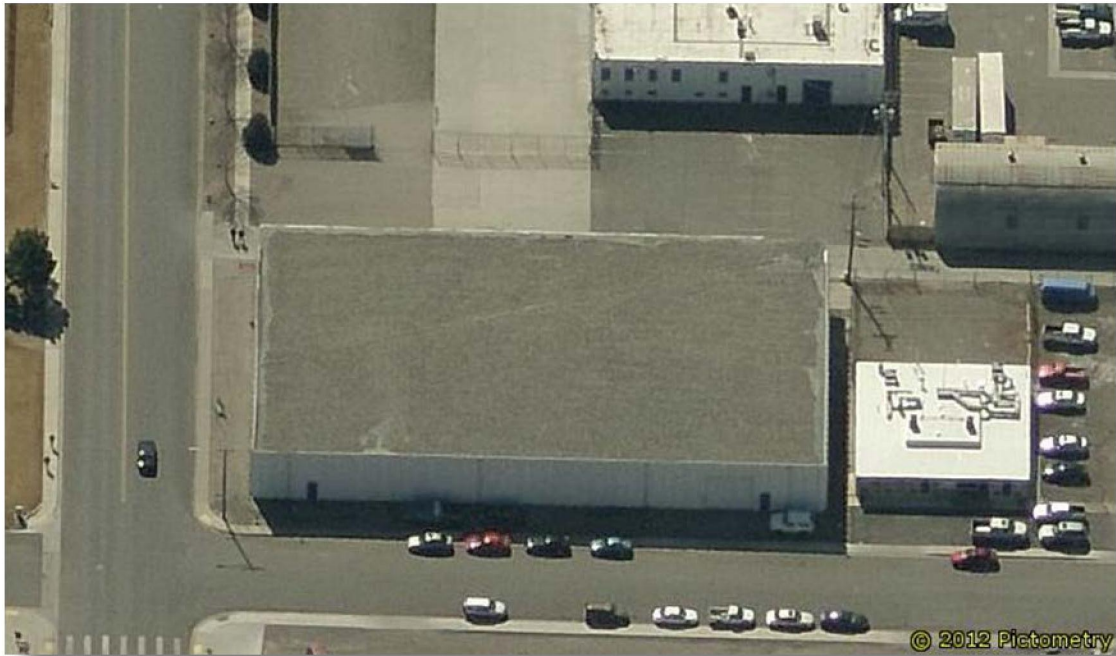
Construction is anticipated to commence as soon as closing of the purchase of the property by applicant is completed. That closing is presently scheduled for March 4, 2013.



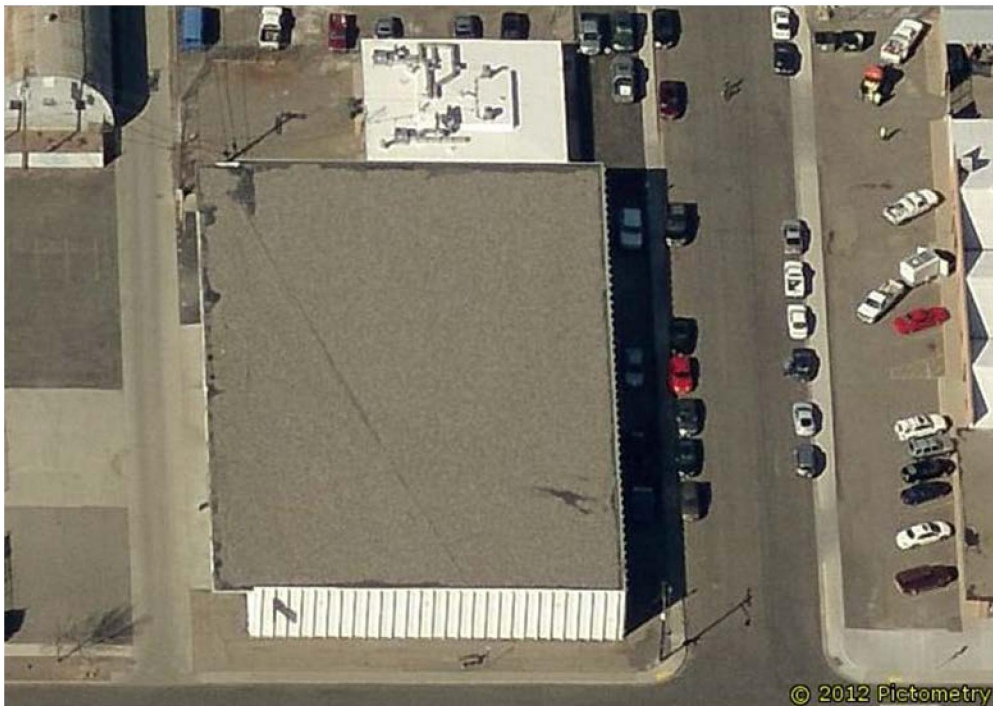
SITE PLAN
1/8" = 1'-0"



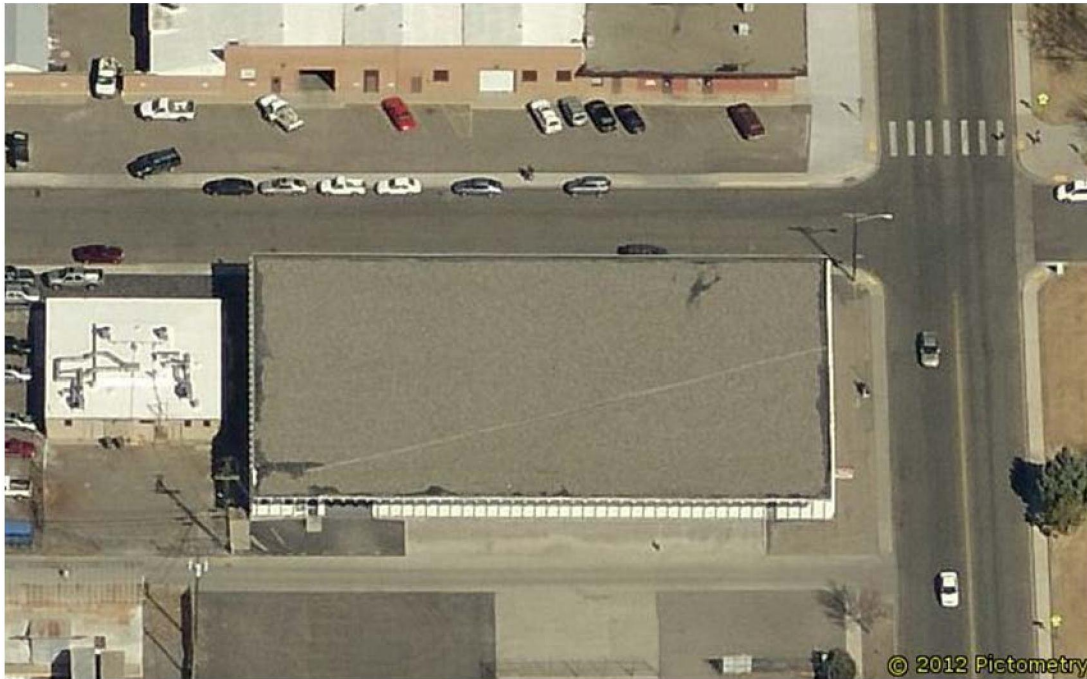
North View



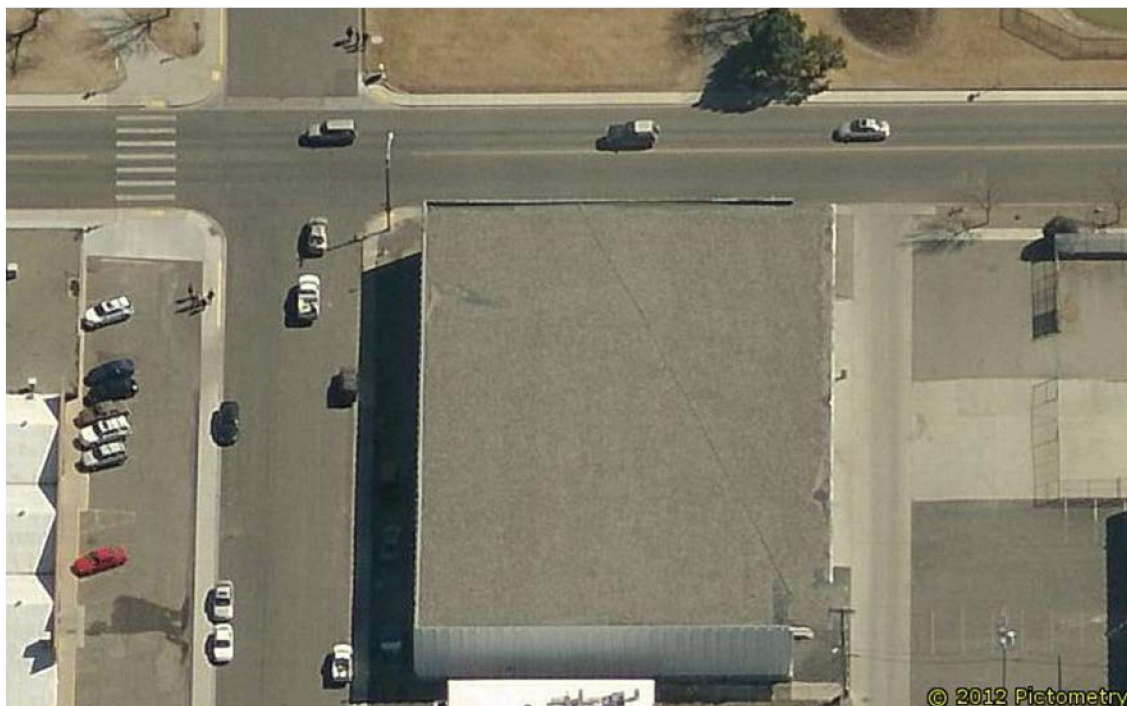
East View



South View



West View



CITY OF GRAND JUNCTION, COLORADO**SPECIAL PERMIT PURSUANT TO SECTION 21.02.120 OF THE GRAND JUNCTION MUNICIPAL CODE (ZONING AND DEVELOPMENT CODE) FOR AN INTERIM USE OF WAREHOUSE WITH INDOOR STORAGE AND INDOOR OPERATION ON PROPERTY LOCATED AT 461 GLENWOOD AVENUE IN GRAND JUNCTION, COLORADO**Findings:

An application for a special permit has been reviewed by staff in accordance with the Zoning and Development Code (Code). K & N Investors LLC, is the owner of the property located at 461 Glenwood Avenue in Grand Junction Colorado, consisting of one lot and is under contract to sell the property to ATD Investments LLC, dba Premier Tire.

The applicant is requesting approval to use the property for indoor operations/storage with outside loading from the south door warehouse.

The property is zoned C-2, while the Comprehensive Plan's designation for the property is Neighborhood Center. To resolve the tension between the zoning and the community's vision for future uses that conflict with current zoning, the City Council provided for a Special Permit in the Code (Section 21.02.120).

The business hours are 7:30 am – 5:30 pm and use three delivery trucks consisting of two pickup trucks and one 18' box truck, which leave the site between 8:00 am and 8:30 am Monday – Friday and return at approximately 5:00 pm the same day. The trucks will be loaded from the alley and parked in the parking spaces south of the building after returning in the evening. The business will use the alley access for ingress/egress from the property. The delivery vehicles will be traveling south from the site to access the main thoroughfare – North Avenue. The warehouse will also receive inventory deliveries Monday – Thursday at approximately 7:30 am that arrive on a 16'-24' box truck. The use of the alley for ingress/egress and the vehicles traveling south rather than toward the high school minimize any conflicts and/or interaction with the pedestrian traffic, which primarily consists of students crossing North 5th Street from the main high school campus to the classroom building located on the northwest corner of Glenwood Avenue and North 5th Street (see map below).

The C-2 zone district permits the proposed use of an indoor operations/storage with outside loading from the south door warehouse. The landowner has submitted a site plan. A special permit provides flexibility when considering a land use that may be less than permanent or temporary in nature, and may be permitted under circumstances particular to the proposed location and subject to conditions that provide protection to adjacent land uses. A special permit is a possibility when more flexibility is required beyond that afforded to the Director of Public Works and Planning through the administrative adjustment process.

The Special Permit allows applicant's use as particularly described herein, subject to the stated conditions, while adequately providing for future redevelopment of the property in accordance with the applicable zoning and the Comprehensive Plan. In approving the Special Permit, the City Council has considered the approval criteria for a Special Permit as set forth in the Staff Report. The findings and conclusions in the Staff Report support the issuance of this Special Permit.

Approval of the Special Permit promotes the following goals of the Comprehensive Plan:

Goal 6: Land use decisions will encourage preservation and appropriate reuse.

Goal 12: Being a regional provider of goods and services the City and County will sustain, develop and enhance a healthy, diverse economy.

The proposed Special Permit furthers Goals 6 and 12 of the Comprehensive Plan by allowing the interim use of the property for indoor operations/storage with outside loading from the south door warehouse, a necessary service, without substantial site improvements while keeping the potential for the land to be redeveloped for future commercial businesses with more permanent site features such as landscaping, irrigation, structures and screening, where required, when the market is ripe.

The Special Permit furthers the goals and policies of the Comprehensive Plan.

The Permit complies with the underlying zoning district standards for C-2 established in Chapter 21.03 of the Code. It satisfies the review criteria found in Section 21.02.120(c).

NOW THEREFORE, BE IT MOVED BY THE CITY COUNCIL OF THE CITY OF GRAND JUNCTION THAT A SPECIAL PERMIT IS APPROVED, PURSUANT TO SECTION 21.02.120 OF THE GRAND JUNCTION MUNICIPAL CODE (ZONING AND DEVELOPMENT CODE), ALLOWING THE FOLLOWING USES ON THE PROPERTY DESCRIBED BELOW WITH THE FOLLOWING CONDITIONS, WITH THE ABOVE FINDINGS BEING AN INTEGRAL PART HEREOF:

1) The site is described as follows:

LOT 5 BLK 11 SHERWOOD ADDITION SEC 11 1S 1W EXC S 10FT

Also known as 461 Glenwood Avenue. The area governed by this Special Permit includes the entire area of the one lot and shall be referred to herein as the Site.

2) Use of the Site is limited to indoor operations and indoor storage. Outdoor loading from the southern door of the warehouse is allowed; no other outdoor operations are allowed. No outdoor storage is allowed.

- 3) Business hours shall be 7:30 am to 5:30 pm.
- 4) Operations include use of two pickup trucks and an 18' box truck and inventory deliveries that arrive on a 16'-24' box truck that is not housed at the site. All such trucks shall use only the alley to access the site. When leaving the site, trucks shall use the alley and turn south on 5th Street. Trucks are not authorized to use Glenwood Avenue or to turn North on 5th Street, for the safety of high school students who must cross 5th Street at Glenwood in order to access classrooms.
- 5) Trucks shall be parked on the site along south side of building when not in use. Parking of trucks on the streets around the site is not authorized. This condition is also imposed for the safety of high school students crossing 5th Street at Glenwood to access classrooms.
- 6) If the type of items stored is changed from tires to another type of inventory, the permittee must submit the proposed change to the Director, who shall determine if the permit terms are still met by the proposal. If the Director determines there is no substantial change to the intensity of the warehouse use, the permit shall remain valid (until otherwise extinguished). Intensity of use shall be determined with reference to the following, without limitation: number of trips per day, size and number of trucks, change to traffic circulation pattern, scope of services offered. In the event of a non-substantial change, the Director may impose additional permit conditions to ensure compliance with applicable fire, building code, Persigo/waste treatment, health department and/or environmental regulations. If the Director determines that the proposed change is substantial due to the increase in intensity of use, a new appropriate land use approval will be required. The Special Permit shall terminate upon approval of the new appropriate land use.
- 7) Uses not specifically described herein, regardless of type or classification and regardless of whether such uses appear as "allowed" uses in the zone/use table of the City's Zoning and Development Code, are prohibited on this site during the term of this Special Permit, unless the Director determines that such a use is accessory to and reasonably incidental and necessary for the specified uses, in which case the Director shall so specify in writing.
- 8) Historical drainage patterns shall be maintained on the Site.
- 9) The Site Plan is fully incorporated herein. No changes to the site or structure(s) thereon shall be made without prior approval by the Director, who shall determine whether such changes substantially comply with the terms of this permit.
- 10) No additional permanent or temporary, principle or accessory, buildings shall be constructed or installed on the Site.

11) Any proposed signage shall meet with the standards as set forth in the Zoning and Development Code Section 21.06.070.

12) This Special Permit runs with the land but is valid only for the specific use as described herein. The Special Permit shall terminate if indoor operations/storage with outside loading from the south door warehouse (by non-use) for six months or longer or if the property is redeveloped into any other use.

13) The failure of this permit to specify other applicable local, state or federal laws or regulations shall not be construed to affect the enforcement thereof. A violation of such applicable laws or regulations may constitute a basis for revocation of the Special Permit, in addition to and not in lieu of any other appropriate remedies or penalties.

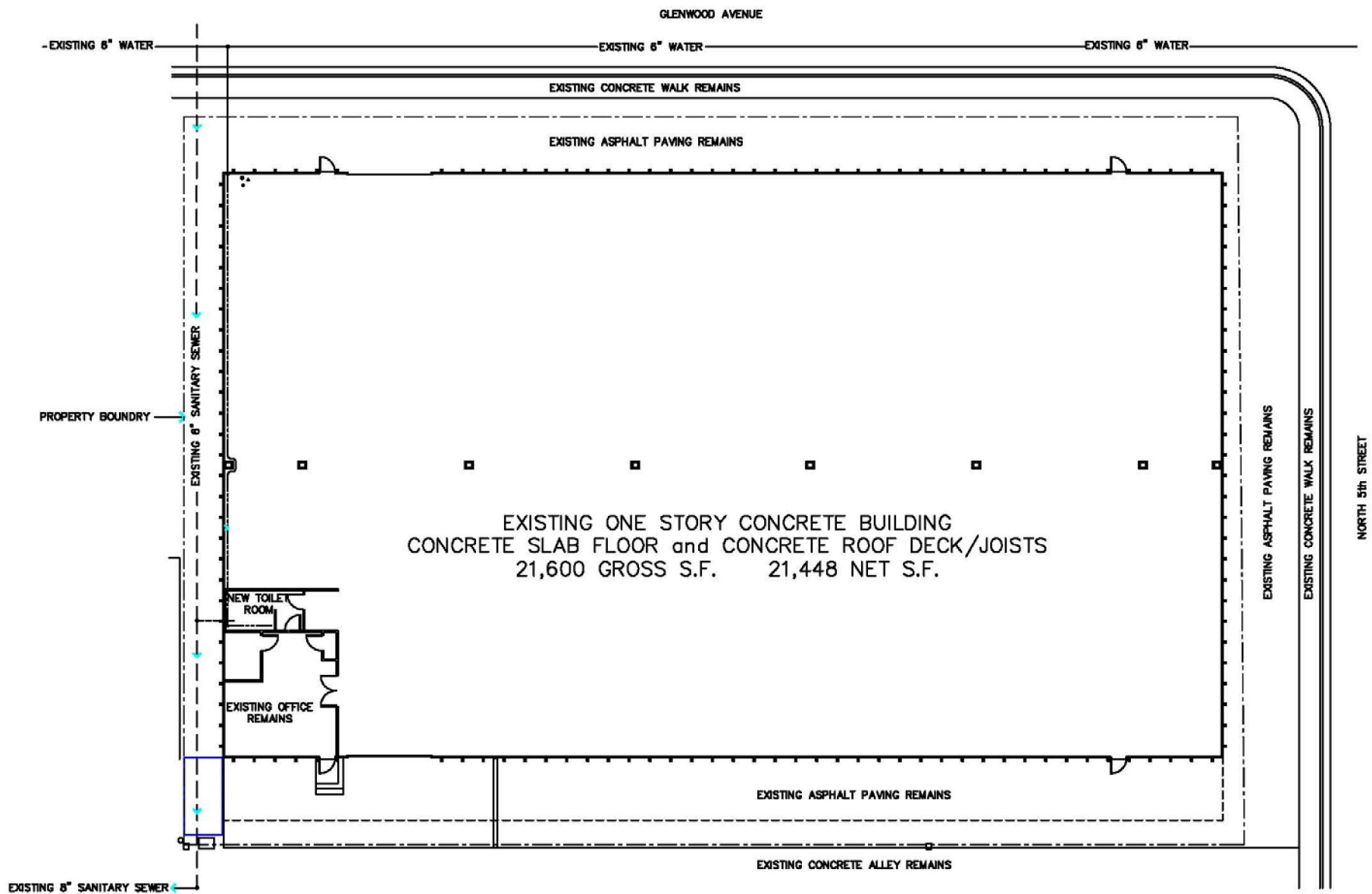
14) The Director may administratively approve minor changes to the Site Plan and this Permit, if he determines that the intent of this Special Permit is maintained, the operational needs of the applicant will be benefitted, and no injury to the public will ensue.

Passed and adopted this _____ day of _____, 2013.

ATTEST:

President of City Council

City Clerk



SITE PLAN

1/8" = 1'-0"



Attach 3
Greater Downtown Plan

Report begins on next page

AGENDA TOPIC:

CPA-2011-1067 – Greater Downtown Plan and Future Land Use Map Amendments
CPA-2012-216 – Comprehensive Plan Text Amendment and Zoning and Development Code Text Amendment
RZN-2012-217 – Rezoning properties within the Greater Downtown Plan Area
ZCA-2012-363 – Zoning Overlays for Corridors and Downtown District

ACTION REQUESTED:

Recommend to City Council the following:

- 1) Comprehensive Plan amendments by adopting the Greater Downtown Plan and the amended Future Land Use Map for the area within the Greater Downtown Plan
- 2) Comprehensive Plan and Zoning and Development Code (“Code”) text amendments to add RO (Residential Office) as a zone district that can implement the Downtown Mixed Use Land Use Designation
- 3) Rezone identified properties within the Greater Downtown Plan area
- 4) Adoption of the Greater Downtown Overlay District

PROJECT DESCRIPTION:

The Greater Downtown area generally encompasses the original square mile of the city and the area between the Riverside Neighborhood to 28 Road and South Avenue to the Colorado River (see map on a following page).

RECOMMENDATION:

Recommend approval to City Council of each of the actions requested.

1. ANALYSIS

Background

A Strategic Downtown Master Plan (SDMP) that encompassed the original square mile was developed through the Downtown Development Authority (DDA) and the guidance of a steering committee of interested downtown merchants, property owners and policymakers during 2007-2008. The SDMP defined an overall vision and goals for downtown and included implementation strategies such as a zoning overlay. The SDMP was considered by City Council on September 14, 2009, but, due to pending adoption of the Comprehensive Plan, Council voted to continue the SDMP to an unspecified future date.

A South Downtown Neighborhood Plan (South Downtown Plan) encompassed the area between the railroad tracks and the Colorado River and the Riverside neighborhood on the west to 28 Road on the east. A plan for the area was developed from 2006-2008 with 15 community focus group meetings, 3 public open houses with 80-100 people in

attendance at each open house. The South Downtown Plan included an existing conditions analysis, goals and implementation including a circulation and trails plan, economic development strategies, rezoning some properties and zoning overlay. The South Downtown Neighborhood Plan was considered by City Council on June 16, 2008, but was not adopted.

A Westside Downtown Redevelopment Plan was adopted by the City in January 2004. This plan included the area bounded on the north by Main Street, on the east by 5th Street, on the south by South Avenue, and on the west by the Railroad. A preferred plan for redevelopment defined redevelopment of various land use and presented concepts for a circulation plan. Design guidelines and standards for the area were considered and incorporated as appropriate within the Greater Downtown Plan. Adoption of the Greater Downtown Plan will repeal and replace the Westside Downtown Redevelopment Plan.

The planning process for the Greater Downtown Plan (GDP) reanalyzed the three previous planning efforts and made revisions as conditions have changed, included areas that had not been covered by either of those plans, and integrated them into a single plan for the downtown area. In addition, the Greater Downtown Plan incorporates elements of the Downtown Development Authority's potential projects in order to support the DDA's Downtown Plan of Development.

For planning purposes, the Greater Downtown area has been divided into three sub districts as shown on the map on the following page: the Downtown, Rail and River Districts.

2. Planning/Public Process

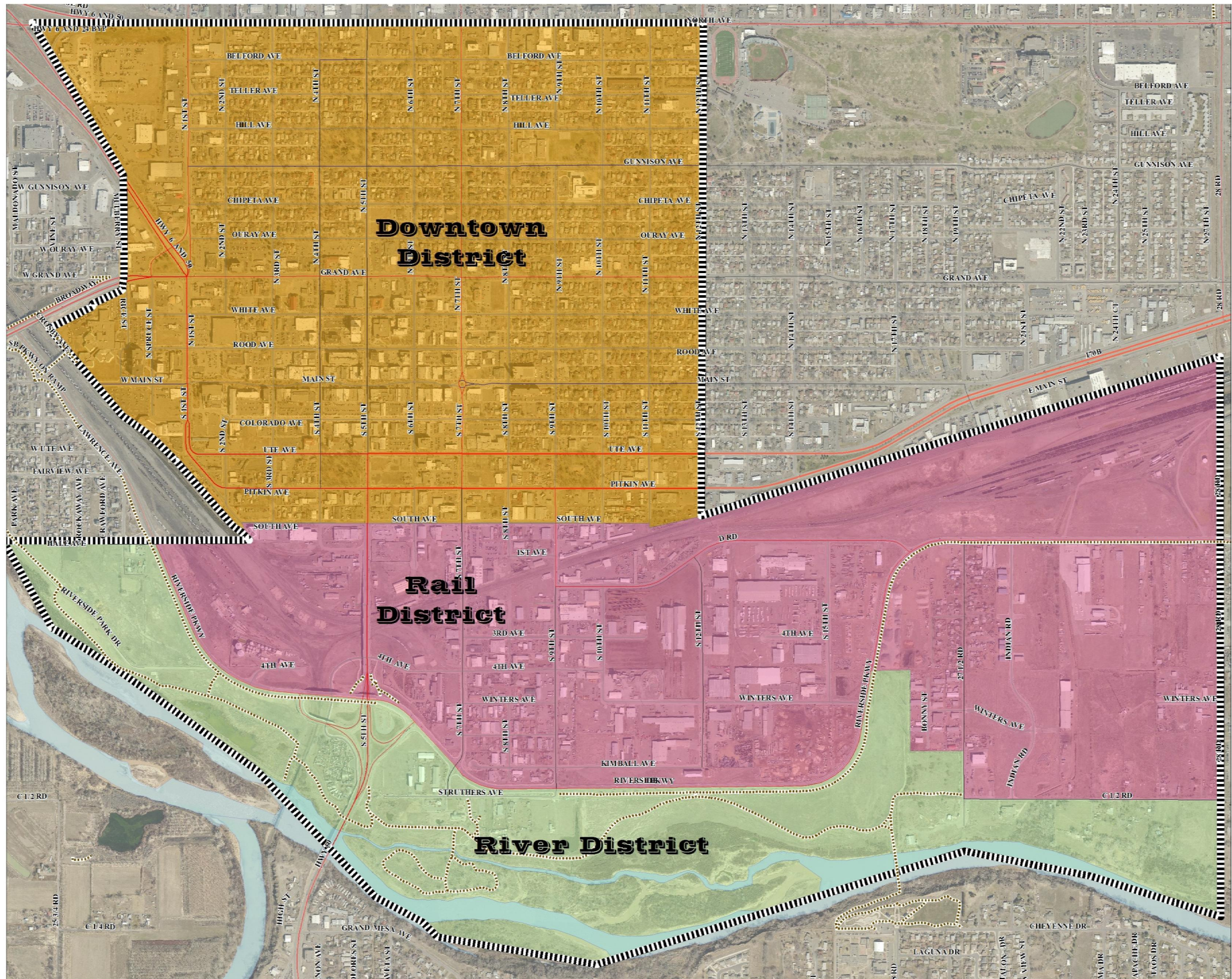
Technical Committee

The Greater Downtown Plan technical committee was comprised of staff members from various public agencies including City Public Works and Planning, City Parks and Recreation, City Geographic Information Systems, Mesa County Planning, the Regional Transportation Planning Office, Mesa County Facilities and Parks, the Downtown Development Authority and the Mesa County Public Library District. The Committee met three times during the course of developing the Greater Downtown Plan and members attended public open houses to discuss concerns and proposals with participants.

Public Open Houses

Two public open houses were held in December 2011 and February 2012 to present concepts and solicit input from property owners and interested citizens.

Notifications/invitations to both public open houses were mailed to all property owners within the Greater Downtown Plan area. Approximately 60 people attended the first open house and 40 attend the second open house. Another public forum to provide information to the public on the proposed zoning overlay for the Central Business District was held on January 31, 2013 which was attended by 30 downtown property owners.



Questionnaires and Comments

A series of questionnaires was available at the December 2011 open house and on the City's web site that were used to solicit public comment and weigh community opinions on design concepts that might be proposed with the Plan. A total of 130 questionnaires were returned. In addition, citizens could provide other written comments at both open houses. The results of the questionnaires and the written comments are included on following pages.

Letters/Meetings with Individual Property Owners

City Public Works and Planning staff coordinated meetings with key individual property owners, businesses or others that contacted the City regarding the Greater Downtown Plan. In addition, individual letters were mailed to property owners along the corridors that may be impacted by the land use and zoning proposals of the Greater Downtown Plan. Follow up meetings or conversations were held with property owners as requested.

Community Presentations/Discussions

Public Works and Planning staff conducted several presentations and discussions regarding the Greater Downtown Plan with community groups and businesses including the Chamber of Commerce, Bray and Company Realty, the Downtown Development Authority and Rail and River District corridor property owners.

City Council, City Planning Commission and Mesa County Planning Commission Workshops

City Public Works and Planning staff attended several workshops with elected and appointed City and County officials to inform and solicit input on the Greater Downtown Plan during its development.

3. Greater Downtown Plan and Future Land Use Map Amendments (CPA-2011-1067 and CPA-2012-216)

Greater Downtown Plan

The public participation process involved community evaluation of various design and planning concepts to determine which of these are most important to the community and should be addressed in greater detail in the Greater Downtown Plan. The concepts addressed four major topics relative to an area plan: land use, circulation, economic (re)development and visual character. The results, along with previous information for the CBD, show strong community support for ideas that were translated to the goals listed below for the Greater Downtown Plan.

A. Area-Wide Goals and Policies

Goal 1: Enhance the transportation system to accommodate automobiles, bikes and pedestrians, and provide adequate, convenient parking.

Goal 2: Establish and improve entry points into the Greater Downtown area.

Goal 3: Promote downtown living by providing a wide range of housing opportunities, primarily in the Downtown District

Goal 4: Redefine the land use along key corridors to provide a mix that will offer the most opportunities for redevelopment and revitalization.

B. Downtown District Goals and Policies

Goal 1: Maintain and enhance the economic, cultural and social vitality of the Downtown District.

Goal 2: Require density/intensity in downtown as prescribed by the Comprehensive Plan, primarily within the Central Business District (CBD).

Goal 3: Develop a pedestrian-oriented, walkable downtown.

Goal 4: Stabilize and enhance the historic residential neighborhoods.

Goal 5: Recognize and promote opportunities to build sub districts/neighborhoods, each with a unique identity.

Goal 6: Jump-start the revitalization and reinvestment in the Downtown District with strategic catalyst projects.

C. Rail District Goals and Policies

Goal 1: Preserve the opportunity for heavy industry and rail service that supports it.

Goal 2: Recognize distinction between “industrial” streets such as 9th and 12th Streets and “public” streets 7th Street and Riverside Parkway.

Goal 3: Promote higher quality, customer and pedestrian friendly development along 7th Street and Riverside Parkway.

Goal 4: Re-establish and improve a street grid in the Rail District.

D. River District Goals and Policies

Goal 1: Create/maintain/enhance a green waterfront

Goal 2: Create retail, general commercial and mixed use opportunities that complement the riverfront use.

Goal 3: Create/enhance redevelopment opportunities and partnerships

Comprehensive Plan

The Grand Junction Comprehensive Plan is based on extensive public input that identifies what kind of community we want to have and identifies ways to achieve our vision. It charts the course to help us become the most livable community west of the Rockies. It establishes a vision that focuses the community on what it should do to sustain the quality of life that all residents desire and expect. The Comprehensive Plan establishes the following guiding principles that will shape growth, all of which apply to development of the Greater Downtown area.

A. Concentrated Centers – The Plan calls for three types of centers; the City Center, Village Centers and Neighborhood Centers.

B. Sustainable Growth Patterns – Fiscal sustainability where we grow efficiently and cost-effectively. Encourage infill and redevelopment.

C. Housing Variety – Allow, encourage more variety in housing types that will better meet the needs of our diverse population.

D. A Grand Green System of Connected Recreational Opportunities – Take advantage of and tie together the exceptional open space assets of Grand Junction, including the Colorado River, our excellent park system, trails and our surrounding open spaces.

E. Balanced Transportation – Accommodate all modes of transportation including air, transit, freight, auto, bike and pedestrian.

F. A Regional Center – Preserve Grand Junction as a provider of diverse goods and services and residential neighborhoods.

Specific policies within the Comprehensive Plan further support the concepts of the Greater Downtown Plan as outlined below.

Goal 4. Support the continued development of the downtown area of the City Center into a vibrant and growing area with jobs, housing and tourist attractions.

Goal 5. To provide a broader mix of housing types in the community to meet the needs of a variety of incomes, family types and life stages.

Goal 6. Land use decision will encourage preservation of existing buildings and their appropriate reuse.

Goal 8. Create attractive public spaces and enhance the visual appeal of the community through quality development.

Goal 9. Develop a well-balanced transportation system that supports automobile, local transit, pedestrian, bicycle, air and freight movement while protecting air, water and natural resources.

Comprehensive Plan Text Amendment and Zoning and Development Code Text Amendment

For some of the parcels in the Downtown District with an existing zoning of Residential Office (RO), the existing Future Land Use Map shows a land use category of Downtown Mixed Use. Presently, the RO district cannot be used to implement the Downtown Mixed Use category. However, the nature of these parcels is that they are small and on the periphery of the Central Business District so a rezone to a zone district that is acceptable in the Downtown Mixed Use designation (e.g. Downtown Business, B-2) would not provide a compatible transition to nearby residential areas. Consequently, the text of the Comprehensive Plan is proposed to be revised to include Residential Office (RO) as an acceptable zone district to implement the Downtown Mixed Use land use designation.

Future Land Use Map Amendments

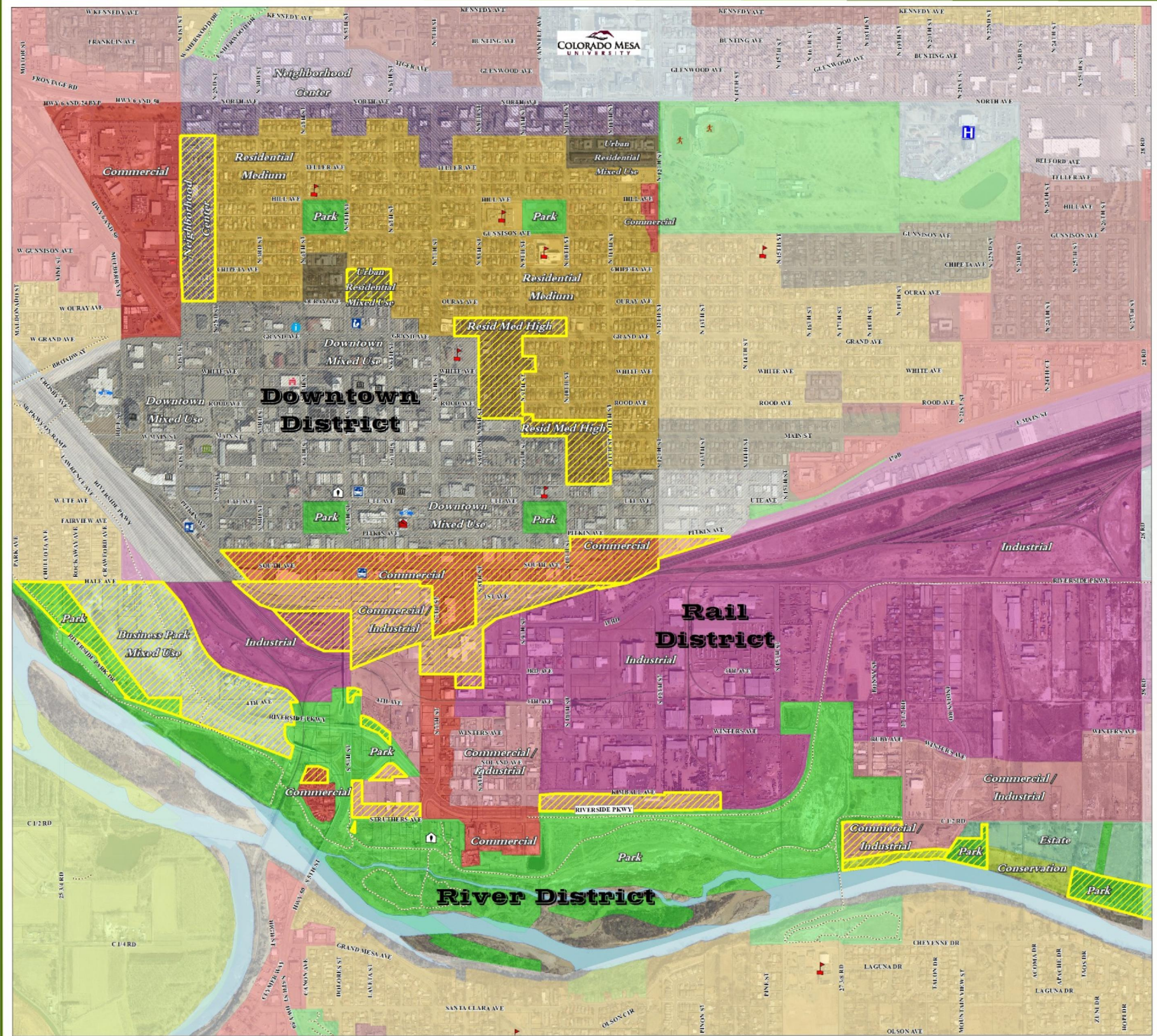
The City of Grand Junction and Mesa County jointly adopted a Comprehensive Plan in February, 2010. The Comprehensive Plan established or assigned new land use designations to implement the vision of the Comprehensive Plan and guide how development should occur. In many cases the new land use designation encouraged higher density or more intense development in some urban areas of the City.

A component of the Greater Downtown Plan is a new Future Land Use Map for the area. The new map will amend the existing Comprehensive Plan Future Land Use Map and is the first step in implementing an overall vision for the Greater Downtown area. The land use categories and their application are further defined in the Greater Downtown Plan report document (attachment 1). Future Land Use Map amendments in the Greater Downtown area are shown as the highlighted areas on the map on the following page. The map on the following page highlights the areas that are proposed to change.

The changes are proposed generally to create better areas of transition between land uses, remove inconsistencies between the future land use and zoning categories, and begin to define the intended character of development in some areas. A more detailed description of each proposed change is included on the following pages.

Regarding the removal of inconsistencies, when the City adopted the Comprehensive Plan, it did not rezone property to be consistent with the new land use designations. As a result, certain urban areas had a land use designation that called for a change of the current zoning of the property. In several cases the zoning was to be upgraded to allow for more residential density or commercial/industrial intensity. In other cases the zoning was to be downgraded to reduce commercial/industrial intensity. However, after further review, some of these changes were in error due to lack of information. In order to remove the inconsistency between the Comprehensive Plan's Future Land Use Map and the zoning of these properties (which has been determined to be more appropriate after reconsideration), the Comprehensive Plan Future Land Use Map needs to be amended.

Greater Downtown Plan - Proposed Future Land Use



Downtown District Future Land Use. The Greater Downtown Plan within the Downtown District is formulated around seven general land use categories: Commercial, Downtown Mixed Use, Neighborhood Center Mixed Use, Urban Residential Mixed Use, Residential Medium High Density, Residential Medium Density and Parks and Open Space. Future Land Use Map amendments in the Downtown District are summarized below.

- North First Street Neighborhood Center – revised from Commercial, expands the North Avenue Neighborhood Center around the corner to the south and provides transition from the commercial corridor to the residential areas of the Downtown District.
- 500 Block of Ouray/Chipeta Avenue – revised from Downtown Mixed Use to Urban Residential Mixed Use in order to better reflect an intended lower intensity of land use on this block and correspond with existing zoning.
- Eastern periphery of the Downtown Mixed Use Area – revised from Residential High Mixed Use to Residential Medium High.
- There are 139 parcels within the Downtown District whose current zoning category conflicts with the current land use category. These conflicts were created in error due to lack of information. With the exception of the blocks along the north side of Grand Avenue, the conflicts will be resolved through the land use changes proposed above. [The parcels along Grand Avenue are proposed to be rezoned (see zoning discussion below) in order to resolve the conflicts.]

Rail District Future Land Use. The Greater Downtown Plan within the Rail District is formulated around five general land use categories: Business Park Mixed Use, Commercial, Commercial industrial, Industrial and Parks and Open Space. Future Land Use Map amendments in the Rail District are summarized below.

- Areas southwest of the railroad tracks to the Riverside Parkway on the west side of 5th Street – revised from Industrial to Business Park Mixed Use to be consistent with existing land uses but also to encourage future redevelopment in these areas with a mix of employment-oriented business and light industrial uses and an allowance of multifamily development.
- Areas in the “wedge” between South Avenue and the railroad tracks – revised from Downtown Mixed Use to Industrial, Commercial/Industrial and Commercial to be consistent with existing zoning.
- There are 69 parcels within the Rail District whose current zoning category conflicts with the current land use category. With the exception of a few parcels along Riverside Parkway, the conflicts will be resolved through the land use changes proposed above. [The parcels along the Parkway are proposed to be rezoned (see zoning discussion below) in order to resolve the conflicts.]

River District Future Land Use. The Greater Downtown Plan within the River District is formulated around six general land use categories: Commercial, Commercial Industrial, Parks and Open Space, Conservation, Estate and Business Park Mixed Use. Future Land Use Map amendments in the River District are summarized below.

- Areas southwest of the Riverside Parkway to the riverfront trail on the west side of 5th Street (mostly City-owned properties) – revised from Industrial and

Commercial/Industrial to Business Park Mixed Use to be consistent with existing land uses, but also encourage future redevelopment in these areas with a mix of employment-oriented business and light industrial uses and an allowance of multifamily development.

- Areas on the east and west side of 5th Street between the Riverside Parkway ramps and Struthers Avenue – revised from Park and Commercial to Commercial (west side) and Commercial/Industrial (east side) to be consistent with existing zoning and better conform to existing parcel boundaries.
- Areas east of 27-1/2 Road, between C-1/2 Road and the Colorado River – revised to be consistent with other properties owned by Colorado State Parks (Park) and add Conservation area along the river where the Riverfront Trail will be completed in this area.
- The three properties along the river just east of 27-1/2 Road known as the Brady properties (labeled as Under Review) do not currently reflect a future land use category since the zoning is under consideration and will be voted upon by the citizens of Grand Junction in the April 2013 election. Once the zoning is established, the Future Land Use Map will be amended accordingly.
- There are 29 parcels within the River District whose current zoning category conflicts with the current land use category. With the exception of a few parcels near the Riverside Parkway/5th Street interchange, the conflicts, created in error due to lack of information, will be resolved through the land use changes proposed above. [The parcels near the interchange are proposed to be rezoned (see zoning discussion below) in order to resolve the conflicts.]

4. Rezoning Properties within Greater Downtown (RZN-2012-217)

Similar to changes in the Comprehensive Plan Future Land Use Map, proposed zoning changes are also one of the means to implement the overall vision for the Greater Downtown area. Proposed zoning changes are minimal, as shown on the map on the following page. Those that are proposed to change (highlighted in yellow) were due to review of information, development of goals and policies for the Greater Downtown area, and public input throughout the Greater Downtown Plan process. The zone districts and their application are further discussed in the Greater Downtown Plan report document (Attachment 1). The rezone ordinance in Attachment 3 also lists each property that is proposed to be rezoned. Zoning changes in the Greater Downtown area are proposed primarily to create better areas of transition between land uses, remove errors and inconsistencies between the zoning and future land use categories, and begin to define the intended character of development in some areas of Greater Downtown.

Downtown District Zoning. Proposed zoning within the Downtown District is shown on maps and the zone districts further discussed in the Greater Downtown Plan report (Attachment 1). The detailed areas are highlighted in yellow on the map on page 12 of this report. Generally, the zoning shall remain the same as currently exists. The few zone changes are described below.

- Southeast corner of Chipeta Avenue and 5th Street – revised from R-8 to R-O to be consistent with the Urban Residential Mixed Use land use category.

- 1100 block of Colorado Avenue – revised from B-1 to B-2 to be consistent with the Downtown Mixed Use land use category.

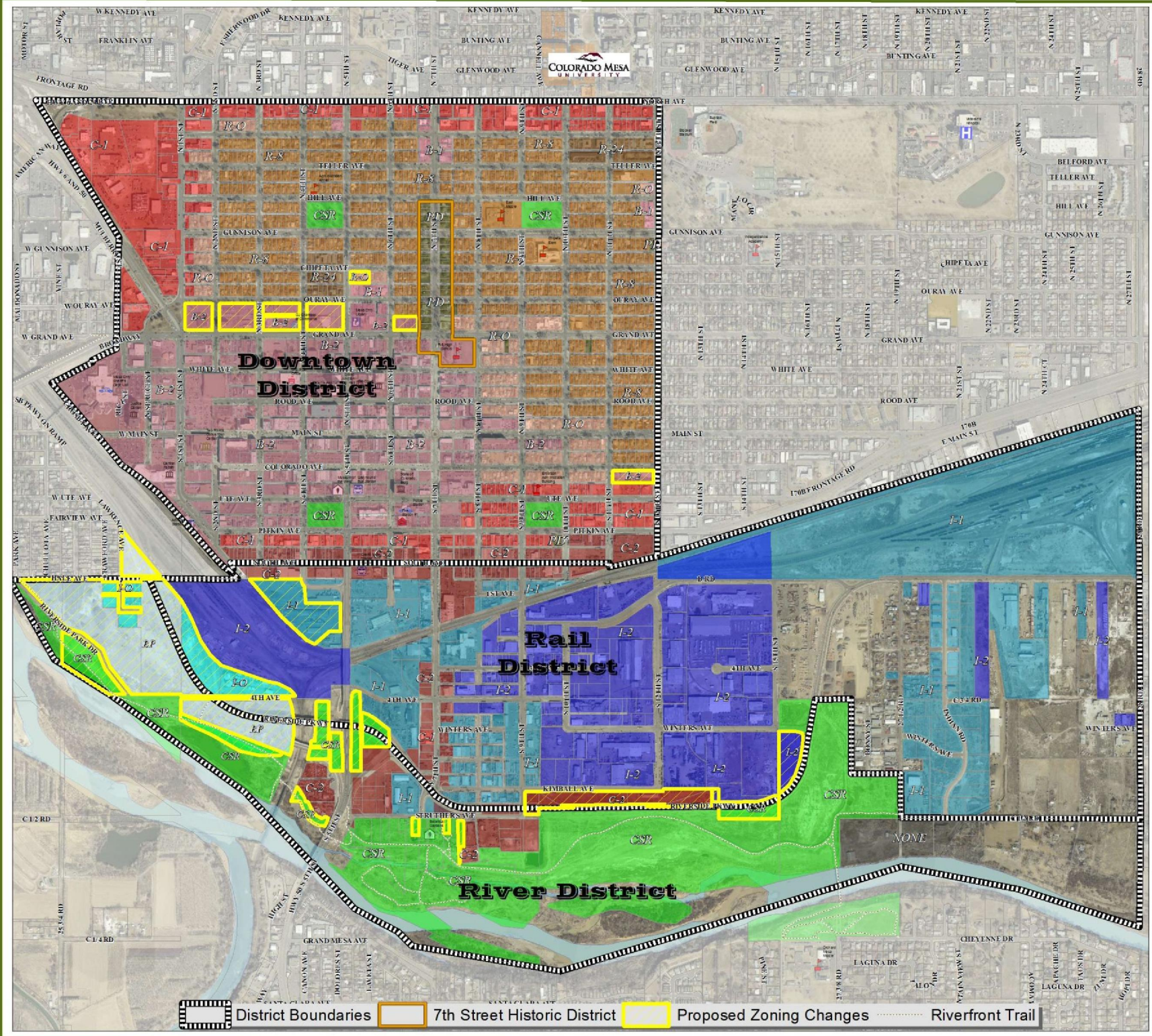
Rail District Zoning. Proposed zoning within the Rail District is shown on maps and the zone districts further discussed in the Greater Downtown Plan report (Attachment 1). The detailed areas are highlighted in yellow on the map on page 12 of this report. Generally, the zoning shall remain the same as currently exists. The few zone changes are described below.

- Two parcels west of South 5th Street revised from I-2 to I-1 and I-O to be consistent with the Commercial/Industrial and Business Park Mixed Use future land use categories.
- City-owned parcels at the interchange of South 5th Street and Riverside Parkway from C-1 to CSR to be consistent with zoning of similar City-owned properties.
- Remnants of Las Colonias Park on the north side of Riverside Parkway from CSR to C-2 and I-2 to reflect existing and potential lease to adjacent property owners.

River District Zoning. Proposed zoning within the River District is shown on maps and the zone districts further described in the Greater Downtown Plan report (Attachment 1). The detailed areas are highlighted in yellow on the map on page 12 of this report. Generally, the zoning shall remain the same as currently exists. The few zone changes are described below, primarily impacting City-owned properties.

- Areas southwest of the Riverside Parkway to the riverfront trail on the west side of 5th Street (mostly City-owned properties) from I-O and I-1 to BP and I-O to be consistent with existing uses (private properties) as well as encourage future redevelopment in these areas with a mix of employment-oriented business and light industrial uses and an allowance of multifamily development.
- Areas along the Colorado River near the Riverside Neighborhood from I-O to CSR to reflect existing riverfront trail use on City-owned property.
- City-owned parcels at the interchange of South 5th Street and Riverside Parkway from C-1 to CSR to be consistent with zoning of similar City-owned properties.
- City-owned parcels near the Botanical Gardens from C-2 to CSR to be consistent with zoning of similar City-owned properties.
- The three properties along the river just east of 27-1/2 Road known as the Brady properties (labeled as NONE) do not currently reflect a zoning as the topic is under consideration and will be voted upon by the citizens of Grand Junction in the April 2013 election.

Greater Downtown Plan - Proposed Zoning



5. Other Implementation Strategies of the Greater Downtown Plan (CPA-2011-1067)

In addition to the future land use and zoning changes proposed, the City has a variety of other tools available through which the goals of the Greater Downtown Plan can be implemented so that the vision for Greater Downtown can materialize and eventually be realized. The GDP represents the first phase of implementation as it includes the basic strategies of designating Future Land Use categories and zoning properties as needed as previously discussed, including a conceptual plan for traffic circulation in Greater Downtown, outlining improvements to the public parks within Greater Downtown, establishing goals and policies for future phases of plan implementation such as economic development strategies (see below), and amending development standards of the zoning districts through a zoning overlay (see Section 6 on following page).

The Conceptual Plan for Traffic Circulation in Greater Downtown (Conceptual Plan) describes proposals for streets and corridors in the Greater Downtown, depicts preferred conceptual designs and proposes additional unclassified streets. This conceptual plan for circulation does not modify the Grand Valley Circulation Plan, but provides preferred concepts and designs for future consideration and development of public rights-of-way. The Conceptual Plan also incorporates the proposed Grand Valley Trails Plan and depicts the location of future bicycle facilities, trails and pedestrian paths within Greater Downtown. As development or redevelopment occurs in Greater Downtown, trails, paths, bike lanes and pedestrian facilities will be constructed in accordance with the adopted Grand Valley Trails Plan. The Conceptual Plan for Traffic Circulation in Greater Downtown is included in Appendix C of the Greater Downtown Plan report.

The Greater Downtown Plan report includes details for ongoing maintenance, improvements, redevelopment and in some cases new development within the Greater Downtown area parks. In addition, the plan summarized the work that has already been completed for potential redevelopment of the City-owned Jarvis property in the eastern portion of the Rail District.

Downtown District Economic Redevelopment. While the Downtown District is the heart of the community, it is but one subset of a larger market and has strengths which can be capitalized on and limitations which should be overcome. Downtown has a tremendous influence on the economic well-being of the entire region. Therefore, it is widely accepted that early projects in any revitalization effort can benefit from public assistance until market conditions reach levels where new construction can support itself.

The Grand Junction SDMP presented guiding principles which; while general in nature, were considered responsive to prevailing conditions, market opportunities, framework elements and stakeholder input. Based on information reviewed and community input received during the Greater Downtown Plan process, these guiding principles are still relevant to the Greater Downtown Plan and are listed below and described in greater detail in the Greater Downtown Plan report (Attachment 1).

- Downtown is one submarket that competes with other submarkets in Grand

Junction.

- Downtown must be market-responsive to changing conditions.
- Downtown infrastructure must be protected and retained.
- Downtown's "tool bag" must contain a variety of strategies and mechanisms that are comprehensive, flexible and creative in order to attract investment.
- Public investment must leverage private investment.
- Public policy must support downtown development.
- Public-private partnerships are essential.

Rail and River District Economic Redevelopment. The changes that have occurred in portions of Greater Downtown such as completion of the Riverside Parkway and planning for the future development of Las Colonias Park have already had a positive influence on the River and Rail Districts. Many properties have been renovated or redeveloped, new uses are relocating to the area and property values are generally on the rise. The Greater Downtown Plan envisions this trend continuing and being enhanced by the following redevelopment concepts:

- Allow existing heavy industry to remain, taking advantage of rail spurs within the area.
- Intensified commercial edge along the north side of the Riverside Parkway with opportunities for mixed use development.
- New general commercial, retail and residential uses will provide activity at the edge of the park after business hours to create a safe park environment that gives "ownership" of the park to the adjacent local business owners and residents.
- New retail and commercial uses such as restaurants, shops and services along South 7th Street to serve the employees, recreational users and residents of the neighborhood.
- Commercial Industrial uses bridge the existing industrial and the commercial corridors.

In addition, discussions with the Grand Junction Economic Partnership, Business Incubator, Manufacturers' Council and Chamber of Commerce during development of the Greater Downtown Plan brought to light many opportunities for the area, the majority of which is within the established Mesa County Enterprise Zone. The GDP outlines goals, policies and strategies that can be used to further the economic (re)development of the Rail and River Districts in Greater Downtown.

- Need for flex space for different types of small business
- Opportunity to develop additional incentives for redevelopment
- Allow for live-work opportunities
- Opportunity to develop partnerships

6. Greater Downtown Overlay District (RZN-2012-218)

The Greater Downtown Overlay District is intended to provide guidance and criteria for the planning, design and implementation of public and private improvements in the

Greater Downtown area. If properly administered and adhered to, the standards and guidelines should result in public and private development improvements (or a combination thereof) that achieve, as a minimum, a common level of quality in terms of site design, architectural design, landscaping and other site improvements.

The general purposes of the standards and guidelines are to support the overall goals of the Greater Downtown Plan.

- Maintain and enhance the economic, cultural and social vitality of Greater Downtown
- Promote downtown living by providing a wide range of housing opportunities in appropriate areas within Greater Downtown
- Enhance the transportation system to accommodate automobiles, bikes and pedestrians, and provide adequate, convenient parking
- Stabilize, preserve, protect and enhance the downtown residential neighborhoods
- Promote and protect the unique identity of Downtown

The standards and guidelines were developed upon an analysis of the existing character of the Greater Downtown area. The area was divided into sub districts and the Downtown District was further divided into subareas (see map in Attachment 2 and on page 18) based on existing zoning, character of existing development and potential for redevelopment opportunities. In addition, primary corridors were identified for which overlay guidance was created. The subareas and primary corridors are shown on the maps on pages 17 and 18.

These standards supplement the City of Grand Junction Zoning and Development Code and identify design alternatives and specific design criteria for the visual character and physical treatment of private development and public improvements within Greater Downtown. The Director will make all decisions and appeals and variance requests will be heard by the City of Grand Junction Planning Commission. The Downtown Development Authority (DDA) will be a review agency for all applications and will make recommendations for proposals in the Central Business District.

Corridor Overlay. The Greater Downtown Overlay District includes standards and guidelines for primary corridors in the River and Rail Districts to begin to implement goals of the plan to 1) improve the visual impact of development along the corridors; and 2) promote higher quality architectural treatment and site design as new development and redevelopment occurs along the corridors. The goals of the corridor guidelines and standards are to:

- Define a vision using examples of what is desired
- Provide design flexibility on a site-by-site basis
- Provide menus of design options so designer/builder can decide what works best for a particular project/site and the vision can be achieved without substantial cost
- Provide design options that provide flexibility for trade-offs in building and site design

- Clearly define what is required for new construction versus building remodels

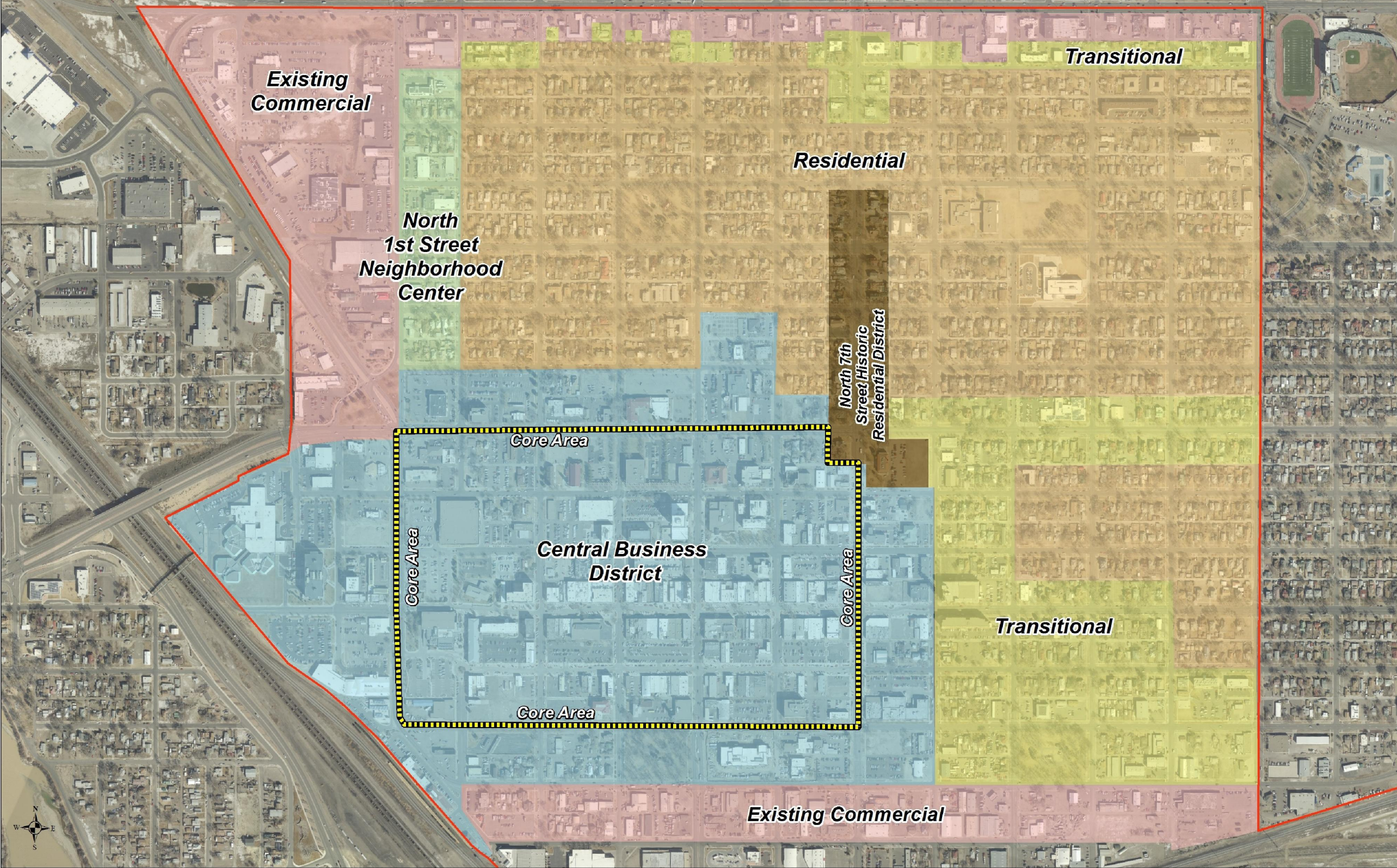
The corridor standards and guidelines are outlined in two areas: 1) Commercial Corridors; and 2) Industrial Corridors. For each type of corridor the standards and guidelines address Site Design, Architectural Design, Landscaping and Signage.

Downtown District Subarea Zoning Overlay. The Greater Downtown Plan includes zoning overlay standards for the subareas of the Downtown District as depicted on the map on page on page 18. Application of the standards and guidelines will begin to implement goals of the plan to:

- Maintain and enhance the economic, cultural and social vitality of Downtown
- Promote downtown living by providing a wide range of housing opportunities
- Enhance the transportation system to accommodate automobiles, bikes and pedestrians, and provide adequate, convenient parking
- Stabilize and enhance the historic residential neighborhoods
- Establish and promote a unique identity
- Preserve and restore significant historic structures
- Activate the edges of the downtown parks with mixed use and programmed/active use of the parks as urban open space rather than passive green parks

The Downtown District subarea standards and guidelines are outlined in five areas and summarized below: 1) Area-Wide; 2) Central Business District-Wide; 3) Central Business District Core Area; 4) Residential; and 5) Transitional.

Downtown District - Subareas

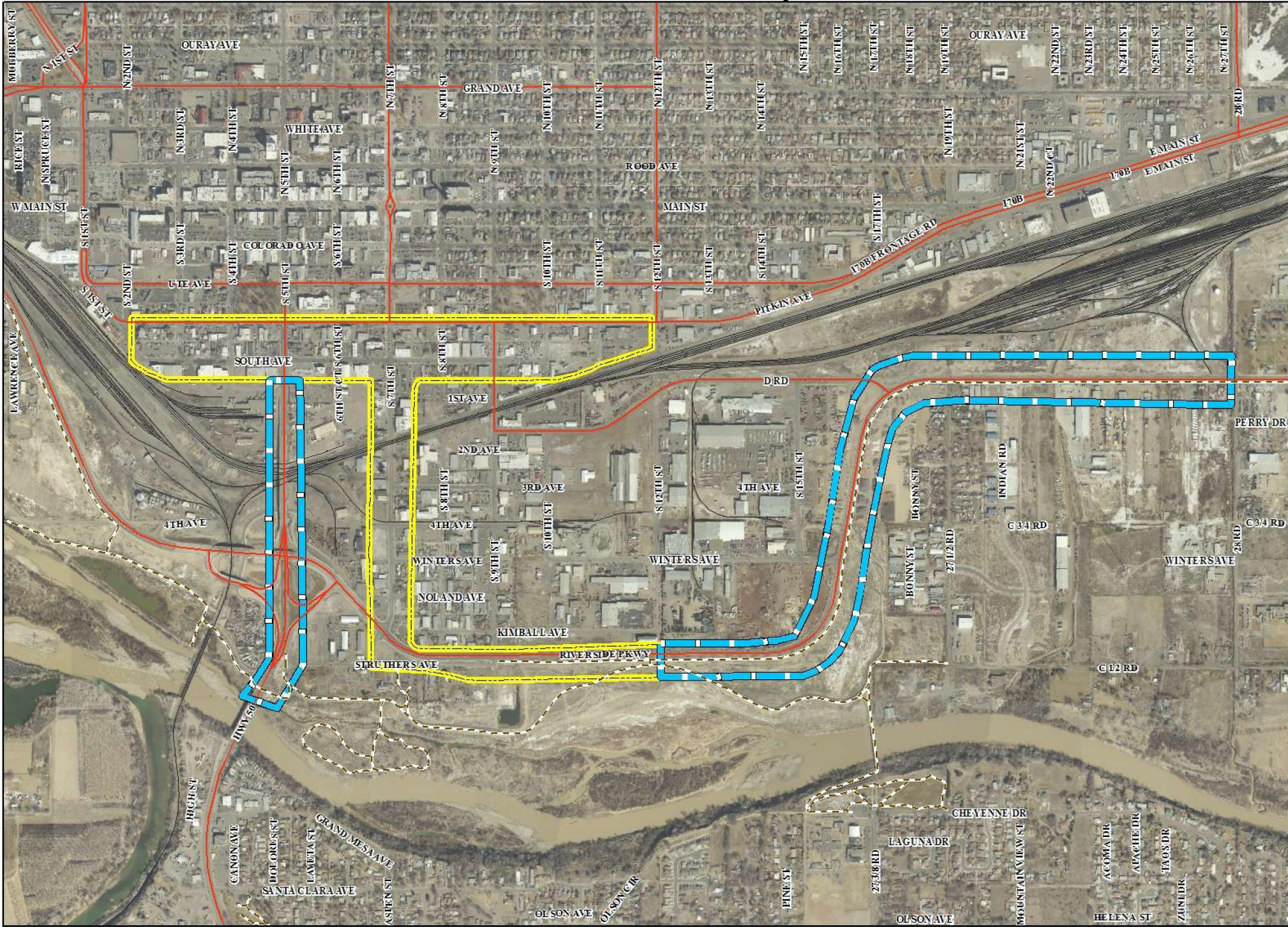


Downtown Subareas Central Business District Core Central Business District North 1st St Neighborhood Existing Commercial Residential Area Transitional Area 7th Street Historic

G:\GIS\DEPARTMENT\PLANNING\NEIGHBORHOOD PLANS\GREATER DOWNTOWN\ZONING OVERLAY.mxd

Greater Downtown Overlay Corridors

Grand Junction
Colorado



Greater Downtown Overlay Corridors — Industrial Corridor — Commercial Corridor

--- Trails

GIS DEPARTMENT / NEIGHBORHOOD PLANS / GREATER DOWNTOWN PLAN Overlay Corridors

Downtown District Area-Wide Standards and Guidelines

- Due to constraints of downtown properties, allows Director to make reasonable exceptions to the provisions of the Zoning and Development Code and the Greater Downtown Plan Zoning Overlay.
- Provide good, interconnected multimodal transportation choices.
- Requires traffic calming measures in public rights-of-way as properties redevelop or infrastructure is reconstructed.

Central Business District (CBD) Guidelines and Standards

Overall Vision/Character

- Activate the downtown core streets through emphasis on higher pedestrian traffic, businesses on the ground level that attract pedestrian traffic, and corner buildings that invite traffic on both streets.
- Encourage high quality, compatible design for all new buildings and establish a cohesive architectural character/theme that complements existing buildings.
- Use building materials that are traditional and weather well and provide a broad variety of appearance.
- Encourage high density, mixed-use development and structures.
- Encourage gradual scale transitions between the CBD and adjacent neighborhoods.
- Minimize single use, surface parking and encourage shared parking.
- Require parking located behind buildings be accessed from the alley.
- Provide streetscape details and landscaping that compliment the architectural character of downtown and exhibit an urban character.

Overall Standards

- Maximum building height 90 feet
- Upper floors of taller buildings shall step back a minimum of 10 feet
- Buildings along Chipeta and Ouray Avenues shall be set back a minimum of 20 feet and step down so the front façade is of a residential scale
- Off-street parking is to be located behind buildings and accessed from the alley
- Minimize curb cuts to maximize on-street parking
- Pedestrian lighting shall be in historical style light poles
- Streetscape design along the northern edge of the CBD shall transition between urban hardscape and more residential streetscape character
- Director may consider variations to landscaping Code, considering existing and proposed streetscape and/or the urban design character of the area

Central Business District Core Area Guidelines

- Only apply to Core Area as depicted on map on page 19
- Façade detailing including entrances and doorways should be compatible with neighboring historic buildings.

Central Business District Core Area Standards

- Only apply to Core Area as depicted on map on page 19
- Minimum building height in the CBD Core Area is 2 stories. Some uses are exempt from the requirement and the Planning Commission may consider other exceptions.
- Maximum building setback of 2 feet, compatible with the mean setback of the immediately adjoining lots on both sides but not greater than 20 feet.
- Building setbacks of up to 10 feet from the abutting street may be allowed if there is a prescribed function for space in front of a building, then maximum building setback is 10 feet.
- Facades shall be visually interesting with varied materials, patterns, definition of bays or other building articulation, 50% minimum in windows on street level façade, façade cap/cornice that cast a shadow. The property owner/developer may choose from a list of architectural elements and choose to meet 4 of the 9 options.

Residential Areas Standards and Guidelines

- Do not allow further encroachment by non-residential uses, higher intensity/density or more intensive zoning but provide a diversity of housing types.
- Maintain and enhance the historic character of the streetscape including use and landscaping of the park strips.
- Maintain the existing character of the house styles. New construction and alterations shall be compatible with key architectural characteristics and site elements of the neighborhood including building mass and scale, setbacks, height, roof shape, window patterns, and exterior materials.
- Ensure accessory structures are subordinate to the primary structures on a site.
- Allow multifamily development where existing zoning allows but site and building design must be compatible with the scale and material finishes of single family residential structures.
- Do not allow off-street parking for multifamily development in the front yard or setback.

Transitional Areas Standards and Guidelines

- Uses as allowed by the Zoning and Development Code but a mix of residential and nonresidential uses on the same lot shall be located in the same structure.
- Hours of operation of nonresidential uses restricted to 7:30 am and 8:00 pm.
- Maximum building size is 10,000.
- Outdoor storage and display are prohibited.
- New residential and non-residential construction shall be designed to have a single family residential character in building mass and scale, setbacks, height, roof shape, window patterns, location of entryways, and exterior materials.
- Signs for non-single family uses are restricted in type, size, location and lighting.

- Non-single family residential uses in the downtown Transitional areas shall be designed and operated not to increase on-street parking in front of single family dwellings in the neighborhood.
- Service entrances, loading areas and dumpsters shall be located only in the rear or side yard.
- Front yards shall be reserved for landscaping, sidewalks, driveway access to parking areas and signage.

REVIEW CRITERIA:

Comprehensive Plan amendments to the Future Land Use Map, zoning changes and the zoning overlays must meet one or more of the following criteria for approval per sections 21.02.130 and 21.02.140 of the Municipal Code. These criteria do not apply for consideration of the text amendment to the Comprehensive Plan and the text amendment to the Zoning and Development Code.

(1) Subsequent events have invalidated the original premise and findings;

The subsequent event that has occurred is the Greater Downtown Plan planning process which has included several general public meetings, meetings with property owners, tenants and local community groups, results of questionnaires and comments solicited from the general public. The Comprehensive Plan did not include this level of planning detail thus, the original premise and findings of that plan have been amended to reflect the findings of the Greater Downtown Plan.

Similarly, the existing zoning Code and map did not take into account the subsequent event of more detailed analysis of zoning in the Greater Downtown area that was done with the Greater Downtown Plan planning process. For some parcels within Greater Downtown, the original premise and findings of the existing zoning map were not consistent with the Future Land Use Map or did not reflect the overall goals of the Greater Downtown Plan. For some areas and corridors in Greater Downtown, the overlay zone will be a tool to accomplish the goals of the Comprehensive Plan and Greater Downtown Plan.

Since the Greater Downtown Plan area encompasses the entire area that had been included in the Westside Downtown Redevelopment Plan, the more recent analysis of land use and implementation strategies will replace what was previously adopted. Consequently, the Westside Downtown Redevelopment Plan needs to be repealed and replaced with the Greater Downtown Plan.

(2) The character and/or condition of the area has changed such that the amendment is consistent with the Plan;

Criterion not met.

(3) Public and community facilities are adequate to serve the type and scope of land use proposed;

Criterion not met.

(4) An inadequate supply of suitably designated land is available in the community, as defined by the presiding body, to accommodate the proposed land use;

Criterion not met.

(5) The community or area, as defined by the presiding body, will derive benefits from the proposed amendment.

Primarily, criterion 5 applies to the proposed Comprehensive Plan amendments to adopt the Greater Downtown Plan and amend the Future Land Use Plan, the zoning map amendments, and the amendment to the Zoning and Development Code to adopt the Greater Downtown Plan Zoning Overlay. The benefit derived by adoption of these items will help the community meet some of its long term goals as expressed in the Comprehensive Plan as discussed in greater detail on page 6 of the staff report.

FINDINGS OF FACT/CONCLUSIONS:

After reviewing the Greater Downtown Plan, files CPA-2011-1067, CPA-2012-216, RZN-2012-217 and RZN-2012-218 for an amendment to the Comprehensive Plan Future Land Use Map and text, rezoning properties within Greater Downtown, and adopting a zoning overlay, staff makes the following findings of fact and conclusions:

1. The Greater Downtown Plan and Zoning Overlay are consistent with the Comprehensive Plan including the following elements:
 - Future Land Use Map and text amendments to the Comprehensive Plan
 - Zoning and Development Code ("Code") amendments to add the Greater Downtown Zoning Overlay and a text amendment to add RO (Residential Office) as a zone district that can implement the Downtown Mixed Use Land Use designation
 - Rezone identified properties within the Greater Downtown Plan area
2. Review criteria 1 and 5 in sections 21.02.130 and 21.02.140 of the Municipal Code have been met.

STAFF RECOMMENDATION:

Staff recommends that the Planning Commission forward a recommendation of approval to City Council of:

- 1) Comprehensive Plan amendments by adopting the Greater Downtown Plan including all revisions per Attachment 5 and repealing the Westside Downtown Redevelopment Plan
- 2) An amended Future Land Use Map for the area within the Greater Downtown Plan

- 3) Comprehensive Plan and Zoning and Development Code ("Code") text amendments to add RO (Residential Office) as a zone district that can implement the Downtown Mixed Use Land Use Category
- 4) Rezone identified properties within the Greater Downtown Plan area
- 4) Adoption of the Greater Downtown Overlay District including all revisions per Attachment 5

RECOMMENDED PLANNING COMMISSION MOTIONS:

Mr. Chairman, on item CPA-2011-1067, a Comprehensive Plan Amendment to adopt the Greater Downtown Plan and Future Land Use Map Amendments included within the plan and repeal the Westside Downtown Redevelopment Plan I move that we forward them to City Council with the recommendation of Approval with the findings of facts and conclusions included within the staff report.

Mr. Chairman, on item CPA-2012-216, a Comprehensive Plan Text Amendment and Zoning and Development Code Text Amendment to include the RO (Residential Office) zoning district as one of the zone districts that implements the Downtown Mixed Use land use designation, I move that we forward them to City Council with the recommendation of Approval with the findings of facts and conclusions included within the staff report..

Mr. Chairman, on item RZN-2012-217, rezoning properties within the Greater Downtown Plan area as set forth in the staff report and in the proposed ordinance, I move that we forward it to City Council with the recommendation of Approval with the findings of facts and conclusions included within the staff report.

Mr. Chairman, on item ZCA-2012-363, a Zoning and Development Code Text Amendment by adopting the Greater Downtown Overlay District, I move that we forward it to City Council with the recommendation of Approval with the findings of facts and conclusion as stated within the staff report.

Attachments:

1. Proposed Ordinance to Amend Comprehensive Plan including:
 - Exhibit A, Greater Downtown Plan Report
 - Exhibit B, Comprehensive Plan Text Amendment
 - Exhibit C, Future Land Use Map
2. Proposed Ordinance to Adopt Greater Downtown Overlay District and Section 21.03.020 (d) of the Zoning and Development Code
 - Exhibit A, Greater Downtown Plan Zoning Overlay Report
 - Exhibit B, Amendment to Section 21.03.020
3. Proposed Ordinance to Rezone Properties Within the Greater Downtown Area
 - Exhibit A, List of Properties to be Rezoned

4. Summary of Public Process and Questionnaires/Comments
5. Proposed revisions to Plan and Overlay documents

ATTACHMENT 1

CITY OF GRAND JUNCTION, COLORADO ORDINANCE NO. _____

AN ORDINANCE ADOPTING THE GRAND JUNCTION GREATER DOWNTOWN PLAN AND AMENDING THE FUTURE LAND USE MAP AND TEXT OF THE COMPREHENSIVE PLAN

AS AN ELEMENT OF THE COMPREHENSIVE PLAN FOR THE AREA GENERALLY INCLUDING THE ORIGINAL SQUARE MILE, SOUTH AVENUE TO THE COLORADO RIVER AND RIVERSIDE NEIGHBORHOOD TO 28 ROAD

RECITALS.

A Strategic Downtown Master Plan that encompassed the original square mile was developed through the Downtown Development Authority (DDA) and the guidance of a steering committee of interested downtown merchants, property owners and policymakers during 2007-2008. The Plan defined an overall vision and goals for downtown and included implementation strategies such as a zoning overlay. The Strategic Downtown Master Plan was considered by City Council on September 14, 2009 but, due to pending adoption of the Comprehensive Plan, Council voted to continue the Plan to an unspecified future date.

A South Downtown Neighborhood Plan encompassed the area between the railroad tracks and the Colorado River and the Riverside neighborhood on the west to 28 Road on the east. A plan for the area was developed from 2006-2008 with 15 community focus group meetings, 3 public open houses with 80-100 people in attendance at each open house. The Plan included a circulation and trails plan, economic development strategies, rezoning some properties and zoning overlay. The South Downtown Neighborhood Plan was considered by City Council on June 16, 2008 but was not adopted.

A Westside Downtown Redevelopment Plan was adopted by the City in January 2004. This plan included the area bounded on the north by Main Street, on the east by 5th Street, on the south by South Avenue, and on the west by the Railroad. A preferred plan for redevelopment defined redevelopment of various land use and presented concepts for a circulation plan. Design guidelines and standards for the area were considered and incorporated as appropriate within the Greater Downtown Plan. Adoption of the Greater Downtown Plan will repeal and replace the Westside Downtown Redevelopment Plan.

The Greater Downtown Plan (Exhibit A) integrates elements of the three previous planning efforts as well as includes areas that had not been covered by either of those plans into a single plan for the downtown area. In addition, the Greater Downtown Plan incorporates elements of the Downtown Development Authority's potential projects in order to support the DDA's Downtown Plan of Development. For planning purposes, the

Greater Downtown area has been divided into three sub districts: the Downtown, Rail and River Districts.

The public participation process involved community evaluation of various design and planning concepts to determine which of these are most important to the community and should be addressed in greater detail in the Greater Downtown Plan. The concepts addressed four major topics relative to an area plan: land use, circulation, economic (re)development and visual character. The results, along with previous information for the CBD, show strong community support for ideas that were translated to the goals listed below for the Greater Downtown Plan.

Area-Wide Goals and Policies

Goal 1: Enhance the transportation system to accommodate automobiles, bikes and pedestrians, and provide adequate, convenient parking.

Goal 2: Establish and improve entry points into the Greater Downtown area.

Goal 3: Promote downtown living by providing a wide range of housing opportunities, primarily in the Downtown District

Goal 4: Redefine the land use along key corridors to provide a mix that will offer the most opportunities for redevelopment and revitalization.

Downtown District Goals and Policies

Goal 1: Maintain and enhance the economic, cultural and social vitality of the Downtown District.

Goal 2: Require density/intensity in downtown as prescribed by the Comprehensive Plan, primarily within the Central Business District (CBD).

Goal 3: Develop a pedestrian-oriented, walkable downtown.

Goal 4: Stabilize and enhance the historic residential neighborhoods.

Goal 5: Recognize and promote opportunities to build sub-districts/neighborhoods, each with a unique identity.

Goal 6: Jump-start the revitalization and reinvestment in the Downtown District with strategic catalyst projects.

Rail District Goals and Policies

Goal 1: Preserve the opportunity for heavy industry and rail service that supports it.

Goal 2: Recognize distinction between “industrial” streets such as 9th and 12th Streets and “public” streets 7th Street and Riverside Parkway.

Goal 3: Promote higher quality, customer and pedestrian friendly development along 7th Street and Riverside Parkway.

Goal 4: Re-establish and improve a street grid in the Rail District.

River District Goals and Policies

Goal 1: Create/maintain/enhance a green waterfront

Goal 2: Create retail, general commercial and mixed use opportunities that complement the riverfront use.

Goal 3: Create/enhance redevelopment opportunities and partnerships

In addition to identifying goals and policies for the area, the Greater Downtown Plan does the following.

1. Includes an amendment to the Comprehensive Plan text (refer to Exhibit B) and amendments to the Comprehensive Plan Future Land Use Map (refer to Appendix A of the Greater Downtown Plan report).

2. Includes zoning changes as required to create better areas of transition between land uses, ensure that the zoning is consistent with the future land use category and begin to define the intended character of development in some areas.

3. Includes Circulation and Trails Plans that depict future street and trail systems for the area and outlines more specific multimodal transportation improvement concepts that serve as future guidance as development and redevelopment occurs in the area.

4. Includes a zoning overlay that provides guidance and criteria for the planning, design and implementation of public and private improvements in the Greater Downtown area. If properly administered and adhered to, the standards and guidelines should result in public and private development improvements (or a combination thereof) that achieve, as a minimum, a common level of quality in terms of site design, architectural design, landscaping and other site improvements.

5. Outlines other implementation tools such as economic development and redevelopment strategies and improvements to the public parks within the Greater Downtown area.

The Grand Junction Planning Commission is charged with the legal duty to prepare and consider and recommend action to City Council regarding master plans for the City.

The Greater Downtown Plan was heard in a public hearing by the Grand Junction Planning Commission on _____, 2013 where the Planning Commission recommended that the City Council adopt the Plan.

NOW THEREFORE, BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF GRAND JUNCTION:

That the Greater Downtown Plan, City of Grand Junction, Colorado, in the form of the document attached hereto, and as recommended for adoption by the Grand Junction Planning Commission is hereby adopted.

The full text of the Ordinance, including the text of the Greater Downtown Plan, in accordance with paragraph 51 of the Charter of the City of Grand Junction, shall be published in pamphlet form with notice published in accordance with the Charter.

INTRODUCED on first reading the _____ day of _____, 2013 and ordered published pamphlet form.

PASSED and **ADOPTED** on second reading the _____ day of _____, 2013 and ordered published in pamphlet form.

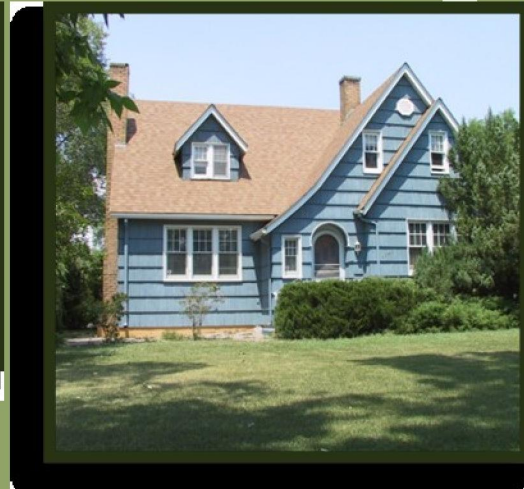
President of City Council

ATTEST:

City Clerk

Greater Downtown Plan

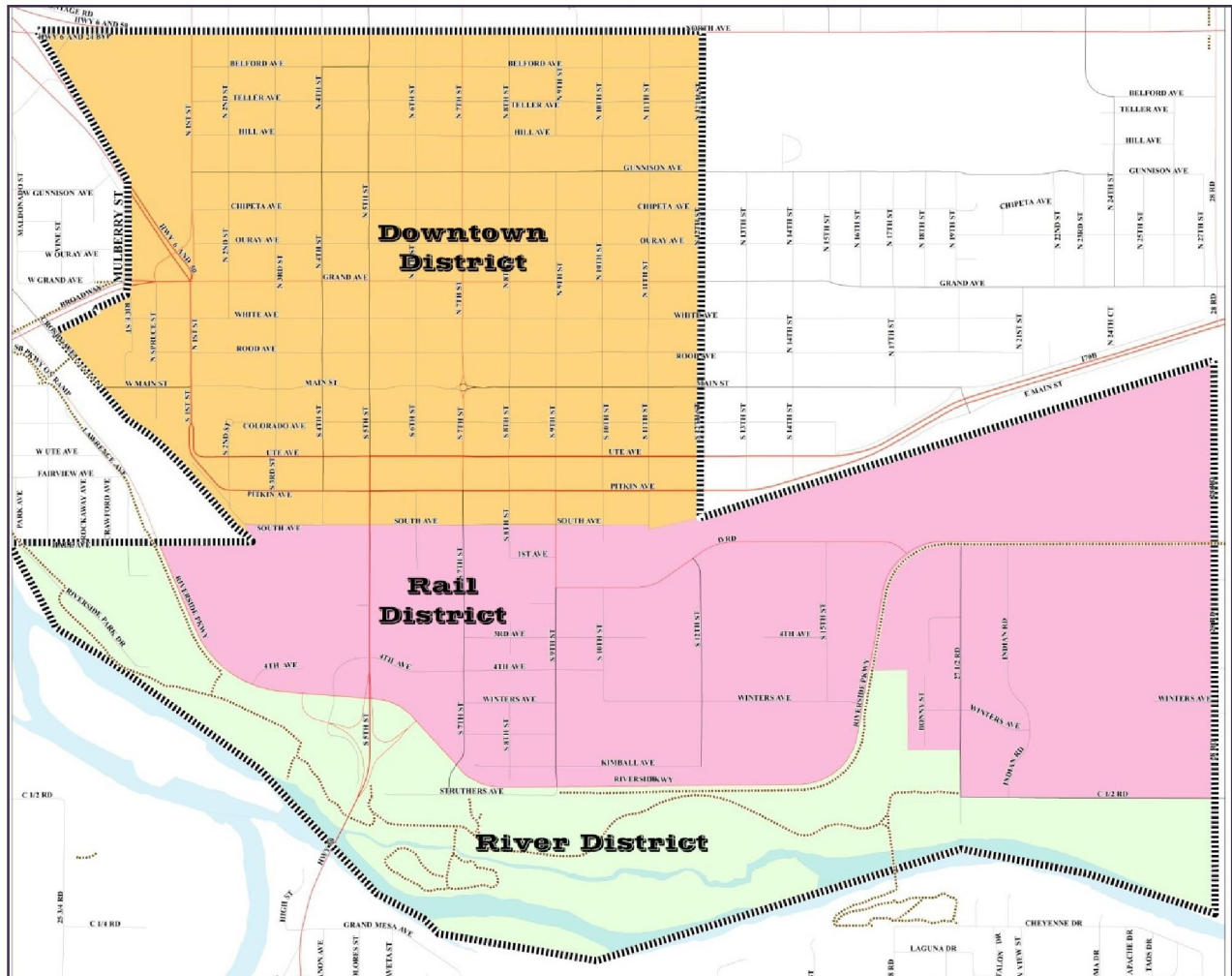
DRAFT



1. STUDY AREA CONTEXT	4
2. PLANNING BACKGROUND	4
3. COMPREHENSIVE PLAN	5
4. SITE ANALYSIS	6
A. Downtown District	6
B. Rail District	10
C. River District	12
5. GREATER DOWNTOWN GOALS AND POLICIES	14
A. Area-Wide Goals and Policies	15
B. Downtown District Goals and Policies	16
C. Rail District Goals and Policies	17
D. River District Goals and Policies	18
6. IMPLEMENTATION STRATEGIES	18
A. Downtown District Future Land Use and Zoning	19
B. Rail District Future Land Use and Zoning	22
C. River District Future Land Use and Zoning	24
D. Development Standards	27
E. Greater Downtown Circulation Plan	27
F. Entryways and Signage	34
G. Economic Redevelopment	36
H. Greater Downtown Area Parks	38
I. City-owned Jarvis Property	40
APPENDICES	43
A. Future Land Use Map	44
B. Zoning Map	45
C. Circulation Plan	46
D. Development Concepts	48
E. Public Participation Process	54

1. STUDY AREA CONTEXT

The Greater Downtown planning area is comprised of three sub-districts: the Downtown, Rail and River Districts. The areas are distinctly different due to their location, influences and historic development but complement each other to form a developed area that is central to the community with uses that range from single family residential to rail-oriented heavy industrial.



2. PLANNING BACKGROUND

A Strategic Downtown Master Plan (SDMP) that encompassed the original square mile was developed through the Downtown Development Authority (DDA) and the guidance of a steering committee of interested downtown merchants, property owners and policymakers during 2007-2008. The SDMP defined an overall vision and goals for downtown and included implementation strategies such as a zoning overlay.

The vision of the SDMP addressed: the transportation network and other infrastructure, introduction of green treatments, creation of design standards and strategies for community marketing and promotion efforts. The goals of the SDMP were to:

- Maintain and enhance the economic, cultural and social vitality of downtown
- Promote downtown living by providing a wide range of housing opportunities
- Enhance the transportation system to accommodate automobiles, bikes and pedestrians, and provide adequate, convenient parking
- Stabilize and enhance the historic residential neighborhoods
- Establish and promote a unique identity
- Jump-start the revitalization and reinvestment in the downtown area with strategic catalyst projects

The Strategic Downtown Master Plan was considered by City Council on September 14, 2009 but, due to pending adoption of the Grand Junction Comprehensive Plan (Comprehensive Plan), Council voted to continue the SDMP to an unspecified future date.

A South Downtown Neighborhood Plan (South Downtown Plan) encompassed the area between the railroad tracks and the Colorado River and the Riverside neighborhood on the west to 28 Road on the east. A plan for the area was developed from 2006-2008 with 15 community focus group meetings, 3 public open houses with 80-100 people in attendance at each open house. The South Downtown Plan included an existing conditions analysis, goals and implementation including a circulation and trails plan, economic development strategies, rezoning some properties and zoning overlay. The goals of the South Downtown Plan were to:

- Create, maintain and enhance a green waterfront
- Recognize existing heavy industry and rail service that supports it
- Recognize the distinction between “industrial” streets and “public” streets
- Promote higher quality, “cleaner” uses in the area generally between 7th and 9th Streets
- Improve entry points and connections to downtown
- Increase light industrial opportunities
- Create areas of mixed uses to screen and transition to the heavy industry
- Create and enhance redevelopment opportunities and partnerships

The South Downtown Neighborhood Plan was considered by City Council on June 16, 2008 but was not adopted.

The planning process for the Greater Downtown Plan reanalyzed the two previous planning efforts and made revisions as conditions have changed, included areas that had not been covered by either of those plans, and integrated them into a single plan for the downtown area. In addition, the Greater Downtown Plan incorporates elements of the Downtown Development Authority’s potential projects in order to support the DDA’s Downtown Plan of Development, as well as incorporating elements of the Comprehensive Plan discussed below.

3. COMPREHENSIVE PLAN

The Grand Junction Comprehensive Plan is based on extensive public input that identifies what kind of community we want to have and identifies ways to achieve our vision. It charts the course to help us become the most livable community west of the Rockies. It establishes a vision that focuses the community on what it should do to sustain the quality of life that all residents desire and expect. The

Comprehensive Plan establishes the following guiding principles that will shape growth, all of which apply to development of the Greater Downtown area.

A. Concentrated Centers – The Plan calls for three types of centers; the City Center, Village Centers and Neighborhood Centers.

B. Sustainable Growth Patterns – Fiscal sustainability where we grow efficiently and cost-effectively. Encourage infill and redevelopment.

C. Housing Variety – Allow, encourage more variety in housing types that will better meet the needs of our diverse population.

D. A Grand Green System of Connected Recreational Opportunities – Take advantage of and tie together the exceptional open space assets of Grand Junction, including the Colorado River, our excellent park system, trails and our surrounding open spaces.

E. Balanced Transportation – Accommodate all modes of transportation including air, transit, freight, auto, bike and pedestrian.

F. A Regional Center – Preserve Grand Junction as a provider of diverse goods and services and residential neighborhoods.

Specific policies within the Comprehensive Plan further support the concepts of the Greater Downtown Plan as outlined below.

Goal 4. Support the continued development of the downtown area of the City Center into a vibrant and growing area with jobs, housing and tourist attractions.

Goal 5. To provide a broader mix of housing types in the community to meet the needs of a variety of incomes, family types and life stages.

Goal 6. Land use decision will encourage preservation of existing buildings and their appropriate reuse.

Goal 8. Create attractive public spaces and enhance the visual appeal of the community through quality development.

Goal 9. Develop a well-balanced transportation system that supports automobile, local transit, pedestrian, bicycle, air and freight movement while protecting air, water and natural resources.

4. SITE ANALYSIS

A. Downtown District

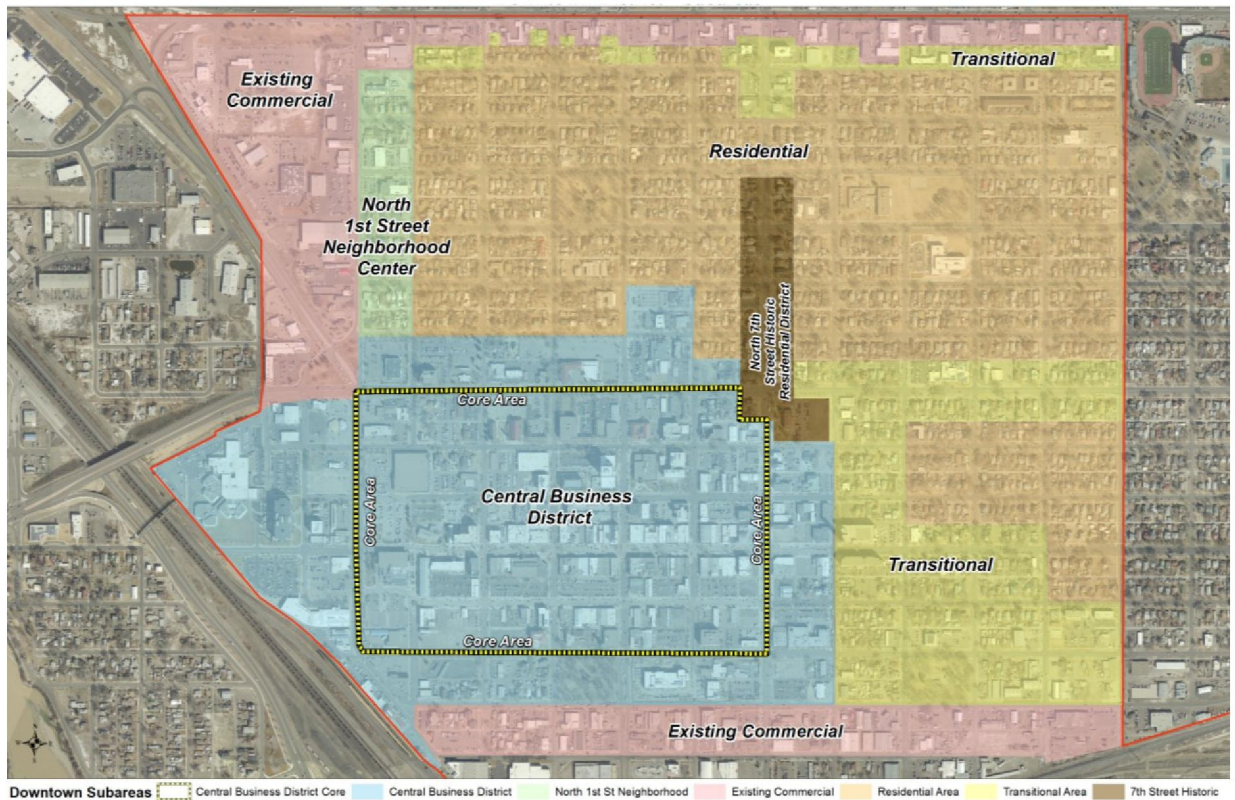
The Downtown District encompasses the original square mile, the area south of North Avenue between 1st Street and Highway 6 & 50, [a small triangle of properties north of Grand Avenue and west of Highway 6 & 50](#), and the area south of Grand Avenue between 1st Street and the railroad tracks (see map on following page).

Downtown District

Map showing the Downtown District in San Francisco, bounded by Market Street to the north, the Embarcadero to the east, and the Golden Gate Bridge to the west. The map displays a grid of streets, including Market Street, Broadway, California Street, and the Embarcadero. The district is highlighted in yellow, and the surrounding areas are in shades of gray. The map is overlaid with a grid of red lines representing street boundaries.

- On average, urban residents spend a greater percentage of household income on retail expenditures, particularly on items such as apparel and food away from home. This indicates an opportunity for additional specialty retail and entertainment space in the Downtown District.
- Housing within the Downtown District has been and is expected to continue to increase in density with smaller households comprised of young and old, and moderate and lower-income residents. However, with a growing concentration of middle-aged, moderate- to high-income households in the City as a whole, there is an opportunity for the Downtown District to attract more diverse, higher-income resident base. The entirety of Grand Junction (urban and fringe) faces a growing shortage of quality affordable housing for its very low- and moderate-income residents, as well as working-wage families. This planning process emphasized the need for a set of strategies tailored specifically to the housing challenges present within the Downtown District.
- Downtown commercial vacancy and rental rates are approaching levels required to support new development and/or redevelopment. However, “seed” money will likely be necessary to leverage private investment in projects that will catalyze reinvestment activity throughout the Downtown District.
- Among the higher growth employment sectors in Mesa County are service industries often consisting of small businesses. This represents an opportunity for the Downtown District to develop not only additional live/work units, but also to promote the adaptive re-use of historically significant buildings and less traditional spaces including former church facilities.
- Forecasts indicate that more than one million square feet of employment space (office), more than 1.6 million square feet of retail space and nearly 1,100 residential units could be absorbed in the market over the next ten years, from which the Downtown District could benefit. The degree to which the Downtown District is able to capture new demand within the trade area and beyond will be a function of the redevelopment process itself. Redeveloping key catalyst areas as residential, retail, employment and community destinations will necessarily increase its ability to capture not only a greater share of trade area demand, but also to reach beyond those boundaries.

- The Downtown District is comprised of many uses that result in a number of diverse neighborhoods. While some areas within downtown are comprised of one single use, such as residential or business/commercial, there are many areas that include a range of uses, such as along Grand Avenue. In order to more fully understand specific issues facing the many, diverse neighborhoods within downtown, the Downtown District was divided into seven subareas: Central Business District (CBD), Central Business District Core Area (Downtown Core), Transitional, Residential, North 1st Street Neighborhood Center (Neighborhood Center), Existing Commercial, and the North Seventh Street Historic Residential District.



Downtown District Subareas

Central Business District (CBD). The CBD contains primarily commercial uses and is located generally south of Ouray Avenue. The CBD also contains a significant number of public and civic facilities. While much of the CBD is zoned to permit a wide range of uses, there are several new projects that have integrated uses into one cohesive project. Pedestrian accessibility and amenities, public facilities, traffic patterns, building heights and commercial development densities combine to create an urban character that attracts local businesses, employees, residents and visitors to the CBD. Within the CBD, the Downtown Core includes the streets and development patterns that most depend on and are defined by the heaviest pedestrian activity.

Transitional Subarea. The Transitional areas of downtown contain a mix of residential, commercial and institutional uses. Development densities and



Main Street Character

building heights are not as extreme as in the CBD and residential uses, which includes single family homes adapted to multifamily uses, are prevalent throughout the Transitional areas. With the exception of the main arterial Grand Avenue, traffic is less congested and parking is available, both on-street and in publicly- and privately-owned surface parking lots.

Existing homeowners in the Transitional areas, especially along Grand Avenue, expressed concern over the rapidly expanding commercial character of the area. Traffic congestion, parking issues, vehicle accessibility, lighting and signage were each cited as an adverse impact that existing single family residential homeowners were grappling with as more homes are converted to business uses.



New Infill Development Building on Right



Residential Subarea Character

Residential Subarea. A significant amount of the Downtown Core contains single family residential uses. The large number of older, historic structures results in a definitive architectural character that local residents want preserved. The North Seventh Street Historic Residential District contains 34 historic homes with a high concentration of early Twentieth Century architectural styles and construction methods. The district covers five blocks of North Seventh Street and is the most intact historical residential area in Grand Junction with a variety of Queen Anne, Colonial Revival and Mission style homes.

[A detailed analysis of ,and specific guidelines and standards applicable only to, properties within the North Seventh Street Historic Residential District were adopted in 2012 and thus, the District is not further addressed in the Greater Downtown Plan.](#)

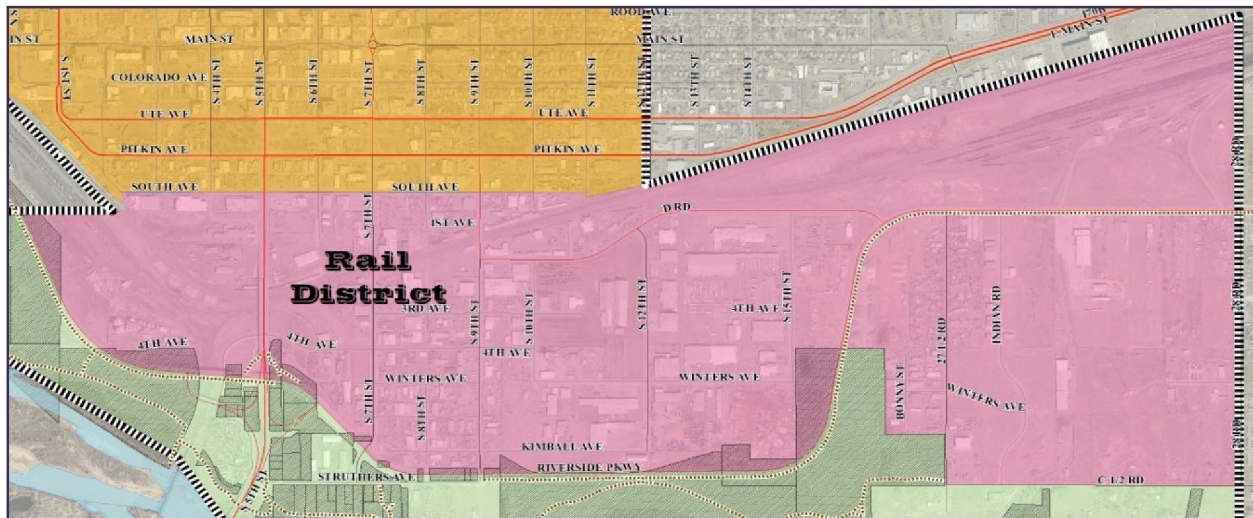
Along the edges of the downtown residential areas, commercial activity is encroaching on established single-family residential neighborhoods. Although existing residents expressed their satisfaction with existing development and uses along North 1st Street and North Avenue, many participants expressed concern over future expansion and potential rezoning along these edges and wanted to see the Greater Downtown Plan respect existing zone districts and development patterns and prohibit any further rezoning or variances.

North 1st Street Neighborhood Center. This subarea of the Downtown District is an extension of the Neighborhood Center defined along the North Avenue corridor. The intent of the Neighborhood Center is to provide for limited employment, residential, open space and limited retail uses that primarily provide convenience to the immediate neighborhood. While much of the area is already developed with commercial area, there is a need to create a better transition between the commercial uses facing North 1st Street and the side streets to the residential areas on the east side of North 2nd Street.

Existing Commercial. This subarea of the Downtown District corresponds with developed commercial corridors on the north/northwest and south sides of the District that have a different character than the type of uses that exist in the other subareas.

B. Rail District

The Rail District is the area bounded by South Avenue and the railroad tracks to the north, Riverside Parkway and C-1/2 Road along the west and south and 28 Road on the east. The man-made framework of the area consists of the buildings and infrastructure that already exist, some of which is very old, and some of which was recently constructed. The railroad tracks along the northern portion of the Rail District as well as the sidings that come into the area are very important considerations. Several spurs continue to be heavily used by the industry in the area, while others have been abandoned but may be available for future use. The former Grand Junction Steel site, located in the center of the Rail District historically received three to four rail cars per week of raw material and finished products were then trucked from the



site. Castings, Inc. handles approximately 480,000 pounds of materials and products in containers by rail each week. Thus, as in the past and as long as it continues to be used as a means of transport, the railroad is a valuable asset to this area and to the community as a whole.



Existing Sidings in the Rail District

The existing street network in the Rail District is incomplete. The plan for the area addresses how the existing streets should be used and how new streets should be planned in the eastern and western ends of the Rail District to continue to provide and enhance access for many modes of transportation to, from and through the area. Presently, 9th, 12th and 15th Streets are the primary north-south streets utilized by the heavy commercial and industrial uses in the area. 7th Street is generally perceived by the community as the “public” access to and from the Rail District and points south. Certainly, the completion of the Riverside Parkway through and along the southern edge of the Rail District has had a major impact on the area with a positive influence.

Part of the existing conditions of the built environment is the pattern of land ownership and use. In the central part of the Rail District there are numerous small parcels. Some have been aggregated into large holdings such as for the larger industries in the area including the former Grand Junction Steel site, Whitewater Building Materials Castings, Inc. and Munroe Pump. The railroad has large landholdings in the area as do various public entities. City-owned properties primarily include remnant parcels that were acquired for construction of the Parkway. Mesa County recently disposed of several small parcels and the State of Colorado has the Department of Transportation complex on the east side of 9th Street and D Road. To the east, the parcels are larger but some are not configured very conducive to development. There are also still remnants of the early days of this area. There are some remaining pockets of occupied single family residential homes.

There are also a few isolated commercial structures with historic significance, most notably the one pictured below which is a remnant of a sugar beet factory complex. The building was previously most visible from the riverfront trail and Orchard Mesa but it is now very visible from the Riverside Parkway. As this area becomes more familiar to people passing through on the trail and on the Parkway, perhaps some of the buildings like this one can become a more integral part of redevelopment.



Existing Single Family Residence

It is estimated that there is a daytime population of over 1,000 employees in the Rail District that have very little available in the way of goods, services, restaurants and other commercial uses in close proximity. There appears to be a need and a desire to promote and develop uses that could not only service the daytime working population but also support the recreational and park users in evenings and on weekends.



Remnant Building from Sugar Beet Factory



Existing Business on South 7th Street

There are surrounding influences that impact the Rail District such as the proximity of the area to the Downtown District. This plan considers the main entrances and connections to the other Districts within the Greater Downtown area and how they can be improved as well as determine if or where there are barriers such as the railroad crossing and how those can be mitigated.

An inventory of existing land uses within the Rail District was completed as part of the planning process, identifying patterns of development and architectural character. Existing land use in the more developed central area of the Rail District is and has historically been primarily heavy commercial and industrial with remnant pockets of residential. The area between 5th Street and 9th Street is characterized by smaller parcels with older structures. A few businesses have aggregated parcels into larger parcels which are more conducive to future improvement and/or redevelopment. This area presents the best opportunities within the Rail District for redevelopment that could be accomplished through creative incentives and partnerships.

The heavy industries are primarily located between 9th and 15th Streets, clustered around the existing railroad spurs on larger parcels. Since the rail and its users are valuable assets to the area and the community as a whole, the core of this area is unlikely to change. However, there appears to be some opportunity and community support to create transitional areas of varied land uses in a tier surrounding the core industrial area. These transitional areas can be used to create compatibility between adjacent uses such as the park and the heavy industrial as well as help visually screen the industrial areas.



Existing Heavy Industrial Uses

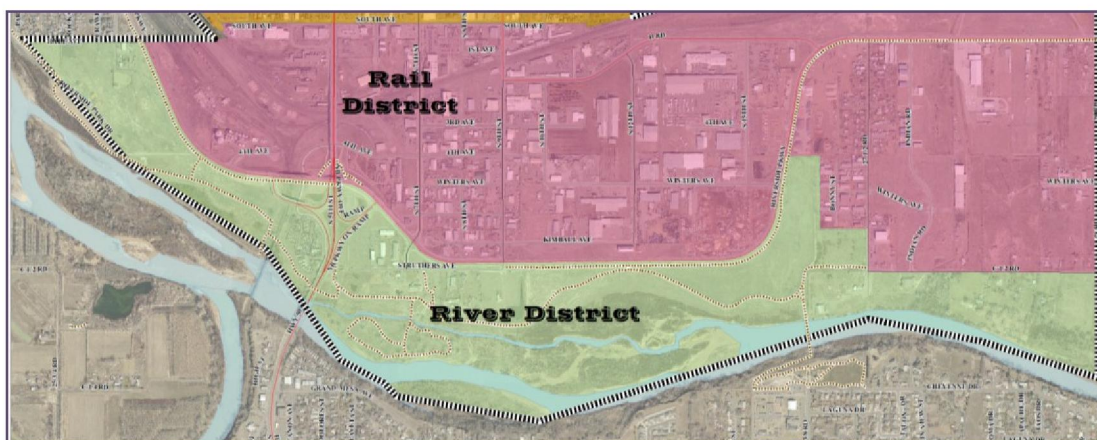
On the eastern side of the Rail District, much of the property is held in larger ownerships and is vacant or underutilized. There is also a small pocket of existing residential use along 27-1/2 Road just south of the Riverside Parkway but which have been zoned for future commercial/industrial uses for many years. Much of the eastern area presents the greatest opportunity for increasing heavy commercial and industrial use within the Rail District and the Greater Downtown area as a whole.

C. River District

The River District is located on either side of the confluence of the Gunnison and Colorado Rivers, roughly between the Riverside neighborhood to the northwest to 28 Road on the east and the Riverside Parkway to the Colorado River. At this “grand junction”, the area transformed from an agricultural based community into the commercial and industrial area it is today. Over time, the area has been used to store mill tailings along the river, process sugar beets in the historic beet packing complex, and more recently the community has rediscovered its natural value for green space, trails and public uses such as the

Botanic Gardens. The majority of the property in the River District is publicly owned including the Las Colonias Park site, the Botanic Gardens property, the Jarvis property, some remnants of land that were acquired for construction of the Parkway and State Parks properties on the east end of the District.

There is an existing trail through the area that must be recognized as the area develops and redevelops. The plan considers that the trail will eventually be extended to the east along the river and that improved public access to the trail system throughout the area is a necessity.



Located within walking distance from the other downtown areas, the River District offers both easy access to recreational amenities along the river, as well as convenient access to shopping and businesses in the adjacent Central Business District. It also functions as a gateway into downtown from Highway 50. Its location and context establishes it as one of the most important places in the City.

The River District is a critical area of the community. It virtually is THE Grand Junction. The Colorado River has a big influence on the area which presents both constraints and opportunities. The floodplain associated with the Colorado River has been altered by construction of a levee that protects a large part of the River District from flood inundation but there are some areas that are still impacted by potential flooding of the Colorado River.



Botanical Gardens

On the other hand, the Colorado River does present excellent opportunities to maintain and enhance amenities that have already been placed along the Colorado River including the Botanic Gardens, the riverfront trail system, the Old Mill pedestrian bridge and the community investment of the Riverside Parkway

The proximity of the area to Orchard Mesa influences the plan both physically and visually. There are also recreation and open space uses within and nearby – Eagle Rim Park on Orchard Mesa and the Botanic Gardens along the Colorado River. The Greater Downtown Plan addresses how these spaces should be connected, enhanced and integrated into redevelopment of the area.

The topography of the site is also an important consideration. While the River District area itself is flat, it is significantly lower than Orchard Mesa to the south. This makes it a very visible area as well as presents some unique opportunities for views and vistas.

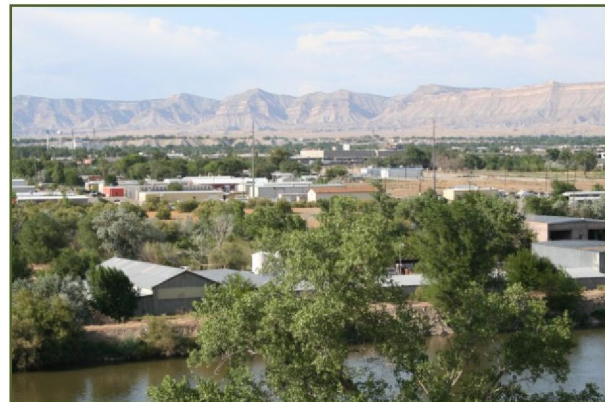
In addition, the plan considers the planning efforts that have been completed for areas within the River District including the Botanical Gardens, Las Colonias Park and the City-owned Jarvis property. The Greater Downtown Plan integrates with the adjacent uses to the east that were included in the Pear Park Neighborhood Plan.



Views of the River District from Eagle Rim Park

Several design concepts have been developed for the Las Colonias Park site which lies south of the Riverside Parkway (Parkway) from 9th Street to 27-1/2 Road. A trail has been constructed through the property and now that the Parkway has been in use, there is renewed interest in developing a more specific plan. Some amenities being considered are parking, restrooms, shelters, play area, open space, expanded walk/bike trails, open space for festivals, music, and educational presentations, various wetlands and arbor education opportunities, a kayak park, tree walk and a disc golf course.

The Jarvis property was historically used as a salvage yard until purchased and cleaned up by the City in the late 1980s. Since then, the property has been vacant with the exception of the recent trail construction through the site. Other uses on the west end of the River District are industrial along the south side of the railroad tracks. A mixed use conceptual plan has been developed for the Jarvis property which was considered through the development of the Greater Downtown Plan.



Views of the Rail and River District from Eagle Rim Park

There are areas of low density residential uses along the south side of C-1/2 Road just west of 28 Road. These uses are intended to remain, along with future development of the riverfront trail system from 27-1/2 Road to 28 Road. The Colorado State Parks has acquired several properties in this area in anticipation of continued trail development.

5. GREATER DOWNTOWN GOALS AND POLICIES

City staff held a series of meetings with property owners, including elected City officials, representatives of large industries, economic redevelopment interests and owners of small businesses and properties. The meetings were conducted as informal brainstorming sessions in order to define more specific issues,

constraints and opportunities and continue to discuss the community's vision for the Greater Downtown area. A more detailed outline of the public planning process is included as Appendix E.

In addition to these meetings, two public open houses were held (December 2011 and February 2012) and questionnaires and information were available on the City's web site for several weeks to invite similar input from other property owners, tenants and citizens at large. Participants were asked to evaluate various design and planning concepts to determine which of these are most important to the community and should be addressed in greater detail in the Plan. The concepts addressed four major topics relative to an area plan: land use, circulation, economic (re)development and visual character. The results of the evaluations and comments gathered on the concepts were tabulated as included in Appendix E. The results, along with previous information for the CBD, show strong community support for ideas that are translated to the goals and policies listed below for the Greater Downtown Plan.

A. Area-Wide Goals and Policies

Goal 1: Enhance the transportation system to accommodate automobiles, bikes and pedestrians, and provide adequate, convenient parking.

Policy 1a: Street design will accommodate travel lanes, parking, bike lanes, medians, sidewalks, and street trees, appropriate to and complementary of the adjacent land use.

Policy 1b: Street design will achieve a balance between travel mobility, land use access and livability and improve connections to the Greater Downtown area and the connections between subdistricts.

Policy 1c: Emphasize "walkability" of the downtown area through street design that is pedestrian friendly to provide a foundation for a safe, active and livable area, including sidewalks, accessibility improvements, bicycle facilities, off-street trail connections and safe crossings, where appropriate.

Goal 2: Establish and improve entry points into the Greater Downtown area (refer to the Wayfinding and Signage Map on page 31).

Policy 2a: Street and streetscape design will include signage, landscaping and other design elements to delineate appropriate entry points into Greater Downtown.

Goal 3: Promote downtown living by providing a wide range of housing opportunities, primarily in the Downtown District.

Policy 3a: Support a regional housing strategy with an emphasis on infill, downtown housing.

Policy 3b: Promote development patterns and regulations that accommodate vertical mixed-use development, primarily in the Central Business District.

Goal 4: Redefine the land use along key corridors to provide a mix that will offer the most opportunities for redevelopment and revitalization.

Policy 4a: Define subareas and corridor areas for groupings of land uses that are complimentary to the rest of the Greater Downtown area.

Policy 4b: Mixed uses, including residential, will be encouraged in appropriate subareas and corridors.

B. Downtown District Goals and Policies

Goal 1: Maintain and enhance the economic, cultural and social vitality of the Downtown District.

Policy 1a: Define subareas and corridor areas for groupings of land uses that are complimentary to the rest of the Greater Downtown area.

Policy 1b: Implement infill and redevelopment policies that support downtown.

Policy 1c: Encourage a wide mix of uses, offering retail and commercial services at ground level and business/office/residential on upper floors in all but residential areas.

Policy 1d: Maintain and expand public amenities and services in the Downtown District.

Policy 1e: Enhance and preserve Whitman and Emerson Parks to integrate the space into the downtown fabric and encourage use by the community.

Policy 1f: The City with assistance from the Downtown Development Authority will explore the alternative street configuration to relocate the one-way couplet of streets that are currently Ute and Pitkin Avenues to utilize Pitkin and South Avenues for this purpose.

Policy 1g: Study alternatives ~~The study of the one-way couplet will include an analysis of alternatives~~ for 4th and 5th Streets including returning these streets to the 2-way grid system between Ute Avenue and North Avenue.

Policy 1h: Within the CBD, encourage shared parking, discourage single-use, surface parking and minimize surface parking in the CBD and develop new means of paying for shared parking (e.g. develop a fee in lieu of required on-site parking that will be used to fund shared parking structures).

Goal 2: Require density/intensity in the Downtown District as prescribed by the Comprehensive Plan, primarily within the Central Business District.

Policy 2a: Strengthen means of implementation that promote vertically mixed-use structures, primarily within the CBD.

Policy 2b: Require minimums in height and density/intensity for new development in the CBD.

Policy 2c: Require minimal or no building setbacks within the Downtown Core to maximize site intensity/density.

Goal 3: Develop a pedestrian-oriented, walkable Downtown Core.

Policy 3a: Prohibit uses on ground level that do not support pedestrian activity.

Policy 3b: Require building façade details that activate the ground floor, particularly on corner buildings to activate north-south streets.

~~**Policy 3c:** Within the CBD, encourage shared parking and discourage single-use, surface parking.~~

Goal 4: Stabilize and enhance the Residential Subareas.

Policy 4a: Discourage further encroachment of non-residential uses into the established residential neighborhoods.

Policy 4b: Establish design standards to address conservation and enhancement of the residential development patterns and streetscape.

Policy 4c: Establish design standards for the Transitional Subareas to emphasize use and development compatibility with adjacent residential areas.

Policy 4d: Promote the establishment of neighborhood watch and neighborhood organizations.

Goal 5: Recognize and promote opportunities to build subareas/neighborhoods, each with a unique identity.

Policy 5a: Develop a set of guidelines within each subarea to address building and façade design, streetscape, landscape and other elements of site development.

Goal 6: Jump-start the revitalization and reinvestment in the Downtown District with strategic catalyst projects.

Policy 6a: Plan and budget for strategic property acquisition for future development.

Policy 6b: Identify locations for and promote concepts of catalyst projects, including public building/housing/mixed use, live/work units, mixed-use retail/residential and mixed-use retail/office.

C. Rail District Goals and Policies

Goal 1: Preserve the opportunity for heavy industry and rail service that supports it.

Policy 1a: The City will maintain industrial zoning in those areas served by rail lines and sidings.

Goal 2: Recognize distinction between “industrial” streets such as 9th and 12th Streets and “public” streets 7th Street and Riverside Parkway.

Policy 2a: Develop street sections that reflect the differences in development patterns along and the use of the street.

Policy 2b: Discourage industrial and commercial traffic from using C-1/2 Road and direct this traffic to travel north and east using the Riverside Parkway. ~~In as much as possible, encourage~~

~~traffic generated from the eastern area of the Rail District to travel north and then east rather than directly east through the low density residential areas.~~

Goal 3: Promote higher quality, customer and pedestrian friendly development along 7th Street and Riverside Parkway.

Policy 3a: Implement design guidelines and standards along corridors that will result in site and building design improvements along the corridors.

Goal 4: Re-establish and improve a street grid in the Rail District.

Policy 5a: The City will implement the Grand Valley Circulation Plan in the Rail District, including construction of new streets as development occurs in the Industrial and Commercial/Industrial areas.

D. River District Goals and Policies

Goal 1: Create/maintain/enhance a green waterfront

Policy 1a: Take advantage of and create opportunities and partnerships to enhance the riverfront trail system.

Policy 1b: Take advantage of and create opportunities and partnerships to develop Las Colonias Park and open space areas within the Jarvis property.

Goal 2: Create retail, general commercial and mixed use opportunities that complement the uses along the riverfront.

Policy 2a: Utilize zoning, overlay districts and incentives for development and redevelopment of complimentary uses.

Goal 3: Create/enhance redevelopment opportunities and partnerships

Policy 3a: The City will work with the Downtown Development Authority to expand its boundaries.

Policy 3b: The City will consider implementation of incentive strategies for redevelopment.

Policy 3c: The City will consider redevelopment opportunities for the Jarvis property including the potential for public-private partnerships.

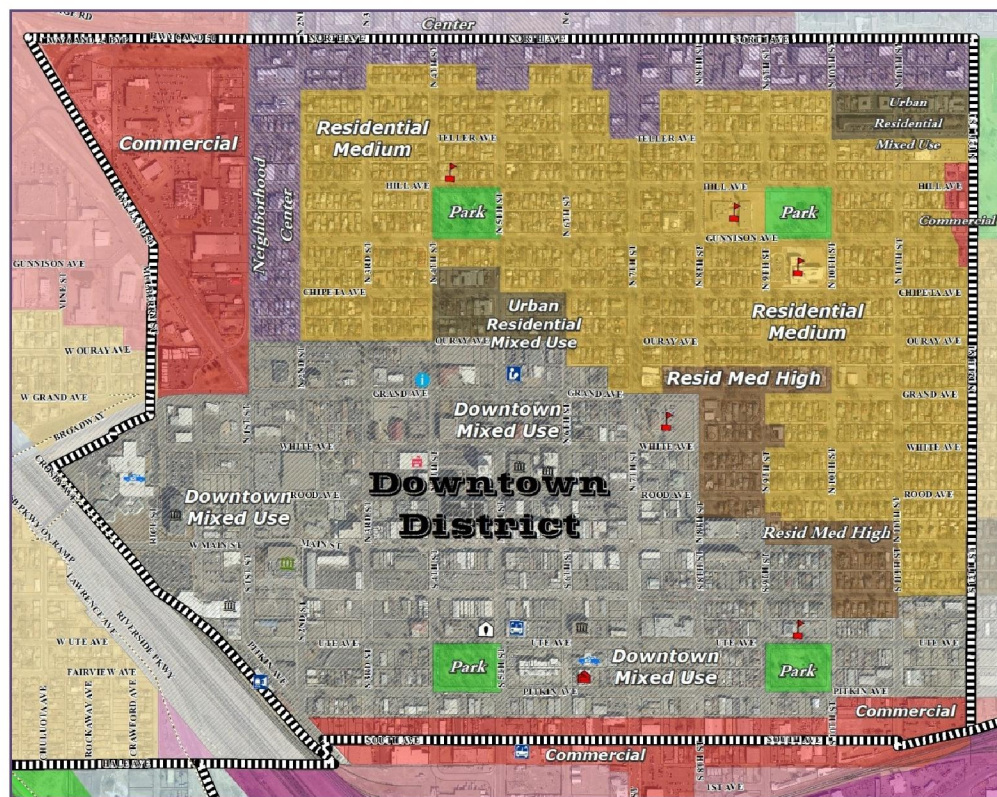
6. IMPLEMENTATION STRATEGIES

The City has a variety of tools available through which these goals can be implemented so that the vision for Greater Downtown can materialize and eventually be realized. This Plan represents the first phase of implementation and includes the basic implementation strategies of designating Future Land Use designations, zoning properties accordingly as needed, amending development standards of the zoning

districts through a zoning overlay and establishing goals and policies for future phases of plan implementation such as economic (re)development strategies.

A. Downtown District Future Land Use and Zoning

Future Land Use. The Downtown District is formulated around seven general land use designations: Commercial, Downtown Mixed Use, Neighborhood Center Mixed Use, Urban Residential Mixed Use, Residential Medium High Density, Residential Medium Density and Parks and Open Space. These designations correspond with those established in the City-County Comprehensive Plan and are illustrated on the map below. A more detailed description of the land use designations may be found in the Comprehensive Plan. [The Blended Land Use Map that is included in the Comprehensive Plan and allows for increases in residential density in compatible areas does not apply to properties within the Downtown District. This is due to the strong public sentiment in the Downtown District residential neighborhoods that the existing historic character of the single family development be protected.](#)



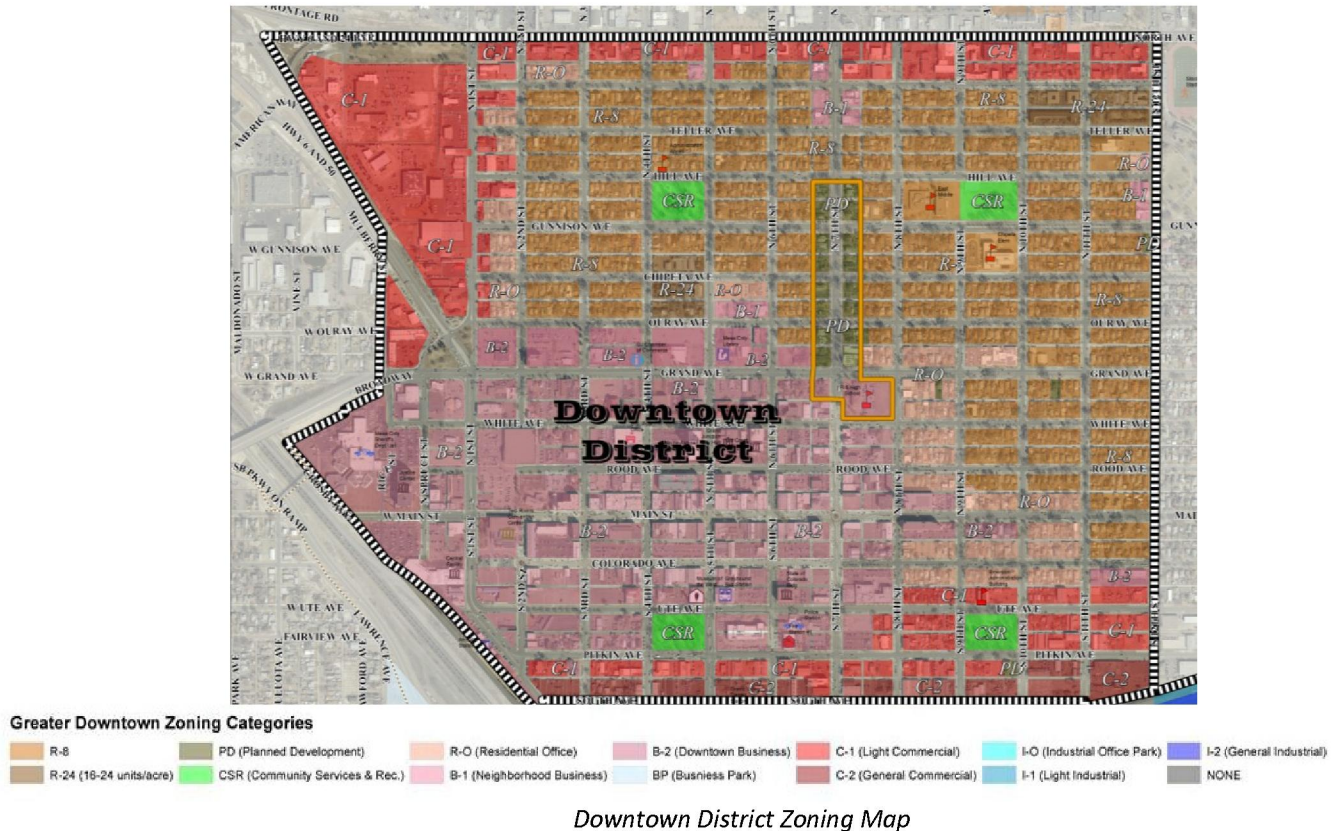
Greater Downtown Future Land Use Categories

Neighborhood Center (NCMU)	Commercial (COM)	Residential Med High (RMH)	Conservation (CON)
Downtown Mixed Use	Industrial (IND)	Residential High Mixed Use or Resid. Med High	Urban Residential Mixed Use (URMU)
Business Park Mixed Use	Commercial Industrial (CI)	Park (PK)	

Downtown District Future Land Use Plan

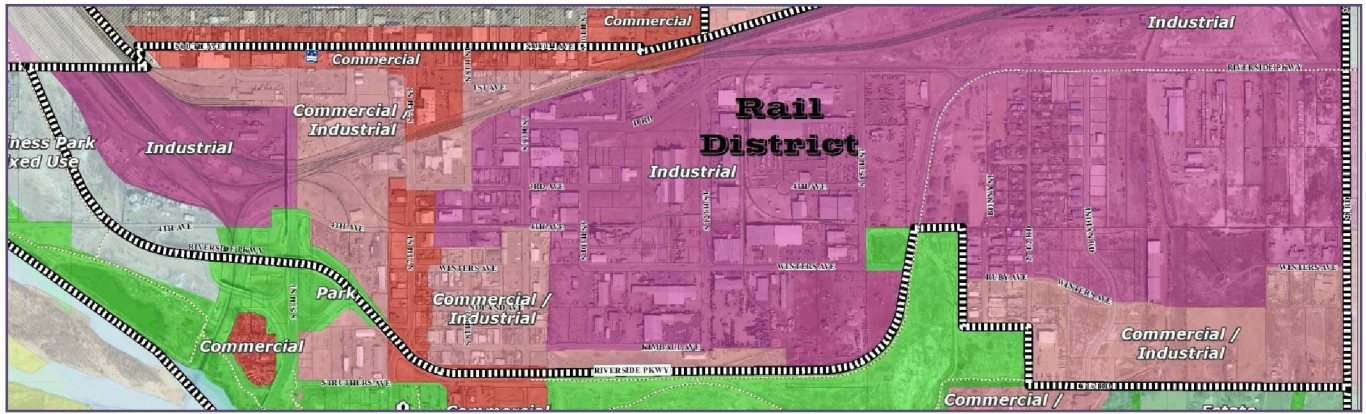
Zoning. Proposed zoning within the Downtown District is shown on the map on the following page and includes the following zone districts: Residential 8 units per acre (R-8), Residential 24 units per acre (R-24), Residential Office (R-O), Neighborhood Business (B-1), Downtown Business (B-2), Light Commercial (C-1), General Commercial (C-2), and Community Services and Recreation (CSR). Generally, the zoning is

proposed to remain the same as currently exists. A more detailed description of the zone districts within the Downtown District may be found in the Zoning and Development Code.



B. Rail District Future Land Use and Zoning

Future Land Use. The Rail District is shown on the map on the following page and is formulated around five general land use designations: Business Park Mixed Use, Commercial, Commercial Industrial, Industrial and Parks and Open Space. These designations correspond with those established in the City-County Comprehensive Plan. A more detailed description of the land use designations may be found in the Comprehensive Plan.



Greater Downtown Future Land Use Categories

Neighborhood Center (NCMU)	Commercial (COM)	Residential Med High (RMH)	Conservation (CON)
Downtown Mixed Use	Industrial (IND)	Residential High Mixed Use or Resid. Med High	Urban Residential Mixed Use (URMU)
Business Park Mixed Use	Commercial Industrial (CI)	Park (PK)	

Rail District Future Land Use Plan

Zoning. Proposed zoning within the Rail District is shown on the map below and includes the Light Commercial (C-1), General Commercial (C-2), Community Services and Recreation (CSR), Business Park Mixed Use (BP), Industrial/Office Park (I-O), Light Industrial, and General Industrial (I-2) zone districts. The majority of the zoning is proposed to remain the same as currently exists. A more detailed description of the zone districts within the Rail District may be found in the Zoning and Development Code.



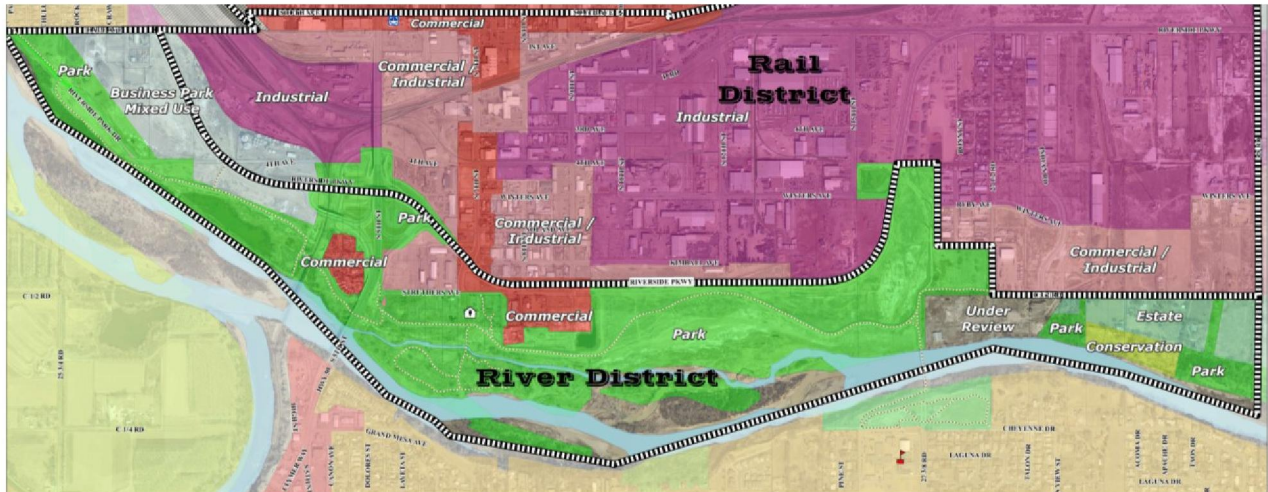
Greater Downtown Zoning Categories

Proposed Zoning Changes	B-2 (Downtown Business)	C-1 (Light Commercial)	I-O (Industrial Office Park)	I-2
R-O (Residential Office)	BP (Business Park)	C-2	I-1 (Light Industrial)	CSR (Community Services & Rec.)

Rail District Zoning Map

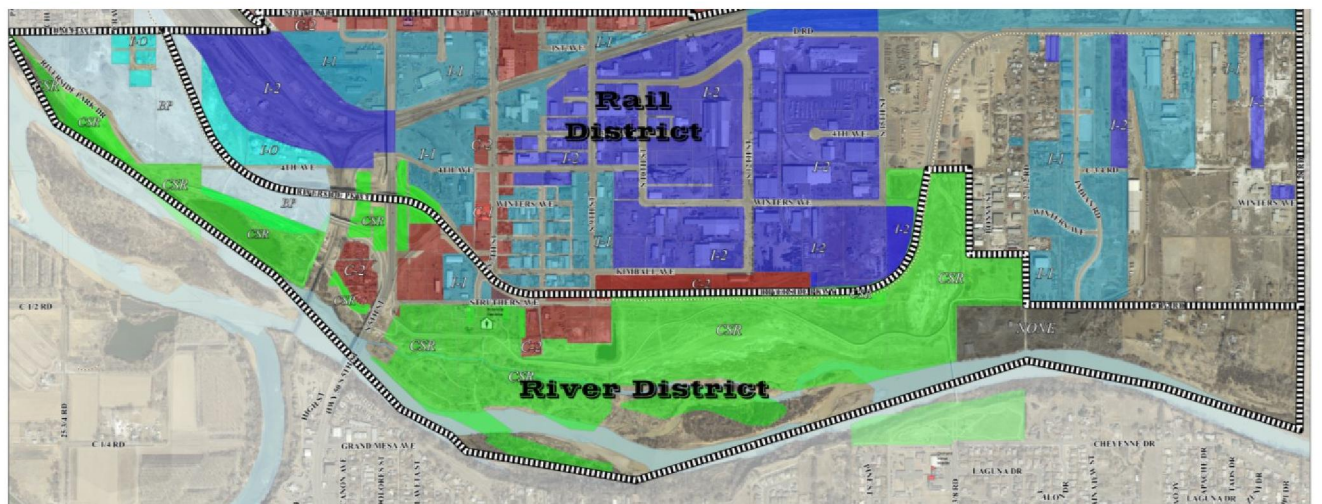
C. River District Future Land Use and Zoning

Future Land Use. The River District as shown on the map on the following page is formulated around six general land use designations: Commercial, Commercial Industrial, Parks and Open Space, Conservation, Estate and Business Park Mixed Use. These designations correspond with those established in the City-County Comprehensive Plan. A more detailed description of the land use designations may be found in the Comprehensive Plan.



River District Future Land Use Plan

Zoning. Proposed zoning within the River District is shown on the map on the below and includes the following zone districts: Light Commercial (C-1), General Commercial (C-2), Community Services and Recreation (CSR), Business Park Mixed Use (BP), Industrial/Office Park (I-O), and Light Industrial (I-1). The majority of the zoning is proposed to remain the same as currently exists. A more detailed description of the zone districts within the River District may be found in the Zoning and Development Code.



River District Zoning Map

D. Development Standards

The community desires to improve the visual character of the Greater Downtown areas that are most visible along major public corridors or from the major public spaces. Consequently, the Plan promotes a higher quality built environment through improved architectural character, reduced visual clutter and enhanced streetscape. These elements are addressed through the Greater Downtown Zoning Overlay detailed in a companion document to this Plan. The elements of the overlay are intended to augment the zoning district standards in the Zoning and Development Code.

E. Conceptual Plan for Traffic Circulation in Greater Downtown

The Grand Valley Circulation Plan adopted by the City of Grand Junction and Mesa County serves as the traffic circulation plan for the Greater Downtown area. The following conceptual plan for traffic circulation in Greater Downtown describes the policies for streets and corridors in the Greater Downtown, depicts preferred conceptual designs and proposes additional unclassified streets which support those policies. This conceptual plan for circulation does not modify the Grand Valley Circulation Plan, but provides preferred concepts and designs for future consideration and development of public rights-of-way.

Policies:

- a. Conduct a more detailed traffic analysis of the area to determine needed intersection control, street cross-sections, pedestrian and vehicle facilities, additional streets and access control, and amend the Grand Valley Circulation Plan accordingly.
- b. Continue the review process for the Grand Valley Trails Plan to determine needed pedestrian and bicycle transportation facilities such as trails, pedestrian paths and bike lanes, and amend the Grand Valley Trails Plan accordingly.

In the Grand Valley Circulation Plan, urban streets are classified according to their function in the transportation network. The two components of function of the Grand Valley Circulation Plan are (1) to provide access to properties and (2) to carry traffic safely and efficiently from point to point. In order to preserve safety and capacity and to enhance the quality of living, these two components should be inversely related, such that busier streets have limited access (resulting in more efficient flow) and quieter streets provide access to properties.

Major Street Corridors. The components of the major street system are identified on the Grand Valley Circulation Plan. These corridors primarily function as carriers of traffic from point to point. The conceptual circulation plan in the Greater Downtown Plan includes proposed unclassified roadways shown in yellow on Appendix C.

As properties develop and redevelop within the greater downtown area, the Grand Valley Circulation Plan will be implemented through construction or improvement of streets warranted by the proposed development, based on adopted street cross sections in the Transportation Engineering Design Standards (TEDS). The conceptual circulation plan in the Greater Downtown Plan provides preferred alternatives for specific design and layout of certain streets, corridors and intersections in the planning area. For example, in commercially-zoned areas, the Commercial Street cross-sections of TEDS will apply, but modifications may be made according to the conceptual circulation plan illustrated and described here.
[street cross-sections, illustrations and text]

E. Greater Downtown Circulation Plan

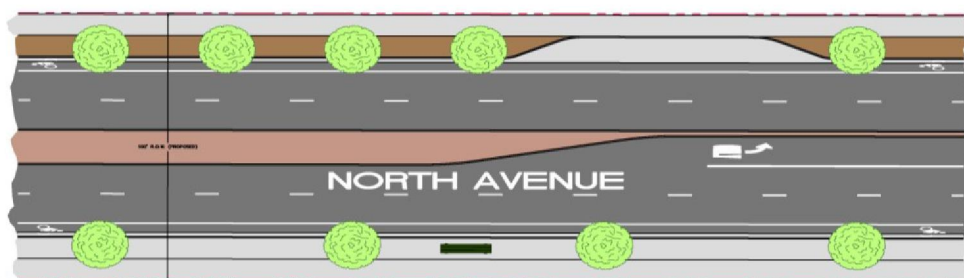
The Grand Valley Circulation Plan that serves as the Circulation Plan for Greater Downtown is shown in Appendix C. The plan identifies a street network that includes both existing and proposed streets and both major and minor streets. In addition to the plan itself, the Greater Downtown Circulation Plan also identifies potential enhancements within Greater Downtown that could be implemented over a number of years. The suggested desired cross-sections and level of streetscape development along the streets within Greater Downtown support the overall Plan goals, land uses and circulation to and from the area for pedestrian, bicycle and vehicles.

Policy: Conduct a more detailed traffic analysis of the area to determine needed intersection control and street cross-sections.

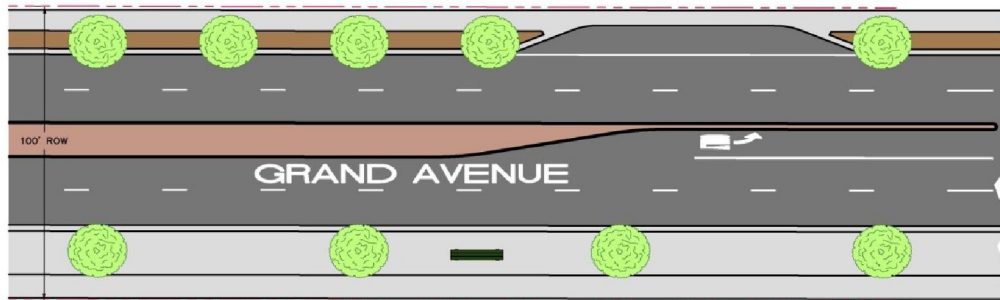
Major Street Corridors. Major streets in the Grand Junction urbanized area are classified according to their function in the transportation network. The two components of function are to provide access to properties and to carry traffic from point to point. In order to preserve safety and capacity and enhance the quality of living, the relation of these two components should be inversely proportionate, with the busier streets having limited access and the quieter streets providing access to properties. The components of the major street system have been identified on a functional classification map, known as the Grand Valley Circulation Plan that has been adopted by the City of Grand Junction and accepted by Mesa County. The Greater Downtown Circulation Plan is intended to augment the Grand Valley Circulation Plan and provide more specific guidance on multimodal circulation improvements as development and redevelopment occurs in the Greater Downtown area.

As properties develop and redevelop within Greater Downtown, the Grand Valley Circulation Plan and the concepts of the Greater Downtown Circulation Plan will be implemented through construction or improvement of streets when warranted by the proposed development. The specific design of each street is generally based on the land use and zoning of the properties along it. For example, in areas that will be zoned Commercial, the Commercial Street cross-section will apply, unless, during actual design and construction, modifications to the standard cross-section are made based on suggestions in the Greater Downtown Plan. Similarly, streets within Industrial areas are to be developed/improved according to the City's adopted Industrial street cross-section, unless, during actual design and construction, modifications to the standard cross-section are made based on suggestions in the Greater Downtown Plan. The street classifications and proposed street sections for the major corridors in Greater Downtown are described below. The concept drawings included on the following pages illustrate suggestions of the Greater Downtown Circulation Plan.

- **North Avenue** – Arterial Street. A potential layout for North Avenue between 1st and 12th Streets was adopted with the North Avenue West Corridor Plan. The layout is incorporated into the Greater Downtown Plan as depicted below.

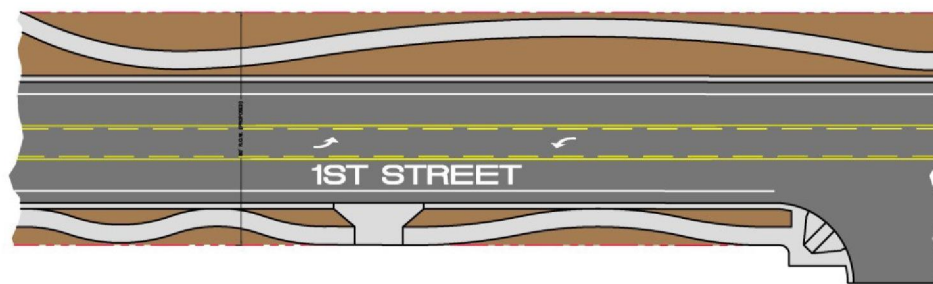


- Grand Avenue** – Arterial Street. While Grand Avenue is an established street with improved streetscape elements the length of the segment within Greater Downtown (1st to 12th Streets), there could be improvements made in some blocks that would enhance traffic flow, pedestrian safety and visual aesthetics. A typical, potential median improvement to Grand Avenue is depicted below. The segment of Grand Avenue between 1st Street and 7th Street also has the potential to be used as a “sharrow”. A sharrow is marked as in the photograph below so that the travel lane may be used by both bicycle and vehicular traffic.

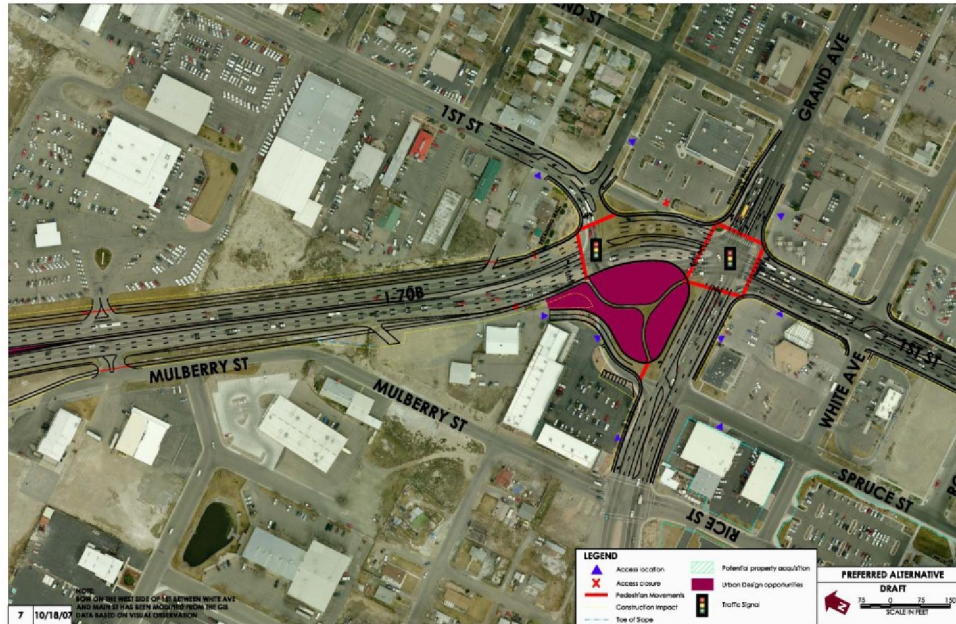


Example of a Sharrow

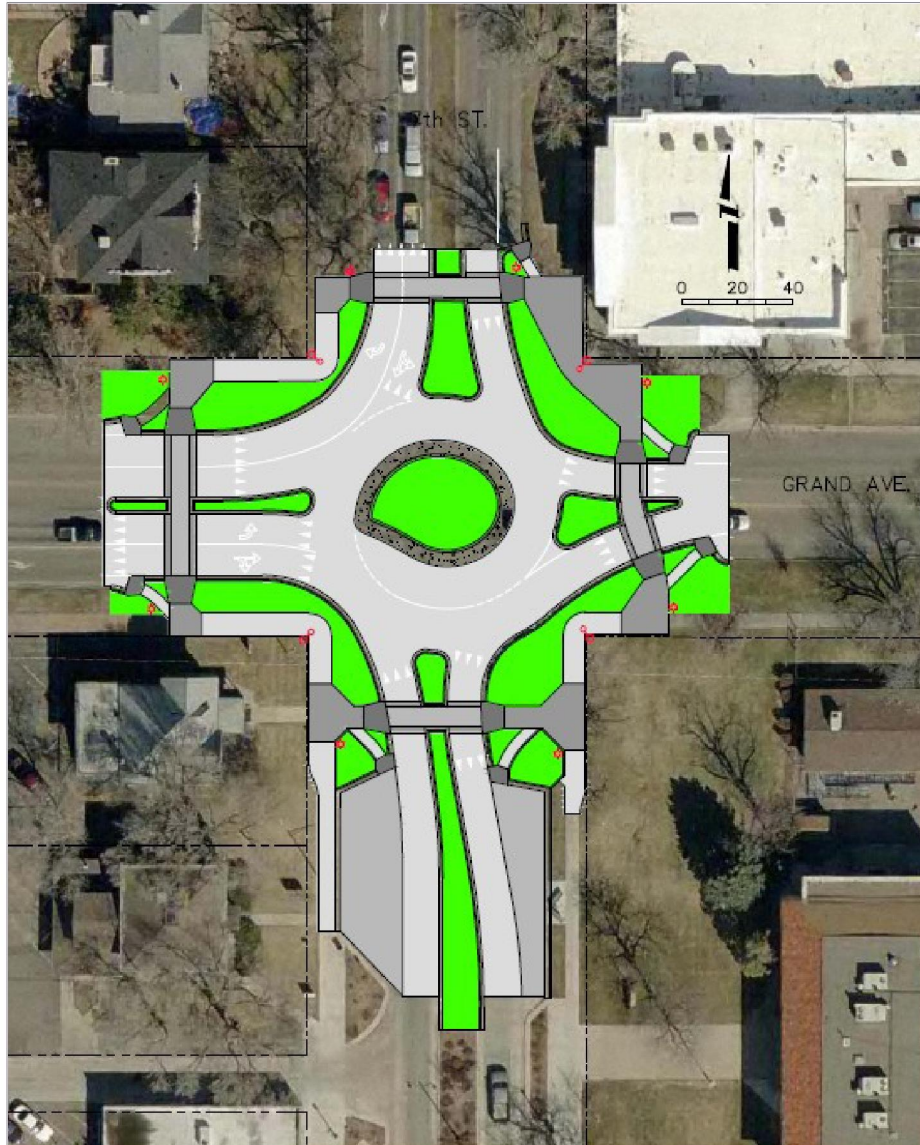
- North 1st Street** – Arterial Street. The segment of North 1st Street from Grand Avenue to North Avenue is deficient in street design and pedestrian amenities. The Greater Downtown Plan envisions eventual redevelopment of this street to improve safety, drainage, pedestrian circulation, access and improve on-street parking. A potential layout is depicted below.



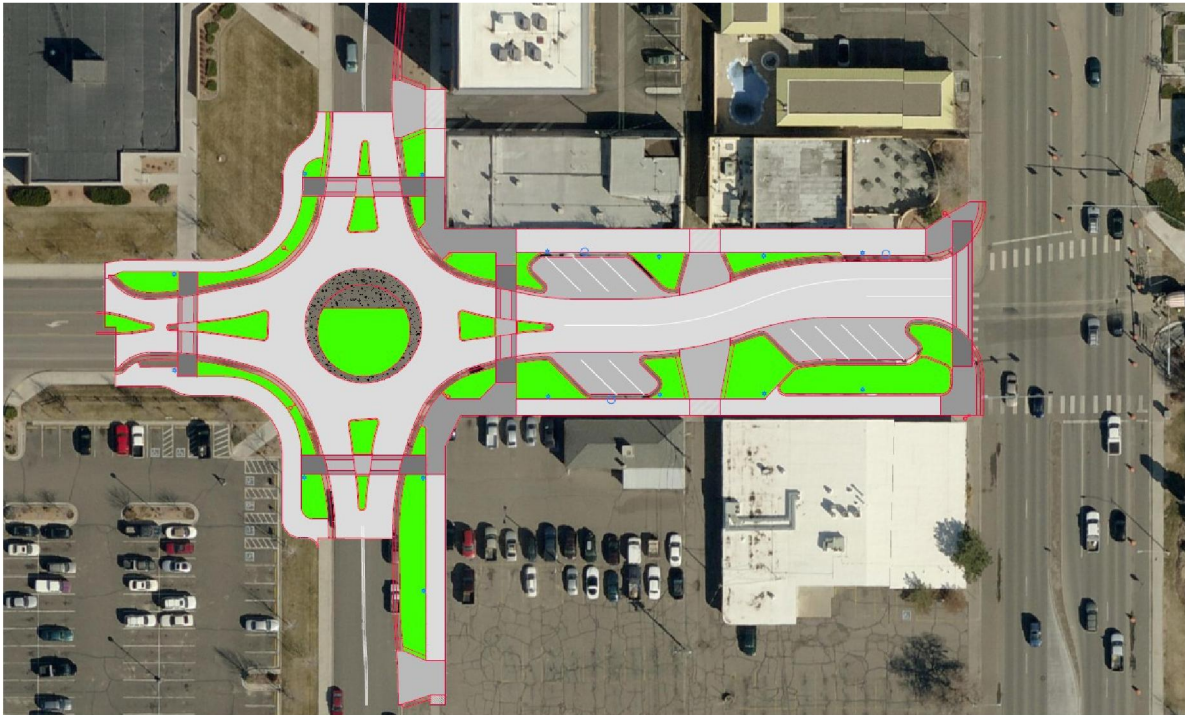
- 1st Street and Grand Avenue Intersection** – The Colorado Department of Transportation (CDOT) completed an Environmental Assessment for the I-70B/6 & 50 corridor which recognized circulation needs at the 1st and Grand intersection. A potential design is shown below. This design is incorporated into the Greater Downtown Plan as are any potential improvements CDOT may include in this project on 1st Street from Grand Avenue to Pitkin Avenue.



- **7th Street and Grand Avenue Intersection** – A design for a roundabout was included with the plans for 7th Street improvements that were completed in 2007-2008. The roundabout was not constructed at that time, but the intersection still warrants vehicular and pedestrian improvements. The plan as originally proposed is shown below.



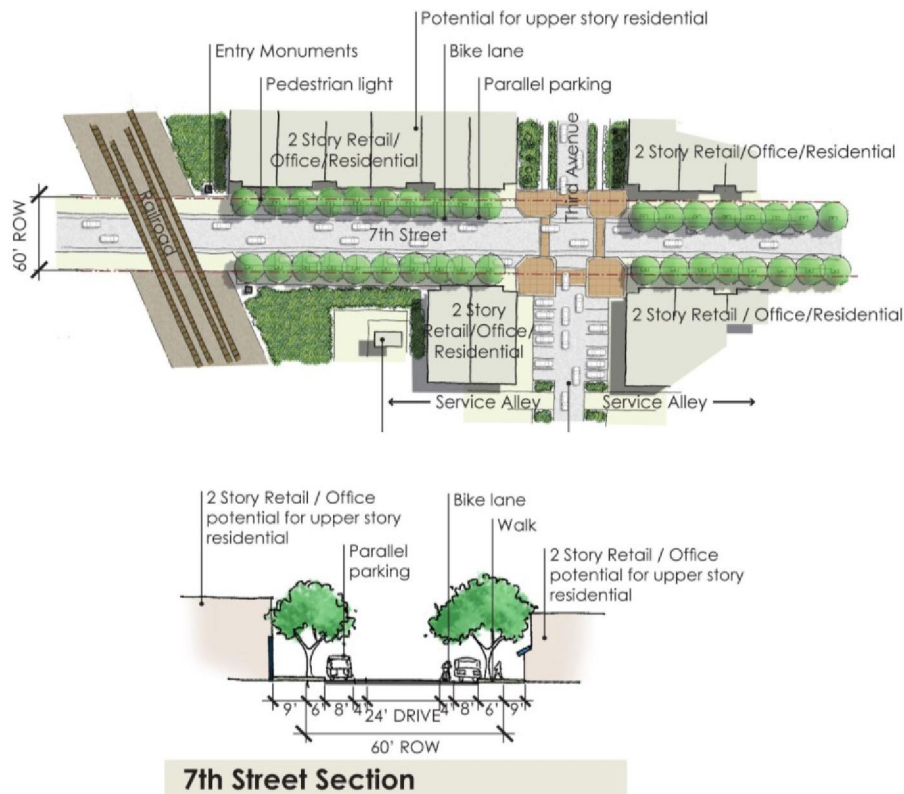
- West Main Street and Spruce Street Intersection** – Vehicular and pedestrian traffic has increased on the West Main Street corridor due to activity at the County Justice Center and the Mesa County Central Services building. To ease traffic flow through the area and improve pedestrian safety, this potential project for a roundabout at Spruce and West Main Streets was originally conceived during the visioning of Main Street in 2008. The roundabout would serve as a visual anchor to the west end of Main Street just as the one at 7th Street serves the east end. The roundabout would also allow for a circulator bus to serve the Main Street corridor from Spruce Street to 7th Street.



- Riverside Parkway** – Arterial Street. The right-of-way width varies; multi-lane; bike lanes; detached walk on the south side; no on-street parking. Completed in 2007.



- South 7th Street** – Suggest 2 lanes; bike lanes; on-street parking both sides; detached walks with landscaping. The streetscape plan for South 7th Street should be enhanced with a similar design as was constructed on 7th Street south of Grand Avenue. Generally, the design would continue the 7th Street boulevard treatments from downtown, Ute and Pitkin to the Botanical Gardens and riverfront area with additional street trees, historic street lights, street furniture and public art. This concept would create a more consistent visual character to connect the River and Rail Districts to the traditional downtown area and improve the visual design of the corridor and emphasizes its use as the primary public north-south corridor through the neighborhood. The concept features enhanced pedestrian facilities with colored concrete, pedestrian safe zones at the “bulb-outs” for easier crossing and additional landscaping.



- South 9th Street and 27-1/2 and C-1/2 Roads** – Suggest 2 lanes; on-street parking both sides; detached sidewalk preferred where possible. The streetscape plan for South 9th Street is to develop similar to what presently exists along South 7th Street with a more defined hardscape of curb and gutter, enhanced pedestrian facilities and street trees. This concept improves the visual quality of the corridor without requiring improvements on private property or compromising adjacent uses. The concept also allows the street to function for the commercial/light industrial traffic that it carries as well as provides for a more comfortable pedestrian or bicycle environment since South 9th Street may be used by the public to access business/commercial areas and Las Colonias Park.



- **Kimball Avenue** - Suggest 2 lanes; on-street parking; detached walks with landscaping.



- **D Road (from 9th Street east to the Riverside Parkway)** – Section yet to be determined.

Local Streets. The components of the local street system are identified on the [Grand Valley Circulation Plan](#). The primary function of the local street network is to provide access to and connections among individual parcels and subdivisions, provide a safe network for movement of pedestrians and bicycles, and serve short length trips to and from collector and higher order streets. Trip lengths on local streets should be short with a lower volume of traffic moving at slower speeds. As properties develop and redevelop within the greater downtown area, the Grand Valley Circulation Plan will be implemented through

construction or improvement of streets warranted by the proposed development, based on adopted street cross sections in the Transportation Engineering Design Standards (TEDS).

Trails. The Grand Valley Trails Plan shows the location of future bicycle facilities, trails and pedestrian paths. As development or redevelopment occurs, trails, paths, bike lanes and pedestrian facilities will be constructed in accordance with the adopted Grand Valley Trails Plan. Also the City may construct these facilities using Transportation Capacity Payments (TCP) funds as part of one or more capital improvement project(s). The Grand Valley Trails Plan is currently undergoing review by the City of Grand Junction and Mesa County. The conceptual circulation plan of the Greater Downtown Plan includes, in Appendix C, the a preferred alternative for trails, paths, bike lanes and pedestrian facilities in the Greater Downtown planning area. The preferred alternative shown comes from a 2012 draft of proposed amendments to the Grand Valley Trails Plan, and it includes the following:

- Extension of Riverfront Trail from Las Colonias Park east to 28 Road (and beyond).
- Bike routes (signs but no separate lane) on Grand Avenue from 7th Street east and 28 Road between the Riverside Parkway and C ½ Road.
- Sharrow bike routes (bikes share slightly wider lane with cars as depicted in the photograph on page 25) on Grand Avenue between 1st and 7th Streets.

Riverside Parkway Pedestrian Overpass. This conceptual circulation plan envisions one or more pedestrian overpasses from the commercial areas of the Rail District to the riverfront areas and Las Colonias Park. Activities and uses in the future park, types of development along the north side of the Parkway will determine where such facilities may be needed. An overpass on the western end in the vicinity of 7th or 9th Streets could also serve as an entrance feature of the neighborhood as further discussed in Section F. below.

Public Transportation. Grand Valley Transit (GVT) presently serves the Downtown District but does not provide service in the Rail and River Districts. Future transit needs within the Rail and River Districts should be monitored as development or redevelopment occurs in these areas and as Las Colonias Park sees more activity.

Local Streets. The Local Street network provides access to individual parcels and serves short length trips to and from collector and higher order streets. Trip lengths on local streets should be short with a lower volume of traffic along with slower speeds. Design of local streets occurs through the development process and will be in accordance with the City's adopted Transportation Engineering Design Standards (TEDS). It is important in the design process to provide connections to adjacent parcels and subdivisions for efficient vehicle travel and a safe network for pedestrians and bicycles.

Trails. The Grand Valley Trails Plan is a planning document that shows the location of future bicycle facilities, trails and pedestrian paths. It is currently under review prior to adoption by the City and County. Implicit in the plan is the construction of sidewalks in accordance with the adopted street cross sections. One of the major purposes of the City's Urban Trails Committee is facilitating linkages from the riverfront trail system to the urban area. As development or redevelopment occurs, construction of trails, paths, bike lanes and pedestrian facilities in accordance with the adopted Grand Valley Trails Plan either occurs with the development or the City constructs the same with the collection of the Transportation Capacity Payment (TCP) as part of a more comprehensive capital improvement project.

The Grand Valley Trails Plan shows the following proposals within Greater Downtown.

- ~~Extension of Riverfront Trail from Las Colonias Park east to 28 Road (and beyond).~~
- ~~Bike Routes (signs but no separate lane) on Grand Avenue from 7th Street east and 28 Road between the Riverside Parkway and C-1/2 Road.~~
- ~~Sharrow Route (bikes share lane with cars) on Grand Avenue between 1st and 7th Streets.~~

~~**Riverside Parkway Pedestrian Overpass.** It is envisioned that eventually there may be need for one or more pedestrian overpasses from the commercial areas of the Rail District to the riverfront areas and Las Colonias Park. Development, activities and uses in the future park and types of development along the north side of the Parkway will dictate where these may be needed based on the level of pedestrian traffic. An overpass on the western end of the area in the vicinity of 7th or 9th Street could also serve as an entrance feature to the neighborhood as further discussed in section F. below.~~

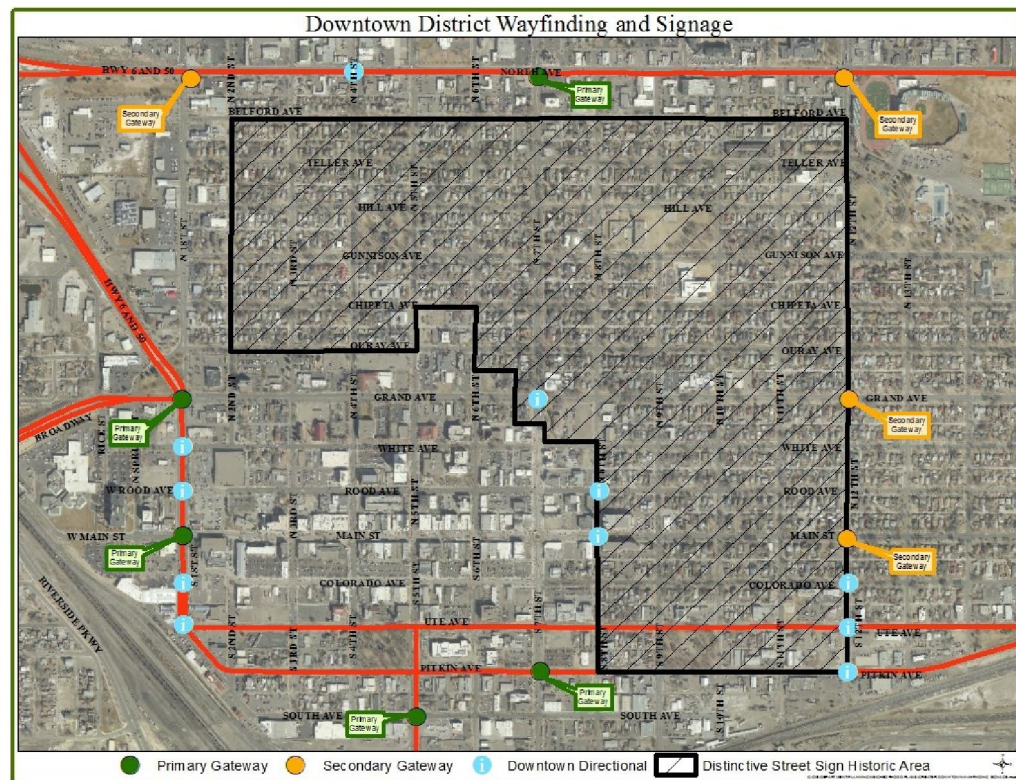
~~**Public Transportation (GVT).** Grand Valley Transit (GVT) presently serves the Downtown District but does not provide service in the Rail and River Districts. Future transit needs within the Rail and River Districts will need to be monitored as more areas are developed or redeveloped and as Las Colonias Park becomes more developed and active.~~

F. Entryways and Signage

Important intersections in the street network offer opportunities to develop a unique theme and identity for the Greater Downtown area. The primary intersections vary in scale and include entries to the Downtown, Rail and River Districts. Each of these should be developed according to general concepts and criteria that are appropriate for their scale, function and importance.

Downtown District Entryways and Signage

While the Downtown District has a strong base of local and regional users, approaches to the District offer no distinguishing features or directional signage that orient first time visitors. When approaching or traversing perimeter streets, visitors have no sense that they are within eyesight of the historic town site or even that they are headed in the right direction toward Downtown. Creating gateways at key entry points, as well as simple perimeter identifiers, will orient visitors, as well as strengthen and celebrate the heritage of the Downtown



Districts. Because of the diversity of uses and neighborhoods within the Downtown District, the design of entryways should include a range of sizes, poles and ornamentation of signage and varied landscape elements, lighting and other features that are integrated with the signage. The primary gateways to the Downtown District and between Downtown and the Rail District include the following intersections.

- 7th Street and North Avenue
- 1st Street and Grand Avenue
- 1st Street and Main Street
- 5th Street and South Avenue
- 7th Street and Pitkin Avenue

In addition, the distinctive public sign palette already started in the Downtown District should be continued and expanded to include street signs and directional signs that have recognizable poles, ornamentation, colors, fonts and logos. The City, together with the DDA, will further develop sign standards and guidelines for private signage placed on buildings or as freestanding signs.

Rail and River District Entryways

5th Street/Riverside Parkway. There are opportunities to celebrate the entry into Grand Junction and the Rail and River Districts at the 5th Street bridge and Struthers Avenue area in conjunction with the Western Colorado Botanical Gardens with attractive low scale signage and sculpture. In addition, there are smaller monuments at various points along the Riverside Parkway that indicate to motorists that they are approaching or traveling on the Riverside Parkway. Due to the scale of the 5th Street/Riverside Parkway intersection and the publicly-owned area around it, this intersection affords the opportunity to create a monument/sculpture of a much larger scale to mark the entrance and give identity to the Rail and River Districts and/or to this “Grand Junction”.

Another possibility in this vicinity is if a pedestrian overpass is desired/needed near the 7th Street/Riverside Parkway intersection it could serve several purposes: pedestrian access across the Parkway, include design elements that give a distinct character to the Rail and River Districts and integrate with surrounding open space, pocket parks and/or water features at the landings on each side of the Parkway.

South 7th Street/Railroad Tracks. There is an existing silo on the Mesa Feed property that identifies the Rail District as you travel south on 7th Street. It is of a scale that is visible from the southern perimeter of downtown and represents the historical agriculture and industrial base upon which the Rail and River Districts have developed. Such a structure could be enhanced and/or replicated to become an even stronger element at this major entrance to the area.

28 Road/Riverside Parkway. This intersection is a smaller scale than the others, but a neighborhood entry could be created, particularly along the north side. The sense of arrival at this location could be created through a water feature, public art, an architectural feature with signage paving patterns and/or landscaping. The design of the entry feature should be of the same character of those that might be created at the other major entry points.

G. Economic Redevelopment

Downtown District Economic Redevelopment

While the Downtown District is the heart of the community, it is but one subset of a larger market and has

strengths which can be capitalized on and limitations which should be overcome. The Downtown District has a tremendous influence on the economic well-being of the entire region. Therefore, it is widely accepted that early projects in any revitalization effort could be publicly assisted until market conditions reach levels where new construction can support itself.

The Grand Junction Strategic Downtown Master Plan presented guiding principles which; while general in nature, were considered responsive to prevailing conditions, market opportunities, framework elements and stakeholder input. These guiding principles are still relevant to the Greater Downtown Plan and are listed and described below.

- **The Downtown District is one submarket that competes with other submarkets in Grand Junction.** The downtown environment, while presenting tremendous opportunity for investment in a setting uniquely positioned to offer both heart and history, carries with it certain limitations, particularly for land-intensive non-destination-oriented land uses. Several market sectors, however, not only survive, but also thrive in a downtown setting. Recognize the obstacles associated with downtown development and encourage regulatory and financial solutions including public subsidies and creative financing mechanisms.
- **The Downtown District must be market-responsive** to changing conditions, with implementation tools and mechanisms in place to both offset competitive disadvantages and capitalize on competitive assets. Market conditions should be continually monitored and information distributed to a broad audience including developers, business and property owners, lenders, city staff and elected and appointed officials.
- **The Downtown District infrastructure must be protected and retained** including physical features, service organizations, mix of employers, historic residential neighborhoods and community attitudes toward the Downtown District. Unlike many communities across the country, Grand Junction's Downtown District existing infrastructure is more than sufficient to promote itself as a downtown neighborhood. These assets, which provide the impetus for investment, need to be protected and promoted.
- **The Downtown District's "tool bag" must contain a variety of strategies and mechanisms to attract investment.** These tools can be financial, physical, market, or organizational in nature and can be used independently or in various combinations. Given the obstacles associated with downtown development, it is imperative that whatever mix of tools is put in place it be comprehensive, flexible and creative.
- **Public investment must leverage private investment.** Historically, the planning, financing and implementation of projects in the downtown market were the primary responsibility of public sector entities such as the City and the DDA. However, while the public sector continues to play a significant role in most downtown efforts, a critical component to the success of any revitalization strategy today is participation by both the public and private sectors. Leveraging of resources is key, as no one entity, public or private, has sufficient resources alone to sustain a long-term downtown improvement effort.
- **Public policy must support development in the Downtown District.** Experience has proven that downtown development will best succeed if regional growth management programs reward efficient development patterns. If growth is allowed to occur in a land extensive, inefficient way

that effectively subsidizes lower densities, downtown development will operate at a competitive disadvantage. Given Grand Junction's existing land use patterns, the Downtown District is susceptible to continued dilution of its role as the community's central business and shopping district.

- **Public-private partnerships are essential.** Under any investment strategy, local government needs to have strong involvement, a visible presence, perhaps be the entity that provides continuing leadership, regulatory incentives, and seed capital for early projects. Not only does government have the legal responsibility to address many of the implementation components, but it is also the logical conduit to local, regional, state and federal funding sources.

Rail and River District Economic Redevelopment

The changes that have occurred in portions of Greater Downtown such as completion of the Riverside Parkway and planning for the future development of Las Colonias Park have already had a positive influence on the River and Rail Districts. Many properties have been renovated or redeveloped, new uses are relocating to the area and property values are generally on the rise. The Greater Downtown Plan envisions this trend continuing and being enhanced by the following redevelopment concepts:

- Allow existing heavy industry to remain, taking advantage of rail spurs within the area.
- Intensified commercial edge along the north side of the Riverside Parkway with opportunities for mixed use development.
- New general commercial, retail and residential uses will provide activity at the edge of the park after business hours to create a safe park environment that gives "ownership" of the park to the adjacent local business owners and residents.
- New retail and commercial uses such as restaurants, shops and services along South 7th Street to serve the employees, recreational users and residents of the neighborhood.
- Commercial Industrial uses bridge the existing industrial and the commercial corridors.

In addition, discussions with the Grand Junction Economic Partnership, Business Incubator, Manufacturers' Council and Chamber of Commerce during development of the Greater Downtown Plan brought to light many opportunities for the area, the majority of which is within the established Mesa County Enterprise Zone. The Greater Downtown Plan outlines goals, policies and strategies that can be used to further the economic (re)development of the Rail and River Districts.

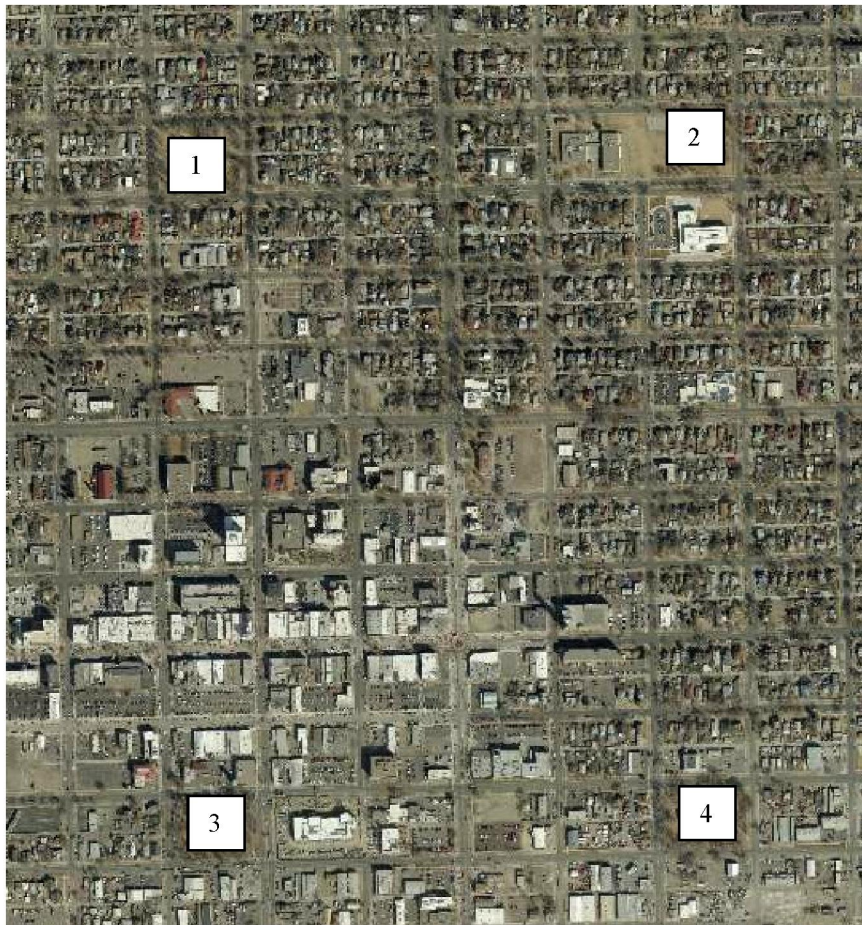
- Need for flex space for different types of small business – new to area or graduating from the Business Incubator. Opportunity for these businesses to serve employee base, residents and recreational users in the area.
- Opportunity to develop additional incentives for redevelopment that has taken advantage of partnerships and/or assembled parcels of land totaling a minimum of ½ acre or more
- Allow for live-work opportunities
- Opportunity to develop partnerships

H. Greater Downtown Area Parks

The four downtown parks were included on the original town plat created by the Grand Junction Town Company in 1882. All four parks still exist as downtown open space. The City acquired Las Colonias Park from the State Parks Department in 1997. It encompasses the mill tailings clean-up site.

1. Hawthorne Park – 400 Gunnison Avenue

- New restroom/shelter constructed 2010
- South picnic shelter requires new roof; scheduled 2013
- Playground surfacing replaced in 2012
- Playground equipment on a 5 to 7 year replacement schedule
- Irrigation is good, 10-15 year replacement



2. Washington Park – 10th Street and Gunnison Avenue

- No restrooms or shelter at this park
- Playground equipment scheduled for replacement in 2013
- Playground surface scheduled for replacement every 2 years
- Irrigation will be replaced every 5 to 7 years

3. Whitman Park – 5th Street and Pitkin Avenue

- Restroom scheduled for replacement in 2013
- Development of a new master plan for the park recommended (see possible concepts below)

4. Emerson Park – 9th Street and Pitkin Avenue

- Restroom scheduled for replacement in 2013
- Playground area for ages 2-5 is good
- Playground for ages 5-15 scheduled for replacement in 5 to 7 years
- Playground surfacing replaced in 2012
- Development of a new master plan for the park recommended (see possible concepts below)

Potential Concepts for Whitman and Emerson Parks

- Provide for a diversity of uses to create a higher level of utilization.
- Minimize the impact of adjacent streets and automobile traffic.
- Activate the edges of the parks with mixed use.
- Contemplate programmed, active use.
- Redevelop as more active, flexible urban open space rather than as passive, green parks.

5. Las Colonias Park – Struthers Avenue and 7th Street

- A master planning process for the park is anticipated to occur in 2013
- Some amenities being considered are parking, restrooms, shelters, play area, open space, expanded walk/bike trails, open space for festivals, music, and educational presentations, various wetlands and arbor education opportunities, kayak park, tree walk and a disc golf course.



I. City-Owned Jarvis Property

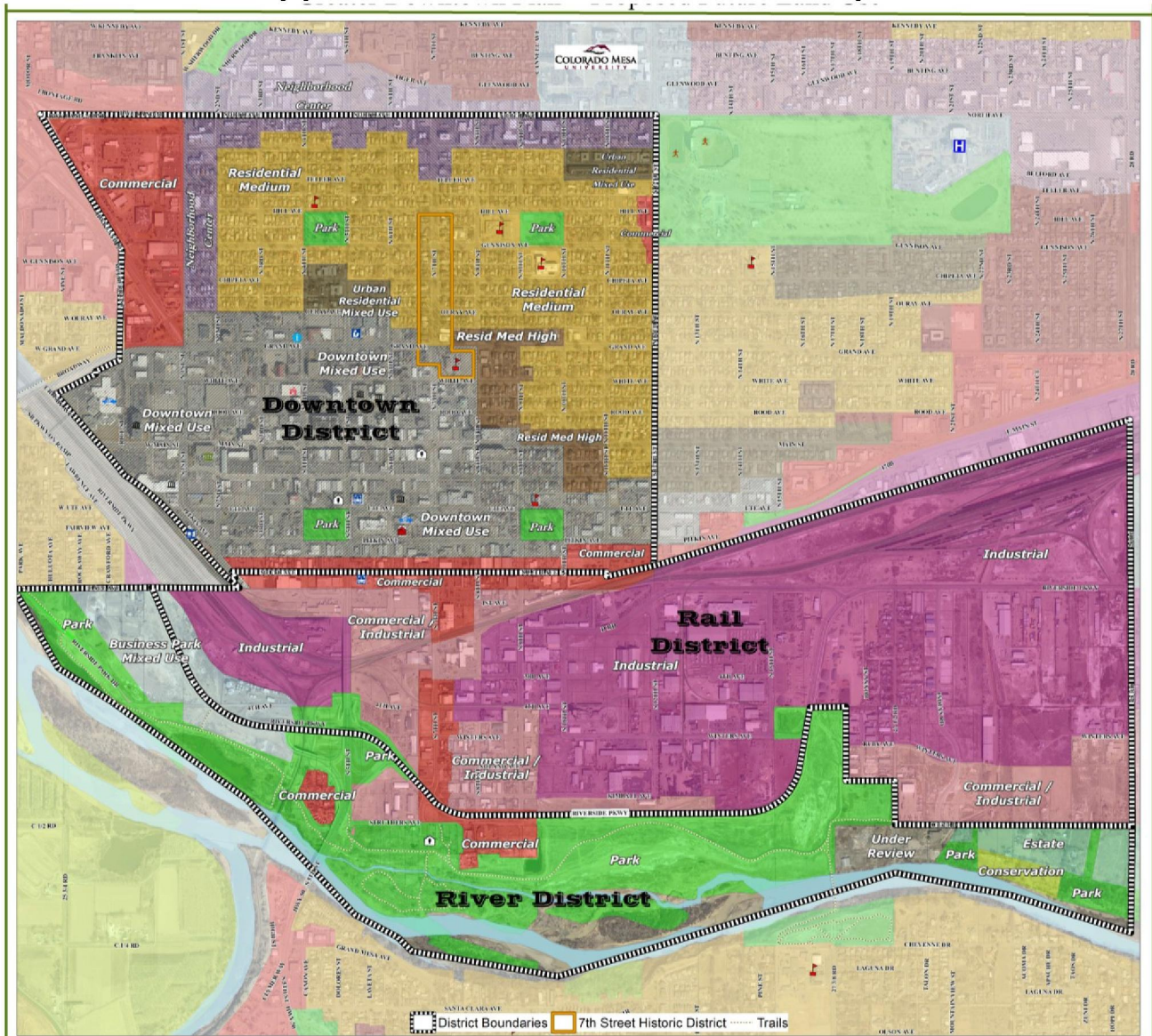
The City completed an initial planning analysis for the City-owned property on the west side of 5th Street between the Colorado River and the Riverside Parkway known as the Jarvis Property. The property is constrained by natural features and the encroachment of the Parkway, but does have approximately 43 acres of developable land.

The initial study was to chart a direction for revitalization of the property. It summarized the key assets, identified some important issues and potential impediments to development, analyzed current market conditions and outlined a concept for organizing potential development of the property. The property and potential project are viewed as a unique opportunity for the City to chart the future of a rare property type, a place where it may be possible to provide a mix of uses, including residences, along the bank of the Colorado River. The major opportunities and constraints identified for potential development of the Jarvis Property were to: continue the riverfront greenway through the property, restore habitat, relocate the high voltage power facilities that run through the site, and provide flood protection for the property.

APPENDICES

- A. Future Land Use Map
- B. Zoning Map
- C. Conceptual Plans for Traffic Circulation and Trails
- D. Development Concepts

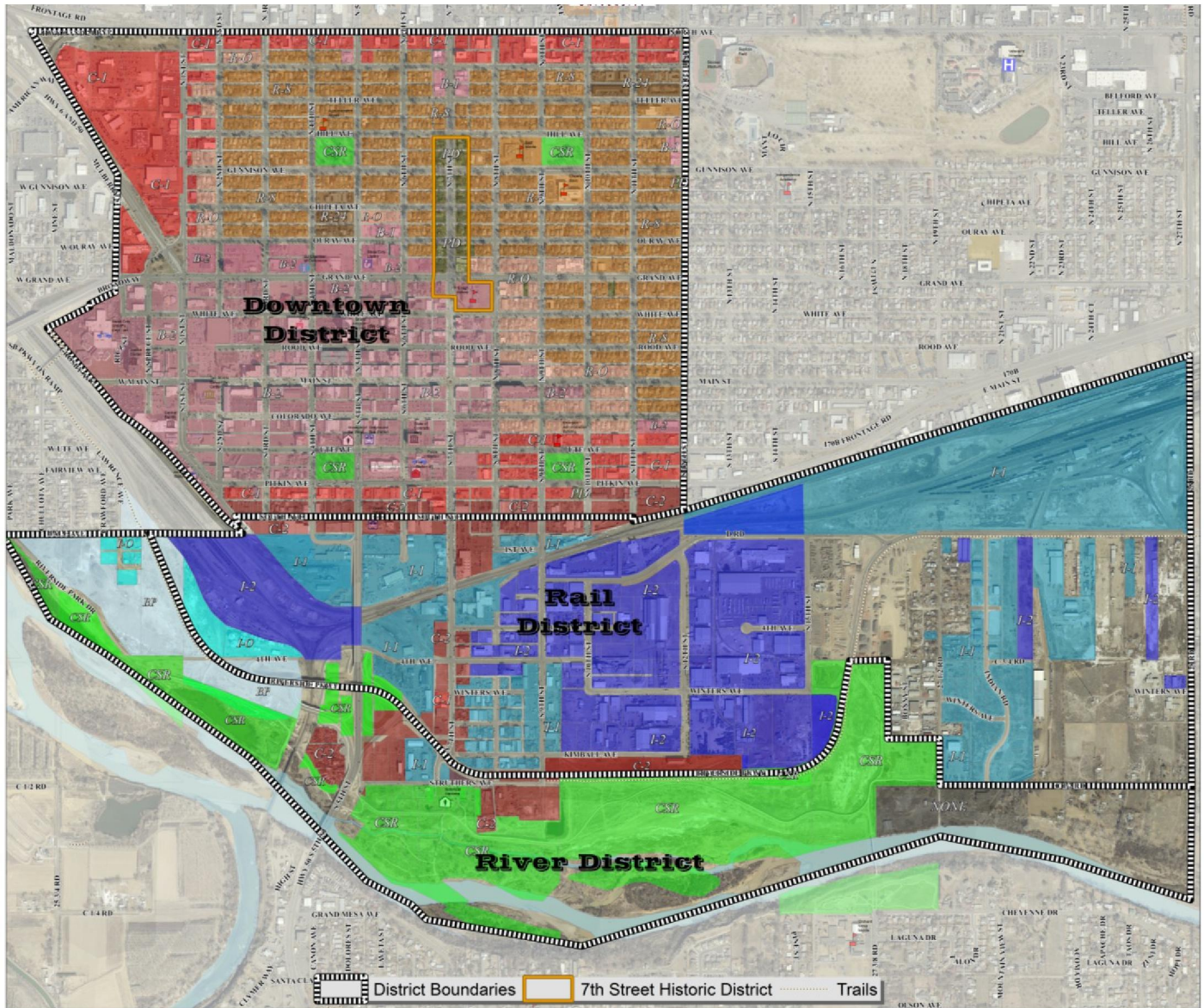
Appendix A – Future Land Use Map



Greater Downtown Future Land Use Categories

Neighborhood Center (NCMU)	Commercial (COM)	Residential Med High (RMH)	Conservation (CON)
Downtown Mixed Use	Industrial (IND)	Residential High Mixed Use or Resid. Med High	Urban Residential Mixed Use (URMU)
Business Park Mixed Use	Commercial Industrial (CI)	Park (PK)	

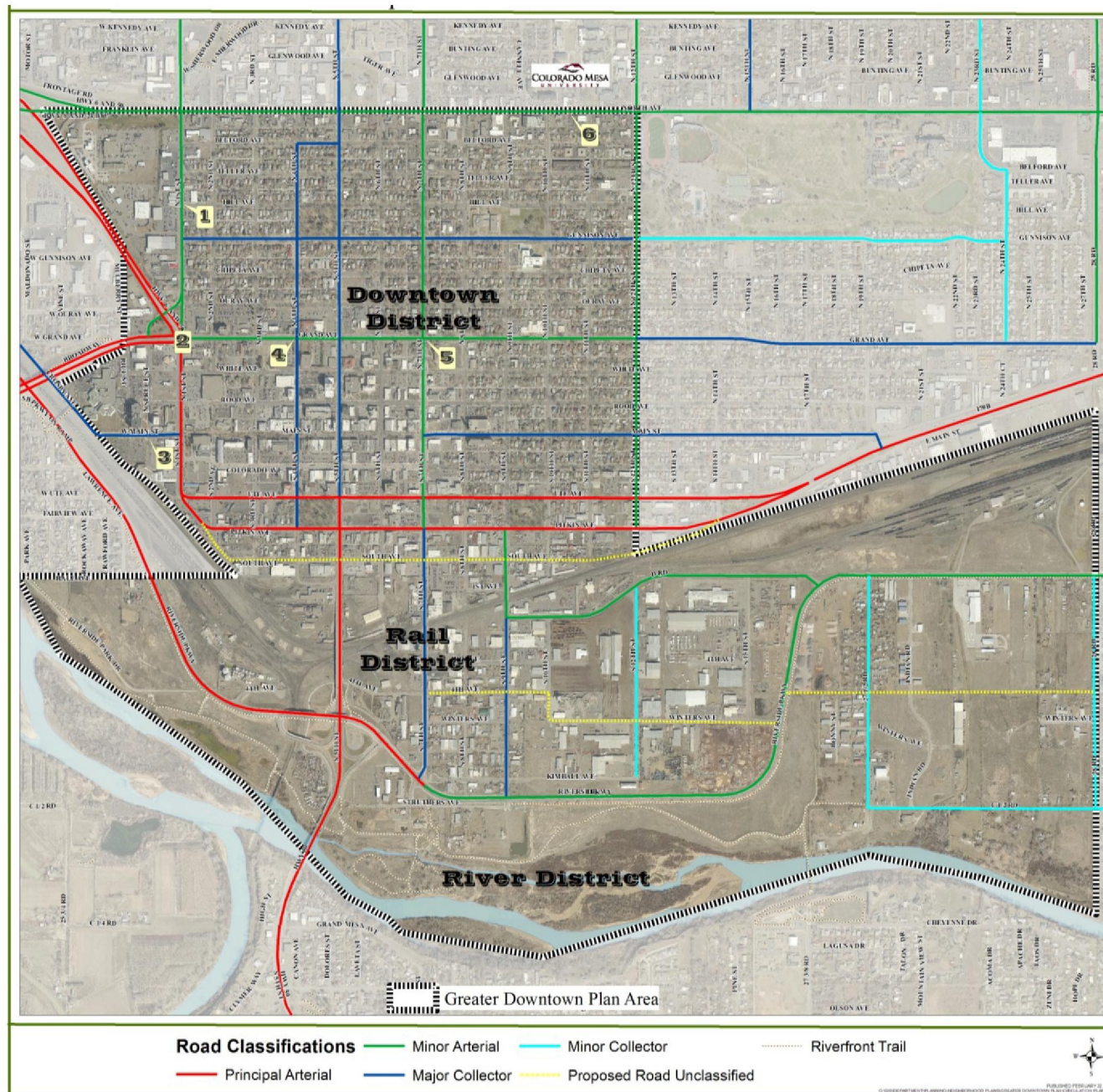
Appendix B – Zoning Map

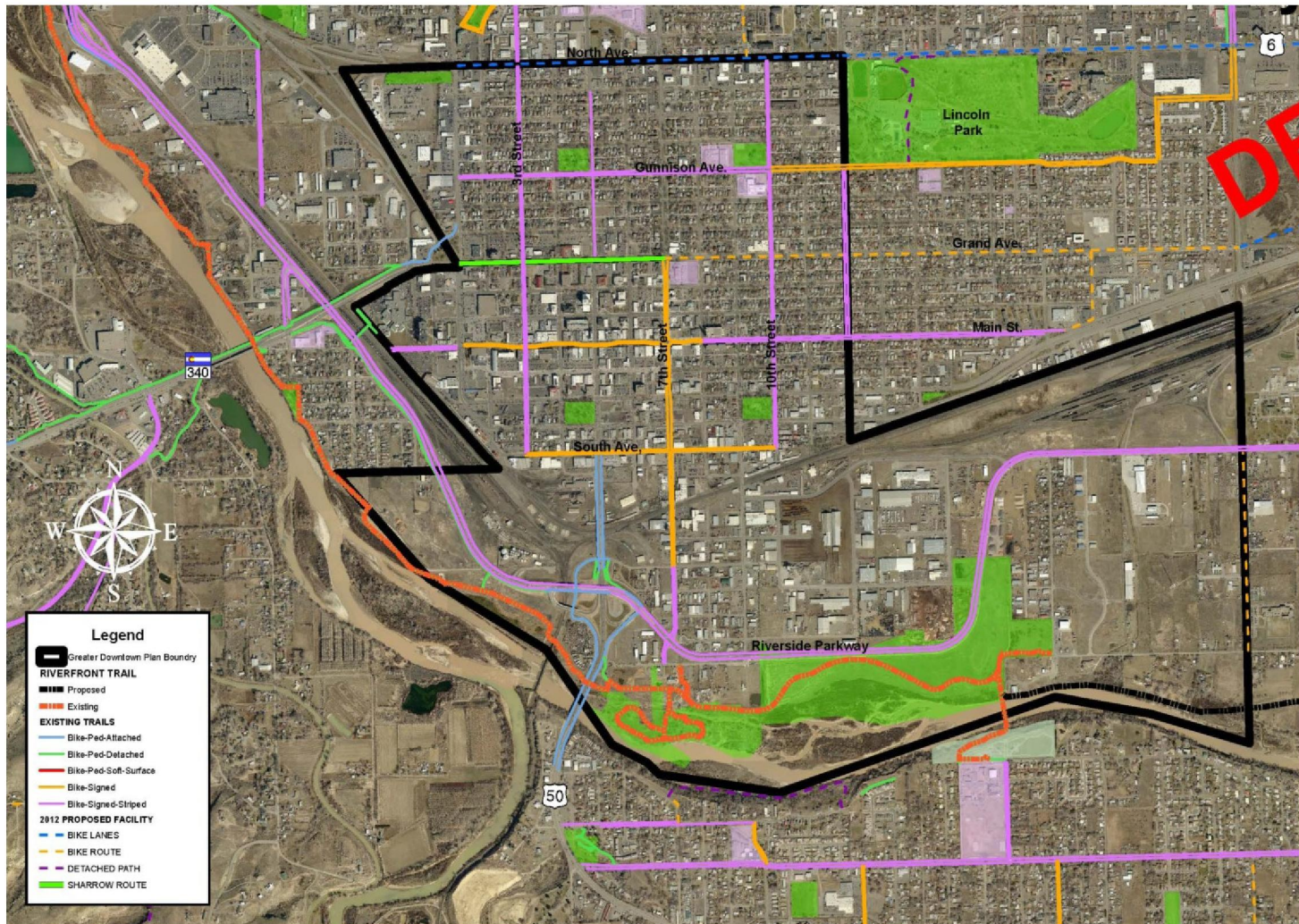


Greater Downtown Zoning Categories

R-8	PD (Planned Development)	R-O (Residential Office)	B-2 (Downtown Business)	C-1 (Light Commercial)	I-O (Industrial Office Park)	I-2 (General Industrial)
R-24 (16-24 units/acre)	CSR (Community Services & Rec.)	B-1 (Neighborhood Business)	BP (Business Park)	C-2 (General Commercial)	I-1 (Light Industrial)	NONE

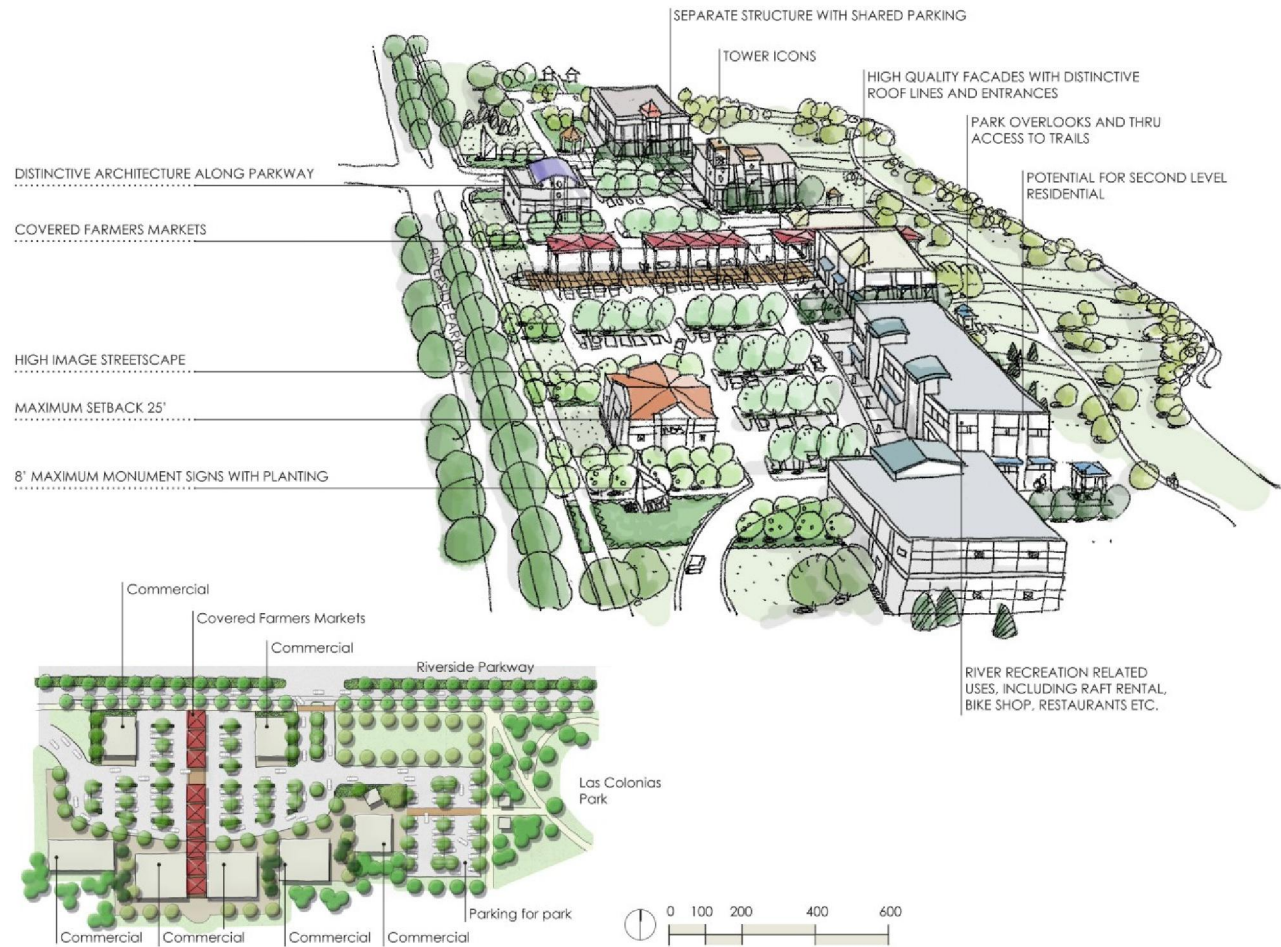
Appendix C – Proposed Conceptual Plans for Traffic Circulation and Trails



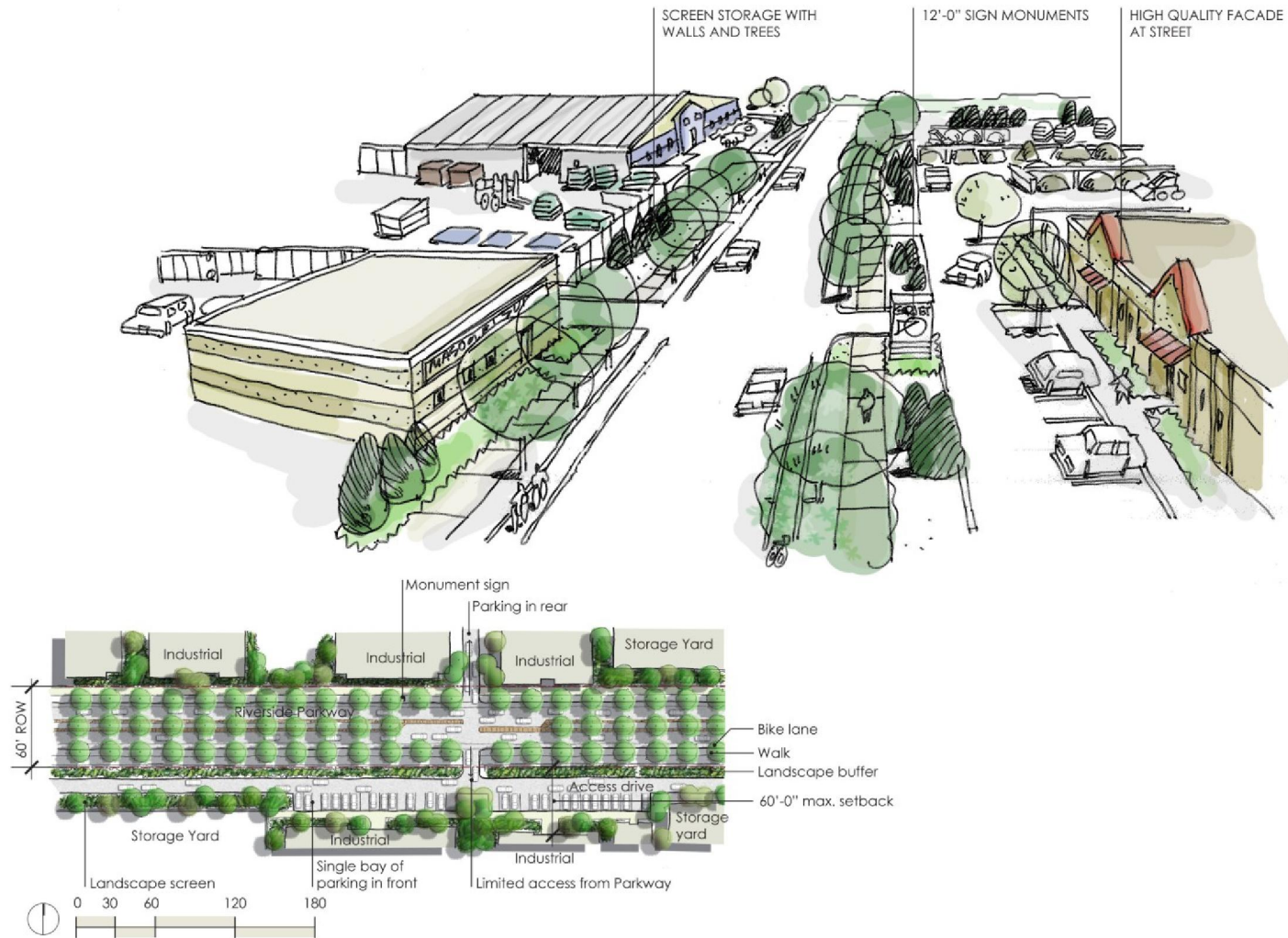


PROPOSED TRAILS PLAN

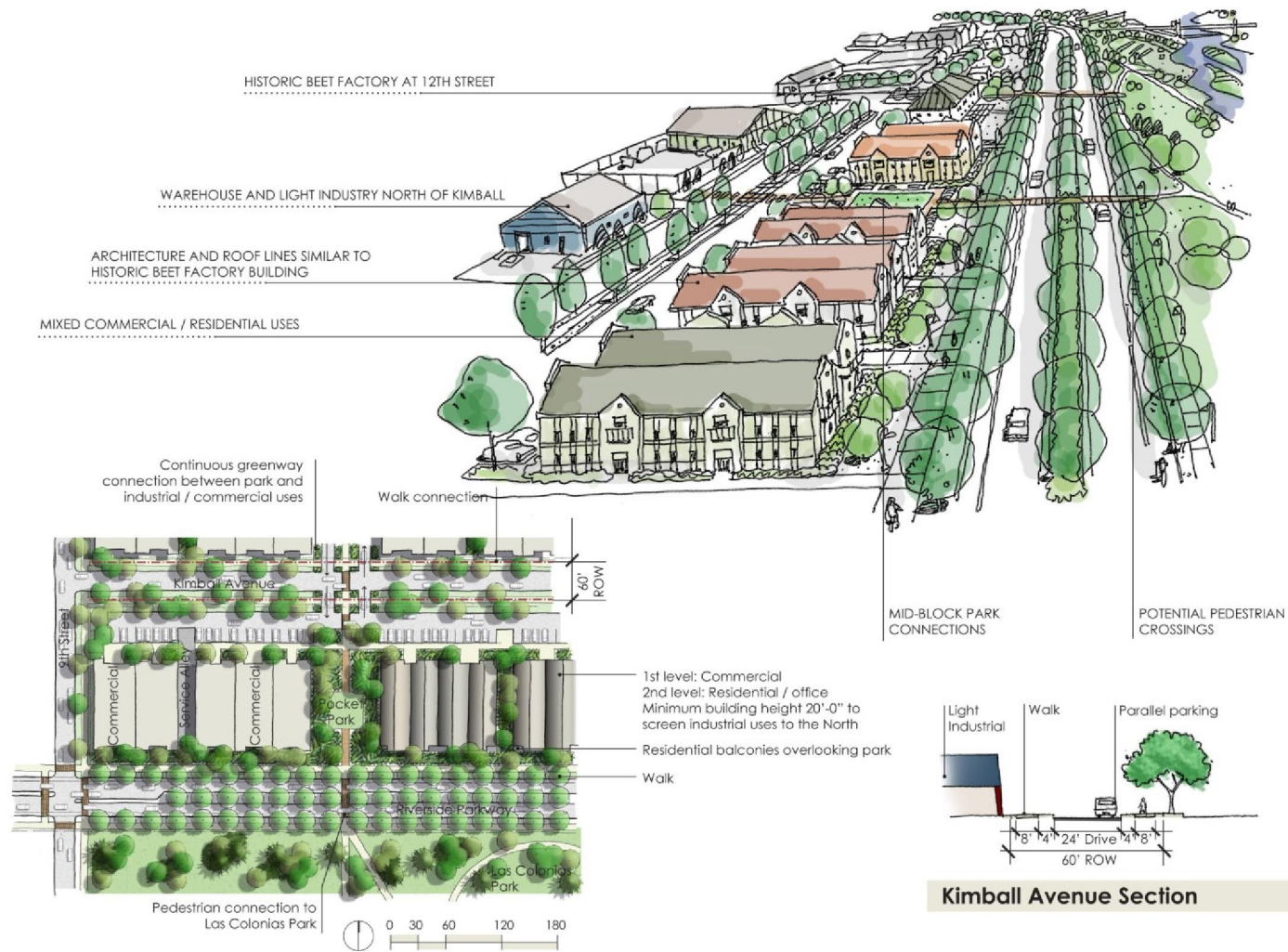
Appendix D – Development Concepts



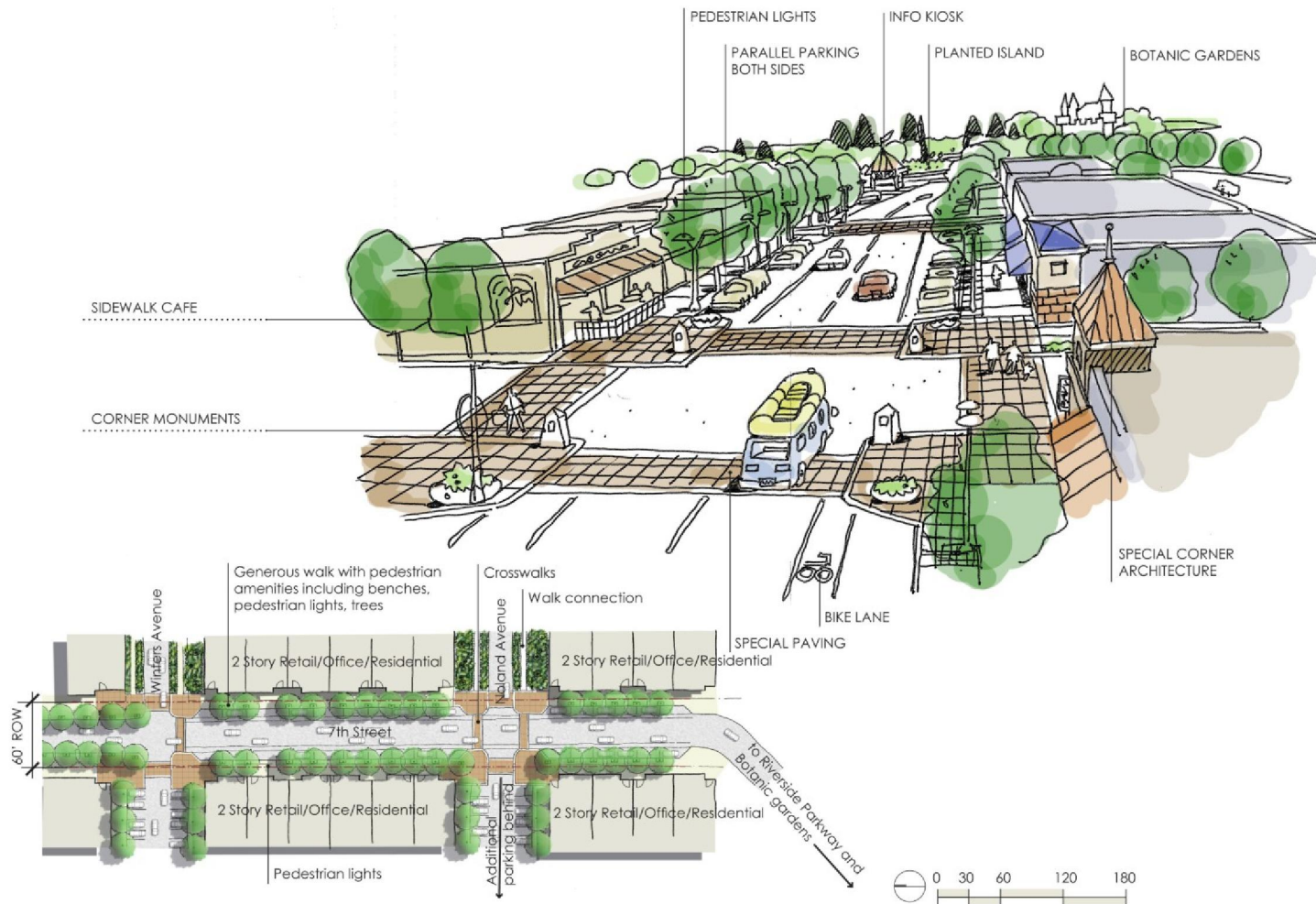
7th and Struthers Redevelopment



Riverside Parkway Between 27 ½ and 28 Road



Riverside Parkway Between 5th Street East and 12th Street



7th Street Looking South



Existing Conditions

FRONT YARD PATIO

TREE LAWN

NO PARKING ON SOUTH SIDE

GARDEN GATES

NEW BUILDING
OF COMPATIBLE
STYLE AND SCALE

IRON FENCE

6'-0" GARDEN STRIP

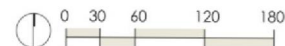
NEW SIDEWALK, CURB
AND GUTTER

REHABILITATE EXISTING
STRUCTURES



ON STREET PARKING
NORTH SIDE

RAISED BRICK CROSSWALK
WITH CORNER PLANTING



Noland Avenue

EXHIBIT B: Amendment to the text of the Comprehensive Plan

Existing Text:

Downtown Mixed Use (DMU)

Density: 24+ du/acre

Employment, residential, retail, office/business park uses allowed. A mix of uses (2 to 8 stories), either horizontal or vertical is expected. Residential densities may exceed 24 du/acre.

Applicable Zones

R-16

R-24

B-2

C-1

MXR – 3, 5, 8

MXG – 3, 5, 8

MXS – 3, 5, 8



Comprehensive Plan
Grand Junction



33

Proposed Text:

Applicable Zones

R-16

R-24

RO

B-2

C-1

MXR – 3, 5, 8

MXG – 3, 5, 8

MXS – 3, 5, 8

ATTACHMENT 2

CITY OF GRAND JUNCTION, COLORADO ORDINANCE NO. _____

AN ORDINANCE AMENDING THE ZONING AND DEVELOPMENT CODE TO ADD SECTION _____, TO BE KNOWN AS THE GREATER DOWNTOWN OVERLAY DISTRICT AND AMENDING SECTION 21.03.020(d) TO INCLUDE THE RO ZONE IN THE DOWNTOWN DISTRICT IN THE DOWNTOWN MIXED USE LAND USE DESIGNATION

RECITALS.

The Greater Downtown Plan recommends adoption of standards and guidelines as an zoning overlay for certain areas of Greater Downtown.

Overlay zoning allows flexibility in the application of zoning standards to areas where special considerations apply. An overlay district is superimposed on one or more established zoning districts to supplement regulations on development in these districts, to permit uses otherwise disallowed, or to implement incentives to development.

An overlay zone supplements the underlying zone with additional standards, guidelines and/or incentives while generally preserving the character of the underlying zone. Examples include different setbacks, increased height allowance or varied allowed uses. A parcel within the overlay district is simultaneously subject to two sets of zoning regulations: the underlying and the overlay zoning standards.

Overlay zone boundaries are not restricted by the underlying zoning district's boundaries. An overlay zone may or may not encompass an entire underlying zoning district. Likewise, an overlay zone can cover more than one zoning district, or even portions of several underlying zone districts.

The Greater Downtown Overlay District covers the Central Business District (CBD), Residential and Transitional subareas of the Downtown District as well as those properties that abut the following defined street corridors: South 5th Street, South 7th Street, Pitkin Avenue, South Avenue and the Riverside Parkway.

For some of the parcels in the Downtown District with an existing zoning of Residential Office (RO), the existing Future Land Use Map shows a land use category of Downtown Mixed Use. Presently, the RO district cannot be used to implement the Downtown Mixed Use category. However, the nature of these parcels is that they are small and on the periphery of the Central Business District so a rezone to a zone district that is acceptable in the Downtown Mixed Use Category (e.g. Downtown Business, B-2) would not provide a compatible transition to nearby residential areas. Consequently, the text of the Comprehensive Plan is revised to include Residential Office (RO) as an acceptable zone district to implement the Downtown Mixed Use land use category.

The Grand Junction Planning Commission is charged with the legal duty to prepare and consider and recommend action to City Council regarding amendments to the Zoning and Development Code for the City.

The Greater Downtown Overlay District was heard in a public hearing by the Grand Junction Planning Commission on _____, 2013 where the Planning Commission recommended that the City Council adopt the Overlay.

NOW THEREFORE, BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF GRAND JUNCTION THAT:

The Zoning and Development Code is hereby amended to add section ____ entitled "Greater Downtown Overlay District" as shown in the attached Exhibit A and as generally described in the recitals above.

The Greater Downtown Overlay District, City of Grand Junction, Colorado, in the form of the document attached as Exhibit A hereto, and as recommended for adoption by the Grand Junction Planning Commission is hereby adopted.

The full text of the Ordinance, including the text of the Greater Downtown Overlay District, in accordance with paragraph 51 of the Charter of the City of Grand Junction, shall be published in pamphlet form with notice published in accordance with the Charter.

INTRODUCED on first reading the ____ day of _____, 2013 and ordered published pamphlet form.

PASSED and **ADOPTED** on second reading the ____ day of _____, 2013 and ordered published in pamphlet form.

President of City Council

ATTEST:

City Clerk

Greater Downtown Overlay District

DRAFT

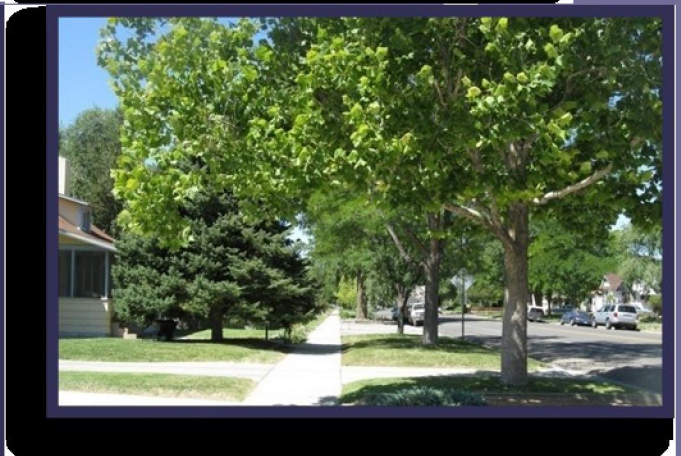


TABLE OF CONTENTS

INTRODUCTION

CORRIDOR STANDARDS AND GUIDELINES

1. Overall Corridor Vision/Concepts
 - A. Architectural Features
 - B. Building Materials
 - C. Streetscape and Site Design
2. Commercial Corridor Standards and Guidelines
 - A. New Site Development or Redevelopment
 - B. Architectural Design of New Buildings/Additions or Substantial Exterior Remodel
 - C. Landscaping for New Site Development or Redevelopment
 - D. Signage
3. Industrial Corridor Standards and Guidelines
 - A. 5th Street Industrial Corridor Standards
 - B. Riverside Parkway Industrial Corridor Standards

DOWNTOWN DISTRICT STANDARDS AND GUIDELINES

1. Overall Downtown District Vision/Concepts
2. Area-Wide Standards and Guidelines
 - A. General
 - B. Transportation
 - C. Entryways and Public Signage
3. Central Business District (CBD) Standards and Guidelines
 - A. Application of Standards and Guidelines
 - B. Land Use / Architecture
 - B. Site / Area Improvements
4. Residential Areas Standards and Guidelines
 - A. Policies
 - B. Standards
 - C. Guidelines
5. Transitional Areas Standards and Guidelines
 - A. Policy
 - B. Standards
 - C. Guidelines

INTRODUCTION

One of the first steps in implementing the Greater Downtown Plan is a zoning overlay. The overlay is intended to provide guidance and criteria for the planning, design and implementation of public and private improvements in the Greater Downtown area and is set forth in this document to be known as the Greater Downtown Overlay District (Zoning Overlay). If properly administered and adhered to, the standards and guidelines should result in public and private development improvements (or a combination thereof) that achieve, as a minimum, a common level of quality in terms of site design, architectural design, landscaping and other site improvements.

The general purposes of the standards and guidelines are to support the overall goals of the Greater Downtown Plan.

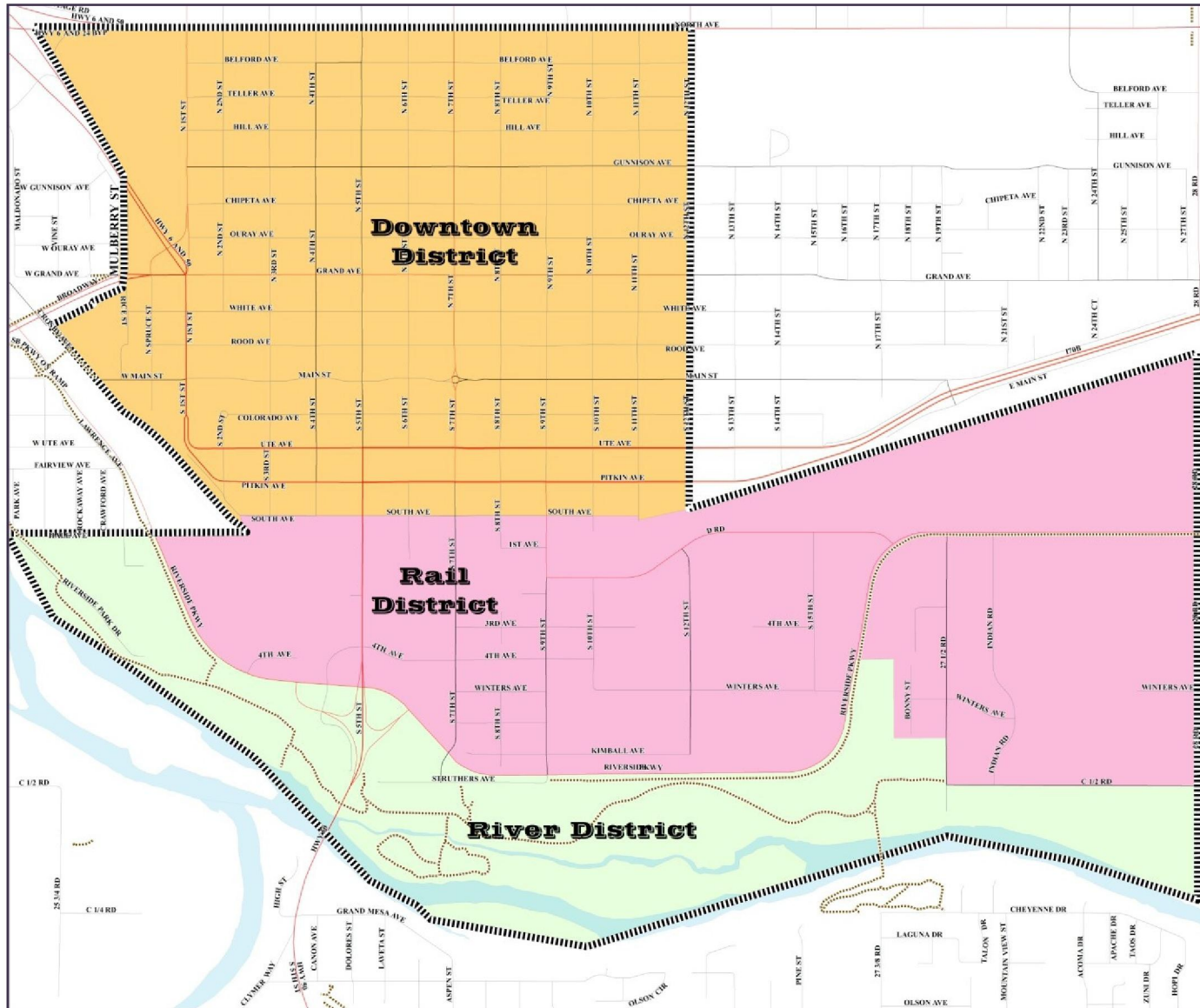
- Maintain and enhance the economic, cultural and social vitality of Greater Downtown Promote downtown living by providing a wide range of housing opportunities in appropriate areas within Greater Downtown.
- Enhance the transportation system to accommodate automobiles, bikes and pedestrians, and provide adequate, convenient parking.
- Stabilize, preserve, protect and enhance the downtown residential neighborhoods.
- Promote and protect the unique identity of the Greater Downtown area.

The standards and guidelines were developed upon an analysis of the existing character of the Greater Downtown area. The area was divided into sub districts and the Downtown District was further divided into subareas based on existing zoning, character of existing development and potential for redevelopment opportunities. In addition, primary corridors were identified for which overlay guidance is created with the adoption of the document. The sub districts, subareas and primary corridors are shown on the maps on the following pages.

These standards supplement other development regulations such as the City of Grand Junction Zoning and Development Code, which includes detailed criteria by zone district, planned development regulations, design and improvement standards, supplemental use regulations, sign regulations and the City Transportation and Engineering Design Standards (TEDS). In the instance the following standards are silent on a development concern, the existing regulations shall apply. None of the guidelines and standards within the Zoning Overlay are intended to apply to properties within the North Seventh Street Historic Residential District or the North Avenue Corridor Overlay since separate guidelines and standards have been established through overlays for those areas.

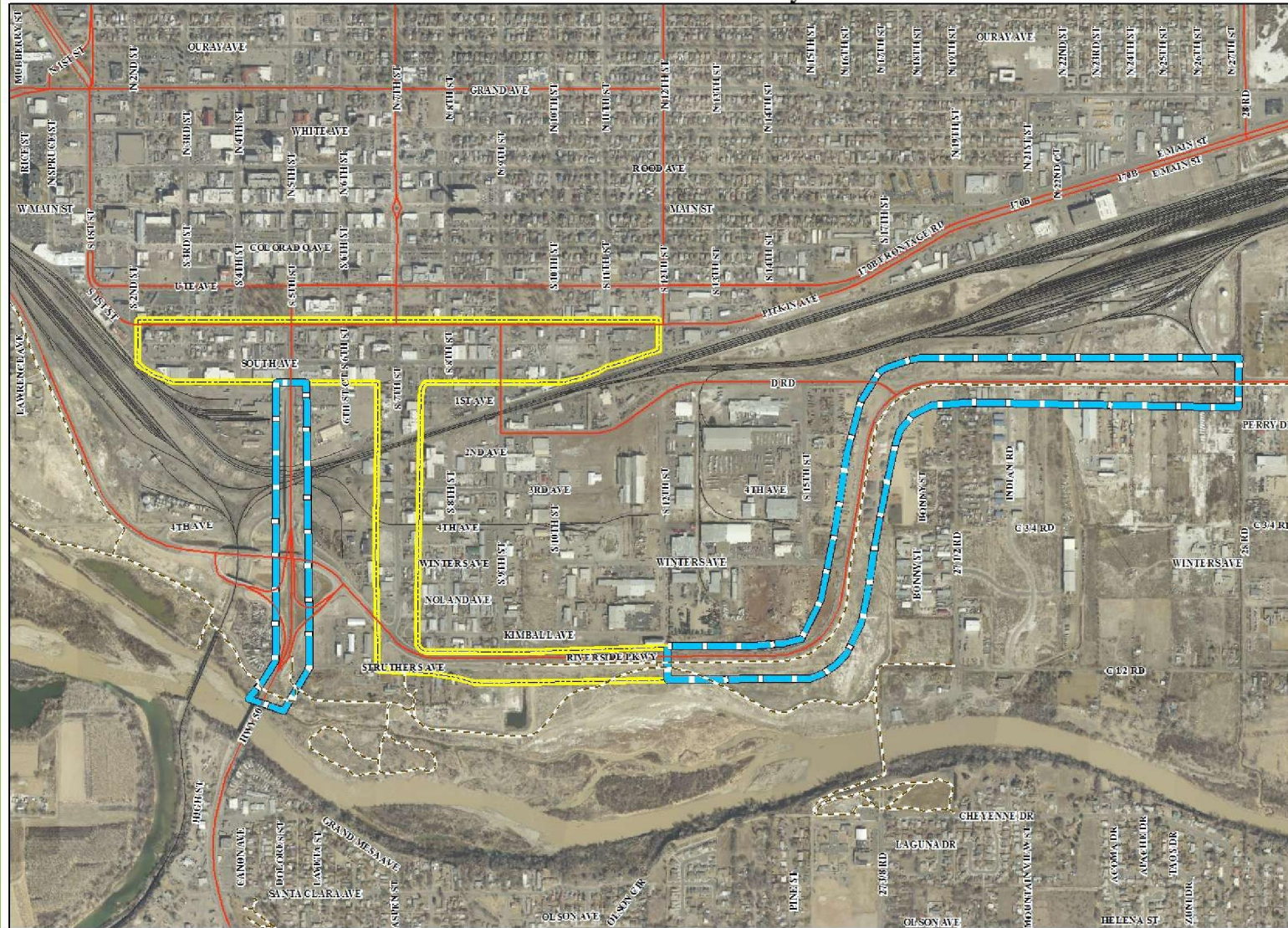
The standards identify design alternatives and specific design criteria for the visual character and physical treatment of private development and public improvements within Greater Downtown. They are adopted through an overlay zoning district, which will establish the means by which the standards are administered and enforced. The Director will make all decisions and appeals and variance requests will be heard by the City of Grand Junction Planning Commission. The Downtown Development Authority (DDA) will be a review agency for all applications and will make recommendations for proposals in the Central Business District.

Greater Downtown Plan Sub Districts



Greater Downtown Overlay Corridors

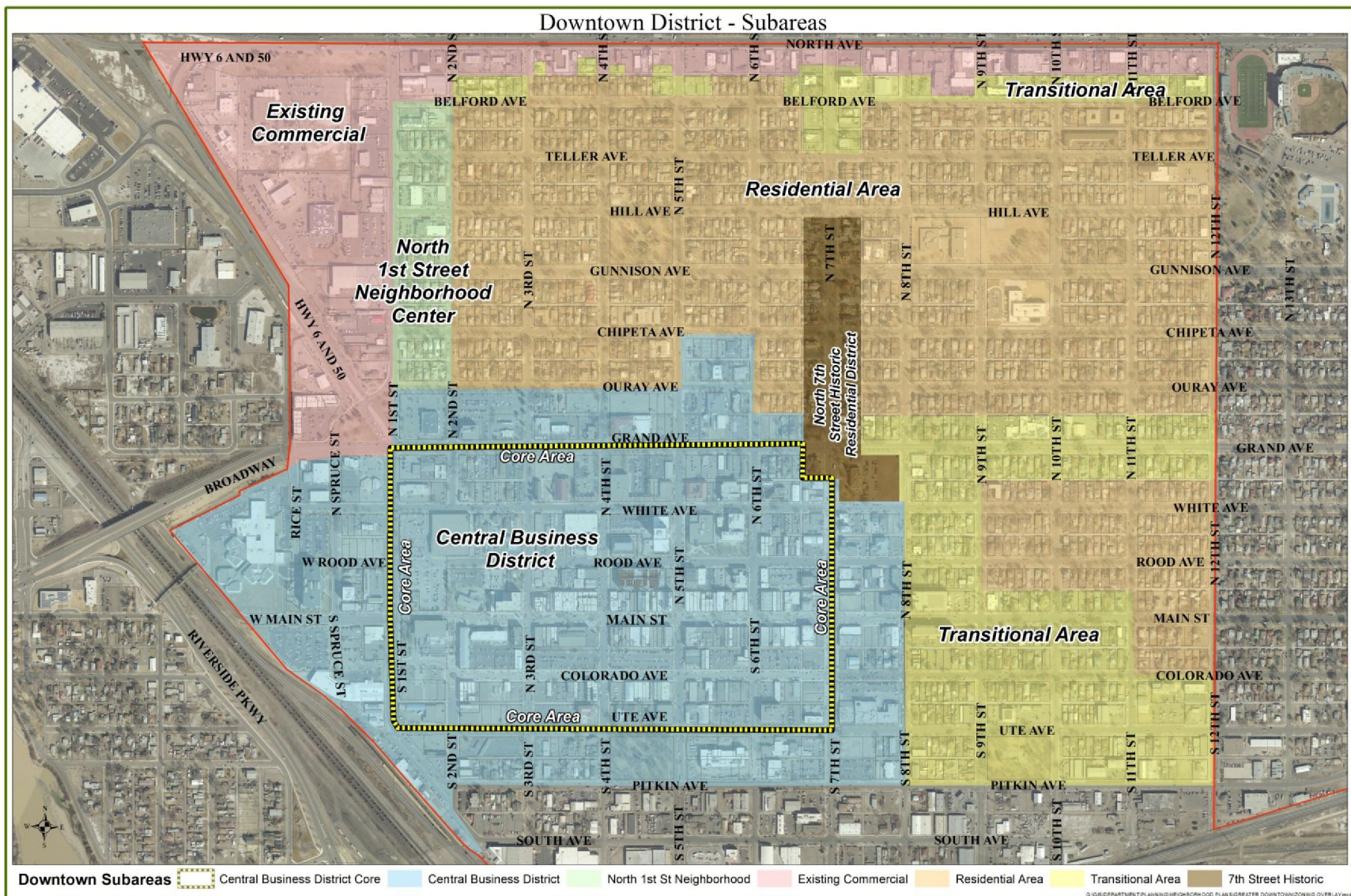
Grand Junction
REGIONAL DEVELOPMENT



Greater Downtown Overlay Corridors — Industrial Corridor — Commercial Corridor

--- Trails

© 2018 GRAND JUNCTION REGIONAL DEVELOPMENT. ALL RIGHTS RESERVED. GRAND JUNCTION, COLORADO.



Greater Downtown Overlay District

CORRIDOR STANDARDS AND GUIDELINES

1. OVERALL CORRIDOR VISION/CONCEPTS

The Greater Downtown Overlay District standards for the Greater Downtown Commercial and Industrial Corridors in the Rail and River Districts begin to implement goals of the *Greater Downtown Plan* to 1) improve the visual impact of development along the corridors; and 2) promote higher quality architectural treatment and site design as new development and redevelopment occurs along the corridors. The visual features identified below define the concepts proposed along the corridors and, as implemented as new development and redevelopment occurs, will shape the desired character of the built environment. The features are grouped into three categories: Architectural Features; Building Materials; and Streetscape and Site Design.

A. Architectural Features

1. Façade modulation and roofline variations. The corridors can be improved through use of visually interesting architectural features that are designed to reduce mass and scale, including variation in the building form with recessed or projecting bays and variation in the rooflines.



2. Façade design. Design details can be used to emphasize architectural features such as the modulation and roof line changes discussed above or other features such as entryways and windows. Façade design details that break up a façade and add visual interest include:

- a. Columns and pilasters that help break up a horizontal plane of a building or other site feature.
- b. Change of material such as on an exterior wainscoting panel.
- c. Accent colors that help define and/or accentuate architectural features.

3. Defined entry. Many of the other architectural features above can be used to accentuate and define the primary entrance to a building, add architectural interest, as well as make the building more pedestrian- or customer-friendly.

4. Window sizes and shapes. The shape and sizes of windows used on a building break up the façade, provide visual variety and provide a pedestrian- and customer friendly character.

5. Awnings and porticos. Awnings and porticos are encouraged to help provide architectural interest.

B. Building Materials

1. Mix of materials. Variations of materials used on exterior facades break up large building forms and walls.



2. Different textures, colors and tones. Use of different textures, colors and tones provide visual interest and can be used to accentuate architectural features.



C. Streetscape and Site Design

Streetscape features along the primary corridors within the Rail and River Districts in Greater Downtown also help establish the visual character of the corridors. Examples of the desired characteristics are illustrated below.

1. Building Placement. Buildings are encouraged to be brought forward on a site to the street or sidewalk.

2. Landscaping. Many of the streets within the Rail and River Districts do not have, nor are planned to have, a sidewalk. However, streetscape interest can be created through landscaping, encouraging use of xeriscape concepts.



Example Existing Streetscape Landscaping

3. Streetscape Design. An urban streetscape is desired that includes hard surfaces, tree grates and street trees. This design concept exists along some of the corridors in the Rail and River District. The urban streetscape along the street can be blended with the site design of the adjacent parcel so that on-site landscaping complements and enhances the existing streetscape.



Existing 7th Street Landscaping

4. Detached sidewalks . Where possible, detached sidewalks are desired along the commercial and industrial overlay corridors and already exist along some streets. The park strip between the curb and sidewalk can provide the landscape relief for an abutting developed property.



Existing Detached Sidewalk on Riverside Parkway (left) and South Avenue (right)

5. Outdoor storage, display and operations. Minimize the visual impact of outdoor storage, display and operations areas through placement on a site behind a building, to the rear of the property or otherwise screened.



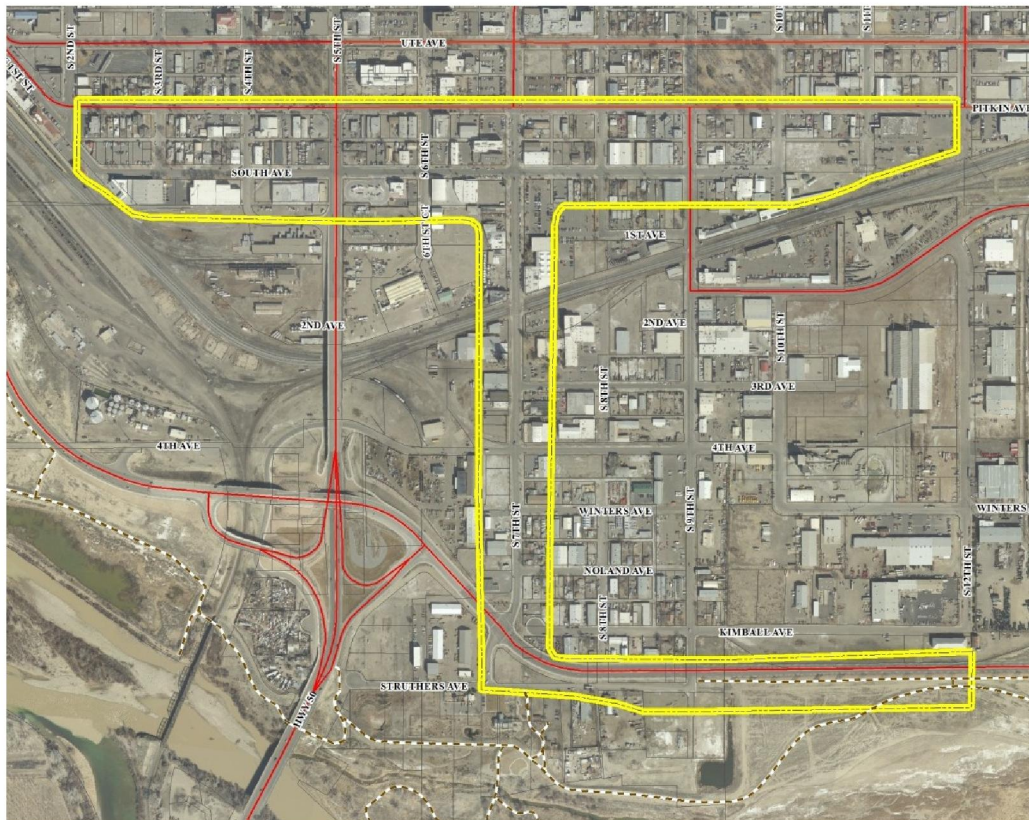
Examples of Outdoor Storage and Operations Located Behind Buildings on 7th Street (left) and Riverside Parkway (right)



Screening Example on Riverside Parkway

2. COMMERCIAL CORRIDOR STANDARDS AND GUIDELINES

The following Commercial Corridor Standards and Guidelines apply to those parcels on the south side of Pitkin Avenue between 2nd Street and 12th Street as shown on the map below: all parcels, or aggregation of parcels to be developed that have frontage on: 1) the east side of 2nd street between Pitkin Avenue and South Avenue; 2) South Avenue between 2nd Street and 12th Street; 3) South 7th Street between Pitkin Avenue and the south side of the Riverside Parkway; 4) Struthers Avenue and Riverside Parkway area from just west of South 7th Street to the alignment of 12th Street; and all parcels between South 7th Street and 12th street within 100 feet immediately north and south of the centerline of the Riverside Parkway . This area shall be referred to hereafter as the Commercial Corridor.



A. New Site Development or Redevelopment

Policies

~~1. In order to prevent parking from dominating the visual setting of the Commercial Corridor, front yards shall allow only principal structures, landscaping, sidewalks, driveway access to parking areas and signage. No parking shall be allowed in the front yard.~~

Standards

1. Outdoor storage and permanent display areas shall only be allowed in the rear half of the lot, beside and/or behind the principal structure. For properties with more than one street frontage, the front of the lot shall be considered, for purposes of this standard, to be the side abutting the higher or highest order street. Portable display of retail merchandise may be permitted subject to the provisions of the Zoning and Development Code.

2. 1. In order to prevent parking from dominating the visual setting of the Commercial Corridor, front yards shall allow only principal structures, landscaping, sidewalks, driveway access to parking areas and signage. No parking shall be allowed in the front yard. All parking that is accessory to a principal use shall be located behind or to the side of the building.

3. Front yard setbacks for principal structures on parcels within the Commercial Corridor are reduced to zero feet.

4. Residential uses are allowed as allowed in the Light Commercial (C-1) zone district in the Zoning and Development Code, regardless of how the property is zoned.

B. Architectural Design of New Buildings/Additions or Substantial Exterior Remodel

Policy Standards

For construction of new buildings and additions, or substantial exterior remodel (see definition below), any façade(s) on all buildings that face a street within the Commercial Corridor shall be designed to relate directly to and reinforce the pedestrian scale and quality of the abutting streets, civic and open spaces.

Standards

21. Any façade(s) of a new building, addition or substantial exterior remodel (see definition in 3 below) that face a street within the Commercial Corridor or that face the public property containing the Riverfront Trail, shall have visually interesting architectural features and patterns that are designed to reduce mass and scale and reflect the desired vision for the corridors as described in Section 1 on pages 8 through 11. In order to do so, the façade(s) of a new building, addition or substantial remodel that face a street within the Commercial Corridor shall exhibit a minimum of three of the following nine architectural design elements.

- a. Variation in materials, material modules, expressed joints and details, surface relief and texture to break up building forms and wall surfaces. Such detailing may include sills, headers, belt courses, reveals, pilasters, window bays or similar features.
- b. Façade articulation/variation such as recessed or projecting bays or pilaster/column projections a minimum of every 30 feet.
- c. Variation in color.
- d. Facade feature that emphasizes the primary building entrance through projecting or recessed forms, detail, color and/or materials.
- e. Variation in roof lines/roof materials in order to add interest to and reduce the scale of buildings or expanses of blank wall. This can be accomplished through design elements such as overhangs/eaves, recesses/projections, raised cornice parapets over doors or bays and peaked roof forms.
- f. Screening of mechanical equipment either located on the roof or on the ground.
- g. Windows.

h. Window or entry awning.

i. Other architectural features that achieve the goals of the overall corridor vision/concept as outlined on pages 8 through 11 as determined by the Director.

32. Definition of Substantial Exterior Remodel – Exterior building alteration that is greater than or equal to 65 percent of the value of the existing site and building. This increase shall trigger 100 percent site development compliance and 100 percent architectural standards compliance. Value shall be the greater of: total actual value per the Mesa County Assessor; or a current appraisal.

Guidelines

1. Exterior building materials should be durable, economically maintained, and of a quality that will retain their appearance over time.

C. Landscaping for New Site Development or Redevelopment

Standards

1. On-site landscaping shall be required per the Zoning and Development Code. The total amount of required landscaping may be reduced by the Director if a minimum of two of the following five conditions exist or are proposed.

a. A higher quality of architectural design is achieved with greater than three of the architectural design elements listed in B above.

b. Fifty percent (50%) or more of the building façade is setback no more than 15 feet.

c. Street trees and other finished landscaping or hardscape exists within the public street immediately in front of the property.

d. All parking is located to the side and/or rear of the building.

e. All outdoor storage and operations are located behind the building.

D. Signage

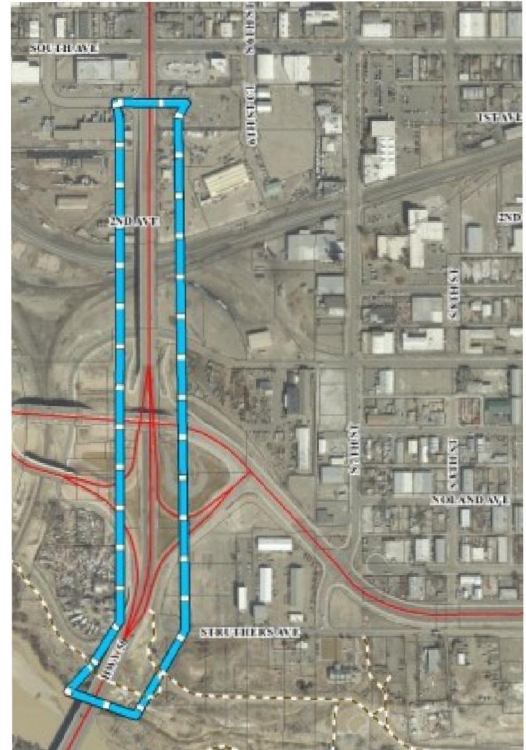
Standards

1. Only flush wall and monument style signs are allowed on the parcels that have frontage on 7th Street, Struthers Avenue, and/or Riverside Parkway within the Commercial Corridor. Flush wall signs are allowed in accordance with the Zoning and Development Code. Monument signs shall be a maximum of 12 feet in height with a maximum total of 100 square feet per sign face allowed per parcel. Illumination shall comply with the Zoning and Development Code.

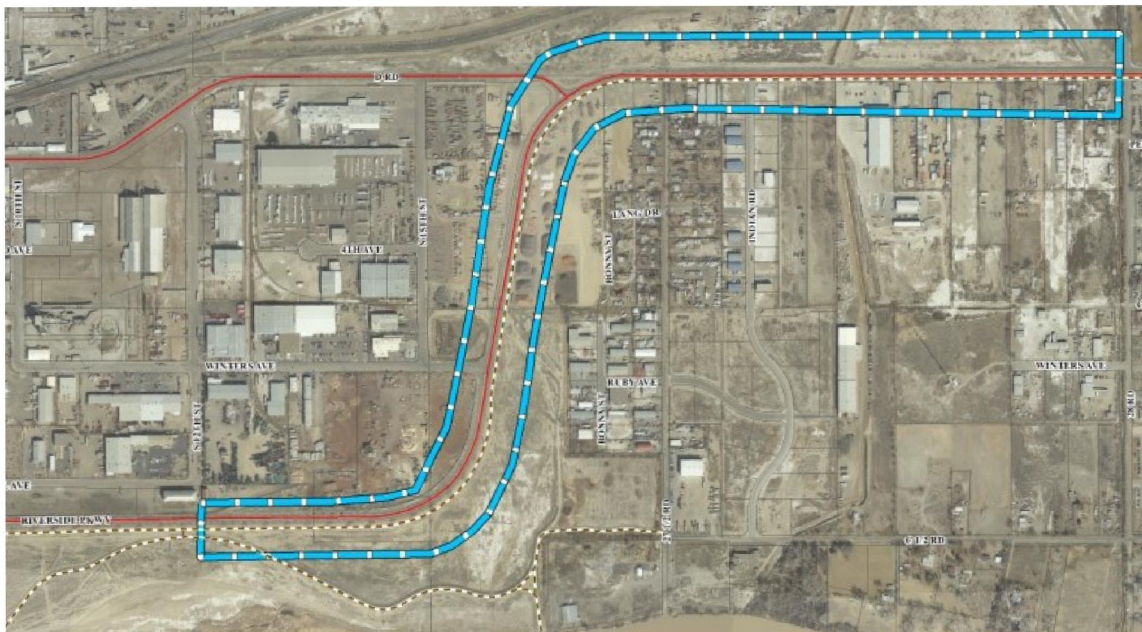
2. ~~New off-premise signs and billboards as defined by the Zoning and Development Code are not allowed on properties within the Commercial Corridor.~~ Off-premise signs and billboards as defined by the Zoning and Development Code that did not exist as of (insert effective date of the ordinance) are not allowed on properties within the Commercial Corridor.

3. INDUSTRIAL CORRIDOR STANDARDS AND GUIDELINES

The following Industrial Corridor standards and guidelines apply to all properties or aggregation of parcels to be developed with frontage on 5th Street or the 5th Street/Riverside Parkway intersection Right-of-way, south of South Avenue to the River, referred to hereafter as the 5th Street Industrial Corridor, and on Riverside Parkway from the east edge of 12th Street, extended along the Riverside Parkway, east to 28 Road, referred to hereafter as the Riverside Parkway Industrial Corridor. Please see the maps below. Collectively, the two shall be referred to as the Industrial Corridors.



5th Street Industrial Corridor



Riverside Parkway Industrial Corridor

A. 5th Street Industrial Corridor Standards

1. No new outdoor uses and operations shall be allowed on parcels that have frontage on 5th Street or on the right-of-way for the 5th Street/Riverside Parkway interchange. Maximum expansion and construction of any outdoor uses and operations (such as outdoor operation of rail yards, salvage yards, etc.) on properties that abut the 5th Street right-of-way shall be 25 percent of the total square footage as it existed on *(insert effective date of the ordinance)*.

2. ~~For uses that require screening per the Zoning and Development Code and will be visible from the elevated portions of the 5th Street viaduct, screening shall be provided on the viaduct that limits views to adjacent uses but still provides longer vistas to the east (Grand Mesa) and west (Colorado National Monument and Uncompahgre Plateau).~~ For uses that require screening per the Zoning and Development Code and will be visible from the elevated portions of the 5th Street viaduct, screening shall be provided on the viaduct that limits views to adjacent uses but still provides longer vistas to the east (Grand Mesa) and west (Colorado National Monument and Uncompahgre Plateau), in accordance with CDOT specifications as follows, "36" x 3/8" x 11 ga Core, 10 ga finish vinyl coated chain link. The City may accept payment in lieu of screening; the amount of the payment-in-lieu may be established by resolution of the City Council.

3. Off-premise signs and billboards as defined by the Zoning and Development Code that did not exist as of *(insert effective date of the ordinance)* are not allowed on properties within the 5th Street Industrial Corridor.

B. Riverside Parkway Industrial Corridor Standards

1. New Site Development or Redevelopment

a. Service entrances, service yards and loading areas shall be located only in the rear or side yard. For properties with more than one street frontage, the front yard shall be considered, for purposes of this standard, to be the side abutting the higher or highest order street. The rear and side yards are any other sides that do not meet the definition of a front yard. Along the Riverside Parkway right-of-way, a six-foot (6') high solid fence or wall of stone, wood or masonry shall screen: each service yard or area from adjoining single family residential zones and uses which are not separated by a street (not counting an alley or any easement).

b. Outdoor storage and permanent display areas shall be allowed per the zone district of the property. Any storage in the front yard adjacent to the Riverside Parkway right-of-way shall be screened with a six-foot high solid architectural wall constructed of stone, masonry or combination thereof with a minimum 14-foot landscape buffer provided outside of the wall unless modified per the landscaping section below.

c. The front yard setback for principal structures on parcels within the Riverside Parkway Industrial Corridor is reduced to zero feet.

2. Architectural Design of New Buildings or Substantial Exterior Remodel

a. Any side of a new building, addition or substantial remodel that faces the Riverside Parkway or the public property containing the Riverfront Trail shall exhibit a minimum of three of the following eight architectural design elements:

- (i) Variation in materials.
- (ii) Façade modulation/articulation a minimum of every 30 feet.
- (iii) Variation in color.
- (iv) Facade feature that emphasizes the primary building entrance.
- (v) Variation in roofline.
- (vi) Windows.
- (vii) Window or entry awning.
- (viii) Other architectural features that achieve the goals of the overall corridor vision/concept as outlined on pages 8 through 11 as determined by the Director.

3. Landscaping for New Site Development or Redevelopment

a. On-site landscaping shall be required per the Zoning and Development Code. The total amount of required landscaping may be reduced by the Director if a minimum of two of the following five conditions exist or are proposed:

- (i) A higher quality of architectural design is achieved with greater than three of the architectural design elements listed in 2 above.
- (ii) Fifty percent (50%) or more of the building façade has no more than a 20-foot setback.
- (iii) Street trees exist within the abutting public street.
- (iv) All parking is located to the side and/or rear of the building.
- (v) All outdoor storage and operations are located behind the building.

4. Signage

a. Only flush wall and monument style signs are allowed on the Riverside Parkway Industrial Corridor properties. Flush wall signs are allowed per the Zoning and Development Code. Monument signs shall be a maximum of 12 feet in height with a maximum total of 100 square feet per sign face allowed per parcel.

b. Off-premise signs and billboards as defined by the Zoning and Development Code that did not exist as of (*insert effective date of the ordinance*) are not allowed on properties within the Riverside Parkway Industrial Corridor.

DOWNTOWN DISTRICT STANDARDS AND GUIDELINES

1. OVERALL DOWNTOWN DISTRICT VISION/CONCEPTS

In order to implement the *Greater Downtown Plan*, the following zoning overlay standards have been developed for the subareas of the Downtown District. Application of the standards and guidelines will begin to implement goals of the plan to:

- Maintain and enhance the economic, cultural and social vitality of the Downtown District.
- Promote downtown living by providing a wide range of housing opportunities.
- Enhance the transportation system to accommodate automobiles, transit, bikes and pedestrians, and provide adequate, convenient parking.
- Stabilize and enhance the historic residential neighborhoods.
- Establish and promote a unique identity for each of the subareas of the Downtown District.
- Preserve and restore significant historic structures.

- Activate the edges of the downtown parks with mixed use and programmed/active use of the parks as urban open space rather than passive green parks.

2. DOWNTOWN DISTRICT AREA-WIDE POLICIES, STANDARDS AND GUIDELINES

A. Policies

1. Maintain and enhance the economic, cultural and social vitality of downtown
2. Promote downtown living by providing a wide range housing opportunities
3. Enhance the transportation system to accommodate automobiles, bikes and pedestrians and provide adequate, convenient parking
4. Stabilize and enhance the historic residential neighborhoods
5. Establish and promote a unique identity
6. Preserve and restore significant historic structures
7. Activate the edges of the downtown parks with mixed use and programmed/active use of the park as urban open space rather than passive green parks.

B. Standards

Due to the constraints of many downtown properties and the City's desire to promote improvement and redevelopment in the Downtown District, the Director may make reasonable exceptions to the provisions of the Zoning and Development Code and the Greater Downtown Overlay District to bulk standards (except for building height), landscaping, parking or other use-specific special regulations for properties that are within the Central Business District (CBD), Transitional or Residential subareas. The following criteria shall be used to consider exceptions from the bulk standards, landscaping, parking or other use-specific special regulations.

1. Hardship Unique to Property, Not Self-Inflicted. There are exceptional conditions creating an undue hardship, applicable only to the property involved or the intended use thereof, which do not apply generally to the other land areas or uses within similar zone districts, and such exceptional conditions or undue hardship was not created by the action or inaction of the applicant or owner of the property;
2. Special Privilege. The exception shall not confer on the applicant any special privilege that is denied to other lands or structures within similar zone districts;
3. Literal Interpretation. The literal interpretation of the provisions of the regulations would deprive the applicant of rights commonly enjoyed by other properties in similar zoning districts and would work unnecessary and undue hardship on the applicant;
4. Greater Downtown Plan Goals. The proposal actually meets overall goals of the Plan better than if standards are followed;
5. Conformance with the Purposes of the Zoning Overlay and the Zoning and Development Code. The granting of an exception shall not conflict with the purposes and intents expressed or implied in this Zoning Overlay or the Zoning and Development Code; and

6. Conformance with Comprehensive Plan. The granting of an exception shall not conflict with the goals and principles in the City's Comprehensive Plan.

C. Guidelines

1. Projects will include good, interconnected transportation choices for better access and better health.
2. Traffic calming measures will be provided where appropriate, including pedestrian refuge areas, medians, landscaping and corner bulb-outs.

3. CENTRAL BUSINESS DISTRICT (CBD) STANDARDS AND GUIDELINES

The following standards and guidelines apply to the CBD area shown on the map (blue area). Further development and implementation of these concepts will be done in coordination with the DDA. The standards and guidelines are intended to apply to new development or redevelopment within the area.

A. Application of Standards and Guidelines

1. Unless otherwise noted below, all of the standards and guidelines shall apply under the following conditions:

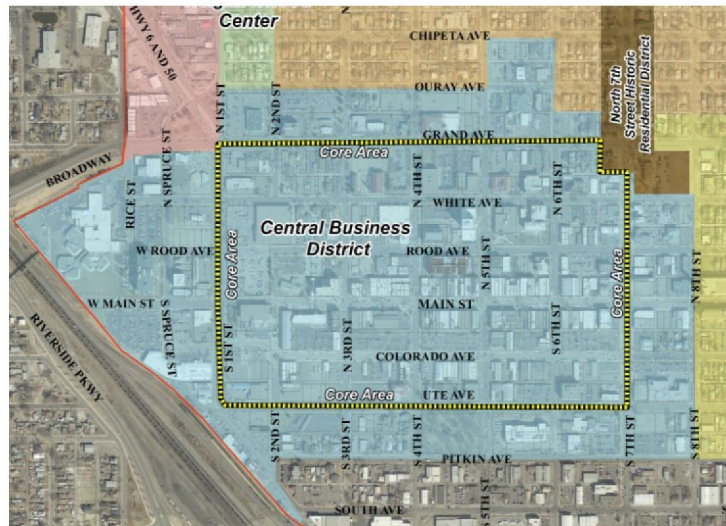
a. Construction of a new building

b. Addition – Construction of an addition to an existing building if the addition increases total building square footage by 100 percent or greater (baseline is building square footage of existing building on *insert date ordinance becomes effective*, 2013). This expansion standard is cumulative after this date in 2013. Once square footage has exceeded 100 percent of the 2013 square footage, the standards and guidelines shall apply to all further expansions.

c. Substantial Exterior Remodel – Exterior building alteration that is greater than or equal to 65 percent of the value of the existing site and building. This increase shall trigger 100 percent site development compliance and 100 percent architectural standards compliance. This shall not trigger the requirement of minimum height. Value shall be the total actual value per the Mesa County Assessor or the appraised value based on an appraisal completed by a certified appraiser licensed to do business in the State of Colorado utilizing the “cost” approach.

2. The standards and guidelines do not apply to:

a. Construction of an addition to an existing building if the addition will increase the total building square footage by less than 100 percent (baseline is building square footage of existing building on *insert date ordinance becomes effective*, 2013). This expansion standard is cumulative after this



date in 2013. Once square footage has exceeded 100 percent of the 2013 square footage, the standards and guidelines shall apply to all further expansions.

b. Exterior building alteration that does not exceed 65 percent of the value of the existing site and building (value determined as set forth above).

c. Interior remodel of an existing building.

d. [This Zoning Overlay](#) does not prohibit rezoning to a form-based zone within the Downtown District [and this Zoning Overlay shall not apply to properties within a form-based zone](#) within the Downtown District.

B. Overall Central Business District (CBD) Vision/Concepts/Policies

1. Activate the Downtown Core area streets through emphasis on higher pedestrian traffic , businesses on the ground level that are oriented towards attracting higher pedestrian volumes, and buildings that “turn the corner” (invite activity on both the primary and cross streets). Refer to the CBD Core Area map on page 19.



Existing Downtown Street Activity

2. Maintain the prominence of buildings along the streets by minimizing building setbacks.

3. Encourage high quality, compatible design for all new buildings and establish a cohesive architectural character/theme that harmonizes new structures with the existing buildings through common materials, scale and basic architectural details as outlined in greater detail in the following standards and guidelines.

4. Typical building materials found in the CBD materials are traditional and weather well, allow a broad variety in appearance and ensure buildings are of high quality. To facilitate the creation of a cohesive architectural character/theme for new buildings, additions or exterior remodels in the Downtown Core, the following exterior finishes are most appropriate: brick, sandstone, stucco, metal cladding, tiles, wood, glazing and decorative concrete masonry units (CMU).



Example Downtown Building Materials

5. Encourage high density, mixed-use development and structures (e.g. retail at street level and residential or office above).



Existing Downtown Mixed Use

6. Encourage gradual scale transitions between the CBD and adjacent neighborhoods. Taller buildings will be located in the center and southern and western perimeter of the CBD, with shorter buildings on the northern and eastern edges of the CBD.

7. Encourage maximizing building scale and intensity/density by offering incentives to build above the required minimum height.

8. Minimize single use, surface parking throughout the CBD.

9. Maintain streetscapes dominated by buildings with parking located behind. Consider elimination of existing curb cuts as a performance benefit.

10. Encourage shared parking.

11. Encourage new infill development on existing, under-utilized surface parking lots.

12. Provide streetscape details and landscaping that compliment the architectural character of the CBD and exhibit urban character.

13. Create entries to the CBD at strategic locations as shown on the Wayfinding and Signage Map in the Greater Downtown Plan report. Enhancements may include landscaped medians, corner bulbs and special signs.

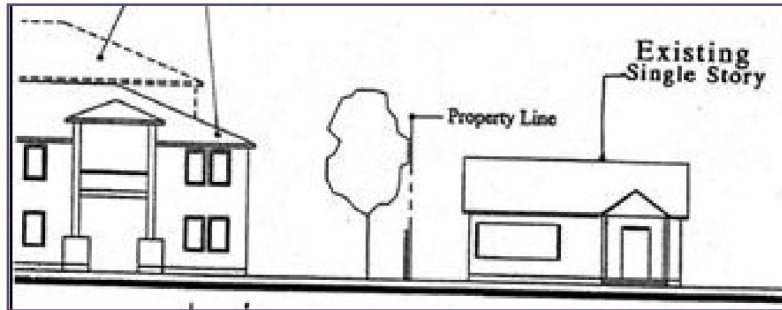
14. The DDA will assist in developing sign standards and guidelines for private signage placed on buildings or as freestanding signs for consideration and adoption by the City.

C. Overall Central Business District (CBD) Standards

1. Maximum building height in the CBD shall be 90 feet. A one-time increase of up to 25 percent per property may be considered by the Grand Junction Planning Commission.

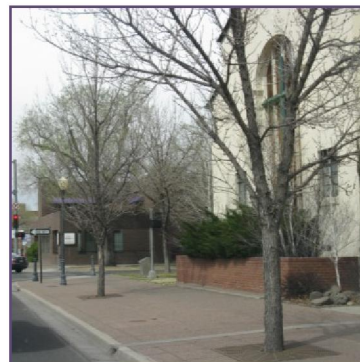
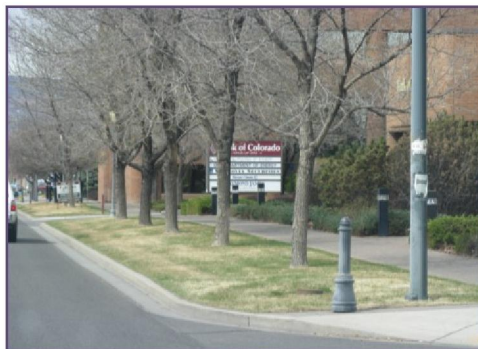
2. Buildings shall be set back a minimum of 20 feet from the street on Chipeta and Ouray Avenues.

3. The buildings will step down so that the front façade of the buildings that are directly across Ouray and Chipeta Avenues from residential buildings or uses are no taller than 40 feet. Minimum depth of the step shall be 10 feet.



Example of Building Stepping Down to Single Family Residential Scale

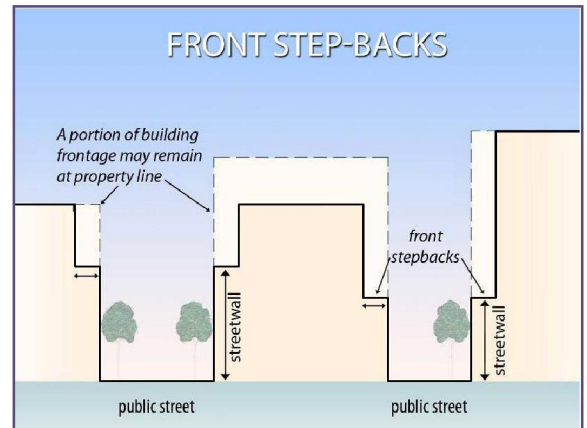
4. Scale and massing of buildings or portions of buildings along Ouray and Chipeta Avenues will be compatible with residential scale.
5. If off-street parking is provided, it shall be located behind buildings on private property. If the property abuts an alley, the parking area shall take access from the alley. If the property has more than one street frontage, “behind the building” shall mean on the opposite side of the building from the front door or the main public door entrance to the building.
6. Maximize opportunities for on-street parking by minimizing curb cuts along the street.
7. All pedestrian level lighting shall be downlit and, if on poles, shall be in a historical style light standard.
8. The streetscape along Grand Avenue and 4th and 5th Streets north to Ouray Avenue within the CBD will continue in a design compatible with the existing improvements along Grand Avenue (e.g. decorative pavement and street trees).
9. Landscaping is expected to comply with the Zoning and Development Code per the requirements of the zone district. The Director may approve variations for new development or redevelopment in the CBD if: 1) street trees exist within the abutting public street; and 2) streetscape elements (plantings, low walls and/or street furniture) are proposed with the development consistent with the urban design character of the CBD.
10. The streetscape along 5th and 6th Streets north of Ouray Avenue to Chipeta Avenue within the CBD shall transition between the urban hardscape and a more residential streetscape character (e.g. detached sidewalk, landscaping in park strip between curb and sidewalk and street trees).



Existing Grand Avenue Streetscape

D. Central Business District Core Area (Downtown Core or Core Area) Guidelines

1. Façade detailing should be compatible with, but not be identical to, that of a neighboring historic building. New facades should have their own, unique design. To create continuity, horizontal lines should be in alignment with neighboring buildings.
2. Entrances are often the primary focal point of a building and, as such, should be designed to fit with the overall character of the area.
3. Doorways may be finished with paints, stains, metal and aluminum cladding set to match the existing trim colors.
4. Single, double, revolving and corner doorways are acceptable in new construction or substantial remodels.
5. On upper floors of street-facing facades on buildings taller than the traditional three stories in the CBD, consider stepping back the upper floors a minimum depth of 10 feet in order to enhance the traditional scale of the CBD and ensure adequate air and light at the sidewalk level.



E. Central Business District Core Area (Downtown Core or Core Area) Standards

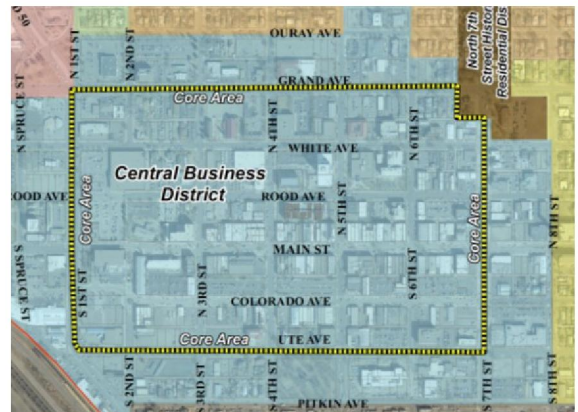
The following standards apply only to the Downtown Core shown on the map (within the yellow-black dashed line).

1. Building Height

- a. Minimum building height in the Downtown Core shall be two stories.

The following uses as allowed in the zone district and as defined by the Zoning and Development Code shall be exempt from the minimum two story requirement:

- Schools, Colleges and Universities
- Vocational, Technical and Trade Schools
- Community Activity Building
- All Other Community Service
- Museums, Art Galleries, Opera Houses, Libraries
- Day Care
- Detention Facilities
- Hospital/Clinic
- Parks and Open Space
- Religious Assembly
- Funeral Homes/Mortuaries/Crematories





Existing Downtown Core Scale

- Safety Services
- Utility Infrastructure and Corridors
- Car Wash, Gasoline Service Station, Quick Lube
- Industrial Services, Contractors and Trade Shops with Indoor Operations and Storage

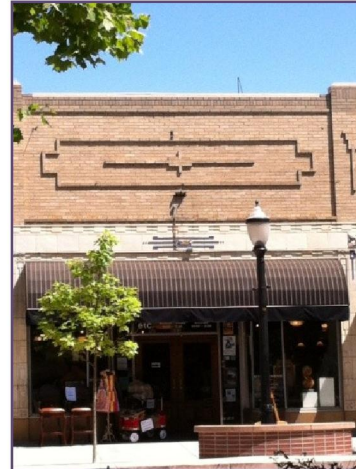
Exemptions to this requirement for other uses of land or occupancies of a building not listed and that is not conducive to a vertical organization of operational space may be considered and approved by the Grand Junction Planning Commission with input from the DDA upon a review of a conceptual level development proposal.

The following criteria shall be used by the Planning Commission to consider exemptions from the bulk standards, landscaping, parking or other use-specific special regulations.

- a. Hardship Unique to Property, Not Self-Inflicted. There are exceptional conditions creating an undue hardship, applicable only to the property involved or the intended use thereof, which do not apply generally to the other land areas or uses within similar zone districts, and such exceptional conditions or undue hardship was not created by the action or inaction of the applicant or owner of the property;
- b. Special Privilege. The exception shall not confer on the applicant any special privilege that is denied to other lands or structures within similar zone districts;
- c. Literal Interpretation. The literal interpretation of the provisions of the regulations would deprive the applicant of rights commonly enjoyed by other properties in similar zoning districts and would work unnecessary and undue hardship on the applicant.
- d. Greater Downtown Plan Goals. The proposal actually meets overall goals of the Plan better than if standards are followed.
- e. Conformance with the Purposes of the Zoning Overlay and the Zoning and Development Code. The granting of an exception shall not conflict with the purposes and intents expressed or implied in this Zoning Overlay or the Zoning and Development Code; and
- f. Conformance with Comprehensive Plan. The granting of an exception shall not conflict with the goals and principles in the City's Comprehensive Plan.

2. Building Setbacks/Site Placement, Scale, Massing and Street Encroachment

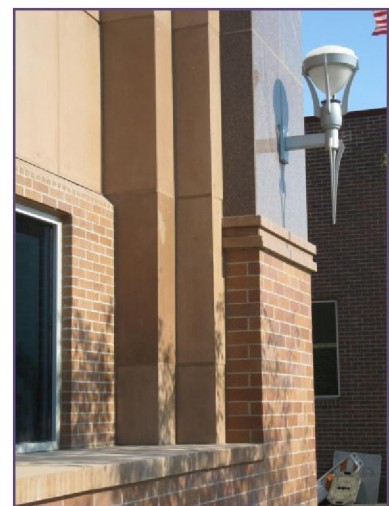
- a. Maximum building setback from the abutting street shall be two feet or compatible with the mean setback of the immediately adjoining lots on both sides of the subject lot but in no case greater than 20 feet .
- b. When building setbacks are not determined as in a above, setbacks of up to 10 feet from the abutting street may be allowed if there is a designed function for the space such as limited outdoor display, seating, outdoor dining areas or a small street park, whether for private or public use.
- c. Architectural features on stories above street level may encroach on the public right-of-way, provided all safety considerations have been met. A revocable permit for such encroachments shall be reviewed and may be approved by the Director or a permanent easement for such encroachments may be reviewed and considered for approval by City Council.
- d. Awnings that overhang windows or entries on street level facades are encouraged and shall be constructed of canvas or heavy cloth or metal (no plastic), utilizing primarily neutral colors.



Existing Canopy on Main Street Building

3. Architectural Character – The façade(s) of a new building, addition or substantial exterior remodel that abut the streets within the Downtown Core shall have articulated architectural features and patterns that are designed to reflect the desired vision for the CBD described in B on pages 20-21. In order to do so, the façade(s) of a new building, addition or substantial exterior remodel within the Downtown Core shall exhibit a minimum of four of the following nine architectural design elements.

- a. On corner parcels, façade design of ground floors “turn the corner” to induce activity and interest in the streetscape on the north-south streets within the Downtown Core.
- b. Facades are articulated and have ornamentation such as varied brick patterns, change in material or color accents and window headers or columns that create shadow lines are examples of acceptable ornamentation.



*Example Ornamentation
for Visual Interest*

c. The street level front façade of the building is “active” with at least 50 percent of the façade in windows, with doors spaced no more than 50 feet apart. Side facades shall meet a minimum of 50 percent of this requirement.

d. Façade features that emphasize the primary building entrance through projecting or recessed forms, detail, color or materials.

e. Building facades are articulated on any street-facing side. Larger buildings are articulated in a hierarchy of smaller volumes and masses that better relate to other buildings and the scale of streets. This is accomplished through establishment of building bays that are distinguished by recessed or protruding elements or a variation in materials or color to break up the façade and reduce the overall scale of large buildings.



Example Façade Articulation to Define Smaller Scale Bays

f. Higher levels of fenestration are required for buildings along both sides of the streets within the CBD Core Area. At a minimum, fenestration shall be concentrated on the street level façade and diminish on upper floors (e.g. window size decreases as the floor level increases).



Existing Facades with Diminishing Fenestration on Upper Floors

g. Buildings include a façade cap. The cap is defined by a distinct roof line or parapet. The design uses ornamentation of these features to enhance the building's identity and support the architectural character of the CBD. The façade cap is in three dimensions that projects and casts a shadow and relates proportionately to the overall building design.

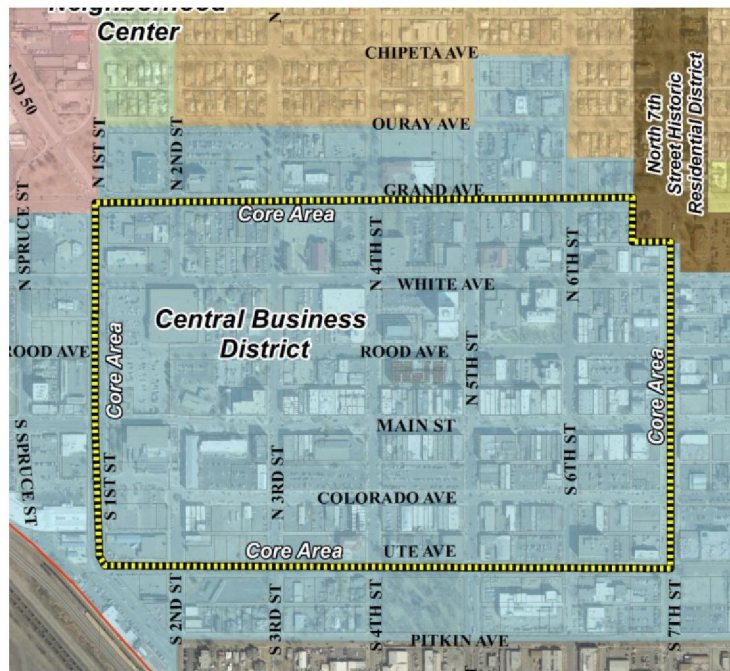


Example Three Dimensional Façade Cap

h. Building design minimizes the visual impact of mechanical equipment located on the roof as viewed from the adjacent streets.

i. Other architectural features that achieve the goals of the overall Central Business District (CBD) vision/concept as outlined in B on pages 20-21 as determined by the Director.

4. On-street parking shall be located and designed to maintain and support a safe pedestrian environment on streets located within the Downtown Core (yellow-black outline on map below). This includes coordinating crosswalks with parking location and eliminating visual and physical obstructions to the pedestrian travel way.

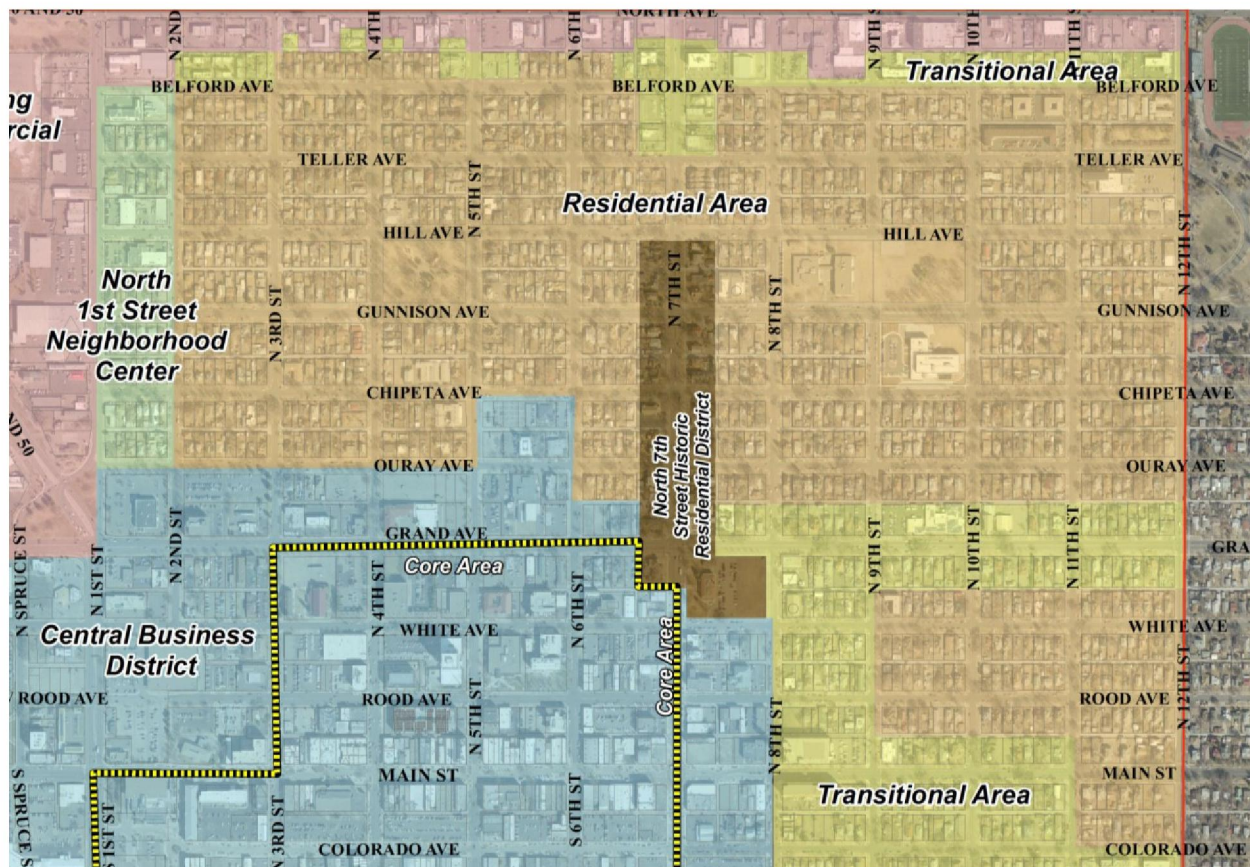


4. RESIDENTIAL AREAS STANDARDS AND GUIDELINES

The following standards and guidelines apply to the Residential areas shown on the map below (orange areas). The standards and guidelines are intended to apply to new development or substantial redevelopment within the area. Substantial redevelopment is any reconstruction, rehabilitation, addition or other improvements to the existing structure(s) on a site where the value of the improvement exceeds 50 percent of the fair market value of the building(s) before the start of construction.

A. Policies

1. The existing historic residential neighborhoods within the Downtown District will be stabilized and enhanced.
2. The existing historic residential neighborhoods within the Downtown District will be preserved for residential uses, with no further encroachment by nonresidential uses.
3. Where existing residential zoning allows, provide a diversity of housing types through development of multifamily housing that is in keeping with the character of the neighborhood (refer to Multifamily Development section on page 30).
4. Enhance access to and improvements within existing public open spaces (e.g. parks and school grounds) within the downtown residential core such as enhanced pedestrian crossings and lighting for safety.



5. Maintain and enhance the historic character of the streetscape with emphasis on the following elements: street trees, landscaping rather than parking or other uses in the park strip between sidewalk and curb, street signs that identify the neighborhoods, lighting and detached sidewalks.



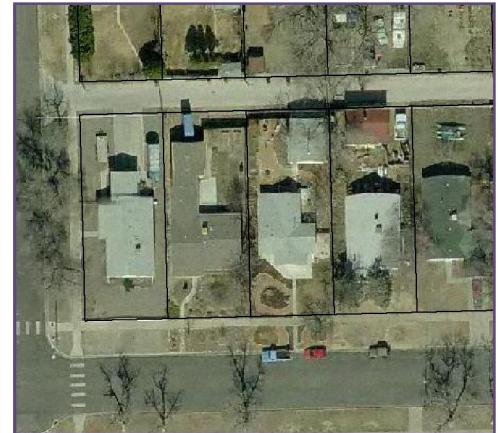
Existing Residential Subarea Streetscape Character

B. Standards

1. Architectural Considerations

a. Building Style and Character. Maintain the existing character of the house styles within the residential neighborhoods in the Downtown District. New construction and alterations shall be compatible with key architectural characteristics and site elements of the neighborhood.

b. Accessory Structure Setbacks. The setback for accessory structures is a zero foot setback from the alley and three feet from neighboring property line(s).



Existing Residential Building Alignment

c. Building Mass/Scale and Proportion. New buildings or additions to existing buildings shall be visually compatible with the area. Visually compatible means compatible with adjacent and neighboring buildings including mass and scale, shape, windows, doors, openings, roof shape, roof pitch and orientation.

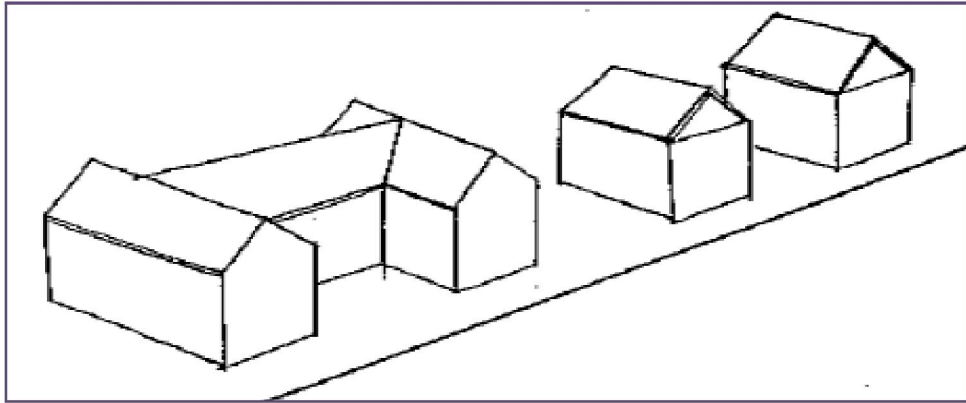
d. Roof Shape. The roofs of new buildings shall be visually compatible with adjacent dwellings. If pitched, the roof pitch shall be at least 4:12.

e. Fenestration. Structures shall be visually compatible with surrounding residential structures. Visually compatible includes the relationship of width to height, and the spacing of windows and doors. For example, tall evenly-spaced rectangular windows are typical of many of the residential styles in the downtown area.



Example Existing Architectural Character

- f. Materials. The exterior materials of all new buildings, additions and alterations shall be similar in size and appearance to adjacent dwellings.
- g. Setbacks. On a corner lot, the front yard setback for that part of the yard that extends to and along the side property line on the street side may be reduced to 10 feet on properties within the Downtown District Residential subareas.
2. Accessory Structures
- a. Accessory structures shall be no taller than the highest eave line of the principal structure.
- b. The footprint size of an accessory structure shall be a maximum of 35 percent of the footprint of the principal structure.
3. Multifamily Development
- Infill of new multifamily buildings may occur where zoning allows within the residential neighborhoods of the Downtown District. However, the site design and structures for this type of development must maintain a scale and character compatible with the residential neighborhoods in the Downtown District. In addition to the Architectural Considerations listed in 1. above, multifamily development shall follow the standards below.
- a. Incorporate forms typical of the single family residential architecture of the Downtown District including sloping roofs, porches, roof dormers and other architectural details.
- b. Break up the mass of larger buildings into forms that are similar in scale to the single family residential character.
- c. Facades must be composed of smaller sections, similar in scale and material finish to single family residential structures.



Example – Break Up Façade of Larger Structure to be Compatible with Single Family Scale

- d. Off-street parking for multifamily development shall not be located in the front yard setback. Parking shall be in the rear or side yards. If the property abuts an alley, the parking area shall take access from the alley. If the property has more than one street frontage, “behind the building” shall mean on the opposite side of the building from the front door or the main public door entrance to the building.
- e. Develop pedestrian links between the front sidewalk and building entrances and between parking and rear or side entrances.

C. Guidelines

1. Demolition of existing historic homes in order to construct new residential structures is strongly discouraged.
2. Maintain and enhance the pattern of landscaped front yards that gives the residential neighborhoods within the Downtown District a distinctive, friendly appearance.
3. Each new building and addition should be located so that it aligns with existing neighborhood buildings. “Aligns” means elevation (e.g. horizontal lines of peaks of roofs, cornices and window sills) and plan (e.g. setbacks from the street and rear property lines and spacing between structures/setbacks from side property lines).
4. Main entrances should open onto a street and should align with those of adjacent residential buildings. For example, on many of the downtown homes, raised foundations and steps that define the main entrance are prevailing characteristics. Door styles should be similar to those found on residential buildings within the area.
5. New buildings and additions should have the same number of stories and a height which is compatible with buildings within the same block.

6. Park strips will be landscaped in a traditional style, including street trees, grass, and low plantings or a combination thereof. Park strip landscaping should include some live material – use of all non-living material such as rock is discouraged. Use of drought-tolerant plants is encouraged.



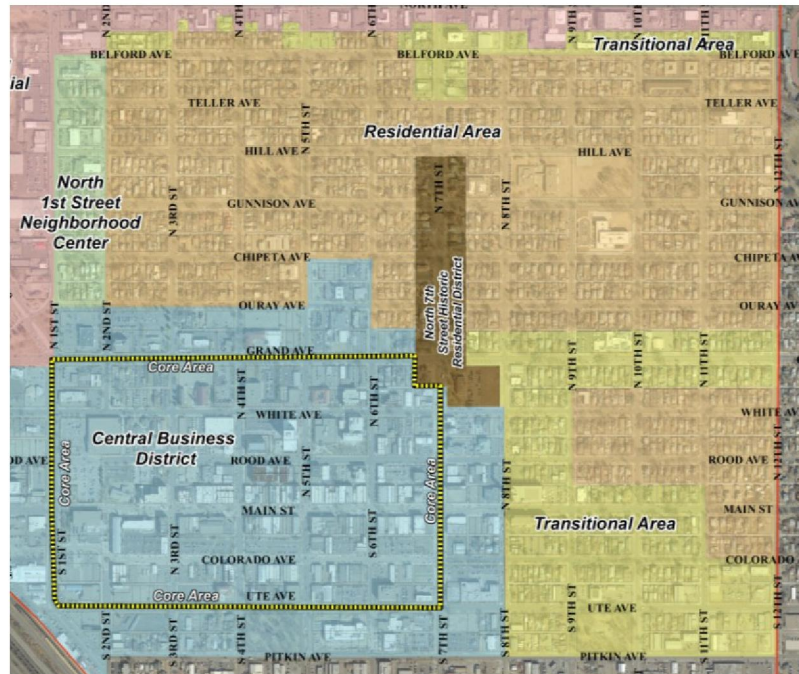
Existing Character of Front Yards and Park Strips

5. TRANSITIONAL AREAS STANDARDS AND GUIDELINES

The following standards and guidelines apply to the Transitional areas shown on the map on the following page (yellow areas). The standards and guidelines are intended to apply to new development or substantial redevelopment within the area. Substantial redevelopment is any reconstruction, rehabilitation, addition or other improvements to the existing structure(s) on site where the value of the improvement exceeds 50 percent of the fair market value of the building(s) before the start of construction.

A. Policy

The peripheral areas of the CBD provide a mix of established residential uses and low intensity, nonretail, neighborhood service and office uses that are compatible with adjacent residential uses and neighborhoods. New development or reuse of existing structures will maintain compatibility with residential building scale and appearance.



B. Standards

1. Land Use and Development Intensity

- a. Any mix of residential and nonresidential uses on the same lot shall be located in the same structure.
- b. No uses within the Transitional Subareas shall open earlier than 7:30 am and shall close no later than 8:00 pm.
- c. Maximum building size shall not exceed 10,000 square feet unless a Conditional Use Permit is issued.
- d. Outdoor storage and display areas are prohibited in the Transitional Subareas.

2. Architectural Considerations

New residential or nonresidential construction, including additions and rehabilitations, in the Transitional Subareas shall be designed to have a single family residential character consistent with existing buildings in the area. "Consistent" means the operational, site design and layout, and architectural considerations described below.

- a. Every new principal building shall be located so that it aligns with existing buildings within the same block. "Aligns" means elevation (*e.g.*, horizontal lines of peaks of roofs, cornices, window sills) and plan (*e.g.*, setbacks from the street and rear property lines and spacing between structures/setbacks from side property lines).



Example Infill Development in Transitional Area - 9th Street and Colorado Avenue

- b. Main entrances shall open onto a street and shall vertically align with those of adjacent residential buildings in the same block. For example, in areas adjacent to the Transitional Subareas, raised foundations and steps that define the main entrance are prevailing residential characteristics. Door styles shall be similar to those found on residential buildings.
- c. Each new principal building, its mass in relation to open spaces and its windows, doors, and openings shall be visually compatible. Visually compatible means compatible with adjacent and neighboring buildings including mass, shape, window, doors, openings, roof shape, roof pitch and orientation. For example, a large building shall be compatible with surrounding smaller dwellings by dividing its mass into smaller components to create a building elevation that is more like the size and proportion of the nearby single family homes.
- d. The roofs of new principal buildings or additions to principal buildings shall be visually compatible with buildings within the same block. When pitched, the roof pitch shall be at least 4:12.
- e. Structures shall be visually compatible with surrounding residential structures. Visually compatible includes the relationship of width to height, and the spacing of windows and doors. For example, tall evenly-spaced rectangular windows are typical of certain residential styles near the Transitional Subareas.

3. Signs

Development of non-single family uses in the downtown Transitional areas may directly abut existing single family residential areas. Thus, in order to maintain compatibility, more restrictive sign regulations shall apply.

- a. Flush wall signs and monument signs shall be the only sign type allowed. Only one real estate sign advertising the property for sale or lease shall be allowed and shall not exceed 10 square feet.
- b. Signs shall be located at least 10 feet behind the front property line. Total sign area, excluding real estate signs advertising the property for sale or lease, shall not exceed 25 square feet per street frontage. The sign allowance for one street frontage may be transferred to a side of a building that has no street frontage, but cannot be transferred to another street frontage. Monument signs shall not exceed eight feet in height.



Example Signs within Transitional Subarea

- c. Signs may only be illuminated between 7:30 a.m. and 8:00 p.m.
- d. Sign enhancement features such as bases, pillars, and other decorative elements as part of monument signs shall not be counted as part of the maximum square footage of the sign, provided such features do not exceed the size of the sign face.

4. Parking and Site Development

- a. Non-single family uses in the Transitional Subareas shall be designed and utilized not to increase on-street parking in front of single family dwellings in the neighborhood.
 - On-site parking shall be provided pursuant to the Zoning and Development Code; and
 - On-site parking spaces shall only be located in the side and rear yards. If the property abuts an alley, the parking area shall take access from the alley. If the property has more than one street frontage, side and rear yards shall mean on the opposite side of the building from the front door or the main public door entrance to the building; and
 - On-site parking shall be screened from nearby single family residential uses by a solid wall, fence or vegetation having a height of not less than four feet nor more than six feet (vegetation may exceed six feet in height).
- b. Service entrances, loading areas and dumpster areas shall be located only in the rear or side yard. If the property has more than one street frontage, the rear or side shall mean on the opposite side of the building from the front door or the main public door entrance to the building; and each loading area shall be screened from each abutting residential use or zone.
- c. Front yards shall contain only landscaping, sidewalks, driveway access to parking areas and signage.

C. Guidelines

1. New buildings should have the same number of stories and a height which is compatible with those of nearby single family residential buildings.
2. The exterior of all new buildings, additions and alterations should be similar in size and appearance to nearby dwellings. Sign materials should be visually compatible with materials used on the building façade.

Exhibit B: Section 21.03.020
(d) Districts to Implement the Comprehensive Plan

Zoning District	Comprehensive Plan Land Use Designation																
	RESIDENTIAL								NONRESIDENTIAL								
	Low				Medium		High										
	Rural	Estate	RL	RML	RM	RMH	RH-MU	UR RH-MU	Commercial	NC-MU	VC-MU	DT-MU	Industrial	C/I	BP-MU	P & OS	Conservation/ Mineral Extraction
RR	•	•	•	•													•
R-E	•	•	•	•													
R-1	•	•	•	•													
R-2	•	•	•	•													
R-4	•	•	•	•	•	•											
R-5	•	•	•	•	•	•											
R-8					•	•				•	•				•		
R-12					•	•				•	•				•		
R-16						•	•	•		•	•	•			•		
R-24+							•	•			•	•			•		
R-O					•	•	•	•	•	•	•	•			•		
B-1								•	•	•	•						
B-2												•					
C-1									•	•	•	•					
C-2									•					•			
CSR	•														•	•	•
M-U									•				•	•			
BP														•	•		
I-O													•	•	•		
I-1													•	•			
I-2													•				
MXR-3 MXG-3 MXS-3										•	•	•					
MXR-5 MXG-5 MXS-5											•	•					
MXR-8 MXG-8 MXS-8												•					

ATTACHMENT 3

**CITY OF GRAND JUNCTION, COLORADO
ORDINANCE NO. _____**

**AN ORDINANCE REZONING PROPERTIES WITHIN THE GREATER DOWNTOWN
PLAN AREA**

RECITALS.

The City has adopted the Greater Downtown Plan which includes a Future Land Use Map for the Greater Downtown area.

In order to implement the Greater Downtown Plan, certain parcels are being rezoned to be consistent with the Future Land Use Map and the goals of the Greater Downtown Plan.

City Council finds that the proposed rezone of property as depicted in the Greater Downtown Plan Zoning Map meets the zoning criteria stated in section 21.02.140 of the Municipal Code.

**NOW THEREFORE, BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF
GRAND JUNCTION:**

| That the properties as listed in Exhibit A are rezoned.

INTRODUCED on first reading the _____ day of _____, 2013 and ordered published pamphlet form.

PASSED and **ADOPTED** on second reading the _____ day of _____, 2013 and ordered published in pamphlet form.

President of City Council

ATTEST:

City Clerk

EXHIBIT A – GREATER DOWNTOWN REZONED PROPERTIES

PARCEL NUMBER	LOCATION	EXISTING ZONING	PROPOSED ZONING
2945-142-39-010	340 GRAND AVE	B-1	B-2
2945-142-39-003	321 OURAY AVE	R-O	B-2
2945-142-38-024	203 OURAY AVE	R-O	B-2
2945-142-38-025	200 GRAND AVE	B-1	B-2
2945-232-04-001	202 RIVERSIDE PKWY	I-2	I-O
2945-142-39-009	360 GRAND AVE	B-1	B-2
2945-232-00-075	NO ADDRESS	I-2	I-O
2945-142-39-001	303 OURAY AVE	R-O	B-2
2945-221-01-005	206 LILA AVE	I-1	I-O
2945-221-00-143	NO ADDRESS	I-1	I-O
2945-142-40-958	447 OURAY AVE	R-O	B-2
2945-142-40-951	402 GRAND AVE	B-1	B-2
2945-142-42-011	604 GRAND AVE	B-1	B-2
2945-142-42-008	624 GRAND AVE	B-1	B-2
2945-142-42-010	608 GRAND AVE	B-1	B-2
2945-142-42-009	616 GRAND AVE	B-1	B-2
2945-142-39-016	NO ADDRESS	R-O	B-2
2945-221-01-008	603 LAWRENCE AVE	I-1	I-O
2945-221-01-003	205 HALE AVE	I-1	I-O
2945-221-01-002	211 HALE AVE	I-1	I-O
2945-221-01-001	219 HALE AVE	I-1	I-O
2945-221-00-080	201 HALE AVE	I-1	I-O
2945-144-25-971	1129 COLORADO AVE	B-1	B-2
2945-144-25-024	1129 COLORADO AVE	B-1	B-2
2945-144-25-021	1105 COLORADO AVE	B-1	B-2
2945-142-42-006	640 GRAND AVE	B-1	B-2
2945-142-39-002	315 OURAY AVE	B-2	B-2
2945-142-32-990	529 CHIPETA AVE	R-8	R-O
2945-142-32-998	520 N 5TH ST	R-8	R-O
2945-142-32-994	501 CHIPETA AVE	R-8	R-O
2945-142-32-999	517 CHIPETA AVE	R-8	R-O
2945-142-32-992	525 CHIPETA AVE	R-8	R-O
2945-144-25-972	1169 COLORADO AVE	B-1	B-2
2945-221-01-010	636 1/2 LAWRENCE AVE	I-1	I-O
2945-221-01-011	636 LAWRENCE AVE	I-1	I-O
2945-142-40-010	460 GRAND AVE	B-1	B-2
2945-142-40-998	433 N 5TH ST	R-O	B-2
2945-142-40-952	453 OURAY AVE	R-O	B-2
2945-142-40-953	460 GRAND AVE	B-1	B-2
2945-142-39-015	330 GRAND AVE	B-1	B-2

PARCEL NUMBER	LOCATION	EXISTING ZONING	PROPOSED ZONING
2945-142-39-004	329 OURAY AVE	R-O	B-2
2945-231-39-001	NO ADDRESS	I-2	C-2
2945-231-39-002	1101 KIMBALL AVE	I-2	C-2
2945-221-01-006	201 LILA AVE	I-1	I-O
2945-142-37-018	400 N 1ST ST	B-1	B-2
2945-232-00-080	NO ADDRESS	I-2	Part to I-1
2945-232-06-003	NO ADDRESS	I-2	I-1
2945-142-39-911	304 GRAND AVE	B-1	B-2

CITY-OWNED PROPERTIES

2945-221-00-942	NO ADDRESS	R-8	B-P
2945-221-00-944	NO ADDRESS	I-O	CSR and BP
2945-221-00-940	543 LAWRENCE AVE	I-1	BP
2945-231-00-945	925 STRUTHERS AVE	CSR	Part to C-2; Part to I-2
2945-232-00-945	1001 S 3RD ST	I-O	Part to CSR; Part to I-2
2945-232-01-941	404 NOLAND AVE	C-2	CSR
2945-232-00-949	926 S 4TH ST	C-2	CSR
2945-232-01-940	426 NOLAND AVE	C-2	CSR
2945-232-00-946	NO ADDRESS	C-2	CSR
2945-231-17-941	919 KIMBALL AVE	I-1	C-2
2945-232-00-942	1001 S 5TH ST	C-2	CSR
2945-232-00-944	940 S 4TH ST	C-2	CSR
2945-232-00-948	950 S 4TH ST	C-2	CSR
2945-232-00-943	910 S 4TH ST	C-2	CSR
2945-232-00-941	1005 S 5TH ST	C-2	CSR
2945-232-00-947	952 S 4TH ST	C-2	CSR
2945-232-01-944	402 NOLAND AVE	C-2	CSR
2945-232-31-941	1030 S 5TH ST	C-2	CSR
2945-233-00-948	641 STRUTHERS AVE	C-2	CSR
2945-232-01-943	1007 S 5TH ST	C-2	CSR
2945-232-02-940	930 S 5TH ST	C-2	CSR
2945-232-01-942	1014 S 4TH ST	C-2	CSR
2945-232-04-942	NO ADDRESS	I-2	BP and I-O
2945-232-02-948	1040 S 5TH ST	C-2	CSR
2945-234-00-930	709 STRUTHERS AVE	C-2	CSR
2945-234-11-941	725 STRUTHERS AVE	C-2	CSR
2945-242-00-945	NO ADDRESS	I-2	CSR
2945-243-00-945	2735 RIVERSIDE PKWY	CSR	I-2

ATTACHMENT 4: PUBLIC PARTICIPATION PROCESS

TECHNICAL COMMITTEE

The Greater Downtown Plan technical committee was comprised of staff members from various public agencies including City Public Works and Planning, City Parks and Recreation, City Geographic Information Systems, Mesa County Planning, the Regional Transportation Planning Office, Mesa County Facilities and Parks, the Downtown Development Authority and the Mesa County Public Library District. The Committee met three times during the course of developing the Greater Downtown Plan and members attended public open houses to discuss concerns and proposals with participants.

PUBLIC OPEN HOUSES

Two public open houses were held in December 2011 and February 2012 to present concepts and solicit input from property owners and interested citizens. Notifications/ invitations to both public open houses were mailed to all property owners within the Greater Downtown Plan area. Approximately 60 people attended the first open house and 40 attend the second open house. Another forum was provided for the property owners within the CBD on January 31, 2013, attended by 30 people.

QUESTIONNAIRES AND COMMENTS

A series of questionnaires were available at the December 2011 open house and on the City's web site that were used to solicit public comment and weigh community opinions on design concepts that might be proposed with the Plan. 130 questionnaires were returned. In addition, citizens could provide other written comments at both open houses. The results of the questionnaires and the written comments are included on following pages.

LETTERS/MEETINGS WITH INDIVIDUAL PROPERTY OWNERS

City Public Works and Planning staff coordinated meetings with key individual property owners, businesses or others that contacted the City regarding the Greater Downtown Plan. In addition, individual letters were mailed to property owners along the corridors that may be impacted by the land use and zoning proposals of the Greater Downtown Plan. Follow up meetings or conversations were held with property owners that responded to the letter. The meetings/conversations including the following individuals or businesses: Marie Ramstetter, Jim Golden, Woodstove Warehouse, 4NR Properties, Struth LLC, Peggy Cox, Kathy Ziola, John Crouch, the Redstone Group (Sugar Beet building), Butch Jarvis, VanGundy's, Castings, Inc., KelMac Industries (GJ Steel site), Sem Materials and Whitewater Sand and Gravel.

COMMUNITY PRESENTATIONS/DISCUSSIONS

Public Works and Planning staff conducted several presentations and discussions regarding the Greater Downtown Plan with community groups and businesses including the Chamber of Commerce, Bray and Company Realty, the Downtown Development Authority and Rail and River District corridor property owners.






CITY COUNCIL, CITY PLANNING COMMISSION AND MESA COUNTY PLANNING COMMISSION WORKSHOPS

City Public Works and Planning staff attended several workshops with elected and appointed City and County officials to inform and solicit input on the Greater Downtown Plan during its development.





In addition to these opportunities for public input, the final draft of the Greater Downtown Plan and the Greater Downtown Plan Overlay were made available to the public 5 weeks prior to the public hearing before the Grand Junction Planning Commission.

Central Business District – Questionnaire Results


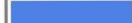



1. Establish a cohesive character/theme that harmonizes new structures with the existing buildings through common materials, scale and architectural details

#	Answer		%
1	Strongly Disagree		7%
2	Disagree		7%
3	Neither Agree nor Disagree		15%
4	Agree		37%
5	Strongly Agree		33%
	Total		100%






2. Promote high density, vertically mixed use structures (e.g. retail at street level and residential or office above)

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		4%
3	Neither Agree nor Disagree		4%
4	Agree		37%
5	Strongly Agree		56%
	Total		100%



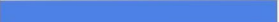
3. Building height restrictions in the CBD should be based on guidelines that address compatibility with surrounding uses.

#	Answer		%
1	Strongly Disagree		7%
2	Disagree		26%
3	Neither Agree nor Disagree		19%
4	Agree		30%
5	Strongly Agree		19%
	Total		100%



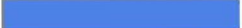
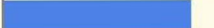
4. Taller buildings should be located in the center of the CBD, with lower buildings on the edges of the CBD.

#	Answer		%
1	Strongly Disagree		4%
2	Disagree		12%
3	Neither Agree nor Disagree		31%
4	Agree		31%
5	Strongly Agree		23%
	Total		100%





5. Preserve and restore significant historic structures

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		0%
3	Neither Agree nor Disagree		13%
4	Agree		30%
5	Strongly Agree		57%
	Total		100%


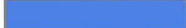

6. Promote infill development that is compatible with the existing downtown character.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		9%
3	Neither Agree nor Disagree		13%
4	Agree		43%
5	Strongly Agree		35%
	Total		100%




7. The streetscape will be dominated by buildings rather than surface parking lots.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		9%
3	Neither Agree nor Disagree		13%
4	Agree		35%
5	Strongly Agree		43%
	Total		100%




8. Shared parking and parking structures should be encouraged in the CBD to reduce the amount of surface parking lots.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		0%
3	Neither Agree nor Disagree		17%
4	Agree		39%
5	Strongly Agree		43%
	Total		100%

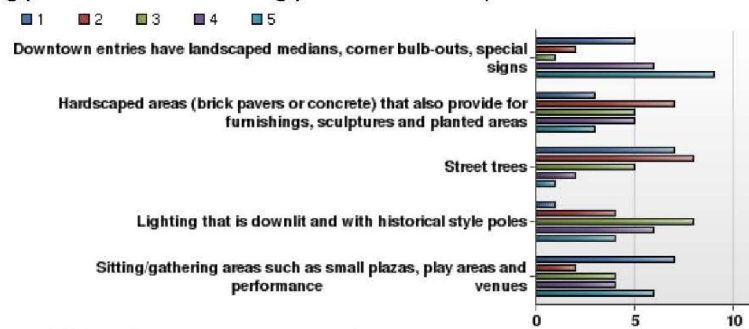
9. Explore new ways to pay for public parking.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		0%
3	Neither Agree nor Disagree		27%
4	Agree		36%
5	Strongly Agree		36%
	Total		100%

10. Provide streetscape details that complement the architectural character of downtown Grand Junction.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		0%
3	Neither Agree nor Disagree		9%
4	Agree		52%
5	Strongly Agree		39%
	Total		100%

11. The most important streetscape characteristics to me are (Please rank the options below in order of your most preferred with 1 being your favorite and 6 being your least favorite):



12. Please provide any additional comments you may have.

Text Response

#2 if you consider parking #4 Keep very tall buildings - like the Alpine Bank Building OUT of downtown.

#11 Xeroscape, dark sky!!, sitting/gathering along rail district as well

The focus within the CBD should be less on specific architectural styles and/or materials, but on those necessary elements to create a streetscape (location of entrances, location of parking, uses of the sidewalk, etc). The collection and range of architectural styles is what illustrates the evolution of a place and brings unique character to that place.

I think there should be a legitimate concrete Skatepark in the downtown park area as an anchor for destination based year round usage... feel free to contact me for any info or questions... (info@bin707.com)






The CBD has a core of historical structures on Main Street and Colorado, but the modern buildings on 7th and what the new Marriott adds is a varied and interesting streetscape with both modern and historic structures. This diversity of design allows for creativity, originality of builders and not a cookie cutter approach to the CBD dictated by code. That said, a 20 story medical center next to the transitional housing areas makes no sense so some type of restrictions should apply. The removal of trees on Main was necessary given the 50 year life cycle, but better planning needs to be done to assure we maintain a natural canopy of vegetation and the City also needs to do a better job of maintaining the fountains assuring they are functioning and serviced as needed. The one located across from the Avalon has malfunctioned much of the time since installation. Also winterization and maintenance of these wonderful outdoor water features must be planned operationally. Parking will always be an issue and the City needs to be more in touch with the reality of what they are approving as evidenced by the latest "VET" housing development done by Catholic Outreach which was totally lacking substantive evidence for the staff justification of parking. The result will be loss of parking for downtown businesses and there is no law that says the facility has to be used for VA occupants forever.

Downtown Area-Wide Concepts – Questionnaire Results


1. Examine the possibility of making 4th and 5th Streets both 2-way streets between Grand and North Avenues

#	Answer		%
1	Strongly Disagree		32%
2	Disagree		18%
3	Neither Agree nor Disagree		14%
4	Agree		18%
5	Strongly Agree		18%
	Total		100%

2. Establish a distinctive public sign palette for the original square mile to include street signs and directional signs that have recognizable poles, ornamentation, colors, fonts and logos.

#	Answer		%
1	Strongly Disagree		9%
2	Disagree		5%
3	Neither Agree nor Disagree		32%
4	Agree		27%
5	Strongly Agree		27%
	Total		100%





3. Emphasize “walkability” of Downtown through ongoing improvements to pedestrian crossings, bicycle facilities (e.g. bike lanes on streets, bike racks at strategic locations).

#	Answer		%
1	Strongly Disagree		4%
2	Disagree		0%
3	Neither Agree nor Disagree		4%
4	Agree		30%
5	Strongly Agree		61%
	Total		100%

4. Establish and improve gateways to the Downtown Area (please select your top 4 choices).

#	Answer		%
1	7th Street and North Avenue		37%
2	1st Street and Grand Avenue		84%
3	12th Street and Grand Avenue		16%
4	1st and Main Streets		84%
5	12th and Main Streets		21%
6	5th Street and South Avenue		68%
7	7th Street and Pitkin Avenue		58%
8	Other – please describe		32%

Other – please describe	
River front trail	
1st - NORTH	
4th & North	
14th & I-70B (Desert Vista Park)	
7th and Main	

2	Disagree		10%
3	Neither Agree nor Disagree		25%
4	Agree		40%
5	Strongly Agree		20%
	Total		100%

5. Please provide any additional comments you may have.

Text Response

Make the accessibility to the river and riverfront trails from downtown area more inviting.

#1 Why?

Make user-friendly ways to get to river trails

#4 improve how? They are all fine now Making 4th & 5th only partly 2-way would be a disaster. There would be auto accidents - people driving in the wrong lanes No more roundabouts!






In time pedestrian traffic will increase yet travel today in the next 20 years most travel will be via cars

#1 but make transition from 1-way to 2-way very clear with islands & other street features Main & Gunnison are ideal 7th from downtown to River is already bike-friendly ok






The directional signage beginning on Horizon Drive and leading to the Downtown along 7th Street already has a distinctive color scheme. However, this scheme was compromised at the intersection of 7th Street and Grand Avenue when the sign pole was painted black. If a color scheme is created, it should build on this work and must not be compromised.

North Central Business District – Questionnaire Results






1. Establish a cohesive character/theme that harmonizes new structures with the existing buildings through common materials, scale and architectural details

#	Answer		%
1	Strongly Disagree		10%
2	Disagree		10%
3	Neither Agree nor Disagree		25%
4	Agree		45%
5	Strongly Agree		10%
	Total		100%



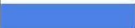

2. Promote vertically mixed use structures (e.g. retail or office at street level and residential or office above)

#	Answer		%
1	Strongly Disagree		5%
2	Disagree		5%
3	Neither Agree nor Disagree		10%
4	Agree		50%
5	Strongly Agree		30%
	Total		100%

3. The maximum building height of 65 feet shall only be allowed on parcels that front Grand Avenue. The buildings should “step down” so that the front of buildings that are directly across the street from residential buildings or uses are only 35 feet in height.

#	Answer		%
1	Strongly Disagree		5%
2	Disagree		10%
3	Neither Agree nor Disagree		25%
4	Agree		40%
5	Strongly Agree		20%
	Total		100%

8. Where available, some parking for non-residential uses may be on the street but only in front of the actual use, not in front of other adjacent uses.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		16%
3	Neither Agree nor Disagree		37%
4	Agree		32%
5	Strongly Agree		16%
	Total		100%

9. Should similar guidelines and standards be applied to the 1st Street Corridor from Grand Avenue to Belford Avenue?

#	Answer		%
1	Yes		78%
2	No		22%
	Total		100%

10. Please provide any additional comments you may have.

Text Response

limit mixed-use intrusion into residential areas

#1 but not exactly alike #8 there's just no space on 4th St






#1 existing bldgs are crap #6 I don't notice anything about Grand west of 7th worth harmonizing with.

Keep it simple. Trees are such a gift. Keep it "big western town" rather than a city. Really, we have a choice western town here that offers relief from cities. May we manage to preserve & maintain that western "wide open spaces" air.



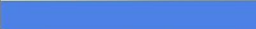

The one-block depth of this subarea creates a challenge. I agree that a maximum setback is necessary, both along Grand and 1st Street. However, this will result in the relocation of parking, loading, etc to the rear of the block, assuming the properties within the block are assembled. I believe that it would be a mistake to severely limit the height and/or building locations along Ouray and 2nd Street. The width of these streets and the streetscape itself would be better used as the transition. This would allow greater utilization of the properties and provide a better defined "edge" between the business and the residential area.

Transitional Areas – Questionnaire Results




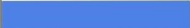

1. Front yards of transitional uses shall be reserved for landscaping, sidewalks and driveway access to parking areas and signage to maintain the residential character.

#	Answer		%
1	Strongly Disagree		5%
2	Disagree		11%
3	Neither Agree nor Disagree		16%
4	Agree		42%
5	Strongly Agree		26%
	Total		100%






2. Keep signs for the non-residential uses subordinate to the residential character.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		5%
3	Neither Agree nor Disagree		16%
4	Agree		58%
5	Strongly Agree		21%
	Total		100%





3. Regulate maximum building size in transitional corridors.

#	Answer		%
1	Strongly Disagree		11%
2	Disagree		21%
3	Neither Agree nor Disagree		16%
4	Agree		42%
5	Strongly Agree		11%
	Total		100%






4. Regulate hours of operation for transitional uses.

#	Answer		%
1	Strongly Disagree		11%
2	Disagree		33%
3	Neither Agree nor Disagree		28%
4	Agree		17%
5	Strongly Agree		11%
	Total		100%






5. Regulate building, site and signage lighting for transitional uses to minimize impact on adjacent residential core.

#	Answer		%
1	Strongly Disagree		5%
2	Disagree		0%
3	Neither Agree nor Disagree		5%
4	Agree		58%
5	Strongly Agree		32%
	Total		100%





6. Regulate parking and screening on non-residential sites to minimize impact on adjacent residential core.

#	Answer		%
1	Strongly Disagree		6%
2	Disagree		6%
3	Neither Agree nor Disagree		11%
4	Agree		39%
5	Strongly Agree		39%
	Total		100%






7. Transitional uses should not be allowed to have outdoor storage areas.

#	Answer		%
1	Strongly Disagree		5%
2	Disagree		16%
3	Neither Agree nor Disagree		37%
4	Agree		16%
5	Strongly Agree		26%
	Total		100%





8. Where available, some parking for non-residential uses may be on the street but only in front of the actual use, not in front of other adjacent uses.

#	Answer		%
1	Strongly Disagree		6%
2	Disagree		0%
3	Neither Agree nor Disagree		35%
4	Agree		41%
5	Strongly Agree		18%
	Total		100%

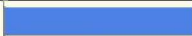






9. Reuse of residential structures and new construction in the transitional corridors shall retain residential character.

#	Answer		%
1	Strongly Disagree		6%
2	Disagree		12%
3	Neither Agree nor Disagree		6%
4	Agree		47%
5	Strongly Agree		29%
	Total		100%

10. Regulate the spacing of non-traditional residential uses (e.g., service organizations, group homes) so as to equitably disburse them throughout the downtown area.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		20%
3	Neither Agree nor Disagree		10%
4	Agree		50%
5	Strongly Agree		20%
	Total		100%

11. The most important residential characteristics to me are (check all that apply):

#	Answer		%
1	Maintain landscaped front yards (no parking in the front yard)		68%
2	Setback of building from street		32%
3	Small, low signage		53%
4	Maintain or construct building forms that are typical of residential architecture (e.g. 1-1/2 to 2 stories, sloping roofs, window pattern, porches)		53%
5	Use materials that are similar in color and texture as those in the residential neighborhood (e.g. roofing, siding)		37%
6	Minimize the visual impact of parking provided for the transitional uses		58%
7	Other – please describe		16%

Other – please describe

Cleanliness and general appearance. In general, the current businesses in this area look nicer than the residences.

blend old & new (modern) architecture thru committee approval/aesthetic guidelines ie: Bozeman, MT

Feel free to tear it up and start over

12. Please provide any additional comments you may have.

Text Response

I am a part owner of a business at 9th and Rood. My main concern is traffic control. There are regularly accidents at 9th and Main, we hear each one and watch them out our windows. In my opinion, having watched this intersection for over a decade, you could drastically reduce the number of these accidents by reducing or eliminating the on-street parking on Main street in the vicinity of this intersection. I'm convinced most of these accidents are caused by visual error. People travelling either way on 9th will stop, look each way, see traffic on Main, and PULL OUT ANYWAY because they think, in a quick glance, the vehicle they saw was parked on Main. In a quick glance, drivers sometimes cannot differentiate between a parked and a travelling vehicle. The City put blinking lights around the stop signs for a while, I see they're gone now, I suspect they statistically did not help reduce the number of accidents. Please try my idea. I'm sure the property owners will complain. But it could be you or your family that is in the next accident at 9th and Main.






Bozeman, MT!!!

#9 the transitional area has no redeeming qualities.





The Transistional Area needs to be redefined. It encompasses existing commercial uses between 7th and 12th Street, many with frontage on North Avenue, which are identified as part of a student commercial/entertainment district. It also encompasses the Ratekin and Qwest towers, both zoned Downtown Business, which should be included in the CBD.

Residential Areas – Questionnaire Results




1. No large-scale redevelopment projects should be allowed within the downtown residential core.

#	Answer		%
1	Strongly Disagree		10%
2	Disagree		20%
3	Neither Agree nor Disagree		20%
4	Agree		20%
5	Strongly Agree		30%
	Total		100%




2. The downtown residential core should be preserved for residential uses only with no further encroachment of non-residential uses.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		24%
3	Neither Agree nor Disagree		29%
4	Agree		10%
5	Strongly Agree		38%
	Total		100%





3. Maintain the existing character of the house styles within the downtown residential core neighborhood – new construction or alteration must be compatible with key architectural characteristics and site elements of the neighborhood.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		15%
3	Neither Agree nor Disagree		0%
4	Agree		45%
5	Strongly Agree		40%
	Total		100%






4. Maintain and enhance the pattern of landscaped front yards that gives the downtown residential core neighborhood a distinctive, friendly appearance.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		0%
3	Neither Agree nor Disagree		14%
4	Agree		48%
5	Strongly Agree		38%
	Total		100%





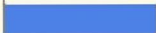
5. Regulate the scale of accessory structures to maintain their character as subordinate to the primary residence.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		14%
3	Neither Agree nor Disagree		10%
4	Agree		43%
5	Strongly Agree		33%
	Total		100%





6. New construction of accessory structures may be allowed to be built at historic setbacks (e.g. there could be a zero foot setback from the alley and only 3 feet from neighboring property line).

#	Answer		%
1	Strongly Disagree		10%
2	Disagree		20%
3	Neither Agree nor Disagree		25%
4	Agree		35%
5	Strongly Agree		10%
	Total		100%





7. Where existing residential zoning allows, provide a diversity of housing types through development of multi-family housing that is in keeping with the character of the neighborhood.

#	Answer		%
1	Strongly Disagree		5%
2	Disagree		19%
3	Neither Agree nor Disagree		14%
4	Agree		29%
5	Strongly Agree		33%
	Total		100%


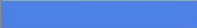

8. Discourage tearing down existing historic homes in order to construct new residential structures.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		10%
3	Neither Agree nor Disagree		19%
4	Agree		38%
5	Strongly Agree		33%
	Total		100%




9. Regulate the spacing of non-traditional residential uses (e.g. service organizations, group homes) so as to equitably disburse them throughout the downtown residential area.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		6%
3	Neither Agree nor Disagree		24%
4	Agree		59%
5	Strongly Agree		12%
	Total		100%







10. Enhance access to and improvements within existing public open spaces (e.g. parks and school grounds) within the downtown residential core.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		0%
3	Neither Agree nor Disagree		18%
4	Agree		47%
5	Strongly Agree		35%
	Total		100%

11. Maintain and enhance the historic character of the streetscape.

#	Answer		%
1	Strongly Disagree		0%
2	Disagree		0%
3	Neither Agree nor Disagree		6%
4	Agree		63%
5	Strongly Agree		31%
	Total		100%

12. The most important streetscape characteristics to me are (check all that apply):





#	Answer		%
1	Street Trees		94%
2	Landscaping rather than parking or other uses in park strips (area between sidewalk and curb)		71%
3	Distinctive street signs for downtown residential core		24%
4	Distinctive street lighting for downtown residential core		41%
5	Minimally maintained landscaping in front yards		6%
6	Detached sidewalks (space between the sidewalk and the curb)		47%
7	Other – please describe		0%
Other – please describe			

13. Please provide any additional comments you may have.





Text Response
<p>Allow for Bed & Breakfast establishments to operate within original square mile, provided that signage, parking areas and lighting can blend in with neighboring properties.</p> <p>#8 depending on the existing quality of the building #11 evergreens!</p> <p>#2 small-scale mixed use would be ok #8 "historic" does not necessarily mean "good" The trees and lawns are pleasant, but GJ could afford to mount a significant effort toward xeriscape concepts. Honestly it seems criminal how folks in this town gush the Colorado River all over the place with little or no thought toward making a more rational truce with the desert.</p> <p>I'd love to see softer lighting, going upward. For good ideas, check out www.darksky.org. Xcel says people need only a 60 watt bulb (or equivalent on houses). I recommend (& long for) banishment of motion-sensor lighting... My plea is keep it simple. Preserve the "big western town" feel of this town. No more narrowing of streets. Please avoid turning into a city (like Boulder). THANKS!</p> <p>Prohibiting "non-residential" uses can create unintended outcomes. For example, should a church be allowed to expand and offer a coffee shop or other gathering place open to the public? This sort of amenity within the residential area is what adds value to the area. It is important to permit multi-family (re)development within the original square mile for two reasons. One, it has excellent infrastructure to service the population and proximity to services. Two, it is the only area within Grand Junction that has lost population in the 2010 Census (Tract 2 and 3), suggesting a loss of families from the area and/or an aging population of smaller households.</p>

Corridor Overlays – Questionnaire Results





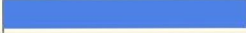
1. Create a mix of higher quality, new uses along South Avenue.

#	Answer		%
1	Strongly Disagree		9%
2	Disagree		0%
3	Neither Agree nor Disagree		18%
4	Agree		9%
5	Strongly Agree		64%
	Total		100%






2. Create a more pedestrian-friendly commercial and service corridor along South Avenue/South 7th Street that complements the street improvements that have already been completed.

#	Answer		%
1	Strongly Disagree		9%
2	Disagree		0%
3	Neither Agree nor Disagree		18%
4	Agree		18%
5	Strongly Agree		55%
	Total		100%






3. Do not allow billboards on the South Avenue/South 7th Street corridor.

#	Answer		%
1	Strongly Disagree		5%
2	Disagree		5%
3	Neither Agree nor Disagree		27%
4	Agree		9%
5	Strongly Agree		55%
	Total		100%






4. Promote higher quality architecture and screening of outdoor uses for sites that face South Avenue and South 7th Street.

#	Answer		%
1	Strongly Disagree		10%
2	Disagree		5%
3	Neither Agree nor Disagree		14%
4	Agree		14%
5	Strongly Agree		57%
	Total		100%






5. Should similar guidelines and standards be applied to the 1st Street corridor from South Avenue to Grand Avenue?

#	Answer		%
1	Strongly Disagree		14%
2	Disagree		0%
3	Neither Agree nor Disagree		24%
4	Agree		14%
5	Strongly Agree		48%
	Total		100%






6. Limit expansion and construction of any new outdoor uses and operations (such as outdoor operation of rail yards, salvage yards, etc.) on properties that abut the South 5th Street right-of-way.

#	Answer		%
1	Strongly Disagree		14%
2	Disagree		14%
3	Neither Agree nor Disagree		14%
4	Agree		9%
5	Strongly Agree		50%
	Total		100%






7. Provide screening on the South 5th Street bridge that limits views to adjacent uses but still provides longer vistas to the east (Grand Mesa) and west (Colorado National Monument and Uncompahgre Plateau).

#	Answer		%
1	Strongly Disagree		23%
2	Disagree		5%
3	Neither Agree nor Disagree		23%
4	Agree		9%
5	Strongly Agree		41%
	Total		100%





8. Promote higher quality architectural features on building facades that directly face or can be seen from the 5th Street bridge on the east and west sides of the bridge.

#	Answer		%
1	Strongly Disagree		9%
2	Disagree		9%
3	Neither Agree nor Disagree		18%
4	Agree		18%
5	Strongly Agree		45%
	Total		100%





9. Do not allow billboards on the South 5th Street corridor.

#	Answer		%
1	Strongly Disagree		9%
2	Disagree		18%
3	Neither Agree nor Disagree		23%
4	Agree		5%
5	Strongly Agree		45%
	Total		100%






10. Create a mix of higher quality, new uses along South 7th Street and Riverside Parkway.

#	Answer		%
1	Strongly Disagree		9%
2	Disagree		5%
3	Neither Agree nor Disagree		27%
4	Agree		18%
5	Strongly Agree		41%
	Total		100%





11. Create a more pedestrian-friendly commercial core for area employees, residents and visitors.

#	Answer		%
1	Strongly Disagree		9%
2	Disagree		0%
3	Neither Agree nor Disagree		23%
4	Agree		14%
5	Strongly Agree		55%
	Total		100%

12. Integrate new commercial and mixed use with park, trail, recreation and open space uses.

#	Answer		%
1	Strongly Disagree		10%
2	Disagree		5%
3	Neither Agree nor Disagree		14%
4	Agree		19%
5	Strongly Agree		52%
	Total		100%






13. Do not allow billboards along the South 7th Street/Riverside Parkway business park mixed use corridor.

#	Answer		%
1	Strongly Disagree		10%
2	Disagree		0%
3	Neither Agree nor Disagree		19%
4	Agree		14%
5	Strongly Agree		57%
	Total		100%






14. Promote commercial and mixed uses along the north side of the Riverside Parkway that would transition to and screen industrial areas behind to the north.

#	Answer		%
1	Strongly Disagree		5%
2	Disagree		15%
3	Neither Agree nor Disagree		20%
4	Agree		30%
5	Strongly Agree		30%
	Total		100%






15. Use the remnant building of the historic sugar beet factory as a guide for scale, height and architectural elements for potential new development along Riverside Parkway.

#	Answer		%
1	Strongly Disagree		10%
2	Disagree		5%
3	Neither Agree nor Disagree		19%
4	Agree		19%
5	Strongly Agree		48%
	Total		100%






16. Promote higher quality architectural treatments for building facades that face the Riverside Parkway.

#	Answer		%
1	Strongly Disagree		14%
2	Disagree		5%
3	Neither Agree nor Disagree		5%
4	Agree		36%
5	Strongly Agree		41%
	Total		100%






17. Regulate screening of outdoor storage in the front yard area to minimize visual impact of these uses along the Riverside Parkway.

#	Answer		%
1	Strongly Disagree		14%
2	Disagree		9%
3	Neither Agree nor Disagree		5%
4	Agree		27%
5	Strongly Agree		45%
	Total		100%

18. Promote higher quality streetscape with minimal building setbacks, use of front yard space, signage, fencing and landscaping regulations.

#	Answer		%
1	Strongly Disagree		5%
2	Disagree		10%
3	Neither Agree nor Disagree		15%
4	Agree		40%
5	Strongly Agree		30%
	Total		100%

19. Do not allow billboards along the Riverside Parkway commercial/industrial corridor.

#	Answer		%
1	Strongly Disagree		5%
2	Disagree		5%
3	Neither Agree nor Disagree		33%
4	Agree		5%
5	Strongly Agree		52%
	Total		100%

20. Please provide any additional comments you may have.

Text Response

NO BILLBOARDS

I think your survey is biased - where do you say you want NO OVERLAYS

Encouraging progressive developers/landowners to make a few "1st moves" needs to be a kick-start for these standards. Once a few are on board this plan can be used as an incentive to get businesses to locate and upgrade in these districts. I envision that as the riverfront and other areas develop, GJ may be able to attract Denver businesses that cannot afford the downtown real estate they want there.

Looks good!

#7 waste of \$;#8 If you do this, no need for a screen; #9 I kinda like them; #10 Keep good reliable businesses in place; #12 if it will help our economy & Bring more businesses downtown; #14 Waste of \$. What is, is. I like seeing the rock yard. Keep it simple. Keep it real. Keep night lighting subtle, friendly, rather than glaring & blinding. Go for quality (which it sounds like you are doing). Yet please make it possible for "old-timers" to maintain their business without undue expense for dubious purpose. Keep our town Western! (that's my plea) open & friendly & not too fancy THANKS

I agree w/ all. The "strongly agree" s are of greatest importance. #15 that and lower

Mistake to not allow residential in rail district. ie: LODO in Denver, has the potential to be urban atmosphere that would attract young professionals to work & reside ie: living/working lofts. Densely populated "downtown" area can prevent urban sprawl, lessen carbon footprint and make this area a desirable place to live & work

1) This is a convoluted process. 2) Driving for answers you want 3) We need jobs, business & honor private property rights

With respect to the South Avenue corridor, is there still discussion about rerouting the I-70B corridor to Pitkin/South? If so, then that changes the type of character along that corridor, though pedestrian access should still be provided. South 5th Street (bridge) does serve as an entry point into the downtown; however, since it essentially "flies over" the existing industrial areas, it is not necessary to completely restrict those uses. The main focal point should be the end of the bridge and the forward vista into downtown, not what is underneath. The orientation of the Riverside Parkway paralleling Las Colonias Park presents something of a challenge for future commercial development, since direct access is only available at 7th St, 9th St, and Winters Avenue. These intersections should be the focus of the integration of commercial to open space concept. The areas along Kimball Avenue adjacent to the sugar beet factory are perhaps too far away from these nodes to be viable for mixed-use development

Greater Downtown Plan Open House
December 8, 2011
Citizen Comments

- I don't want changes in zoning OR overlays – leave it alone for now – that's the best way to stimulate job growth.
- # one priority should be emphasis on maintaining and enhancing the character of the downtown residential and business to provide character, charm and livability within the core area. Increased bike lanes/racks, pedestrian crossings and a strong control of architectural features will serve to ensure an attractive downtown for years to come.
- Would love to see better use of Whitman Park connected to the Museum, Renaissance Fair, Farmer's Market etc. An East/West traffic corridor south of Ute would make the area more pedestrian-friendly to downtown visitors.
- Must have strict sign codes & restrictions; NO lighted signs in residential district; Same for R-O zones.
- R-O must provide parking for residents & employees, clientele during business hours. Parking is a big problem in R-O areas where renters & employees are not allowed to park during business hours and take parking from residents. Sometimes R-O residents tie up parking for days at a time leaving homeowners without parking and must carry groceries and other items from alley or further from home. Limit the amount of families living in existing homes. Some homes have multiple families living there (with numerous vehicles).
- NO parking of vehicles on parkway.
- Support for the Ute/South shift of one-ways.
- Encourage continued support for the "Arts" and Museum in downtown.
- Support for safety for bicyclists and pedestrians. WALKABILITY!
- Support for parks and active recreation in the area.
- Agree with – no billboards on South Ave/S 7th Street/S 5th Street corridors.
- Promote higher architecture and screening for site South Ave.
- Set minimum height requirements for downtown – Build Taller.
- I like the corridor concepts connecting downtown to the river. I live &* work downtown, bike the river and only need a couple connections to get there, so 5th Street & 7th Street corridors bike-friendly are important to me personally; and I think, good for the area in general.
- With proposed rezone of block between 5th & 6th, Ouray & Chipeta – would a credit union or bank be a compatible use? – Eve Tallman 683-2424.
- I agree that building sizes need to blend as they approach the residential districts from Grand Ave.
- We need better ways to notify owners of the meetings. I didn't know of the Chipeta Elementary meeting.
- Clean tamarisk, Russian Olive (tents, dogs) between S 5th Street and Railroad Bridge.
- Improve bike, pedestrian route Main to River trail.
- Put soft path along river thru Los Colonias (something easy to replace if floods).
- Put pedestrian bridge over cut in Jarvis pond from soft path loop.
- Improve pedestrian, bike route W. Main to River (by Dual Immersion Academy school, etc).
- I would like to see more development along the river. Has city planning ever taken a "field trip" to Littleton to see what they have done with their river trail? I grew up there and saw it transform from trash to a treasure.
- Since this town is named after the confluence of 2 rivers, why not have a confluence park? Buy all the trashed area down at the confluence.
- River District – Concerned with Cities priority on present & future Riverfront Trail maintenance. Does not appear to be a priority. Also concerned with City "real" concern & commitment to the River District development for the Park & Future Trail to 29 Road.
- The industrial use of the 2 blocks south of Pitkin should remain due to the already constructed infrastructure i.e. – rail yards, holding tanks, etc. to re-set this area into another location would be very cost prohibited.

Regarding the workshop at City Hall 09.19.2011 regarding
Zoning and Future Land Conflicts

1) South downtown area;

1) South Downtown, the OPTION 3 submitted by Kathy Portner was the plan she said matched current use and that she would propose. The City Council concurred. That the "triangle on 6th Street should be I-1, historical use of rail; take change North to South side of South Ave.; West side of 7th to accommodate current users. Council agreed with one dissenting vote.

There was talk of "conditional use" and "over-lays".

The Mayor's remarks were that both of these 'conditions' on zoning creates unpredictability. Both should be avoided whenever possible. Council concurred.

At the work shop, there was also concern that the South Downtown should be considered separate from the Downtown Plan. However, we note this has been put as one.

Meeting 12.08.2011 at Whitman Center

No property owners have been for a change of zoning in South Downtown. The Council and Planning Department discussed this at length at 9.19.2011 workshop and felt uses should be kept for current users and zoning should remain the same.

- 1) Why are the same meetings necessary – over and over?
- 2) Why are minutes not kept of previous meetings?

Please consider keeping the zoning the same for the South Downtown so businesses have predictability and continue operating – instead of wondering what the City is going to do to them.

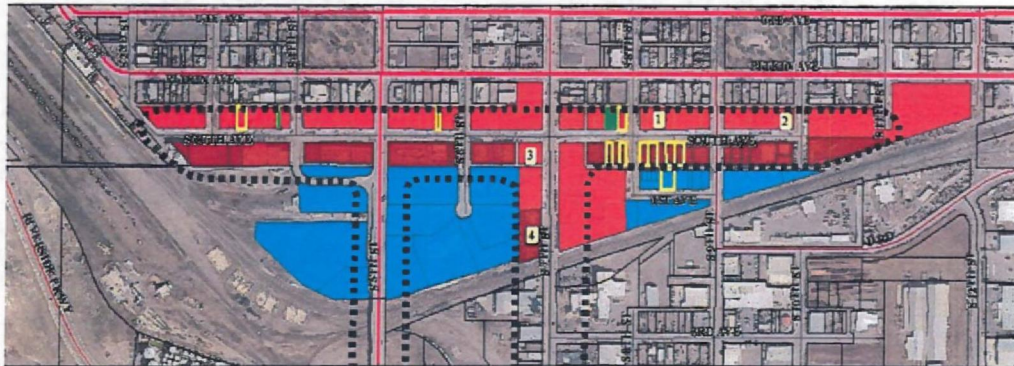
Thank you,



Lois Dunn
PO box 1889
Grand Junction CO 81502
970-243-8843
loisgdunn@gmail.com

Attached: Proposal 3 as presented at the 9.19.2011 workshop

OPTION 3: Omitted Area South Ave to RR Tracks Potential Zoning



Potential Zoning

- C-1 (Light Commercial)
- C-2 (General Commercial)
- I-1 (Light Industrial)

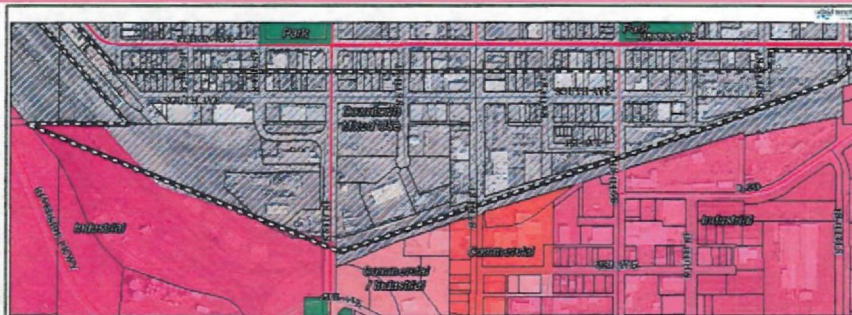
Non-Conforming Use in Potential Zone

- 1 Warehouse - conforms in C-2, not C-1
- 2 Private Gas Pumps - conforms in C-2, not C-1
- 3 Flea Market - conforms in C-1, no outdoor operation
- 4 Dible Oil - conforms in C-2, not C-1

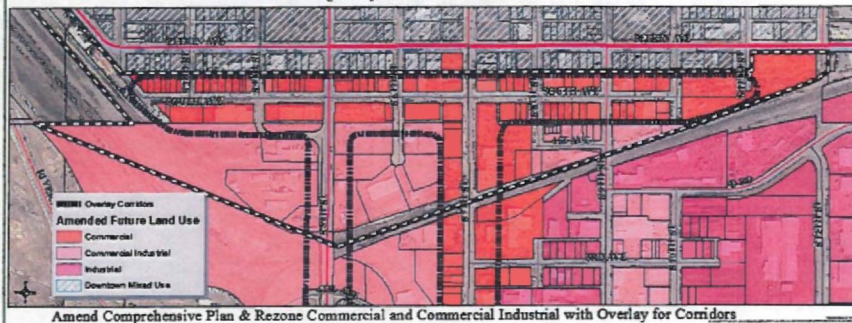
Overlay Corridors

- Non-Conform Use Only with Existing Zoning
- Non-Conforming Use with Existing & Potential Zoning

OPTION 3: Omitted Area South Ave to RR Tracks Extend Concepts from South Downtown Plan



Existing Comprehensive Plan Land Use



Amend Comprehensive Plan & Rezone Commercial and Commercial Industrial with Overlay for Corridors

DOWNTOWN OVERLAYS

- Transitional Areas – Need parking guidelines for business & residential uses. What about parking guidelines for rentals? Currently property owners do not have any requirements to have renters park on or in front of units. What about off street parking requirements? We need them.
- Transitional Area – Text seems to primarily be geared to business discussion. We need guidelines & standards to be inclusive of the residential aspects!! All readers need to understand we are an inclusive neighborhood comprised of residential and business interests. Current zoning states “Residential/Office”.
- 1st Street from Main St south to the depot – slow traffic/Parkway/landscaped median, shift of Ute/Pitkin to Pitkin/South.
- On street parking should be encouraged to slow traffic and act as a buffer on collector streets (e.g. Grand Ave) to buffer traffic from residential yards. Parked cars are a safety buffer for children playing in yards.
- No uses earlier than 7:30 am or after 8 pm. The Yoga Collective would immediately be in violation of this on 10th & Main.

PROPOSED LAND USE MAP

- It doesn't seem to make sense putting a Commercial/Industrial area on the river, breaking up the continuity of the park areas.
- Train Depot – Why not encourage Amtrak, GVT & Greyhound Bus Co. to make a true intermodal center @ 1st/2nd and Pitkin? Then encourage higher density housing and mixed use for this area.
- Isn't the Neighborhood Center supposed to serve clients who “walk” from the immediate neighborhood? On 1st Street there are barely sidewalks to serve the NC. Isn't this commercial area being used by the “Driving” community more than the walking neighbors? Do you expect this to change?

PROPOSED ZONING MAP

- The infamous Brady property will be surrounded by parks on 3 sides according to the FLU. How does the I-1 zoning make any sense? Why not get Brady to trade for some vacant land to its north and east.
- Rail industrial zone inhibits residential uses & remodels – should be mixed use zone. This area is full of houses – not just industrial uses.
- County zoning missing from maps; any changes to County zoning considered?
- Brady Trucking need to be moved to the vacant industrial land to the east of where they are now and the land by the River needs to be part of the Park.
- Winters/Kimball – 7th/9th – There are residences there that should be retained for that use! Why can't we have residential blended use in ANY work areas? It makes not sense to segregate them and force their use change when we supposedly are trying to create a walkable city.
- Why is the Industrial use STILL specified by the Riverfront Trail? OM elevation looks right down on that area. Retain our Riverfront for a beautiful recreational feature, please!!!
- BP – doesn't allow museums; concerned that some retail sales are not allowed in the BP. Compare BP to C-2 uses.
- S 7th Street – look at potential of leaving the C-2 zoning and utilize the overlay zone for design standards.
- Would like to do mixed use, but business – residence requires owner or employee live there.
- Suggest incentives for new uses or upgrades we'd like to see – 1111 S 7th Street.
- Attached letter from John Crouch.
- Attached letter from Margaret Cox.

CIRCULATION/TRAILS/BUS

- NO MORE ROUNDABOUTS. THEY DO NOT AID IN TRAFFIC FLOW! Courtesy is not practiced, yield signs are not followed. They increase traffic congestion!

- The homes that area between 7th & 9th and Winters to Noland should be preserved as residential. This area needs residential if you want such things as a brew pub & other business to make this area a beautiful thriving area – Not a home for the homeless – Thank you, Kathy Jordan.
- 1ST & Grand – Uh.... Roundabout! It's a perfect intersection; DCOT & the City already own the land.
- Main Street east of 8th Street is too wide. Install some central medians or something to slow traffic, increase interest, create more neighborhood identity.
- 7th Street needs a sign that keeps large 53 feet long trucks off of it between Ute and Grand.
- 7th and Grand roundabout may cause vehicle/pedestrian conflict. Have walk signs/lights now. Traffic exiting roundabout does not expect or look for pedestrian traffic. These conflicts exist at 7th and Main roundabout.
- #2 – Spruce St @ Grand Ave – needs to be a right-in/right-out only – unclear if it is planned that way.
- #3 – Spruce & Main improvements look great - & should be a priority with new and increased traffic on South Spruce – Mesa County Central Services. In short term remove diagonal parking spaces closest to the intersection – poor sight distance now.
- Will a pedestrian connection from Ute Ave to Pitkin Ave be built where 6th St is now closed for the Fire & Police Facilities?
- More marked bike lanes would improve safe traffic flow.

CORRIDOR OVERLAYS

- 1101 Kimball owner Bryan Wiman – We support the corridor overly that affects our property “Sugar Beet Factory”. We understand that the existing use is not affected and that is important to us. We also respect the significant beauty of the Colonias Park area and we support re-development if market demands.
- Indian Road plans contradict this?? Follow up on – 396/398 & 397/399 Indian Road
- Moving Pitkin/Ute one way traffic to Pitkin/South should not occur. The Grand Valley Transit facility would be very negatively impacted – both pedestrian users and bus ingress-egress from such a drastic increase in traffic. – John Heidernan.
- A traffic light at S 5th and South Ave would back up north bound traffic significantly worse on the overpass. The one at Pitkin already does. Moving it one block south – not a good idea. - John Heidernan.
- The proposal to move Pitkin & Ute one way traffic to Pitkin & South is of concern to us. That would essentially put our 2 businesses, Enterprise & All Pets Center in the median strip of I-70B. Our access is already a huge problem for our clients and this would make it worse.

PARKS

- Brady trucking should expand to the east NOT on the riverfront.

GENERAL

- Please, no more parking meters. – Rob Rubin

City of Grand Junction
City Neighborhood Services
Attn Kristen Ashbeck, Senior Planner
250 North 5th Street
Grand Junction, CO 81501


05/15/2012

Dear Kristen:

Thank you for speaking with me regarding the proposed rezone from General Commercial (C-2) to Light Commercial (C-1).

The area of concern is commercial and best suited for the highest zoning available.. The railroad, junk yards, metal processors etc. and decades of investment in the area below Pitkin is not conducive to spotty zoning. Keep it all the highest zoning available.

To do what is suggested will force businesses that need higher zoning to move to Fruita or to the County.

Thank you

John Crouch
510 Liberty Cap Court
Grand Junction, Co 81507

Rec'd 4/25/12

Margaret Cox
P.O. Box 2422
Grand Junction, CO 81502

Grand Junction City Council
City Hall
250 N. 5th St.
Grand Junction, CO 81501
April 24, 2012

RE: Re- Zone and Over-Lay Plan for South Downtown Grand Junction

Dear City Council members,

I have recently been made aware of proposed changes to the zoning of south downtown that would negatively affect my commercial property at 805 Struthers Avenue. This 2 acre parcel is presently zoned C-2 and borders Riverside Parkway between 8th and 9th street on the south side. It is adjacent to city land that will someday become Los Colonias Park. Long range city plans include possible commercial development of privately owned land in my specific area. I am not opposed to the gradual development of the area for recreation, retail and even some residential uses. I have reviewed the proposals over the years with some interest. The only consistent conclusion I draw of these proposals is that they are projected endlessly into the future. There is never a start date.

The Planning Department has not been able to give any specific time-line on the creation of the park, saying it will take way more funding than is now in the budget. Re-zoning of that area should not take place until much of the park is completed and can demonstrate a draw for both the public and investors.

One compromise could be to change the zoning to C-1 only for individual parcels as requested by their owners. I would have no objection to that change, but would not want 805 Struthers re-zoned to C-1 at this time.

My property is a non-conforming site with non-conforming use since the zoning changed from Industrial to C-2. In years past I have sacrificed the flexibility of industrial usage. Should I decide to change my use or expand my facilities the C-2 designation already requires a major investment to meet city building codes, screening, landscaping and parking, etc. If the zoning were to change to C-1 I would no longer be allowed to use the land for storage and other outdoor uses if I changed my activities or have a building expansion over 20 percent, or if a potential buyer envisioned another use outside the C-1 zoning specifications. This limits my property's potential. It devalues my property. Quibbling about which zoning is more valuable is not productive at this point. I value my land based on the revenue it generates and its potential to provide me an income far into the future. This is based on the flexibility zoning allows.

Although the argument may someday be made that the potential use will be more valuable as a C-1 property, I see no evidence at all of this being so at this time. There is almost no retail anywhere in the area. There is no city park as a destination for the public. Efforts at upgrading the river corridor have been minimal, with the exception of the parkway itself and are not visible to the traveler on that parkway. There is no entrance to the park, no parking lot or access to the river or the pedestrian bridge to the east from the Parkway. No business has invested in a C-1 business on Struthers or anywhere near. I feel this is not the time to make changes to zoning that effect the possibilities for land owners to find tenants, have a variety of commercial functions, or ultimately to sell the real estate.

One exception, in particular, to the "grandfathering" of my outdoor usage I find unfair and would like addressed. If a property is vacant for more than one year the zoning requirements will kick in and protection as a "non-conforming" site will be lost. This does not accommodate the economic downturn that both Grand Junction and our national economy have experienced. Coming years could even be more challenging. This would represent a "taking" of sorts to force a revision of that non-conforming status. Commercial properties often times cannot be occupied consistently and, unfortunately, can remain vacant for well over twelve months. Owners already have taxes and other related expenses and should not have to lose their effective zoning status because of the misfortune of a vacancy. I would ask that this provision be removed from any Over-Lay or Re-Zoning considerations.

Please leave my property at 805 Struthers Avenue at the current zoning of C-2.
Thank you for your consideration.

Sincerely,



Margaret Cox

970-778-9000

>>> Marie Ramstetter <ramstet@gmail.com> 3/14/2012 3:46 PM >>>
I am still opposed to the down zone

On Wed, Mar 14, 2012 at 12:23 PM, Kristen Ashbeck <kristena@ci.grandjct.co.us> wrote:
Hi Marie,

Thank you for your comment regarding the Greater Downtown Plan. We understand your concerns and would like to provide you with additional information concerning the proposed zone change of your property from C-2 to C-1. If you review the attached information, it outlines the differences between the two zones. You will see that there is not a great difference between the uses that you might consider for your property, especially since the building already exists and there is very little room on the site for expansion or for outdoor uses. The zone change certainly would not impact a current use of the building or the building itself.

If after reviewing the information (or if you are unable to open the attachment), you have further questions or comments, please do not hesitate to contact me.

Kristen Ashbeck
Neighborhood Services / CDBG
[970.244.1491](tel:970.244.1491)
kristena@gjcity.org
[970.256.4114](tel:970.256.4114) fax

City of Grand Junction
250 North 5th Street
Grand Junction CO 81501

>>> Marie Ramstetter <ramstet@gmail.com> 3/13/2012 2:19 PM >>>

I am absolutely opposed to your attempt at down zoning my property, tax id 2945-231-00-008 located at 803 S 7th Street. Consider this a formal protest to the City.

From: Donna <donnap@bresnan.net>
To: <kristena@gjcity.org>
Date: 7/29/2012 7:10 PM
Subject: Greater Downtown Area Plan

Dear Kristen,

Thank you for your hard work and dedication to the development of the Greater Downtown Plan. We have reviewed the current draft and it is our hope that the Plan is approved by the City Council.

Unfortunately, a plan is only as good as the adherence, and we are not optimistic that the City will direct the appropriate departments to assure compliance. As you are aware, several ordinances currently on the books are not enforced. We are not optimistic that this will change with a new Plan.

Sincerely,

James and Donna Patton
341 Gunnison Avenue
970-245-1678

ATTACHMENT 5
Proposed Revisions to Plan and Overlay Documents
as Red-Lined in Planning Commission Version

GREATER DOWNTOWN PLAN REPORT

1. Page 6 – Clarification – added small triangle area in description of Greater Downtown area
2. Page 9 – Clarification – added reference to 7th Street District, applicable guidelines and standards already adopted.
3. Page 10 – Clarification – added brief description of Existing Commercial Subarea, previously omitted.
4. Page 16 – Clarification – Restate study of alternatives for one-way streets.
5. Pages 16-17 – Combine redundant policies – Goal 1, Policy 1h and Goal 3, Policy 3c
6. Page 17 – Clarification of goal
7. Page 18 – Clarification – reword Policy 2b
8. Page 19 – Clarification – Paragraph A – Blended map does not apply to future land use within the Downtown District
9. Pages 23-24 and 31-32 – Rewrite of Circulation section to clarify that it is proposals/suggestions for Greater Downtown and does not amend the Grand Valley Circulation Plan.
10. Page 25 Add photo and description to clarify/define “sharrow”
11. For easier use of final version, removed summary of public process and questionnaires/comments from Plan Report. Included instead as Attachment 4 to the staff report.

GREATER DOWNTOWN PLAN ZONING OVERLAY

1. Page 12 – Delete policy – combine with redundant standard 2 on page 13.
2. Page 14 – Clarify – Standard 1 is a policy, renumber remainder of standards.
3. Page 14 – Clarification – Revise to match the same statement in other corridor signage standards.
4. Page 16 – Clarification – Reword 5th Street viaduct screening requirement.
5. Page 20 – Clarification – add 2.d. re: application of zoning overlay to form-based zone districts.